

**REVISED AGENDA
REGULAR BUSINESS MEETING OF CITY COUNCIL**

**Monday, October 26, 2015, 1:00 p.m.
Council Chamber, City Hall**

Pages

1. NATIONAL ANTHEM AND CALL TO ORDER

2. CONFIRMATION OF AGENDA

15 - 59

Recommendation

1. That the attached letter from M. Hein submitting comments, dated October 26, 2015, be considered during Item 6.1.
2. That the attached letters from the following requesting to speak be received and considered for Item 8.1.7:
 - Jan Norris, dated October 23 and October 26, 2015;
 - Louise Jones, dated October 23, 2015; and
 - Candace Savage, dated October 23, 2015.
3. That the attached letter from Jan Norris submitting comments, dated October 26, 2015, be considered during Item 8.1.7.
4. That the attached letters from the following requesting to speak be received and considered during Item 8.3.2:
 - Anna Cole, dated October 22, 2015; and
 - Tyler Gould, dated October 23, 2015.
5. That the attached letters from the following requesting to speak be received and considered during Item 11.1:
 - John Cragg, dated October 23, 2015;
 - Peter Brown, dated October 23, 2015;
 - Darren Inglis-McQuay, dated October 23, 2015; and
 - Laurel Beaumont, dated October 25, 2015.

6. That the attached letters from the following submitting comments be received and considered during Item 11.1:
 - Matthew Hooper, dated October 21, 2015;
 - Shaun Murphy, dated October 22, 2015;
 - Carolanne Inglis-McQuay, dated October 22, 2015;
 - John and Sheila Patterson, dated October 22, 2015;
 - Colin and Diane Forest, dated October 23, 2015;
 - Lynn LeMesurier, dated October 23, 2015;
 - Kearney and Lori Healy, dated October 25, 2015;
 - Jane McPhee, dated October 25, 2015;
 - Shan Landry, dated October 25, 2015;
 - Linda Nordlund, dated October 24, 2015;
 - Ariel Gomez, dated October 23, 2015;
 - Cynthia Sotnikow, dated October 24, 2015;
 - Don Berton, dated October 25, 2015;
 - Jacelyn and Lars Larson, dated October 25, 2015;
 - Paul Buffel, dated October 23, 2015;
 - Shirley Fredeen, dated October 25, 2015;
 - Wilma Poole, dated October 24, 2015;
 - Pamela Bennett, dated October 25, 2015;
 - Marcel and Janice Gallays, dated October 23, 2015;
 - Mark Ross, dated October 26, 2015;
 - Carmen Thompson, dated October 26, 2015;
 - Jason Klewchuk, dated October 26, 2015; and
 - Janine Champagne, dated October 26, 2015.
7. That the agenda be confirmed as amended.

3. DECLARATION OF PECUNIARY INTEREST

4. ADOPTION OF MINUTES

Recommendation

That the minutes of Regular Business Meeting of City Council held on September 28, 2015, and the minutes of Special Meeting of City Council held on October 13, 2015, be approved.

5. PUBLIC ACKNOWLEDGMENTS

5.1 Budz-Guenter Memorial Scholarship - 2015/2016 (Files CK. 150-5)

Recipient - Keaton Brissaw

5.2 CUPE 59 / City of Saskatoon Joint Scholarship - 2015 (Files CK. 150-5)

Recipients - Eva Yang and Kiana Mah

(Kiana Mah - unable to attend)

6. UNFINISHED BUSINESS

6.1 Prohibition of Smoking in City-Operated Public Places (Files CK. 185-3)

60 - 64

City Council, at its meeting held on September 28, 2015, deferred consideration of the above to this meeting.

The following is provided:

- Report of the City Solicitor, dated September 28, 2015;
- Bylaw No. 9312; and
- Letter from Adrian Wohl, dated October, 15, 2015.

Recommendation

That permission be granted to introduce Bylaw No. 9312, and give same its FIRST reading.

Recommendation

That Bylaw No. 9312 now be read a SECOND time.

Recommendation

That permission be granted to have Bylaw No. 9312 read a third time at this meeting.

Recommendation

That Bylaw No. 9312 now be read a THIRD time, that the bylaw be passed and the Mayor and the City Clerk be authorized to sign same and attach the corporate seal thereto.

7. QUESTION PERIOD

8. CONSENT AGENDA

Recommendation

That the Committee recommendations contained in items 8.1.1 to 8.1.8; 8.2.1 to 8.2.4; 8.3.1 to 8.3.2; 8.4.1 to 8.4.8; and 8.5.1 be adopted as one motion.

8.1 Standing Policy Committee on Planning, Development & Community Services

8.1.1 Application for Municipal Heritage Property Designation – Mann House (1040 University Drive) (Files CK. 710-65 and PL. 907-1)

65 - 72

Recommendation

1. That the City Solicitor be requested to prepare and bring

forward a bylaw to designate the property at 1040 University Drive as a Municipal Heritage Property under the provision of The Heritage Property Act, with such designation limited to the exterior of the building;

2. That the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed designation; and
3. That \$2,500 be allocated from the Heritage Reserve Fund for supply and installation of a recognition plaque for the property.

8.1.2 Missing and Murdered Aboriginal Women Monument (Files CK. 4040-1 and RS. 215-13-0) 73 - 79

Recommendation

That the front plaza of Saskatoon Police Service Headquarters be approved as the location of the Missing and Murdered Aboriginal Women Monument.

8.1.3 Saskatoon Register of Historic Places (Files CK. 710-1 and PL. 710-27) 80 - 109

Recommendation

1. That the properties be added to the Saskatoon Register of Historic Places, pursuant to the recommendations outlined in the City of Saskatoon Heritage Plan; and
2. That future additions to the Saskatoon Register of Historic Places be delegated to the General Manager, Community Services Department, in consultation with the Municipal Heritage Advisory Committee.

8.1.4 City of Saskatoon's Response to Syrian Refugee Crisis (Files CK. 100-21 and RS. 100-1) 110 - 123

Recommendation

1. That the information be received;
2. That the initiatives identified on page 3 of the October 5, 2015 report of the General Manager, Community Services, with no cost implications, including A, C, D, F, G, and H, be supported;
3. That the Administration report further with respect to any cost implications of items B and E on page 3 of the above report; and
4. That the Administration consult with the provincial and federal government on what they are doing in the areas

identified in the above report, including the issues of transit services and issues around the identified options, and check further with organizations in the community, including faith-based organizations and those supporting refugees, on any updated information for further reporting to the Standing Policy Committee on Planning, Development and Community Services.

- 8.1.5 Communications to Council – Jesse Todd – Request for Bylaw – Disposal of Asbestos Material (Files CK.530-1 and 185-1; PL. 541-6)** 124 - 133

Recommendation

1. That the report of the General Manager, Community Services Department, dated October 5, 2015, be received as information; and
2. That the Administration report back on the matter at a later date on other improvements that might be occurring in Saskatoon.

- 8.1.6 After Hours Barking Dog Complaints (Files CK. 152-1 and RS. 151-1)** 134 - 137

Recommendation

That the report of the General Manager, Community Services Department, dated October 5, 2015, be received as information.

- 8.1.7 Meewasin Valley Authority Northeast Swale Master Plan (Files CK. 4131-5, x CK. 4205-5 and PL. 181-13-7)** 138 - 239

A communication from Christine Varnam, dated October 8, 2015, has been provided.

Recommendation

1. That the Meewasin Northeast Swale Master Plan be approved, in principle;
2. That the Administration be directed to work with the Meewasin Valley Authority to develop a funding strategy and communication plan, consider potential regulations, and assist with additional implementation planning, reporting back at the appropriate time; and
3. That the City Solicitor be directed to amend the “Master Agreement” (2007) between the City of Saskatoon and the Meewasin Valley Authority to include the Northeast Swale of this report to City Council, for review and approval.

8.1.8 Proposed Adult Services Licensing Bylaw Amendments

240 - 252

Recommendation

1. That the proposed amendments to Adult Services Licensing Bylaw, 2012 No. 9011, as outlined in the report of the General Manager, Community Services Department dated October 5, 2015, be approved; and
2. That permission be granted to introduce Bylaw No. 9321, and give same its FIRST reading.

Recommendation

That Bylaw No. 9321 now be read a SECOND time.

Recommendation

That permission be granted to have Bylaw No. 9321 read a third time at this meeting.

Recommendation

That Bylaw No. 9321 now be read a THIRD time, that the bylaw be passed and the Mayor and the City Clerk be authorized to sign same and attach the corporate seal thereto.

8.2 Standing Policy Committee on Finance

8.2.1 Amendments to Policy Nos. C02-030 Purchase of Goods, Services and Work, and A02-027 Corporate Purchasing Procedure (Files CK. 1000-5, AF. 421-002 and 185-1)

253 - 257

Recommendation

1. That the amendments to reporting requirements contained in Council Policy No. C02-030 and Administrative Policy No. A02-027 be adopted;
2. That the City Clerk be requested to update the policies as reflected in the October 5, 2014 report of the CFO/General Manager, Asset and Financial Management Department; and
3. That the City Solicitor be instructed to amend Bylaw No. 8174, The City Administration Bylaw, 2003, to reflect the change in Reporting to Council.

8.2.2 Contract Award Report – May 1 to August 31, 2015 Contracts Between \$50,000 and \$75,000 (Files CK. 1000-1 and AF. 1000-1)

258 - 260

Recommendation

That the report of the CFO/General Manager, Asset and Financial Management Department, dated October 5, 2015, be received as information.

- 8.2.3 Proposed Internal Audit Administration Process and Amendments to Council Policy No. C02-032 (Files CK. 1600-1 and AF. 1600-1)** 261 - 275

Recommendation

That the amendments to Council Policy No. C02-032, Internal Audit Charter, be approved.

- 8.2.4 Request to Sell City-Owned Property – Aspen Ridge Neighbourhood (Files CK. 4215-1, x CK. 4131-32, AF. 4131-1 and LA. 4131-30)** 276 - 299

Recommendation

1. That the Director of Saskatoon Land be authorized to sell 134 single-family lots with legal descriptions of Lots 1 to 38, Block 701, Plan No. to be registered; Lots 1 to 13, Block 702, Plan No. to be registered; Lots 1 to 25, Block 703, Plan No. to be registered; Lots 1 to 16, Block 704, Plan No. to be registered; Lots 1 to 28, Block 705, Plan No. to be registered; and Lots 1 to 14, Block 706, Plan No. to be registered on Fast Crescent, Court, Way and Land through a lot draw process;
2. That if the parcels are not sold through the lot draw process, they will be placed for sale over-the-counter on a first-come, first-served basis;
3. That the Director of Saskatoon Land be authorized to sell Lots 1 and 2, Block 701 to the highest bidder through a public tender process for the intended use of developing Type II Residential Care Homes, Child Care Centres or Pre-Schools with tender conditions and reserve bid prices plus applicable taxes;
4. That any of the pre-designated Type II Residential Care Homes, Child Care Centres or Pre-School lots which are not sold through the public tender process be placed for sale over-the-counter on a first-come, first-served basis for the same intended purpose for a period of one year with conditions specified in the Agreement for Sale;
5. That the Director of Saskatoon Land be authorized to make minor adjustments to the approved pricing that may be necessary to account for changes in servicing costs and lots being returned;

6. That the Director of Saskatoon Land be authorized to administer development controls for the 134 single-family lots; and
7. That the City Solicitor be requested to prepare the Agreements for Sale and that His Worship the Mayor and the City Clerk be authorized to execute the agreements under the Corporate Seal.

8.3 Standing Policy Committee Transportation

- 8.3.1 2016 Neighbourhood Traffic Management Reviews (Files CK. 6320-1 and TS. 6320-1) 300 - 307**

Recommendation

That the eight neighbourhoods selected for 2016 traffic reviews, as part of the Neighbourhood Traffic Management Program, include Stonebridge, Willowgrove, Hampton Village, Silverspring, Grosvenor Park, Lakeridge, Sutherland, and Parkridge.

- 8.3.2 33rd Street - Boulevard Removal - Parking and Traffic Operations (Files CK. 6320-1 and TS. 6320-1) 308 - 378**

Recommendation

1. That the 33rd Street Corridor Study be adopted as the framework for future traffic improvements along 33rd Street, to be undertaken as funding is made available through the annual budget process;
2. That the Administration continue consultations with the 33rd Street Business Improvement District and all abutting Community Associations, regarding the future of traffic improvements along 33rd Street; and
3. That the Administration continue discussions with stakeholders regarding pedestrian safety measures (pedestrian bulbging) and the number of lanes of traffic between Avenues B and G.

8.4 Standing Policy Committee on Environment, Utilities & Corporate Services

- 8.4.1 Continuous Improvement Strategy – Internal Process Review Update (Files CK. 116-1 and CP. 100-1) 379 - 388**

Recommendation

That the report of the General Manager, Corporate Performance Department dated October 13, 2015, be received as information.

8.4.2	Environmental Policy (C02-036) Update (Files CK. 7550-1 and CP. 7540)	389 - 399
	Recommendation	
	That the outlined revisions to the Environmental Policy (C02-036) be approved.	
8.4.3	Integrated Waste Management Annual Report 2014 (Files CK. 430-78 and CP. 7542)	400 - 431
	Recommendation	
	1. That the report of the General Manager, Corporate Performance Department dated October 13, 2015, be received as information; and	
	2. That a copy of this report be forwarded to the Saskatoon Environmental Advisory Committee for information.	
8.4.4	2015 Update to Our Environment: The City of Saskatoon's Environmental Leadership Report (Files CK. 7550-1 and CP. 7542)	432 - 448
	Recommendation	
	1. That the report of the General Manager, Corporate Performance Department dated October 13, 2015, be received as information; and	
	2. That a copy of this report be forwarded to the Saskatoon Environmental Advisory Committee for information.	
8.4.5	Community Solar Power Co-operative – Memorandum of Agreement (Files CK. 2000-5 and WT. 2000-10-9)	449 - 453
	Recommendation	
	That the City Solicitor be requested to prepare a Memorandum of Agreement, in accordance with the terms set out in this report with the SES Solar Co-operative Ltd. and that His Worship the Mayor and the City Clerk be authorized to execute the agreement under the Corporate Seal.	
8.4.6	Network Connectivity Services – Request for Proposal Award (Files CK. 233-1 and CC. 15-0179)	454 - 457
	Recommendation	
	1. That a contract with Shaw Business for the provision of network connectivity services for the civic facilities with the two Active Data Centers DC1 (City Hall) and DC2 (SaskTel)	

for up to thirty six months at an estimated fee of \$412,065, plus applicable taxes be approved; and

2. That Purchasing Services be authorized to issue the necessary Purchase Order.

8.4.7 Parks, Design, Construction, and Maintenance – Civic Service Review (Files CK. 4205-1, x 116-1 and CP. 0116-003) 458 - 475

Recommendation

1. That the improved efficiencies and effectiveness in delivering the service as outlined in this report be received as information;
2. That the Service Level for Parks Pathway Snow Clearing be increased to include snow clearing services to 14 kilometres of Naturalized Park pathways;
3. That the Service Level for Shelterbelt/Buffer Strip Maintenance be increased so that a proactive service level that includes mowing, trimming and garbage pick-ups within the shelterbelt areas can be established; and
4. That the Administration report on a Park Infrastructure Asset Management Plan including a funding strategy to achieve desired service levels.

8.4.8 Xylem Water Solutions – Supply of Proprietary Parts – Blanket Purchase Order (Files CK. 1000-3 and WWT. 1000-1) 476 - 482

Recommendation

1. That the Administration prepare a blanket purchase order with Xylem Water Solutions for the supply of Flygt equipment for up to five years, for a maximum total cost of \$750,000 (including taxes); and
2. That Purchasing Services issue the appropriate blanket purchase order.

8.5 Executive Committee

8.5.1 The Marr Residence Management Board – Request to Increase Membership on Board (Files CK. 225-52) 483 - 484

Recommendation

That the terms of Reference of the Marr Residence Management Board be amended to provide an increase in the composition of the number of members appointed from the general public from three to four.

9. REPORTS FROM ADMINISTRATION AND COMMITTEES

9.1 Asset & Financial Management Department

- 9.1.1 **Boychuck Drive and Highway 16 Interchange Funding Application (Files CK. 1860-1, x CK. 6000-1 and AF. 1860-1)** 485 - 486

Recommendation

That it be resolved that City Council supports the application for a Provincial-Territorial Infrastructure Component grant for Highways and Major Road infrastructure, for the Boychuk and Highway 16 Interchange, from the New Building Canada Fund, and City Council agrees to meet legislated standards, to meet the terms and conditions of the Provincial-Territorial Infrastructure Component (PTIC) program, to conduct an open tendering process, to manage the construction of the project, to fund the municipal share of the construction cost, to fund ongoing operation and maintenance costs, and to follow any mitigation measures as required by the *Canadian Environmental Assessment Act* and *The Environmental Assessment Act (Saskatchewan)*.

9.2 Community Services Department

9.3 Corporate Performance Department

9.4 Transportation & Utilities Department

9.5 Office of the City Clerk

9.6 Office of the City Solicitor

- 9.6.1 **Snowmobile Bylaw Amendment (Files CK. 185-1)** 487 - 492

Recommendation

That permission be granted to introduce Bylaw No. 9323, *The Snowmobile Amendment Bylaw, 2015*, and give same its FIRST reading.

Recommendation

That Bylaw No. 9323 now be read a SECOND time.

Recommendation

That permission be granted to have Bylaw No. 9323 read a third time at this meeting.

Recommendation

That Bylaw No. 9323 now be read a THIRD time, that the bylaw be passed and the Mayor and the City Clerk be authorized to sign same and attach the corporate seal thereto.

- 9.6.2 Farm Land Fixed Taxation Agreement Extension (Files CK. 1920-1) 493 - 505**

Recommendation

That permission be granted to introduce Bylaw No. 9322, *The Brian Robert Sutherland Farm Land Fixed Rate of Taxation Renewal Bylaw, 2015*, and give same its FIRST reading.

Recommendation

That Bylaw No. 9322 now be read a SECOND time.

Recommendation

That permission be granted to have Bylaw No. 9322 read a third time at this meeting.

Recommendation

That Bylaw No. 9322 now be read a THIRD time, that the bylaw be passed and the Mayor and the City Clerk be authorized to sign same and attach the corporate seal thereto.

- 9.7 Standing Policy Committee on Environment, Utilities And Corporate Services**

- 9.8 Standing Policy Committee on Finance**

- 9.9 Standing Policy Committee on Planning, Development And Community Services**

- 9.9.1 Growth Plan to Half a Million – Long-Term Directions and Fall 2015 Public Engagement (Files CK. 4110-2 and PL. 4110-12-7) 506 - 515**

Recommendation

That the report of the General Manager, Community Services Department, dated October 5, 2015, be received as information.

- 9.9.2 2014 Annual Report - Saskatoon Housing Initiatives Partnership (Files CK. 750-1 and PL. 950-14) 516 - 530**

Executive Director, Saskatoon Housing Initiatives Partnership (SHIP) Shaun Dyck will provide a PowerPoint presentation.

Recommendation

That the report of the General Manager, Community Services

Department, dated October 5, 2015, be received as information.

9.10 Standing Policy Committee on Transportation

9.11 Executive Committee

9.12 Other Reports

10. INQUIRIES

11. MOTIONS (NOTICE PREVIOUSLY GIVEN)

- 11.1 9th Street and Lorne Avenue Resolution of City Council - Councillor E. Olauson (May 25, 2015) [Files CK. 6320-1, x CK. 6295-1] 531 - 573

Councillor E. Olauson provided the following Notice of Motion:

"Take notice that at the next Regular Business Meeting of City Council I will move the following motion:

'That the following May 25, 2015 resolution of City Council be rescinded:

"That the directional closure be installed at 9th Street and Lorne Avenue/Idylwyld Drive to restrict westbound vehicular usage.""

Letters from the following are provided:

Request to Speak:

- Robert Clipperton, dated October 11, 2015;
- Tony Scissons, dated October 13, 2015;
- Bruce Stone, dated October 14, 2015; and
- Mike McKague, dated October 20, 2015.

Submitting Comments:

- Peter Brown, dated October 1, 2015;
- Robert Sykes, dated October 12, 2015;
- Gordon Punter, dated October 13, 2015;
- Carolyn Jones, dated October 16, 2015;
- Rob Young, dated October 16, 2015;
- Karen Dyck, dated October 16, 2015;
- Mark Radu, dated October 18, 2015;
- Bob Trach, dated October 18, 2015;
- Marie Jensen, dated October 19, 2015;
- Charles Foster, dated October 19, 2015;

- Derek and Nora Potts, dated October 19, 2015;
- Randall and Suzanne Winnitowy, dated October 19, 2015;
- Lisa Kozmyk, dated October 20, 2015;
- Gerard Rodier, dated October 20, 2015;
- Vincent Martin, dated October 20, 2015;
- Lyndsay Zelinski, dated October 20, 2015;
- L. Neault, dated October 20, 2015;
- Neale Buettner, dated October 20, 2015;
- Eric Cline, dated October 20, 2015;
- Ksenija Smiljic, dated October 20, 2015; and
- Laurel Beaumont, dated September 15, 2015 and October 9, 2015, submitting approximately 80 form letters, including 96 signatures opposed to the proposed closure. (Samples attached containing additional comments).

11.2 9th Street at Lorne Avenue Traffic Restriction Update [File No. CK. 6320-1] 574 - 578

Recommendation

That the information be received.

12. GIVING NOTICE

13. URGENT BUSINESS

13.1 8th Street East and Lorne Avenue Sanitary Chamber Installation Emergency Design-Build [File No. CK. 7820-1] 579 - 582

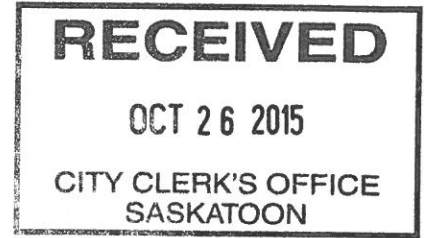
Recommendation

1. That the Administration prepare a contract with Hamm Construction Ltd. for the supply and installation of a sanitary chamber, and upgrade of sanitary and water mains at the intersection of 8th Street East and Lorne Avenue for a total estimated value of \$660,000 (including GST); and
2. That the City Solicitor be requested to prepare the appropriate agreement and that His Worship the Mayor and the City Clerk be authorized to execute the agreement under the Corporate Seal.

14. IN CAMERA SESSION (OPTIONAL)

15. ADJOURNMENT

From: M Hein <mehein@sasktel.net>
Sent: October 26, 2015 8:33 AM
To: Web E-mail - City Clerks
Subject: Proposed smoking bylaw change



Submitted on Monday, October 26, 2015 - 08:33
Submitted by anonymous user: 96.125.241.213
Submitted values are:

First Name: M
Last Name: Hein
Email: mehein@sasktel.net
Confirm Email: mehein@sasktel.net
Phone Number: (306) 683-3697

==Your Message==

Service category: City Bylaws & Policies

Subject: Proposed smoking bylaw change

Message: The proposed smoking bylaw change is absolutely ridiculous. I fully agree with indoor facility bans but an outdoor ban goes way too far. Do we live in a communist country all of a sudden? Where are the rights of smokers? Come on city council concentrate on running the city...not violating a persons rights

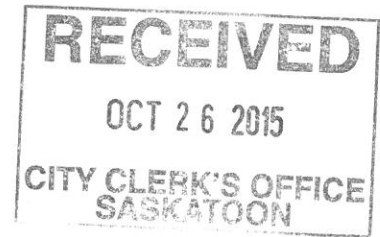
Attachment:

Would you like to receive a short survey to provide your feedback on our customer service? The information you share will be used to improve the service we provide to you and all of our customers.:
No

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/405/submission/47196>

4131-5
x4205-5

From: Jan Norris <jannorri@gmail.com>
Sent: October 23, 2015 3:44 PM
To: Web E-mail - City Clerks
Subject: Request to speak at Council Meeting Oct. 26



Submitted on Friday, October 23, 2015 - 15:44
Submitted by anonymous user: 70.64.84.163
Submitted values are:

First Name: Jan
Last Name: Norris
Email: jannorri@gmail.com
Confirm Email: jannorri@gmail.com
Phone Number: (306) 665-3430

==Your Message==

Service category: City Council, Boards & Committees
Subject: Request to speak at Council Meeting Oct. 26

Message:

Hello.

I would like to address Council this Monday Oct. 26 regarding the Meewasin Valley plan for the NE Swale.

Thank you.

Jan Norris

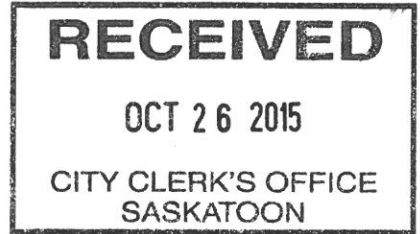
Attachment:

Would you like to receive a short survey to provide your feedback on our customer service? The information you share will be used to improve the service we provide to you and all of our customers.:
No

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/405/submission/46963>

4131-5
x4205-5

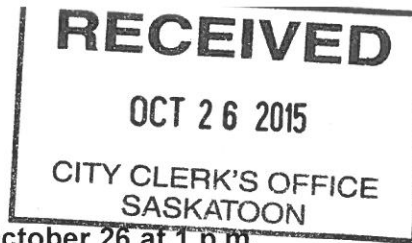
From: Louise Jones <louise.jones@sasktel.net> on behalf of Louise Jones <louise.jones@sasktel.net>
Sent: Friday, October 23, 2015 4:06 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Friday, October 23, 2015 - 16:05
Submitted by anonymous user: 204.83.109.202
Submitted values are:

Date: Friday, October 23, 2015
To: His Worship the Mayor and Members of City Council
First Name: Louise
Last Name: Jones
Address: 206 111th Street
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 1T2
Email: louise.jones@sasktel.net
Comments: I would like to speak to council concerning File No. Ck. 4131-5,x CK 4205-5 and PL. 181-13-7

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46966>



4131-5
x 4205-5

Presentation to Council on Monday, October 26 at 1 p.m.

Your Worship, Council thank you for the opportunity to speak on behalf of the Northeast Swalewatchers. On October 6, I addressed the Standing Committee on Planning, Development and Community Service. You will have read the presentation made at that time.

As you are aware, the Northeast Swalewatchers is an umbrella group of concerned citizens, including experts in many fields related to the health of the 26 kilometre long Northeast Swale, and many organizations dedicated to conserving and protecting natural species and spaces. These organizations include the Saskatoon Nature Society, Saskatchewan Environmental Society, Saskatoon Wildlife Federation, Ducks Unlimited, Native Plant Society of Saskatchewan, Saskatoon Light Pollution group, and Wild About Saskatoon. Many citizens of Saskatoon share our interest in protecting this jewel of Saskatoon's natural areas. Having little parcels of land is not the same thing as having continuous wildlife corridors. The one may appear to serve the same function, but it doesn't. As Toddi Steelman, now Chair of the MVA Board said at a meeting of the MVA Board in December 2014, "**the value of the Swale is in its wholeness.**" But with current development plans, the "wholeness" of the Swale is already becoming just the remnants of the previously undisturbed Swale.

We appreciate the work that the MVA and City have done to try to reduce damage to the part of the Swale under their jurisdiction through the Northeast Swale Guidelines and now the Master Plan. Nevertheless it is obvious that building roads through, and subdivisions adjacent to, this area will have long lasting negative effects. At this stage our group is most concerned about reducing these impacts through construction's careful adherence to the strictures in place through a high standard of environmental monitoring, through modification to some of the design features, and even additional provisions to ensure that human behaviour is appropriate to accessing this invaluable resource.

Even with various protections such as the predevelopment monitoring protocol and the NE Swale guidelines in place, unnecessary damage has already occurred . Without an ongoing and concerted effort, more errors are bound to be made, whether as a result of lack of communication between departments, or through misunderstandings on the part of the construction people on the ground. Time pressures often result in priority placed on quick action rather than on environmental protection. There is an urgent need for a change in administrative process to ensure that what is planned is executed as intended.

The City also needs to TAKE RESPONSIBILITY for the Swale as a conservation area, and do everything that is required to support the MVA in the conservation effort. As future users enter the Swale and use the roadway crossings, they must immediately physically recognize that this is a Special Conservation Area. This is only somewhat done with reduced road speeds, animal crossing signs, Swale identification signs, but could be MADE SPECIAL by design. With this objective in mind, the Swale Watchers strongly suggest the following:

1. The Zoning Bylaw should be amended to designate the Swale as a Special Conservation Area. Obvious signage must be erected at the entrance and exit of all Swale-crossing roadways to make people aware of its special status. In

addition, roadways and neighbourhoods near the Swale should be zoned as "buffer areas" that require special consideration under the City's Environmental Plan.

2. All Swale-crossing roadways must be designed for the conservation environment, and not just as standard thoroughfares! As such, they should all observe the same speed limit for traffic – 50 km/hr. Steps must be taken to ensure that this speed limit is observed, possibly with speed cameras or in-road rumble strips.

3. Roadway lighting for all crossings of the Swale must be consistent, with well shielded, glare-free orange lighting, and not blue LED lighting that will disrupt the ability of animals to safely cross. In addition, the Zoning Bylaw should be amended to ensure that excessive noise and stray light from commercial, industrial or residential areas do not enter the Swale, including the prohibition of electronic message boards and lit signage within direct view of the Swale.

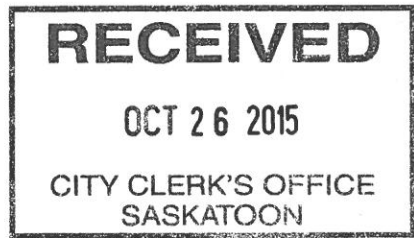
4. The development of Environmental Policies must be accelerated so the Plans resulting from the Policies can be integrated into construction plans immediately. At present, most environmental policies are scheduled to be completed AFTER roadway construction for Central Avenue and the North Commuter Parkway is complete! The *intention* of these plans and policies must be relayed to design and construction crews, even if the documents have not been finalized.

5. The City must quickly create an overall integrated Master Plan that includes all aspects of the Swale vision and integrates with the MVA Swale Master Plan

Urban growth pressures continue, relentlessly, to have a deleterious effect on the natural world and therefore on us, whether we fully realize this or not. We need to get this right for the Swale and for other important natural areas in the future.

4131-5
x4205-5

From: Candace Savage <candace.savage@sasktel.net>
Sent: Friday, October 23, 2015 1:23 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Friday, October 23, 2015 - 13:23
Submitted by anonymous user: 71.17.144.227
Submitted values are:

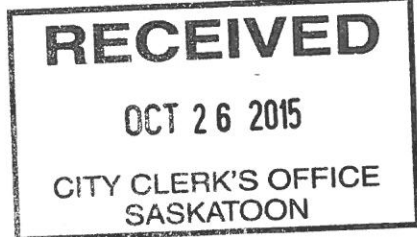
Date: Friday, October 23, 2015
To: His Worship the Mayor and Members of City Council
First Name: Candace
Last Name: Savage
Address: 302 Albert Avenue
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N1G1
Email: candace.savage@sasktel.net

Comments:
I would like to speak to Council at its Regular Business meeting on Monday, October 26, 2015, to comment on the Meewasin Valley Authority Northeast Swale Master Plan.

Thank you.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46934>

4131-5
x 4205-5



From: Jan Norris <jannorri@gmail.com>
Sent: October 26, 2015 8:34 AM
To: City Council
Subject: Form submission from: Write a Letter to Council

Submitted on Monday, October 26, 2015 - 08:34
Submitted by anonymous user: 70.64.84.163
Submitted values are:

Date: Monday, October 26, 2015
To: His Worship the Mayor and Members of City Council
First Name: Jan
Last Name: Norris
Address: 422 10th St. E
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0C9
Email: jannorri@gmail.com

Comments:
First of all I think it's important to recognize that the City's projects in the NE swale runs contrary to our new strategic plan, developed after much consultation with the people, involving hundreds of meetings and costing a lot of money, and on those grounds the plan should have been altered or rejected.

Our goal for example is to be a Greener City, and our strategic plan says:

"We will know we are achieving our goal when significant natural features, including ... wetlands and remnant patches of natural prairie... are protected and many have been enhanced."

There is no way in which building 3 housing developments around it and 2 highways through it can be seen as protecting and enhancing the swale.

Unfortunately today I cannot ask Council to reconsider the new suburbs or the roads and highways - which will be built by the way on Saskatoon's most fertile soil.

Nor can I ask you to reexamine the questionable assumption behind all this: that our population will reach 500,000 in the near future.

I can only address the MVA's proposal before us.

I am happy that some of its \$14 million price tag will be for culverts for amphibians to go under the roads and highways. A member of SwaleWatchers pointed out that these culverts should have no bottoms, so that the frogs and salamanders could travel through them on mud, water or earth rather than corrugated steel or concrete. This could be a species-saving amendment that I hope will be instituted.

But I believe there is an important omission in the Meewasin plan that needs to be addressed, and this is the lack of effective corridors for other wildlife; overpasses that would allow animals to cross

above the roads and highways safely, as have been built in hundreds of other places all over the world.

Without such corridors I think we can be sure there will be lots of roadkill and possibly serious accidents that may even result in human fatalities.

In this regard I have three questions:

- 1) What makes us think there won't be deer – or worse, moose – trying to cross these roads and highways? They are attracted to wetlands, and the swale is, by definition, a wetland, one they have been using for a very long time.
- 2) Driving on a four lane highway, with no curves and not much traffic, maybe running late, how many of us can be sure we would rigorously obey a speed limit of 50 kph?
- 3) When are we going to start taking the lives of other animals seriously?

The consultants said there would not be enough animals trying to cross the swale's roadways to justify spending money on wildlife overpasses. I would like to know how they came to this conclusion. I have read there are 50 deer living in or around the swale and there has been at least one moose sighting.

Therefore I encourage members of Council to ask:

If we are going to spend \$14.1 million on paths, signage, bathrooms, parking lots and a buffer zone so that we humans can use the swale, can we not spend something so the rabbits, porcupines, ferrets, deer, foxes, and other animals have a chance against our lethal cars and trucks?

Furthermore, since there was apparently no effort made to determine the cost of constructing wildlife corridors, how can we know whether it would be worth it or not?

How do we determine how much we should spend to save a non-human life, or a human one? And what if that non-human is one of the last of its kind on earth?

These are difficult questions I know, and should probably not be left to one or two consultants.

So I urge you to take the time, before it's too late, to consider what's really important, now and for future generations, and not to be swayed by false economies.

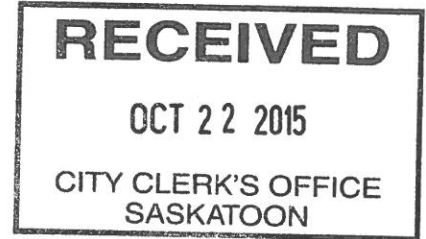
We have one chance with the swale, and if this precious place is wounded beyond recovery, we will be blamed - and our descendants may consider our treatment of wild animals as barbaric we consider the treatment of human slaves.

It's time to consider all the earth's species as precious, not just our own.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47197>

6320-1

From: Anna Cole <uncannycanuck@gmail.com>
Sent: October 22, 2015 11:10 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Thursday, October 22, 2015 - 11:10
Submitted by anonymous user: 167.129.128.45
Submitted values are:

Date: Thursday, October 22, 2015
To: His Worship the Mayor and Members of City Council
First Name: Anna
Last Name: Cole
Address: 1110 Tiffin Cres
City: Saskatoon
Province: Saskatchewan
Postal Code: S7L 3R3
Email: uncannycanuck@gmail.com

Comments:

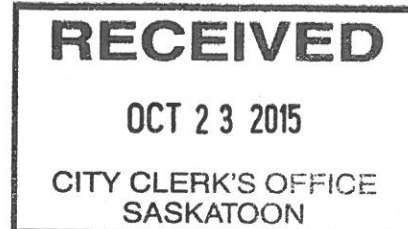
As the President of the Hudson Bay Park - Mayfair - Kelsey Woodlawn Community Association, I request to make a presentation on item 8.3.2 33rd Street - Boulevard Removal - Parking and Traffic Operations (Files CK. 6320-1 and TS. 6320-1). I wish to speak in support of the requests of the 33rd St. BID:

1. A designation of 33rd Street as a minor arterial corridor (not major);
2. Single lane traffic from the furthest west corner of Ave B to Ave G;
3. Parking limits of 2 hours from the furthest west corner of Ave B to Ave G, and up to the allies north and south of 33rd Street; and
4. Pedestrian bulbing from the furthest west corner of Ave B to Ave G with each intersection having 2 bulbs (one on the north and one on the south side of 33rd Street) at designated crossing areas.

Thank you.
Anna Cole

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46730>

From: Tyler Gould <tyler.gould@33rd.ca>
Sent: Friday, October 23, 2015 11:21 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Friday, October 23, 2015 - 11:20
Submitted by anonymous user: 174.2.88.82
Submitted values are:

Date: Friday, October 23, 2015
To: His Worship the Mayor and Members of City Council
First Name: Tyler
Last Name: Gould
Address: Suite J, 511 - 33rd Street West
City: Saskatoon
Province: Saskatchewan
Postal Code: S7L0V7
Email: tyler.gould@33rd.ca

Comments:

Hi there,

I would like to request to speak to Council on Monday, October 26th on behalf of the 33rd Street Business Improvement District. I would like to identify issues and request the following:

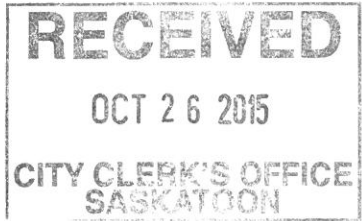
1. A designation of 33rd Street as a minor arterial (not major);
2. 33rd Street be limited to single lane traffic from the furthest west corner of Ave B to Ave G;
3. Parking limits of 2 hours from the furthest west corner of Ave B to Ave G, and up to the alleys north and south of 33rd Street; and
4. Pedestrian bulbing from the furthest west corner of Ave B to Ave G with each intersection having 2 bulbs (one on the north and one on the south side of 33rd Street) at designated crossing areas.

Thank you,
Regards,
Tyler

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46918>

63207

From: JP cragg <thepaulk@hotmail.com>
Sent: October 23, 2015 2:17 PM
To: Web E-mail - City Clerks
Subject: Request to speak at city council meeting Monday oct 26 at 1 pm



Hello,

I am writing to request to speak at the city council meeting Monday oct 26 at 1 pm regarding the closer of the 9th street on ramp

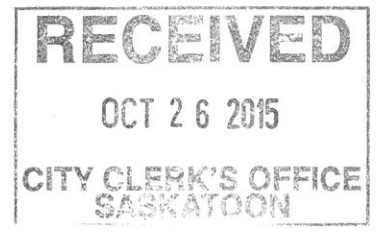
Thank you,

John Cragg

Sent from my iPhone

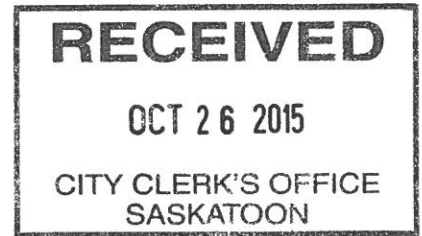
6320-1

From: Peter Brown <peterbrown@sasktel.net>
Sent: October 23, 2015 2:17 PM
To: Web E-mail - City Clerks
Subject: Monday's meeting on 9th Street closure



Hi,
I'd like to speak at the meeting. Thanks.
Peter Brown

From: Darren Inglis-McQuay <dinglis_mcquay@icloud.com>
Sent: Friday, October 23, 2015 7:02 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



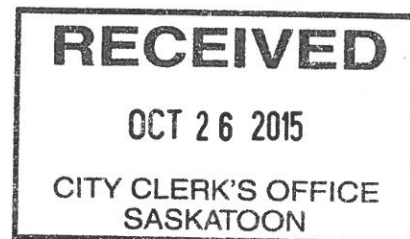
Submitted on Friday, October 23, 2015 - 19:01
Submitted by anonymous user: 64.110.197.3
Submitted values are:

Date: Friday, October 23, 2015
To: His Worship the Mayor and Members of City Council
First Name: Darren
Last Name: Inglis-McQuay
Address: 117 9th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A1
Email: dinglis_mcquay@icloud.com

Comments: I would like to speak at the October 26th meeting to discuss the motion to rescind the approval for the temporary direction closure at 9th St and Lorne Ave.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46977>

From: Laurel Beaumont <busstop@sasktel.net>
Sent: Sunday, October 25, 2015 10:40 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Sunday, October 25, 2015 - 10:40
Submitted by anonymous user: 207.47.243.71
Submitted values are:

Date: Sunday, October 25, 2015
To: His Worship the Mayor and Members of City Council
First Name: Laurel
Last Name: Beaumont
Address: 1012 McPherson Ave
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N0Y5
Email: busstop@sasktel.net

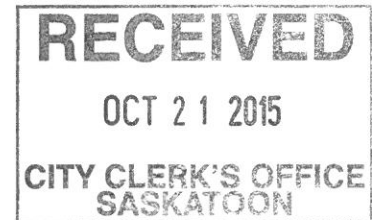
Comments:

I wish to advise The Office of The City Clerk that I request an opportunity to address Council at the October 26th Meeting in regards to the Motion to Rescind the resolution to close 9th St at Lorne Ave.

Thank you,
Laurel Beaumont

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47097>

From: Matthew Hooper <hooper.holdings@gmail.com>
Sent: Wednesday, October 21, 2015 10:10 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Wednesday, October 21, 2015 - 10:09
 Submitted by anonymous user: 184.69.57.250
 Submitted values are:

Date: Wednesday, October 21, 2015
 To: His Worship the Mayor and Members of City Council
 First Name: Matthew
 Last Name: Hooper
 Address: 2927 Calder Avenue
 City: Saskatoon
 Province: Saskatchewan
 Postal Code: S7J 4X2
 Email: hooper.holdings@gmail.com
 Comments:
 RE: Closure of 9th St access to Idylwyld Drive.

My corporation, Hooper Holdings Inc. owns the 12 suite apartment block at 1010 McPherson Avenue. I feel that the discussed closure would negatively affect my street, and my property, and I ask that you please consider the following items in your discussions on October 26th.

-The 9th street access is used by people coming from Rotary park, and Saskatchewan Crescent E as McPherson Avenue is the most convenient access to the park. Closing the 9th St access would cause this traffic to continue South down McPherson Avenue to 8th St.

-There is a stop sign at McPherson and 8th which will mean vehicles will need to stop, and line up to turn right on to busy 8th st whereas it's free flow onto the access at 9th st.

-Several houses on McPherson have front drive access, and the increased traffic will hinder property access. This will be especially true for the house on the corner of 8th st and McPherson which has a front drive right before the stop sign. Only one 9th st property has a front drive, and it's back near McPherson, not near the freeway access. Nearly all 9th st properties on the North side use the back alley to access their properties.

-9th St is much wider than McPherson Avenue, and can support bidirectional traffic, and parking while McPherson avenue is only wide enough to allow one directional traffic with parked cars on both sides of the street. Not only is 9th St wider, but population density on McPherson is significantly higher due to my apartment building, and the fourplex across the street.

-The counters will confirm this I'm sure, but by watching this intersection for many years, it's fairly obvious that the majority of 9th st traffic turns North toward Rotary Park, rather than continue West onto the freeway, and therefor closing the access will cause a negligible decrease in traffic flow down 9th St.

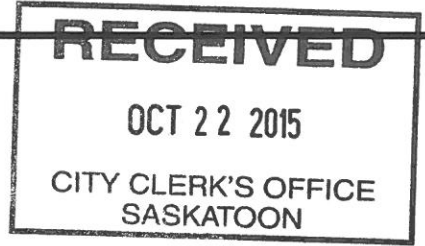
Thank you very much for considering my comments on this matter. I really do think that this closure will have a very negative impact, (It already has) and I truly hope that this decision is rescinded on October 26th. I truly feel that if 9th St. traffic is a concern, the very best thing would be to restore the access to Sask Crescent E from Victoria Avenue. This would help everyone.

Thanks,

Matt Hooper
Hooper Holdings Inc.
306-341-1263

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46378>

6320-1



From: Murphy, Shaun <shaun.murphy@usask.ca>
Sent: October 22, 2015 9:25 AM
To: Web E-mail - City Clerks
Subject: 9th St closure

Dear Councillors,

I am writing to support the closure of 9 th street. I live on the 200 block of 9 th street and I have noted the reduction of traffic volume. I also note the reduced speed of vehicles. This is residential neighbourhood and the recent reduction supports the community climate of this area. I have also noted more children playing in front yards along 9th. I cannot claim this is part of the closure, but it is interesting that it was noticeable post closure. The closure of the freeway entrance was decided upon in community consultations. Consultations which I and many residents attended. At the last community consultation there was not a single dissenting voice against the closure of the entrance. I must express my dismay at the revisiting of this issue after so many hours went into the consultation process. I was heartened by the consultation process and thought them well attended and well run. To now reverse decisions arrived at during the meetings raises concerns about the spending of tax dollars and a lack of respect for democratic action.

Please keep the closure in place.

yours truly,

Shaun Murphy
216 9 St E

6320-1

From: Carolanne Inglis-McQuay <inglis_mcquay@mac.com>
Sent: October 22, 2015 8:44 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Thursday, October 22, 2015 - 08:44
Submitted by anonymous user: 64.110.197.3
Submitted values are:

Date: Thursday, October 22, 2015
To: His Worship the Mayor and Members of City Council
First Name: Carolanne
Last Name: Inglis-McQuay
Address: 117 9th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A1
Email: inglis_mcquay@mac.com

Comments:

My name is Carolanne Inglis-McQuay, and I live at 117 9th Street East. I have been an active participant in the City-led process to consult and engage with community members about measures to make the Nutana neighbourhood safer, and participated in the public meeting of City Council where the City voted to undertake two important activities towards this end - the permanent partial closure of the on-ramp at the end of 9th street east, and some important work in the Temperance Street area. I was also present for the discussion and debate by the Councillors, which saw the issue raised about protecting the westerly most Elm tree on 9th Street East while designing the closure.

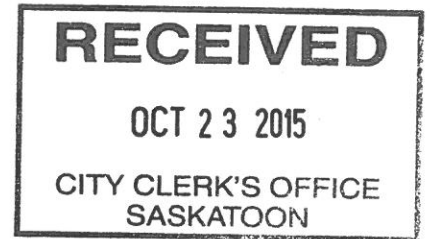
I would like to reiterate my support for the permanent partial closure at the end of 9th Street East, subject to the incorporation of the tree into the closure design. I would also like to reiterate my support for the City-led consultation process that was undertaken which led to the outcome of our elected leaders voting in favour of these two projects.

The process that led to the decision which is being questioned now, after the fact, involved over two years of work, three open public meetings where any person could raise comments to the staff members designing the solutions to the issues, used real-time data as evidence to support the recommendations to the Council, and ultimately led to a decision being made by our democratically elected leaders in a public forum, where any member of the public was afforded an opportunity to speak on the matter. This was an excellent and fair process that was followed by the City. Just because people don't like the decision that was made, doesn't mean that they get to unravel the process afterwards by garnering 'support' for their position using a factually incorrect petition. This is contrary to the fair and due process what our City regularly undertakes.

The temporary partial closure has turned our street into a proper residential street where I am not afraid to let my child walk up and down the sidewalk now for fear of people racing along in excess of 50-60km/h. Thank you for that. And I don't believe that it has put any undue safety pressure on other parts of the neighbourhood - but I will let your traffic experts speak to that.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46701>

From: John & Sheila Patterson <jcmpatterson@gmail.com>
Sent: Thursday, October 22, 2015 5:57 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Thursday, October 22, 2015 - 17:57
Submitted by anonymous user: 71.17.212.253
Submitted values are:

Date: Thursday, October 22, 2015
To: His Worship the Mayor and Members of City Council
First Name: John & Sheila
Last Name: Patterson
Address: 118 9th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A2
Email: jcmpatterson@gmail.com

Comments:

Saskatoon City Council:e

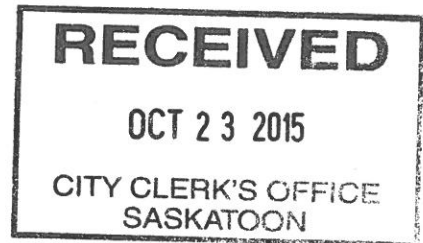
Please be advised that as property tax paying residents of 9th Street East, we request City Council move to retain the traffic diversion recently installed on our residential 9th Street East. Finally our street is not serving as a commercial corridor to the freeway, and we are enjoying the safety from the commercial and commuter traffic this change has provided.

John & Sheila Patterson, 118 9th St. E. tel: 306-665-4749.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46815>

From: Colin & Diane Forest <dianems@sasktel.net>
Sent: Friday, October 23, 2015 10:43 AM
To: City Council
Subject: Form submission from: Write a Letter to Council

Submitted on Friday, October 23, 2015 - 10:42
Submitted by anonymous user: 71.17.148.185
Submitted values are:



Date: Friday, October 23, 2015
To: His Worship the Mayor and Members of City Council
First Name: Colin & Diane
Last Name: Forest
Address: 323 - 9th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A5
Email: dianems@sasktel.net
Comments:

We wish to address the issue of the closure of the 9th Street freeway access. We wish to thank the City for going through the process of organizing traffic calming meetings. Everyone in our neighborhood received an invitation to attend these traffic calming meetings. These meetings were held on Tuesday, October 1, 2013, Tuesday, September 9, 2014 and Thursday, January 8, 2015. My husband and I attended each and every one of these meetings because we felt it necessary to help make decisions when it came to traffic on our street. We have recently been told that there has been a petition circulating in our neighborhood to rescind this closure. As a resident who is directly affected by the rescinding of this closure, we were not approached to sign this petition. We also understand that the majority of people who signed the petition do not live on 9th Street. They now realize they may be inconvenienced by the extra 30 seconds it takes to drive to 8th Street. The time for these people to voice their opinions was at the meetings, which everyone had the opportunity to attend. Now after all the time and effort put into the meetings by both the City and the neighborhood, these people want the closure rescinded. Where were they when all this was being discussed?

As part of the process the City performed several traffic studies which concluded that traffic between Victoria and the Freeway ramp was well above levels for a residential neighbourhood, with some vehicles traveling in excess of 80 km/hr. Much of the traffic on 9th Street to the freeway consisted of impatient people taking a short-cut to avoid traffic on 8th Street. When the meetings were held there were many discussions on methods of slowing traffic on 9th Street. Over the last few years, several traffic-calming methods have been tried without much success. Since the closure of the Freeway access there has been a noticeable reduction of traffic volume and associated noise with less cars moving down 9th Street. The City is currently conducting traffic counts along 9th Street, which we believe will confirm a great reduction in traffic. We feel this issue has gone through due process, which provided the neighbourhood with an excellent solution. The time to make those changes was at the neighbourhood traffic calming meetings.

We are directly affected by the 9th Street closure and wish to inform you that the closure is welcomed and appreciated by us; 9th Street is no longer a raceway due to this closure. The nature of our neighborhood is transitioning back to a family-oriented neighborhood. There are currently 12 children on the 300 block of 9th street. Now the families of the 12 little children on our street can breathe a

sigh of relief and not have to worry about the cars racing to get to the freeway. Since the closure of the access to the freeway, our daughter can safely cross the street to catch her bus in the morning. Before the closure, she was dodging traffic that sped down 9th Street. With 8th Street being the major arterial route there is no need for 9th Street to act as a second access. The loss of a child being hit by a car greatly outweighs any inconvenience for people having to access the Freeway via 8th Street.

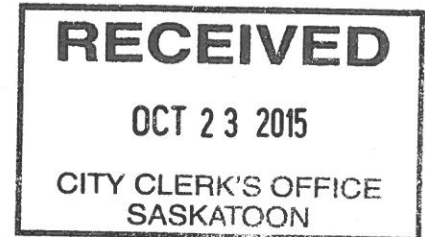
This is a neighbourhood issue and has no effect on traffic flows throughout the rest of the City. When this issue comes before council again we ask that you put yourselves in the place of people who actually live on 9th Street and who will once again have to deal with the danger and noise associated with increased traffic if the ramp is reopened. This closure has been recommended by the City traffic department as a result of the neighbourhood traffic calming meetings. Our primary concern is for the safety of the residents in the neighborhood. We sincerely implore you to vote to keep the ramp closure in place.

Thank you,

Colin and Diane Forest

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46902>

From: Lynn LeMesurier <lynnlemesurier@gmail.com>
Sent: Friday, October 23, 2015 11:10 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Friday, October 23, 2015 - 11:09
Submitted by anonymous user: 64.110.233.42
Submitted values are:

Date: Friday, October 23, 2015
To: His Worship the Mayor and Members of City Council
First Name: Lynn
Last Name: LeMesurier
Address: 113 9th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A1
Email: lynnlemesurier@gmail.com

Comments:

Dear City Council,

I am writing this letter with hope that the council will uphold their decision for the directional closure of 9th Street East. This decision was arrived at in a fair and democratic way. The City and this neighbourhood followed the proper procedures to arrive at this decision that speaks to the issue of safety for residents of this neighbourhood.

The temporary closure which is now in effect, while not being obeyed by all, has certainly seen a reduction in the number of problem vehicles at this time.

Our household firmly believes that a directional change in traffic on McPherson, in other words, making the one block of McPherson between 8th & 9th Street, a one way street would help to diversify the traffic in the area. This decision would allow the utilization of Victoria Avenue, which was designed to withstand heavier traffic, play the proper role in feeding this portion of the Nutana neighbourhood

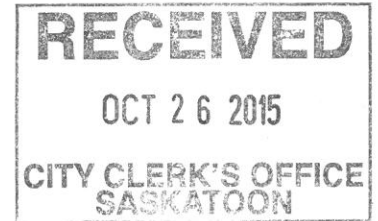
Again, I would ask council members to vote to uphold the decision to make the directional closure of 9th Street East a permanent one.

Respectfully submitted,
Bob & Lynn LeMesurier

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46916>

6320-1

From: Kearney Healy <kearneyh@yahoo.com>
Sent: October 25, 2015 3:24 PM
To: Web E-mail - City Clerks
Cc: Clark, Charlie (External)
Subject: Urgent for October 26th City Council Meeting



To Mayor Atchison and City Council,

Re: Ninth Street Directional Closure

Please do not prematurely end the temporary closure of Ninth Street. We think that the closure may serve the city as it considers its future.

We've lived on the 200 block of Ninth Street East for 30 years. About 10 years ago a young doctor and his young family, who lived on our block, told us that they were moving away. They explained that they felt the high speed traffic racing down 9th to the Idylwyld Freeway was too dangerous for children. They said it was wrong for 9th Street to be used that way.

Recently, I experienced that. At about 8:00am on a winter day when there were deep ruts on 9th Street, I was scraping snow from my car's windows. I went from the driver's side windows to go to the rear window. I slipped on the ice and fell to my right, my head in one of the deep ruts. I was dazed but I saw a small car coming straight at me. I rolled away as quickly as I could. The little car tried to steer left out of the ruts. It temporarily lost control and slid in the fresh snow. Ultimately, no harm or damage was done. The small car got back into the ruts and hurried to the exit onto the freeway.

I realize the young doctor was right. It is wrong to treat a residential street as a feeder street for a highway. And it's wrong to treat a feeder street as a residential street. There are many variables like 9th Street not being plowed and having the snow removed after snow storms as 8th Street would be. There are so many unforeseen consequences, some with very serious implications, for example, children playing while cars are hurrying with so little benefit...8th Street is a short block away.

Please do not prematurely end the gathering of evidence.

Kind regards,

Kearney and Lori Healy

6320-1

From: Jane McPhee <mcjane@sasktel.net>
Sent: October 25, 2015 4:55 PM
To: Web E-mail - City Clerks
Subject: Current Temporary direction closure at 9th Street & Lorne Ave



To Whom It May Concern:

I am a resident on the 200 block of 9th Street, (219) and it has come to my attention that some neighbourhood residents want to rescind the approved closure.

As a resident who had my dog struck by a car racing down this street to the freeway prior to the roundabout placement at 9th and McPherson, and who values the lives of the children and others in this block, I respectfully request that the Motion be defeated at the October 26th Council Meeting to rescind the approval for the current temporary direction closure at 9th Street and Lorne Avenue.

I believe that the appropriate assessment of the suitability of the closure in deterring cars speeding down 9th Street depends on the temporary measure being in place long enough to assess the outcome of the change, as specified in the Nutana Traffic Review Report.

Please defeat the motion to rescind the approval for the closure.

Respectfully,

Jane McPhee (219 9thSt)

Sent from my iPad

6320-1

RECEIVED

OCT 26 2015

**CITY CLERK'S OFFICE
SASKATOON**

From: Shan Landry <shanl@sasktel.net>
Sent: October 25, 2015 11:00 PM
To: Web E-mail - City Clerks
Subject: Current Temporary Direction Closure at 9th Street and Lorne Avenue

To Whom it May Concern,

As a resident at 215 9th Street East, it has come to my attention that some of my neighbours want City Council to rescind the recently approved closure of 9th street that prevents access to Lorne Avenue and Idylwyld.

Over the years we have had growing concerns about the speed of vehicles using 9th Street to access Idylwyld freeway. I respectfully request that the motion to rescind the temporary closure be defeated at the October 26th Council meeting and the closure remain for the planned trial period. This trial period will allow sufficient time to determine the outcomes of this closure as specified in the Nutana Traffic Review Report.

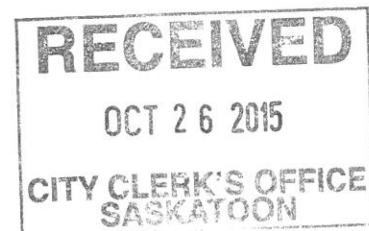
Please defeat the motion to rescind the approval for the closure.

Respectfully, Shan Landry 215 9th Street East

Sent from my iPad

6350-1

From: Linda Nordlund <lindanordlund4@gmail.com>
Sent: October 24, 2015 5:42 PM
To: Web E-mail - City Clerks
Subject: Ninth Street Directional closure



Dear City Council,

I am writing to strongly support leaving the direction closure at ninth street and Lorne avenue in place.

Please defeat the motion at the OCT 26 Council meeting to rescind the approval for the current temporary direction closure at 9 th street and Lorne Avenue. I request that it be left in place so that its suitability can be assessed for the period specified in the Nutana traffic Review Report.

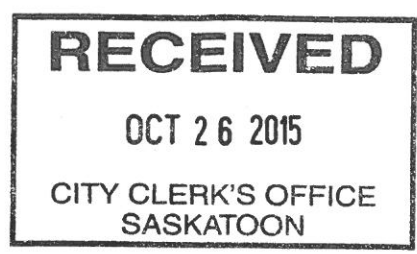
There has been much , much less traffic down our street since this has been put in place. We especially notice that there are much fewer speeding cars racing down the street , building up speed to enter the freeway. The reduction in traffic is amazing...our street is sooooo much more peaceful without the speeding cars. Even the traffic calming circle did not seem to slow them down or deter people from speeding on ninth street.

Please. please leave the ninth street directional closure in place...it has made such a difference in our neighborhood. I can't believe there is anyone who would want it removed,

Yours truly
Linda Nordlund
218-9 th street east
S7N 0A4
306 653-5649

6320-1

From: Linda Nordlund <lindanordlund4@gmail.com>
Sent: Saturday, October 24, 2015 5:48 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Saturday, October 24, 2015 - 17:48
Submitted by anonymous user: 204.83.111.47
Submitted values are:

Date: Saturday, October 24, 2015
To: His Worship the Mayor and Members of City Council
First Name: Linda
Last Name: Nordlund
Address: 218 - 9 th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A4
Email: lindanordlund4@gmail.com

Comments:
Dear City Council,

I am writing to strongly support leaving the direction closure at ninth street and Lorne avenue in place.

Please defeat the motion at the Oct 26 Council meeting to rescind the approval for the current temporary direction closure at 9 th street and Lorne Avenue. I request that it be left in place so that its suitability can be assessed for the period specified in the Nutana traffic Review Report.

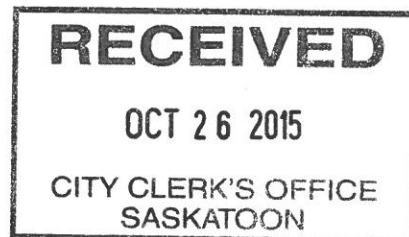
There has been much , much less traffic down our street since this has been put in place. We especially notice that there are much fewer speeding cars racing down the street , building up speed to enter the freeway. The reduction in traffic is amazing...our street is sooooo much more peaceful without the speeding cars. Even the traffic calming circle did not seem to slow them down or deter people from speeding on ninth street.

Please. please leave the ninth street directional closure in place...it has made such a difference in our neighborhood.

Yours truly
Linda Nordlund

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47050>

From: Ariel Gomez <ariel.gomez@lightsource.ca>
Sent: Friday, October 23, 2015 6:37 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Friday, October 23, 2015 - 18:36
Submitted by anonymous user: 71.17.243.101
Submitted values are:

Date: Friday, October 23, 2015
To: His Worship the Mayor and Members of City Council
First Name: Ariel
Last Name: Gomez
Address: 212-10 St. East, Apt 310
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N2T6
Email: ariel.gomez@lightsource.ca

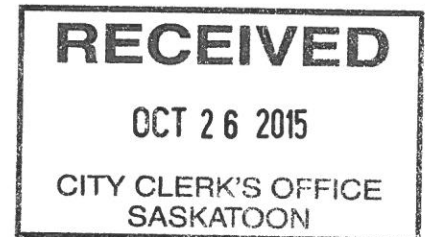
Comments:

I am writing to oppose the closure of the 9th street exit to Idylwyld. This is a route used by me, many neighbors and our visitors to access Idylwyld.
I don't think this decision was consulted with all the affected neighbors and should not be carried out without taking into consideration all the affected people's opinion.

Best,
Ariel

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46976>

From: Cynthia Sotnikow <cynsot@sasktel.net>
Sent: Saturday, October 24, 2015 9:11 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Saturday, October 24, 2015 - 21:10
Submitted by anonymous user: 64.110.218.146
Submitted values are:

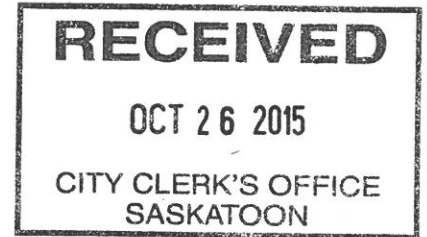
Date: Saturday, October 24, 2015
To: His Worship the Mayor and Members of City Council
First Name: Cynthia
Last Name: Sotnikow
Address: 234 4th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7H 1J1
Email: cynsot@sasktel.net
Comments:

Regarding the partial closure of access onto Idylwyld Drive from the 100 block of 9th Street East on October 1, 2015 after efforts made through a 2 year community consultation process. I now understand that in less than 30 days since the partial closure was implemented a motion has been introduced to reverse City Councils decision. Community consultation provides a forum for opinions to be heard over an appropriate time frame. This same courtesy along with an appropriate timeframe should be exercised when a motion is made to reverse a decision. It is disappointing to think that our City Council would take such an unbalanced approach to decision making. Please consider the safety concerns presented through the community consultation process and evaluate the resulting change in traffic flow over a reasonable time frame.

Regards, Cynthia

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47065>

From: Don Berton <donberton@gmail.com>
Sent: Sunday, October 25, 2015 8:48 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



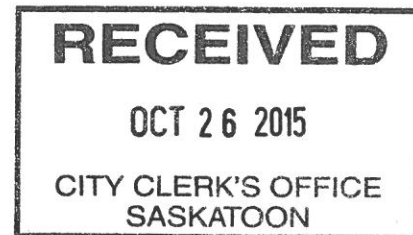
Submitted on Sunday, October 25, 2015 - 20:47
Submitted by anonymous user: 204.83.111.47
Submitted values are:

Date: Sunday, October 25, 2015
To: His Worship the Mayor and Members of City Council
First Name: Don
Last Name: Berton
Address: 218 - 9th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A4
Email: donberton@gmail.com

Comments: I am responding to the trial directional closure at 9th Street and Lorne Avenue. This closure is working. Traffic flow is reduced including speeds. It finally feels like this is a residential street with residential traffic flows. There is no need for an access to Circle Drive at 9th Street when there is an access one block away on 8th Street. There is enough traffic noise on 9th Street now from the 8th Street traffic. Allowing access to Circle Drive from 9th Street is dangerous. It is icy in winter and Lorne Avenue is curved from 8th Street to 9th Street and offers poor visibility of the oncoming traffic that is already at merging speed from 8th Street. The traffic circle that was installed at 9th Street and McPherson did nothing to slow down traffic. There were many near misses and of course accidents. Drivers did not yield nor did they slow down. I observed a fire truck unable to negotiate this circle efficiently. Good thing they weren't responding to fire. Please make this closure permanent. Removing this closure only benefits a very small group of people.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47147>

From: Jacelyn & Lars Larson <jacelyn.larson@me.com>
Sent: Sunday, October 25, 2015 9:08 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Sunday, October 25, 2015 - 21:07
Submitted by anonymous user: 70.64.67.148
Submitted values are:

Date: Sunday, October 25, 2015
To: His Worship the Mayor and Members of City Council
First Name: Jacelyn & Lars
Last Name: Larson
Address: 303 - 9th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A5
Email: jacelyn.larson@me.com

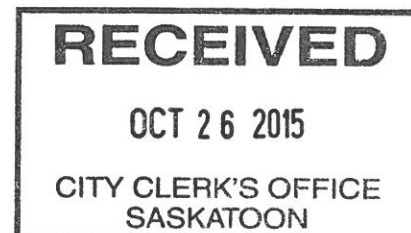
Comments:

Please defeat the motion at the October 26th Council Meeting to rescind the approval for the current temporary direction closure at 9th St and Lorne Ave. We ask that the temporary measure be left in place for the purposes of assessing its suitability for the period specified in the Nutana Traffic Review Report. We have already found, in the short period of time, improvement in reducing traffic flow. The flow of traffic was also often noted to be at increased speed going down the street.

Thank you,
Jacelyn & Lars Larson

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47151>

From: Paul Buffel <paul.b@sasktel.net>
Sent: Friday, October 23, 2015 8:29 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Friday, October 23, 2015 - 20:29
Submitted by anonymous user: 64.110.217.53
Submitted values are:

Date: Friday, October 23, 2015
To: His Worship the Mayor and Members of City Council
First Name: Paul
Last Name: Buffel
Address: 216 9th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A4
Email: paul.b@sasktel.net
Comments:
RE: 9th Street Directional Closure

I am a resident on the 200 block of 9th Street. I attended the city organized public traffic meetings that began in 2014. I was present during the discussions on options of how to address the issue of the on ramp access to the freeway from 9th Street. I was impressed with the organization and thoroughness of the process and pleased with the outcome of a one year temporary closure to be studied regarding its effectiveness.

I was home when a neighbour from McPherson Ave came to my door with a petition against the road closure. We had a friendly conversation (I had never met her before) in which I learned she had not attended the public traffic meetings and had not known about them. She was also unaware that several points on her petition were false and misleading including a point about a large Elm tree that the city was to be removing. (I know of some neighbours who signed the petition simply due to this false statement).

We discussed the process that had led to the temporary closure and I urged her to get the correct information before proceeding any further. It appears that she did not do this.

It is my hope that the time and effort myself and many of my neighbours have put into the public consultation process since 2014 will be given serious consideration over a petition signed by 20 people who were presented compelling yet misleading and/or false statements. The city's letter that was distributed to our mailboxes adequately explained the process and dispelled concerns that were falsely stated in the petition.

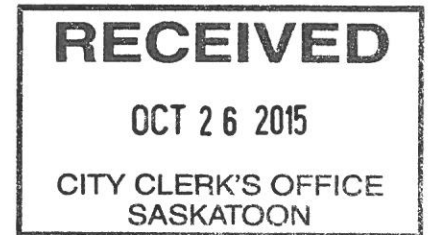
I ask that City Counsel defeat the motion at the October 26th Council Meeting to rescind the approval for the current temporary direction closure at 9th Street and Lorne Ave.

I ask that the temporary measure be left in place for the purpose of assessing its suitability for the period specified in the Nutana Traffic Review Report.

Most sincerely,
Paul Buffel

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46982>

From: Shirley Fredeen <shirley.fredeen@usask.ca>
Sent: Sunday, October 25, 2015 7:56 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



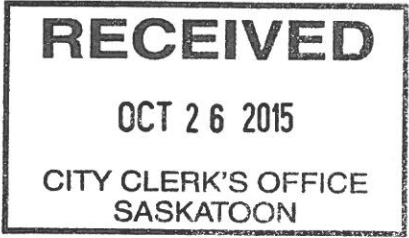
Submitted on Sunday, October 25, 2015 - 19:56
Submitted by anonymous user: 96.125.246.97
Submitted values are:

Date: Sunday, October 25, 2015
To: His Worship the Mayor and Members of City Council
First Name: Shirley
Last Name: Fredeen
Address: e207 9th St
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A3
Email: shirley.fredeen@usask.ca

Comments: Hello, I understand that some people in this neighbourhood have asked that the pilot closing of the 9th St entrance to the freeway be halted. I would like to register my concern. The decision was arrived at through a lengthy and open process. Furthermore, since this is just a pilot, we need the information in order to make a good decision.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47139>

From: Wilma Poole <handson87@hotmail.com>
Sent: Saturday, October 24, 2015 9:46 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



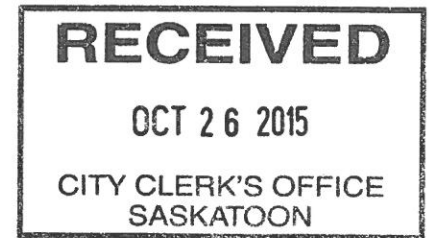
Submitted on Saturday, October 24, 2015 - 09:46
Submitted by anonymous user: 198.245.113.186
Submitted values are:

Date: Saturday, October 24, 2015
To: His Worship the Mayor and Members of City Council
First Name: Wilma
Last Name: Poole
Address: 325-9th Street
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N0A5
Email: handson87@hotmail.com

Comments: Please defeat the motion at the October 26th Council Meeting to rescind the approval for the current temporary direction closure at 9th St and Lorne Ave. I have noticed a considerable decrease in fast traffic since the closure, which is great. I ask that the temporary measure be left in place for the purposes of assessing its suitability for the period specified in the Nutana Traffic Review Report. Thank you.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47017>

From: Pamela Bennett <pmbennett2008@gmail.com>
Sent: Sunday, October 25, 2015 11:05 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Sunday, October 25, 2015 - 11:05
Submitted by anonymous user: 70.64.100.86
Submitted values are:

Date: Sunday, October 25, 2015
To: His Worship the Mayor and Members of City Council
First Name: Pamela
Last Name: Bennett
Address: 1206 Melrose Ave
City: Saskatoon
Province: Saskatchewan
Postal Code: S7H1Y6
Email: pmbennett2008@gmail.com
Comments:
RE: partial closure on the 100 block of 9th Street East.

In May 2015, after 2 years of consultations, the City voted to close the second on ramp onto Idylwyld because of safety issues. I support this decision to help increase safety for residents in the area, particularly those on the 100 block of 9th Street East.

I have recently learned someone is now opposing the closure and has asked a city a councillor to introduce a motion to undo the decision.

I am in support of the consultation process that took place. It was done appropriately and the outcome will allow for a more safer residential street. Tax payers should not have to pay for reopening the discussions and possibly public consultation on this topic.

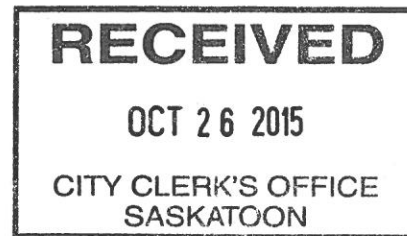
Thank you,
Pam Bennett

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47100>

6320-1

From: Marcel & Janice Gallays <marcel.gallays@sasktel.net>
Sent: Friday, October 23, 2015 4:25 PM
To: City Council
Subject: Form submission from: Write a Letter to Council

Submitted on Friday, October 23, 2015 - 16:25
Submitted by anonymous user: 71.17.148.141
Submitted values are:



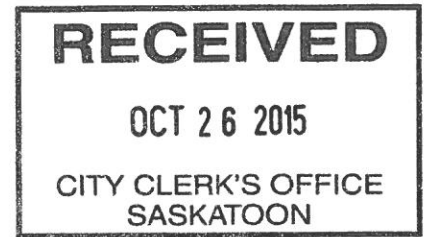
Date: Friday, October 23, 2015
To: His Worship the Mayor and Members of City Council
First Name: Marcel & Janice
Last Name: Gallays
Address: 312 - 9th St. E.
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N0A6
Email: marcel.gallays@sasktel.net

Comments: We wish to support the city initiative to address traffic concerns in our neighbourhood by asking City Council to follow through with the temporary installation of a traffic barrier at 9th St. and Lorne Ave. including the study of its effects for the one year duration. Since its recent installation, the barrier has, in our opinion, already reduced the number and speed of vehicles using 9th St. as an alternate route to the Buckwold Bridge, thus making our street safer. Our daily visiting grandchildren and our neighbours' children as well as our aging residents are grateful for the work done thus far. We beseech you to defeat the motion at the Oct 26th Council meeting to rescind the approval of the current temporary direction closure at 9th St. & Lorne Ave. We really need to give this temporary measure time to be studied to gauge its effectiveness at making our neighbourhood safer. Thank you for the opportunity to address this matter.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46968>

6320-1

From: Mark Ross <markross@sasktel.net>
Sent: October 26, 2015 8:32 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



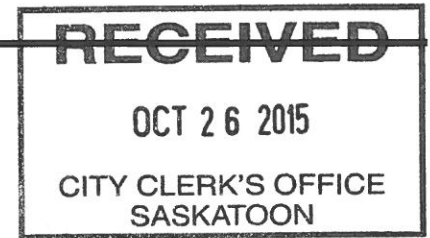
Submitted on Monday, October 26, 2015 - 08:31
Submitted by anonymous user: 204.83.109.134
Submitted values are:

Date: Monday, October 26, 2015
To: His Worship the Mayor and Members of City Council
First Name: Mark
Last Name: Ross
Address: 129 9th street east
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A1
Email: markross@sasktel.net

Comments: Please defeat the motion at the October 26th Council Meeting to rescind the approval for the current temporary direction closure at 9th street and Lorne ave. We ask that the temporary measure be left in place for the purposes of assessing its suitability for the period specified in the Nutana Traffic Review Report.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47195>

6320-1



From: marross2@sasktel.net
Sent: October 26, 2015 8:24 AM
To: Web E-mail - City Clerks
Subject: 9th street directional closure

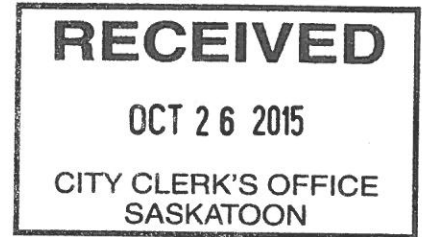
Please defeat the motion at the Oct. 26th council meeting to rescind the approval for the current temporary directional closure at 9th street and Lorne ave. We ask that the temporary measure be left in place for the purposes of assessing its suitability for the period specified in the Nutana Traffic Review Report.

It is incredibly sad that some people won't let this project take its natural course for the sake of safety in the neighborhood. My family and I support the study and anticipate the findings will make our street and our community safer. Any person who does not support safety for all in the community is not considered a community member in my books.

Thank you
Mark Ross
129 9th street east

6350-1

From: Carmen Thompson <carmen.thompson@sasktel.net>
Sent: October 26, 2015 8:30 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



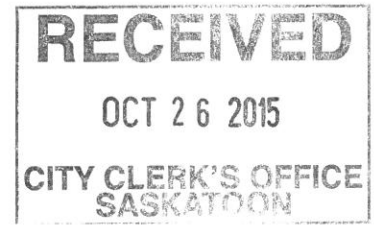
Submitted on Monday, October 26, 2015 - 08:30
Submitted by anonymous user: 71.17.246.189
Submitted values are:

Date: Monday, October 26, 2015
To: His Worship the Mayor and Members of City Council
First Name: Carmen
Last Name: Thompson
Address: 115-9th St E
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N0A1
Email: carmen.thompson@sasktel.net

Comments: Please defeat the motion at the Oct. 26th council meeting to rescind approval for the temporary directional closure at 9th St and Lorne Ave. We ask that the measure be left in place. As a homeowner in the 100 block of 9th Ave, we constantly have to worry about the large volume of traffic going by my home to access the on ramp and the directional closure has seemed to make a very positive difference.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47194>

From: Jason Klewchuk <jklewchuk@gmail.com>
Sent: Monday, October 26, 2015 8:55 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Monday, October 26, 2015 - 08:54
Submitted by anonymous user: 204.83.110.237
Submitted values are:

Date: Monday, October 26, 2015
To: His Worship the Mayor and Members of City Council
First Name: Jason
Last Name: Klewchuk
Address: 212 9th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A4
Email: jklewchuk@gmail.com
Comments:
Good morning.

I am writing to add my opinion to the discussion/controversy surrounding the 9th Street directional closure where 9th Street East intersects with the ramp to Idylwyld Drive.

It is my understanding that the current closure, which is a temporary test to see if a difference is made with traffic routing through the neighborhood and the streets that are used by local traffic, was actually a properly researched and documented proposal. This is in stark contrast to the information provided by the individual who came to our door asking for signatures to stop this test and remove the installation, and is the major reason that our household refused to sign the petition.

We live in the 200 block of 9th Street East, and have already noticed a marked reduction in the number of cars that go speeding down our street, many quite obviously well over the speed limit. I can't say for sure that all of the traffic that used to go down our street was headed for the freeway and was bypassing the heavier traffic on 8th Street, but you can see from our house the intersection where the roundabout used to be, and comments indicating that people would speed around the traffic stop and not use it as intended are accurate, which makes me believe that most of these vehicles were NOT just local traffic.

I would like voice my support that this motion to remove the closure be defeated, and that this test, based on actual research and recommendation, be give the opportunity to show whether or not the closure is a viable option to the traffic problems we have in this community. The desire of a few individuals to slightly reduce the amount of actual local traffic on their street at the expense of the safety of the people on another street due to non-local traffic does not seem like a good enough reason to totally reverse and eliminate the efforts to find a solution that have been made so far. Particularly since it seems that the proposed solution might actually be making a difference.

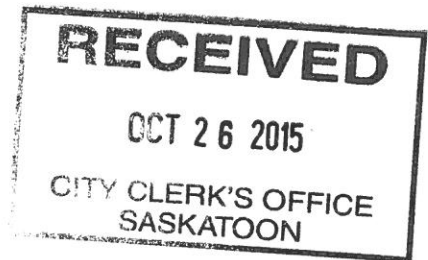
Sincerely,

Jason Klewchuk
212 9th Street East
Saskatoon, Saskatchewan
S7N 0A4

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47203>

6320-1

From: Janine Champagne <jjc2285@gmail.com>
Sent: October 26, 2015 9:50 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Monday, October 26, 2015 - 09:50
Submitted by anonymous user: 64.110.197.187
Submitted values are:

Date: Monday, October 26, 2015
To: His Worship the Mayor and Members of City Council
First Name: Janine
Last Name: Champagne
Address: 114 9th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A2
Email: jjc2285@gmail.com
Comments:

Good Morning,

My note is in response to the directional closure of Ninth Street to Idywyld Dr.

This year I made the choice to relocate to this beautiful city and yes, I did choose this neighbourhood. Since the closure, myself and my neighbours as well as their infant babies and toddlers have finally enjoyed being outside without having to worry about drivers that race down this street to get on to Idywyld when they could very easily and without time restraints travel another one half of a block and use 8th Street which I believe is a main artery and connector.

I do not want to see these barricades removed. As a mother and grandmother the possibility of someone being injured or killed by a speeding car on this street scares me. We have several small children and babies here now that will soon be travelling back and forth to visit each other. Naturally I am very concerned for their safety.

I also understood that the barricades would be up for a minimum of one year followed by a review. What is the sudden rush by some people to take them down? Is it possibly due to the fact that they chose not to be involved with the process when it started? That's not a good enough reason for me. They did have a chance to be heard.

Please defeat the motion to remove the barricades.

Sincerely,

Janine Champagne

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47211>

Prohibition of Smoking in City-Operated Public Places

Recommendation

That City Council consider Bylaw No. 9312

Topic and Purpose

The purpose of this report is to provide City Council with Bylaw No. 9312, *The Smoking Control Amendment Bylaw, 2015* which implements City Council's decision to amend *The Smoking Control Bylaw, 2004* to expand existing smoking prohibitions to include outdoor public places which are owned or operated by the City of Saskatoon.

Report

On August 20, 2015, at its Regular Business Meeting, City Council received a report from the City Solicitor, recommending options for the expansion of existing smoking prohibitions to include outdoor public spaces operated by the City such as parks, outdoor sports-fields, public squares and recreation areas. City Council resolved that the expanded prohibitions be enforced through bylaw and that the City Solicitor prepare the necessary bylaw amendment.

Further, on August 20, 2015, during the Regular Business Meeting of City Council, a question was raised regarding a possible exemption under *The Tobacco Control Act* (the "Act") for the use of tobacco within specialty shops, such as cigar shops. While the Act no longer contains a specific "exemptions" section (section 12 was repealed in 2004), section 11 provides that the prohibition does not apply to "a separate enclosed ventilated place ...that meets the prescribed requirements" or "a prescribed place, premises or vehicle". *The Tobacco Control Regulations* define a separate enclosed ventilated place, and limit them to "within a special-care home or a personal care home". This exemption would not apply to cigar/specialty shops.

The Tobacco Control Regulations do not define or list any prescribed place, premises or vehicle, therefore none currently exist as an exemption and, to conclude, cigar/specialty shops would not be exempt to the application of the Act.

Another report, prior to year's end, will be brought to City Council which further amends the Bylaw to prohibit the use of electronic cigarettes (or "vaping") in all places where smoking is currently prohibited. The prohibitions, as with smoking, will not allow for an exemption in vaping shops. It may be noted that, Edmonton City Council recently voted to allow for such an exemption in vaping shops.

Prohibition of Smoking in City-Operated Public Places

In accordance with City Council's instructions, we are pleased to submit Bylaw No. 9312, *The Smoking Control Amendment Bylaw, 2015*, for Council's consideration. The proposed Bylaw No. 9312 will come into force January 1, 2016.

The January 1, 2016 implementation date is proposed for a number of reasons. It will allow for public education of the expanded scope of the Bylaw. In addition, the January 1, 2016 date will provide adequate time to permit the City to bring its "public spaces" now subject to the smoking prohibition into compliance with the Bylaw requirements. For example, the time will permit signage to be erected and smoking paraphernalia such as ashtrays to be removed.

Finally, we would propose to have the amendments contained in Bylaw No. 9312 come into force concurrent with the enactment of the vaping prohibition amendments. We undertake to have those amendments to Council for its consideration in advance of the new year.

Attachment

1. Proposed Bylaw No. 9312, *The Smoking Control Amendment Bylaw, 2015*

Report Approval

Written by: Derek Kowalski, Solicitor
Reviewed by: Christine G. Bogad, Director of Administrative Law
Approved by: Patricia Warwick, City Solicitor

Admin Report – Prohibition of Smoking in City-Operated Public Places.docx
237.0004-djk-2.docx

BYLAW NO. 9312

The Smoking Control Amendment Bylaw, 2015

The Council of The City of Saskatoon enacts:

Short Title

1. This Bylaw may be cited as *The Smoking Control Amendment Bylaw, 2015*.

Purpose

2. The purpose of this Bylaw is to amend *The Smoking Control Bylaw, 2004* to expand existing smoking prohibitions to include all public places which are operated by the City of Saskatoon.

Bylaw No. 8286 Amended

3. *The Smoking Control Bylaw, 2004* is amended in the manner set forth in this Bylaw.

Preamble Amended

4. The Preamble is amended:

- (a) by striking out the following:

“And whereas, under the provisions of Section 12 of *The Tobacco Control Act*, the Province of Saskatchewan has designated certain public places or premises in which smoking is permitted except in designated non-smoking areas;” and

- (b) by adding the following to the final paragraph after “licensed premises,”:

“outdoor public places operated by the City,”.

Section 2 Amended

5. Section 2 is amended:

(a) by adding the following after subsection 2(e):

“(e.1) “outdoor public place” means any outdoor space owned or operated by the City of Saskatoon that is open to the public or to which the public is customarily admitted or invited, and includes parks, swimming pools, playgrounds, outdoor sports-fields, public squares or recreation areas, but does not include streets or sidewalks;”

(b) by adding the following after subsection 2(i):

“(i.1) “sidewalk” means the part of the street adapted to the use of or ordinarily used by pedestrians;”

(c) by adding the following after subsection 2(j):

“(j.1) “street” means a street as defined in *The Cities Act*;”

Subsection 4(1) Amended

6. Subsection 4(1) is amended by adding the following after clause 4(1)(e):

“(e.1) outdoor public places operated by the City;”

Coming into Force

7. This Bylaw shall come into force on January 1, 2016.

Read a first time this day of , 2015.

Read a second time this day of , 2015.

Read a third time and passed this day of , 2015.

Mayor

City Clerk

From: Adrian Wohl <a_j_a_x_jr@msn.com>
Sent: October 15, 2015 7:49 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Thursday, October 15, 2015 - 19:48
Submitted by anonymous user: 174.2.78.83
Submitted values are:

Date: Thursday, October 15, 2015
To: His Worship the Mayor and Members of City Council
First Name: Adrian
Last Name: Wohl
Address: 3814 John a macdonald road
City: Saskatoon
Province: Saskatchewan
Postal Code: S7L5L3
Email: a_j_a_x_jr@msn.com

Comments: I noticed plenty of people on Facebook complaining about cigarette smoking bans on parks grounds and various other places, my comment to them is to not ban smoking everywhere, but to ban sales of tobacco in general to save lives and promote less litter from cigarette butts, if there are no sales of cigarettes then there is less litter and less people complaining about second hand smoke issues because tobacco would be banned completely and it would save lives so it's not a bad thing, then nobody can access tobacco in Saskatchewan and it would be a banned product, that's what I think is better than trying to put up cheesy no smoking signs nobody cares to obey anyhow, like the ones at the hospital of royal university area, everyone doesn't care about the no smoking sign, public and patients of the hospital still smoke there, it was a dumb idea putting a sign there anyhow, nobody really cares everyone is going to smoke where ever they feel like, so if you want to ban cigarette sales completely so nobody can smoke in Saskatoon or in Saskatchewan as a whole I'm all for a tobacco ban and I've smoked for like fifteen years since being in high school is where I started because other students smoked too, a total ban is for a cleaner environment and cleaner air, no cigarette butts on grounds and no second hand smoke if tobacco is banned, so it's a good thing.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/45613>



STANDING POLICY COMMITTEE ON PLANNING, DEVELOPMENT AND COMMUNITY SERVICES

Application for Municipal Heritage Property Designation – Mann House (1040 University Drive)

Recommendation of the Committee

1. That the City Solicitor be requested to prepare and bring forward a bylaw to designate the property at 1040 University Drive as a Municipal Heritage Property under the provision of The Heritage Property Act, with such designation limited to the exterior of the building;
2. That the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed designation; and
3. That \$2,500 be allocated from the Heritage Reserve Fund for supply and installation of a recognition plaque for the property.

History

At the October 5, 2015 meeting of the Standing Policy Committee on Planning, Development and Community Services, a memo of support from Municipal Heritage Advisory Committee, dated September 2, 2015 was considered. A report of the General Manager, Community Services Department, dated October 5, 2015, was also considered.

Your Committee considered and supports the recommendation in the report of the General Manager, Community Services Department dated October 5, 2015.

Attachment

October 5, 2015 report of the General Manager, Community Services Department.

Application for Municipal Heritage Property Designation – Mann House (1040 University Drive)

Recommendation

That a report be forwarded to the Standing Policy Committee on Planning, Development and Community Services with a recommendation to City Council:

1. That the City Solicitor be requested to prepare and bring forward a bylaw to designate the property at 1040 University Drive as a Municipal Heritage Property under the provision of *The Heritage Property Act*, with such designation limited to the exterior of the building;
2. That the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed designation; and
3. That \$2,500 be allocated from the Heritage Reserve Fund for supply and installation of a recognition plaque for the property.

Topic and Purpose

The purpose of this report is to consider an application by the property owner requesting 1040 University Drive (The Mann House) to be designated as a Municipal Heritage Property.

Report Highlights

1. The Mann House is located on a corner lot in the Varsity View neighbourhood. This historic place features a one- and three-quarter storey dwelling built in 1912.
2. The heritage value of the Mann House resides in its long association with Owen Mann, a long-time member of City Council. Other notable individuals who lived at this location include: Thomas Watson, physician and head of the Saskatoon Cancer Clinic; and W.E. Walter, a Special Representative for the Canadian National Railway (CNR) Colonization Department.
3. A formal evaluation of the 1040 University Drive was conducted. The Administration is recommending designation of 1040 University Drive as a Municipal Heritage Property.

Strategic Goal

The report supports the City of Saskatoon's (City) Strategic Goal of Quality of Life. As a community, we find new and creative ways to showcase our city's built, natural, and cultural heritage.

Background

The current owners of 1040 University Drive have requested Municipal Heritage Designation of this property.

Application for Municipal Heritage Property Designation – Mann House (1040 University Drive)

Report

Description of the Historic Place

The Mann House features a one- and three-quarter storey dwelling at the corner of University Drive and McKinnon Avenue North (see Attachment 1). Designed by Architect Norman Thompson, the 1912 home has a stucco exterior, multiple window groupings with rectangular panes on the upper level, a hipped roof, and an enclosed front veranda. While not considered to be exemplary of a particular architectural style, the large home does add character to the Varsity View neighbourhood despite modifications to its original exterior.

Heritage Value

The heritage value of the Mann House resides in its long association with Owen Mann. Mann was a long-standing City Councillor, who served from 1969 to 1979, and again from 1980 to 1994. He was a member of the Engineering Faculty at the University of Saskatchewan and brought his professional skills to City Council, where he served on countless committees over the years and took a strong interest in Saskatoon's infrastructure. Mann lived at 1040 University Drive from 1963 until his passing in 2009.

Other notable individuals who have lived at 1040 University Drive include W.E. Walter, who worked as a Special Representative for CNR's Colonization Department and Dr. Thomas Watson - physician and head of the Saskatoon Cancer Clinic.

Additional information on the heritage value of 1040 University Drive is included in the property's Statement of Significance (see Attachment 2).

Evaluation

A formal evaluation of the exterior of the building has been conducted, and the Administration is of the opinion that the property is eligible for designation as a Municipal Heritage Property. The home's exterior remains in fair condition, and conservation work will be required in the future. Particular attention to the stucco, wood details on the upper level, and exposed rafter tails is required. The small attached garage may require extensive rehabilitation or removal in the future. It should be noted that the attached garage, veranda, stucco, and wood trim detailing are not original to the 1912 home.

The current property owners are committed to maintaining the original character of the home and have already undertaken a number of restoration projects to the dwelling's interior. The Administration is recommending that only the building's exterior be designated as a Municipal Heritage Property.

Options to the Recommendation

City Council has the option of not designating this building as a Municipal Heritage Property.

Public and/or Stakeholder Involvement

Public and/or stakeholder consultations are not required.

Application for Municipal Heritage Property Designation – Mann House (1040 University Drive)

Communication Plan

All municipal heritage properties are marked with a bronze plaque on site that describes the heritage significance of the property. If designation is approved, the property will also be added to the Saskatoon Register of Historic Places.

Policy Implications

The proposal complies with Civic Heritage Policy No. C10-020.

Financial Implications

The amount of \$2,500 from the Heritage Reserve Fund would be allocated for the fabrication of the bronze plaque and installation on the property.

Other Considerations/Implications

There are no environmental, privacy, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

If City Council recommends designation of the property, a date for a public hearing will be set. This date will be set in accordance with the provisions in *The Heritage Property Act*.

Public Notice

Public notice, pursuant to Section 3 of Public Notice Policy No. C01-021, is not required.

If designation as a Municipal Heritage Property is recommended, the designation will be advertised in accordance with the provisions in *The Heritage Property Act*, which requires that the Notice of Intention to Designate be advertised at least 30 days prior to the public hearing.

Attachments

1. Location Plan
2. Statement of Significance
3. Photographs of Subject Property

Report Approval

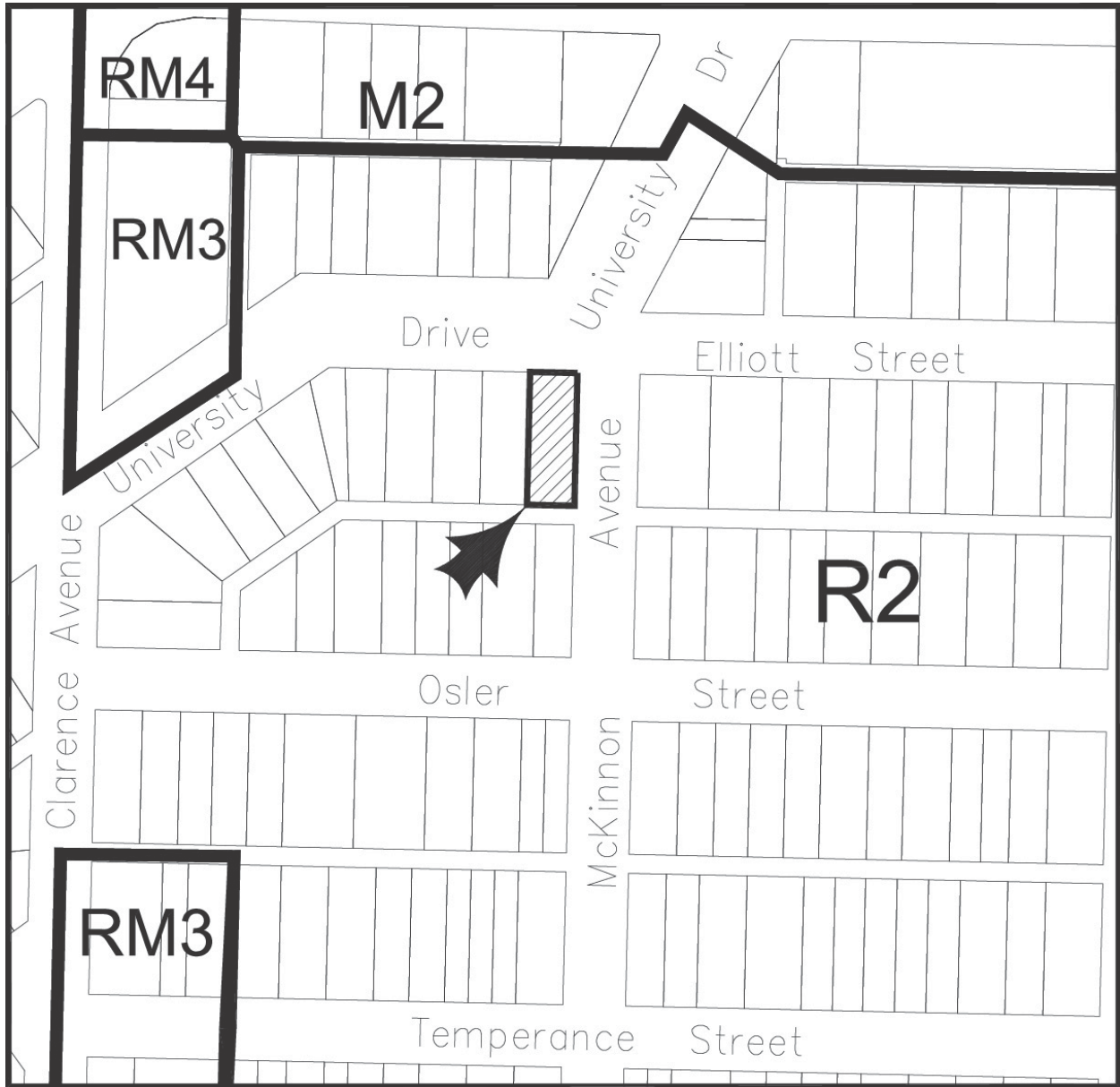
Written by: Catherine Kambeitz, Heritage and Design Coordinator, Planning and Development

Reviewed by: Laura Hartney, Acting Director of Planning and Development

Approved by: Randy Grauer, General Manager, Community Services Department

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Location Plan



MUNICIPAL HERITAGE PROPERTY

1040 University Drive



N:\Planning\MAPPING\Requests\Internal\Heritage\Heritage Site - 1040 University Drive.dwg

Statement of Significance

Mann House – 1040 University Drive

Neighbourhood:	Varsity View
Date Constructed:	1912
Development Era:	1906 – 1913 (Pre WWI)
Architectural Style:	-
Architect:	Norman L. Thompson
Builder:	-
Designation:	Municipal
Original Use:	Private Residence
Current Use:	Private Residence



Source: City of Saskatoon

Description of Historic Place

The Mann House features a one and three-quarter storey dwelling at the corner of University Drive and McKinnon Avenue North. Designed by Architect Norman Thompson, the 1912 home is located in the neighbourhood of Varsity View and was once home to former City Councillor and University of Saskatchewan Engineer, Owen Mann.

Heritage Value

The heritage value of the Mann House lies in its association with Owen Mann, who was a City Councillor with one of the longest tenures in Saskatoon's history. Mann was known for his interest in civic infrastructure, careful city planning and prudent spending. Born in Frances Saskatchewan in 1923, Mann was raised in Flin Flon Manitoba. Following completion of high school, Mann worked for Hudson Bay Mining. Through a company scholarship, Mann came to the University of Saskatchewan (U of S) to study mechanical engineering. He became the outstanding graduate in Engineering in 1945 at the U of S, and later studied at the University of Iowa for his Masters Degree.

Following completion of his studies, Owen Mann began a 42 year career as a professor at the College of Engineering at the University of Saskatchewan. When Mann was elected to City Council in 1969, he brought with him his professional skills and engineering expertise. Mann served on City Council from 1969 to 1979, and again from 1980 to 1994. Among his many contributions to the City of Saskatoon, Mann assisted with drawing up plans for the City's new sewage treatment plant. He served on countless civic committees, and also spent several years as a Director with the Saskatchewan Urban Municipalities Association (SUMA). Mann, along with his family, purchased the home at 1040 University Drive in 1963, and lived there for over 45

years. Owen Mann Park, located in the neighbourhood of Stonebridge, was named in his honour in 2010.

Two other notable individuals, who resided at 1040 University Drive, include Thomas A. Watson and W.E. Walter. Watson, in 1949 at the Saskatoon Cancer Clinic, became the first physician to use the betatron, an electrical method of producing 25 million volt x-rays to treat patients with late stage cancer. In 1951, Watson, with the help of physicist Harold John, created the Watson-Johns cobalt treatment unit – the first of its kind in the world. Watson also worked as a professor at the University of Saskatchewan's department of therapeutic radiology, which he headed from 1954 to 1963 before moving to Ontario to become director of the Ontario Cancer Foundation.

W.E. Walter, a Special Representative for the Canadian National Railway (CNR) Immigration and Colonization Department, lived in the home at 1040 University Drive from 1930 to 1947. The Immigration and Colonization Department, had offices in Saskatoon, Winnipeg, and Edmonton, and were responsible for the recruitment of thousands of settlers to the west.

Over the years, the Mann House has seen a number of changes to its exterior. In 1918 the attached garage was built; in the 1930's the glassed-in veranda was constructed; and around the same time the existing exterior stucco was added covering the original wood siding that is still present in the veranda's interior. In 1987 the garage roof was replaced. Despite some of these modifications, the Mann House adds character to the Varsity View neighbourhood and serves as a tribute to Owen Mann, Thomas A. Watson and W.E. Walter.

Source: City of Saskatoon Built Heritage Database

Character Defining Elements

The heritage value of the Mann House resides in the following character-defining elements:

- Its aesthetic value, evident in: its hipped roof with multiple gables, enclosed veranda, windows (including multiple rectangular planes on upper story); false half-timbering; brick chimneys with decorative chimney pots; and exposed rafter tails.
- Those elements associated with Owen Mann, Thomas A. Watson and W.E. Walter, such as its existence on its original lot in Varsity View and its proximity to the University.

Photographs of Subject Property



North Facade (University Drive)



East Facade (Mckinnon Avenue)



Rafter Tails



Chimney



STANDING POLICY COMMITTEE ON PLANNING, DEVELOPMENT AND COMMUNITY SERVICES

Missing and Murdered Aboriginal Women Monument

Recommendation of the Committee

That the front plaza of Saskatoon Police Service Headquarters be approved as the location of the Missing and Murdered Aboriginal Women Monument.

History

At the October 5, 2015 meeting of the Standing Policy Committee on Planning, Development and Community Services, a report of the General Manager, Community Services Department, dated October 5, 2015, was considered.

Attachment

October 5, 2015 report of the General Manager, Community Services Department.

Missing and Murdered Aboriginal Women Monument

Recommendation

That the Standing Policy Committee on Planning, Development and Community Services recommend to City Council that the front plaza of Saskatoon Police Service Headquarters be approved as the location of the Missing and Murdered Aboriginal Women Monument.

Topic and Purpose

The Saskatoon Police Service (SPS), in partnership with the Saskatoon Tribal Council (STC), commissioned a commemorative artwork to remember missing and murdered Aboriginal women. The subject for commemoration was assessed, as per Commemorations and Monuments Policy No. C09-038, and has met the necessary criteria for commemoration. A rendition of the artist's monument was reviewed by the Public Art Advisory Committee (PAAC) for its artistic merit, as per Public Art Policy No. C10-025.

The purpose of this report is to recommend that the Missing and Murdered Aboriginal Monument be located on the front plaza of SPS Headquarters.

Report Highlights

1. SPS and STC partnered to commission a commemorative artwork to remember missing and murdered Aboriginal women.
2. As per Commemorations and Monuments Policy No. C09-038, a Commemoration Review Committee (Committee) was created to assess whether the subject for commemoration was of significant meaning, importance, and relevance to the community of Saskatoon. The Committee unanimously agreed that the subject was worthy of commemoration in Saskatoon.
3. From the start of this project, SPS and STC identified the front plaza of SPS Headquarters as a meaningful location for this monument. The Committee, along with Civic Administration and community stakeholders, support the monument in this location.
4. SPS and STC have selected Saskatchewan artist Lionel Peyachew to design the Missing and Murdered Aboriginal Women monument. The artist's rendition of the monument was reviewed by the PAAC for its artistic merit.

Strategic Goal

This report supports the Strategic Goal of Quality of Life and short-term strategy to strengthen relations with local Aboriginal organizations and the long-term strategy of implementing the Municipal Culture Plan.

Missing and Murdered Aboriginal Women Monument

Background

Commemorations contribute to Saskatoon's urban landscape. Whether as statues, monuments, or artworks, they describe our connections to the past, express community and individual values, contribute to telling our story, and help to create a sense of place. In response to the importance that community places have on commemoration, the Administration developed a comprehensive policy to govern requests for commemorative art, statues, and monuments in Saskatoon. Commemorations and Monuments Policy No. C09-038 was adopted by City Council in May of 2013.

Report

Commemorative Artwork to Remember Missing and Murdered Aboriginal Women

SPS and STC partnered to commission a commemorative artwork to remember missing and murdered Aboriginal women. For SPS and the STC, it is critically important to acknowledge the pain and loss associated with missing and murdered Aboriginal women by building a memorial at the new police station. The decision to convey respect and acknowledge the alarming rates of missing and murdered Aboriginal women by establishing a memorial at the new police station is remarkable and possibly a first in Canada. This project is strongly supported by the families and community members affected.

Subject for Commemoration Assessed by Commemoration Review

As per Commemorations and Monuments Policy No. C09-038, the Committee was created to assess whether the subject for commemoration was of significant meaning, importance, and relevance to the community of Saskatoon. The Committee met on November 5, 2014, and unanimously agreed that the subject was worthy of commemoration in Saskatoon. The Committee also congratulated SPS and STC for "their commitment with the community to lead this commemorative art project."

Front Plaza of SPS Headquarters is the Recommended Location

From the start of this project, SPS and STC identified the front plaza of SPS Headquarters as a meaningful location for this monument (see Attachment 1). The Committee, along with Civic Administration and community stakeholders, support the monument in this location. While the selected sculpture does require power for lighting, the plaza is equipped with an electrical connection.

Saskatchewan Artist Lionel Peyachew Selected to Design the Monument

After public and stakeholder consultation, SPS and STC selected Saskatchewan artist Lionel Peyachew to design the Missing and Murdered Aboriginal Women monument (see Attachment 2).

Mr. Peyachew's design is a life-size bronze sculpture of a woman named "Wicanhpi Duta Win" or "Red Star Woman." According to the artist, the sculpture "represents the artistry of a fancy dancer, with her shawl as her wings, her arms gliding in the sky, like an eagle dancing in the clouds."

Missing and Murdered Aboriginal Women Monument

The monument was favourably reviewed, for its artistic merit, by the PAAC at its June 19, 2015 meeting.

Options to the Recommendation

The option exists to not approve the front plaza of SPS Headquarters as the location for this monument. In this case, further direction would be required.

Public and/or Stakeholder Involvement

From the outset, this project has generated considerable public and stakeholder involvement. The two main proponents, SPS and STC, have engaged the community on a number of occasions, including a well-attended community open house held on June 17, 2015, at SPS Headquarters.

Communication Plan

If this recommendation is approved, the Administration will include the commemorative artwork on its interactive public art map application (iMap), located on the City website.

Policy Implications

This recommendation is in accordance with Commemorations and Monuments Policy No. C09-038.

Financial Implications

Like all proponents of commemorations, SPS and STC are responsible for all costs associated with the project management, development, and fabrication of the monument.

Preventative maintenance and conservation costs will be included within the Public Art Maintenance budget as managed by Facilities and Fleet Management, Asset & Financial Management Department. The estimated operating impact is \$1,500 per year.

Safety/Crime Prevention Through Environmental Design (CPTED)

The CPTED Review Committee has been requested to review the commemorative art installation at the proposed location.

Other Considerations/Implications

There are no environmental or privacy implications or considerations.

Due Date for Follow-up and/or Project Completion

If this location is approved, the Administration will remain in frequent communication with the artist and request updates on its creation and installation. The artist will have up to three years to install the monument.

Public Notice

Public notice pursuant to Section 3 of Public Notice Policy No. C01-021, is not required.

Missing and Murdered Aboriginal Women Monument

Attachments

1. Recommended Location of Missing and Murdered Aboriginal Women Monument
2. Artist's Rendering of Missing and Murdered Aboriginal Women Monument

Report Approval

Written by: Kevin Kitchen, Community Initiatives Manager
Monica Goulet, Aboriginal Relations Consultant, Saskatoon Police Service

Reviewed by: Lynne Lacroix, Director of Community Development
Clive Weighill, Chief of Saskatoon Police Service

Approved by: Randy Grauer, General Manager, Community Services Department

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Recommended Location of Missing and Murdered Aboriginal Women Monument



Artist's Rendering of Missing and Murdered Aboriginal Women Monument





STANDING POLICY COMMITTEE ON PLANNING, DEVELOPMENT AND COMMUNITY SERVICES

Saskatoon Register of Historic Places

Recommendation of the Committee

1. That the properties be added to the Saskatoon Register of Historic Places, pursuant to the recommendations outlined in the City of Saskatoon Heritage Plan; and
2. That future additions to the Saskatoon Register of Historic Places be delegated to the General Manager, Community Services Department, in consultation with the Municipal Heritage Advisory Committee.

History

At the October 5, 2015 meeting of the Standing Policy Committee on Planning, Development and Community Services, a memo of support from Municipal Heritage Advisory Committee, dated September 2, 2015 was considered. A report of the General Manager, Community Services Department, dated October 5, 2015, was also considered.

Your Committee considered and supports the recommendation in the report of the General Manager, Community Services Department dated October 5, 2015.

Attachment

October 5, 2015 report of the General Manager, Community Services Department.

Saskatoon Register of Historic Places

Recommendation

That a report be forwarded to the Standing Policy Committee on Planning, Development and Community Services with a recommendation to City Council:

1. That the properties be added to the Saskatoon Register of Historic Places, pursuant to the recommendations outlined in the City of Saskatoon Heritage Plan; and
2. That future additions to the Saskatoon Register of Historic Places be delegated to the General Manager, Community Services Department, in consultation with the Municipal Heritage Advisory Committee.

Topic and Purpose

The purpose of this report is to add the properties, outlined in Attachment 1, which have significant heritage qualities or attributes, for inclusion on the new Saskatoon Register of Historic Places (Register). The intent of the Register is to serve as a promotional and educational tool for heritage resources in Saskatoon.

Report Highlights

1. The Register will be the City of Saskatoon's (City) official public listing of historic places and is a non-statutory listing. The primary purpose of the Register is to promote public awareness, education, and appreciation of heritage in Saskatoon.
2. Designated and non-designated properties are proposed to be included on the Register, as outlined in Attachment 1.
3. A Statement of Significance (SOS) will be prepared for each property and will describe what the historic place is, why it is of heritage value, and what character-defining elements should be retained in order to preserve its heritage value.

Strategic Goals

Under the City's Strategic Goal of Sustainable Growth, this report supports the preservation of the character of heritage buildings and historical landmarks. This report also supports the Strategic Goal of Quality of Life. As a community, we find new and creative ways to showcase our city's built, natural, and cultural heritage.

Background

The Heritage Policy and Program Review, completed in 2012, recommended that the City:

- a) establish the Register by City Council resolution;
- b) make the Register easily available to the public;
- c) initiate an evaluation of the Built Heritage Database to identify the sites of highest heritage value that can be included on the Register; and

Saskatoon Register of Historic Places

- d) identify heritage resources other than buildings, such as cultural landscapes, that can be included on the Register.

The creation of the Register was formalized in the City's updated Civic Heritage Policy No. C10-020 and the City of Saskatoon Heritage Plan in 2014. At its March 31, 2014 meeting, City Council resolved that the Register be created and made available to the public. In 2014, the Administration researched and evaluated a number of properties on the City's Built Heritage Database to establish a list of resources recognized as having significant heritage value, and thus be appropriate for inclusion on the Register.

Report

The Register

Identifying heritage resources is an important step in conserving Saskatoon's heritage. The Register will be the City's official public listing of historic places in Saskatoon considered to have significant heritage value. These historic places contribute to our sense of place and help define the unique attributes of our city. It is important to note, the Register is a non-statutory listing, meaning that there will be no legal restrictions or development implications on properties as a result.

Properties have been evaluated and recommended for inclusion on the Register for a number of reasons, which may include, or be a combination of, the following:

- a) the historic place is representative of a distinct or unique architectural style;
- b) the property is associated with a significant person(s);
- c) the property is associated with a particular historical event or theme;
- d) the property exhibits social, cultural, environmental, archeological and paleontological significance; and/or
- e) the property adds value in the context of its surrounding area or landscape.

The Register will include buildings, structures, monuments, memorials, and natural and cultural landscapes.

Properties

The Register is proposed to include the following:

- a) designated municipal heritage properties, provincial heritage properties, and/or national historic sites;
- b) properties listed on Bylaw No. 6770 - A Bylaw of The City of Saskatoon to deny a permit for the demolition of certain property (Holding Bylaw); and
- c) other historic places that meet the criteria to become eligible for municipal heritage designation but have received no such designation and have not been placed on the Holding Bylaw.

Statement of Significance

Each listing on the Register will include the following information (if applicable):

Saskatoon Register of Historic Places

- a) physical address;
- b) neighbourhood;
- c) date of construction;
- d) architectural style;
- e) architect;
- f) builder;
- g) original and current use;
- h) a current photograph/historical photograph (if available); and
- i) an SOS.

An SOS summarizes the description, heritage value, and character-defining elements of the property. As stated in the Standards and Guidelines for the Conservation of Historic Places in Canada, an SOS for a historic place is a key document in determining goals, standards, and techniques that are appropriate for conserving these historic places.

Public and/or Stakeholder Involvement

All property owners, with the exception of those who own designated heritage properties, and those properties that are government or publically owned, were notified, by mail, of the intent to include their property on the Register, along with an opportunity to submit comments and concerns. That notification generated very little feedback. The majority of property owners who did contact the City were supportive of their property being included on the Register. Only one property owner has requested that their property not be included on the Register, and as such, that property has been removed from the Register at this time.

Other Considerations/Implications

There are no options, policy, financial, environmental, privacy, or CPTED implications or considerations. A communication plan will be developed in consultation with the Municipal Heritage Advisory Committee.

Due Date for Follow-up and/or Project Completion

Pending approval by City Council, the Register will be made publically available through the City's website.

Public Notice

Public notice, pursuant to Section 3 of Public Notice Policy No. C01-021, is not required.

Attachment

1. Properties Proposed to be Included on the Saskatoon Register of Historic Places

Report Approval

Written by: Catherine Kambeitz, Heritage and Design Coordinator, Planning and Development
Reviewed by: Darryl Dawson, Acting Director of Planning and Development
Approved by: Randy Grauer, General Manager, Community Services Department

S:\Reports\DS\2015\MHAC – Saskatoon Register of Historic Places\kt

Register No.	Historic Place	Heritage Protection Status	Civic Address	Neighbourhood	Heritage Significance
001	Marr Residence	Designated	326 11th Street East	Nutana	<ul style="list-style-type: none"> • One of the earliest homes built in Nutana • Architectural style
002	Pioneer (Nutana) Cemetery	Designated	Ruth Street West and St. Henry Avenue	Exhibition	<ul style="list-style-type: none"> • Associated with early settlers of Saskatoon • Early landscape design
003	Little Stone School House	Designated	University of Saskatchewan	University of Saskatchewan (U of S) Management Area	<ul style="list-style-type: none"> • Associated with the foundation of public education in Saskatoon • Stonemasonry • One of the first heritage conservation projects in Canada
004	Peter MacKinnon Building (formerly the College Building)	Designated	105 Administration Place (University of Saskatchewan)	U of S Management Area	<ul style="list-style-type: none"> • First College of Agriculture at a Canadian University • Collegiate Gothic architectural style
005	Independent Order of Odd Fellows Temple	Designated	416 21st Street East	Central Business District	<ul style="list-style-type: none"> • Architectural style (Neo-Classical and Beaux Arts influences) • Associated with the development of the Saskatoon community
006	Albert Community Centre	Designated	610 Clarence Avenue South	Varsity View	<ul style="list-style-type: none"> • Representative of the importance of the British tradition in Saskatoon's

					settlement <ul style="list-style-type: none"> • Symbolic of Saskatoon's early education system
007	Fairbanks Morse Warehouse	Designated	14 23rd Street East	Central Business District	<ul style="list-style-type: none"> • Classical Revival architectural style • Role in Saskatoon's commercial history
008	Land Titles Office	Designated	311 21st Street East	Central Business District	<ul style="list-style-type: none"> • Architectural style (Neo-Classical and Romanesque Revival influences) • Associated with growth and prosperity in Saskatchewan
009	Bowerman Residence	Designated	1328 Avenue K South	Holiday Park	<ul style="list-style-type: none"> • Western Stick architectural style • Associated with Allan Bowerman – one of Saskatoon's first postmasters and member of the first Town Council • Associated with history of tuberculosis treatment in Saskatoon
010	Rugby Chapel	Designated	1337 College Drive	U of S Management Area	<ul style="list-style-type: none"> • Associated with the history of Anglican education in Saskatchewan, and of the University of Saskatchewan • Gothic Revival architectural style

011	Sommerville/Pettit Residence	Designated	870 University Drive	Nutana	<ul style="list-style-type: none"> • Architectural style (Tudor Revival and Spanish Revival influences) • Contributes to University Drive streetscape
012	Gustin/Trounce Residence	Designated	512 10th Street East	Nutana	<ul style="list-style-type: none"> • Oldest known building in Saskatoon (Trounce) • Pioneer vernacular architectural style (Trounce) • Associated with Lyell Gustin, pianist and music teacher (Gustin) • Eclectic architectural style and unique interior layout (Gustin)
013	Arrand Block	Designated	520 - 524 11th St E.	Nutana	<ul style="list-style-type: none"> • Neo-classical architectural style • Associated with the Arrand family, prominent Saskatoon contractors
014	Forestry Farm Park (Superintendent's Residence)	Designated	1903 Forestry Farm Park Drive	Sutherland	<ul style="list-style-type: none"> • Role in prairie landscape development
015	Former Fire Hall No. 3	Designated	612 11th Street East	Nutana	<ul style="list-style-type: none"> • Associated with Saskatoon's boom years • Associated with the cold war period • Vernacular architectural style
016	Next of Kin Memorial Avenue NHS	Designated	1502 2 nd Avenue North	Kelsey-Woodlawn	<ul style="list-style-type: none"> • Example of the "Roads of Remembrance" phenomenon which was developed to

					honour casualties of WWI
017	CPR Station NHS	Designated	305 Idylwyld Drive North	Central Business District	<ul style="list-style-type: none"> • Associated with Saskatoon's emerging status as an important prairie city
018	VIA Rail (Union) Station	Designated	1701 Chappel Drive	C.N. Yards Management Area	<ul style="list-style-type: none"> • Reflects changes in rail transportation during the mid-20th century • International architectural style
019	F.P. Martin House (1 & 2)	Designated	716 and 718 Saskatchewan Crescent East	Nutana	<ul style="list-style-type: none"> • Architectural style (Cottage influences) • Associated with the life and work of Saskatoon architect Frank P. Martin • Contributes to the historic streetscape of Saskatchewan Crescent
020	The Broadway Theatre	Designated	715 Broadway Avenue	Nutana	<ul style="list-style-type: none"> • Art Moderne architectural style • Associated with the Broadway Avenue commercial district
021	Hutchinson Building	Designated	144 2nd Avenue south	Central Business District	<ul style="list-style-type: none"> • Vernacular façade • Associated with prominent architect Frank P. Martin • Associated with commercial development in Saskatoon
022	Saskatoon Electrical System Substation	Designated	619 Main Street	Nutana	<ul style="list-style-type: none"> • Modern Classical architectural style • Associated with public works in Saskatoon

					<ul style="list-style-type: none"> • Contributes to the surrounding streetscape
023	Thirteenth Street Terrace	Designated	711 - 723 13th Street East	Nutana	<ul style="list-style-type: none"> • Architectural style • Innovative multiple dwelling unit design at the time
024	Alexander Residence	Designated	1020 Spadina Crescent East	City Park	<ul style="list-style-type: none"> • Craftsman architectural style • Associated with Dr. Harold Alexander, head of the Surgery Department at St. Paul's Hospital
025	Little Chief Service Station	Designated	344 20th Street West	Riversdale	<ul style="list-style-type: none"> • Spanish Revival architectural style • Associated with the commercial development of Saskatoon
026	Knox United Church	Designated	838 Spadina Crescent East	Central Business District	<ul style="list-style-type: none"> • Collegiate Gothic architectural style • Religious and cultural landmark
027	St. John's Anglican Cathedral	Designated	816 Spadina Crescent East	Central Business District	<ul style="list-style-type: none"> • Gothic Revival architectural style • Religious and cultural landmark
028	Landa Residence	Designated	202 Avenue E South	Riversdale	<ul style="list-style-type: none"> • Associated with the Landa family, who were the first Jewish settlers in Saskatoon • Architectural style (Prairie influences)
029	McLean Block	Designated	263 3rd Avenue South	Central Business District	<ul style="list-style-type: none"> • Architectural style • Contributes to 3rd Ave Streetscape

030	Bottomley House	Designated	1118 College Drive	Varsity View	<ul style="list-style-type: none"> • Queen Anne architectural style • Associated with Saskatoon's economic prosperity in the early-20th century
031	Aden Bowman Residence	Designated	1018 McPherson Avenue	Nutana	<ul style="list-style-type: none"> • Associated with business and civic leader Aden Bowman • Associated with architect, R.M. Thompson
032	Cambridge Court	Designated	129 5th Avenue North	Central Business District	<ul style="list-style-type: none"> • Associated with architect David Webster • Architectural style (Romanesque Revival and Chicago influences)
033	Larkin House	Designated	927 5th Avenue North	City Park	<ul style="list-style-type: none"> • Associated with two prominent business families during the 1920s, the Larkin and Perrott families • Colonial architectural style
034	Arthur Cook Building	Designated	306 Ontario Avenue	Central Business District	<ul style="list-style-type: none"> • Contributes to the Warehouse District streetscape • Associated with architect David Webster
035	W.J. English House	Designated	932 University Drive	Nutana	<ul style="list-style-type: none"> • Architectural style (Spanish and California Mission influences) • Associated with a number of prominent citizens
036	City Gardener's Site	Designated	810 Spadina Crescent West	King George	<ul style="list-style-type: none"> • Associated with Old Bone Trail

					<ul style="list-style-type: none"> • Associated with the city's first gardeners
037	Bell House	Holding Bylaw	906 Saskatchewan Crescent East	Nutana	<ul style="list-style-type: none"> • Associated with William J. Bell, pioneer and alderman • Associated with architects Storey and Van Egmond • Architectural style (Colonial influences)
038	Hopkins House	Holding Bylaw	307 Saskatchewan Crescent West	Nutana	<ul style="list-style-type: none"> • Neo-classical architectural style • Associated with William Hopkins, prominent civic politician and businessman
039	Adilman's Department Store	Holding Bylaw	126 – 128 20 th Street West	Riversdale	<ul style="list-style-type: none"> • Art Moderne architectural style • Associated with the Adilman Family's retail business
040	Bessborough Hotel	Holding Bylaw	601 Spadina Crescent East	Central Business District	<ul style="list-style-type: none"> • Historic and cultural landmark • Chateau architectural style
041	Broadway Bridge	Holding Bylaw	Broadway Avenue	Central Business District / Nutana	<ul style="list-style-type: none"> • Historic landmark
042	University Bridge	Holding Bylaw	25 th Street East	Central Business District	<ul style="list-style-type: none"> • Historic landmark
043	Buena Vista School	Holding Bylaw	1306 Lorne Avenue	Buena Vista	<ul style="list-style-type: none"> • Representative of the importance of the British tradition both in Saskatoon's settlement and in the educational system • Associated with architect David Webster
044	Calder House	Holding	848 Saskatchewan	Nutana	<ul style="list-style-type: none"> • Architectural style (Colonial

		Bylaw	Crescent East		<ul style="list-style-type: none"> Revival influences) Associated with T.F. Calder, former Provincial Manager of Standard Trust
045	Board of Trade Office	Holding Bylaw	1022 Temperance Street	Varsity View	<ul style="list-style-type: none"> Architectural style (Classical influences) Associated with early Board of Trade activities
046	Canada Building	Holding Bylaw	105 21 st Street East	Central Business District	<ul style="list-style-type: none"> Chicago architectural style Associated with Allan Bowerman, Saskatoon's first postmaster and member of the first Town Council
047	Senator Hotel	Holding Bylaw	243 21 st Street East	Central Business District	<ul style="list-style-type: none"> Architectural style Historic landmark
048	Hugh Cairns Memorial	Holding Bylaw	Kinsmen Park (945 Spadina Crescent East)	City Park	<ul style="list-style-type: none"> Symbolic of the lives that were lost during WWI
049	Irvine House	Holding Bylaw	416 11th Street East	Nutana	<ul style="list-style-type: none"> Architectural style (Queen Anne influences) Associated with Robert B. Irvine – member of Saskatoon's first City Council and former chairman of the Nutana School District
050	King George School	Holding Bylaw	721 Avenue K South	King George	<ul style="list-style-type: none"> Representative of the importance of education and the British tradition Associated with architect David Webster
051	MacMillan Building	Holding Bylaw	135 21st Street East	Central Business District	<ul style="list-style-type: none"> Chicago architectural style Associated with Frank R.

					MacMillan, former alderman and mayor
052	Normal School	Holding Bylaw	1030 Idylwyld Drive North	Central Industrial	<ul style="list-style-type: none"> • Collegiate Gothic architectural style • Representative of one of the most modern and best equipped institutions in Canada in the 1920s
053	Powe House	Holding Bylaw	100 115 th Street West	Sutherland	<ul style="list-style-type: none"> • One of the first homesteading sites in the Saskatoon area
054	R.J.D Williams School	Holding Bylaw	221 Cumberland Avenue	U of S Management Area	<ul style="list-style-type: none"> • An important educational landmark in Saskatoon and the province of Saskatchewan • Gothic architectural style
055	Roxy Theatre	Holding Bylaw	320 20th Street West	Riversdale	<ul style="list-style-type: none"> • Spanish Villa architectural style • Reminiscent of old Saskatoon theatres • Contributes to 20th Street West streetscape
056	Royal Bank	Holding Bylaw	241 2 nd Avenue South	Central Business District	<ul style="list-style-type: none"> • Architectural style (Classical and Renaissance influences)
057	Rumely Warehouse	Holding Bylaw	224 Pacific Avenue	Central Business District	<ul style="list-style-type: none"> • North American Warehouse architectural style • Representative of Saskatoon's role as a prominent distribution center
058	Saskatoon Club	Holding Bylaw	417 21st Street East	Central Business District	<ul style="list-style-type: none"> • Historic landmark in the Saskatoon business community

059	Nutana Collegiate Institute	Holding Bylaw	411 11th Street East	Nutana	<ul style="list-style-type: none"> • First collegiate in Saskatoon • Role in Saskatoon's art community • French Renaissance architectural style
060	Schrader House	Holding Bylaw	321 6 th Avenue North	Central Business District	<ul style="list-style-type: none"> • Representative of affluence in the early 20th century • Associated with Schrader family • Architectural style
061	St. George's Ukrainian Catholic Church	Holding Bylaw	214 Avenue M South	Pleasant Hill	<ul style="list-style-type: none"> • Byzantine architectural style • Religious and cultural landmark
062	St. Joseph's Church	Holding Bylaw	535 8th Street East	Nutana	<ul style="list-style-type: none"> • Romanesque architectural style • Religious and cultural landmark
063	St. Mark's Anglican Church	Holding Bylaw	1406 8th Avenue North	North Park	<ul style="list-style-type: none"> • Architectural style • Religious and cultural landmark
064	Star Phoenix Clock	Holding Bylaw	5 th Avenue and 24 th Street	Central Business District	<ul style="list-style-type: none"> • Symbolic tribute to those Canadians who sacrificed their lives during WWI
065	Stewart's Drug Store	Holding Bylaw	810 Broadway Avenue	Nutana	<ul style="list-style-type: none"> • One of the oldest surviving buildings on Broadway Avenue • Associated with Charles Stewart, pharmacist
066	Third Avenue United Church	Holding Bylaw	304 3rd Avenue North	Central Business District	<ul style="list-style-type: none"> • English Gothic architectural style • Religious and cultural landmark

067	Thompson Chambers / Avalon Block	Holding Bylaw	206 2 nd Avenue North	Central Business District	<ul style="list-style-type: none"> • Unique in size and scale along 2nd Avenue North
068	Ukrainians Orthodox Cathedral of the Holy Trinity	Holding Bylaw	919 20th Street West	Riversdale	<ul style="list-style-type: none"> • Byzantine architectural style • Religious and cultural landmark
069	The Vimy Memorial	Holding Bylaw	Kiwanis Memorial Park (501 Spadina Crescent East)	Central Business District	<ul style="list-style-type: none"> • Symbolic of the lives that were lost during WWI, and the Battle of Vimy Ridge
070	T. Eaton Co Ltd.	Holding Bylaw	310 21 st Street East	Central Business District	<ul style="list-style-type: none"> • Italian Renaissance architectural Style • Associated with the Eaton's company - once Canada's largest department store retailer
071	Grace-Westminster Church	-	505 10 th Street East	Nutana	<ul style="list-style-type: none"> • Victorian Gothic architectural style • Religious and cultural landmark
072	Nurses Residence	-	1702 20 th Street West	Pleasant Hill	<ul style="list-style-type: none"> • Representative of health care education in the first half of the 20th century
073	Birks Building	-	165 3 rd Avenue South	Central Business District	<ul style="list-style-type: none"> • Architectural style (Beaux Arts influences) • Associated with Henry Birks and Jewellery Company Henry Birks and Sons Ltd
074	Spadina Crescent Bridge	-	Spadina Crescent East	City Park	<ul style="list-style-type: none"> • Historical landmark
075	McKague's Ltd.	-	300 3 rd Avenue South	Central Business District	<ul style="list-style-type: none"> • Architectural style • Associated with the

					McKague family, who provided funeral services And related activities in the early 19 th century
076	City Park Collegiate	-	820 9 th Avenue North	City Park	<ul style="list-style-type: none"> • British architectural style • Role as an educational institution and community centre in Saskatoon
077	Bank of Montreal	-	101 2 nd Avenue North	Central Business District	<ul style="list-style-type: none"> • International architectural style • Associated with Canada's first permanent bank – the Bank of Montreal
078	Arnold's Private Hospital	-	203 8 th Street West	Buena Vista	<ul style="list-style-type: none"> • Representative of Saskatoon's healthcare institutions and early nursing practices
079	Albany Hotel	-	202 20 th Street West	Riversdale	<ul style="list-style-type: none"> • One of the earliest buildings in the Riversdale area
080	King George Hotel	-	157 2 nd Avenue North	Central Business District	<ul style="list-style-type: none"> • One of Saskatoon's oldest hotels
081	Sgt. Hugh Cairns V.C. Armoury	-	930 Idylwyld Drive North	Central Industrial	<ul style="list-style-type: none"> • Symbolic of the lives that were lost during WWI • Associated with Hugh Cairns – WWI Soldier
082	Christ Church	-	515 28 th Street West	Caswell Hill	<ul style="list-style-type: none"> • Gothic Revival architectural Style • Religious and cultural landmark
083	Kiwanis Memorial Park	-	501 Spadina Crescent East	Central Business District	<ul style="list-style-type: none"> • Cultural landmark

084	Glengarry Block	-	245 3 rd Avenue South	Central Business District	<ul style="list-style-type: none"> • Architectural style • Associated with architect, David Webster
085	Connaught Block	-	247 3 rd Avenue South	Central Business District	<ul style="list-style-type: none"> • Architectural style • Associated with architect, David Webster
086	Smith Block	-	723 Broadway Avenue	Nutana	<ul style="list-style-type: none"> • Architectural style
087	Sturdy Stone Building	-	122 3 rd Avenue North	Central Business District	<ul style="list-style-type: none"> • Brutalist architectural style
088	Montgomery Place Memorial Cairn	-	Montgomery Park (3302 Ortona Street)	Montgomery Place	<ul style="list-style-type: none"> • Symbolic tribute to war veterans and formation of Montgomery Place
089	Pioneer Memorial Cairn	-	Cosmopolitan Park (725 Saskatchewan Crescent East)	Nutana	<ul style="list-style-type: none"> • Honours the men and women who were the original founders of Saskatoon
090	Sommerfeld Block	-	813 and 817 Broadway Avenue	Nutana	<ul style="list-style-type: none"> • Architectural style • Associated with historic Broadway area
091	Stone Barn - University of Saskatchewan	-	University of Saskatchewan	U of S Management Area	<ul style="list-style-type: none"> • One of the few French Canada/Eastern Township structures remaining on the Prairies • Symbolic of the study of agriculture in Saskatchewan
092	Bottomley Block	-	155 2 nd Avenue South	Central Business District	<ul style="list-style-type: none"> • Architectural style • Associated with architectural firm, Webster and Noel • Associated with Richard Bottomley, an early investor in Saskatoon

093	Kempthorne Block	-	157 2 nd Avenue South	Central Business District	<ul style="list-style-type: none"> • Architectural style • Associated with architectural firm, Webster and Noel
094	Mendel Art Gallery and Civic Conservatory	-	950 Spadina Crescent East	City Park	<ul style="list-style-type: none"> • Modernist architectural style • Associated with Fred S. Mendel, a local industrialist • Cultural centre
095	Hollywood Studios Building	-	238 2 nd Avenue South	Central Business District	<ul style="list-style-type: none"> • Architectural style • Associated with Hollywood Studio's photography gallery
096	Tees and Persse Building	-	331 1 st Avenue North	Central Business District	<ul style="list-style-type: none"> • Chicago architectural style
097	Mackenzie and Thayer Warehouse	-	303 Pacific Avenue	Central Business District	<ul style="list-style-type: none"> • Representative of the prevailing design of warehouses built during the 1920's
098	Eaton Block	-	735 Broadway Avenue	Nutana	<ul style="list-style-type: none"> • Architectural style • Associated with Frederick Eaton, an early Saskatoon doctor
099	The Diefenbaker Canada Centre	-	University of Saskatchewan	U of S Management Area	<ul style="list-style-type: none"> • Honours Canada's 13th Prime Minister and houses his extensive collection of papers and memorabilia
100	Caswell School	-	204 30 th Street West	Caswell Hill	<ul style="list-style-type: none"> • Representative of Saskatoon's early education system • Associated with R. Caswell, a Saskatoon pioneer
101	Wilson School	-	902 7 th Avenue North	City Park	<ul style="list-style-type: none"> • Representative of Saskatoon's early education

					<ul style="list-style-type: none"> system Associated with James R. Wilson, a Saskatoon Public School Board member
102	Fred Mitchell Memorial Garden	-	410 Saskatchewan Crescent West	Buena Vista	<ul style="list-style-type: none"> Represents early commercial landscaping in Saskatoon
103	Ashworth Holmes Park	-	915 Avenue D North	Caswell Hill	<ul style="list-style-type: none"> Associated with John Ashworth and Joseph Holmes, two prominent business men who founded the Saskatoon Nursery Cultural landmark
104	Hoeschen House	-	309 Saskatchewan Crescent West	Nutana	<ul style="list-style-type: none"> Georgian architectural style Associated with Ben Hoeschen, owner of Saskatoon's first brewing company and active community member
105	MacMillan House	-	302 Saskatchewan Crescent West	Nutana	<ul style="list-style-type: none"> Architectural style (Craftsman influences) Associated with Frank MacMillan, early Saskatoon merchant, alderman, mayor, and Member of Parliament
106	Pendygrasse House	-	1919 St. Henry Avenue	Exhibition	<ul style="list-style-type: none"> Architectural style Associated with Harold Pendygrasse, an early Saskatoon homesteader and businessman
107	Memorial Gates	-	University of Saskatchewan	U of S Management Area	<ul style="list-style-type: none"> Honours those from the University of Saskatchewan who lost their lives in WWI

108	CN Rail Bridge	-	Spadina Crescent East	Exhibition	<ul style="list-style-type: none"> • Historic landmark
109	Bedford Road Collegiate	-	722 Bedford Road	Caswell Hill	<ul style="list-style-type: none"> • Collegiate Gothic architectural style • Symbolizes the expansion of the population on the west side of the river and the importance of education in Saskatoon
112	CPR Train Bridge	-	33 rd Street East	City Park	<ul style="list-style-type: none"> • Historic landmark
113	Pleasant Hill School	-	215 Avenue South	Pleasant Hill	<ul style="list-style-type: none"> • Cultural landmark • Associated with architect, Frank P. Martin
114	St. Andrew's College	-	1121 College Drive	U of S Management Area	<ul style="list-style-type: none"> • Collegiate Gothic architectural style • Religious significance
115	Victoria School	-	639 Broadway Avenue	Nutana	<ul style="list-style-type: none"> • French Renaissance style (Classical Revival and Second Empire influences) • Community landmark • Contributes to Broadway streetscape
116	Westmount School	-	411 Avenue J South	Westmount	<ul style="list-style-type: none"> • British architectural style • Community landmark
117	Wing (Lynx) Building	-	2407 Avenue C North	Airport Industrial	<ul style="list-style-type: none"> • Training School for the R.C.A.F during WWII
118	Laurentian/Union Bank	-	183 2 nd Avenue South	Central Business District	<ul style="list-style-type: none"> • Classical Revival architectural style
119	Federal Building	-	101 22 nd Street East	Central Business District	<ul style="list-style-type: none"> • Beaux Arts architectural style
120	H.M.C.S. Unicorn	-	405 24 th Street East	Central Business District	<ul style="list-style-type: none"> • Naval barracks

121	City Hospital Nurses Residence	-	701 Queen Street	City Park	<ul style="list-style-type: none"> • Representative of health care education in the first half of the 20th century
122	Royal University Hospital	-	103 Hospital Drive	U of S Management Area	<ul style="list-style-type: none"> • Modern Collegiate architectural style • Prominent landmark
123	Cenotaph	-	222 3 rd Avenue North	Central Business District	<ul style="list-style-type: none"> • War monument
124	Ukrainian Museum of Canada	-	910 Spadina Crescent East	Central Business District	<ul style="list-style-type: none"> • Ukrainian Folk architectural style • Cultural and community landmark
125	Chubb Block	-	227 21 st Street East	Central Business District	<ul style="list-style-type: none"> • Architectural style • Associated with Benjamin Chubb who served on town Council and was part of the establishment of the Quaker Oats plant
126	Drinkle Building No. 3	-	117 3 rd Avenue South	Central Business District	<ul style="list-style-type: none"> • Architectural style • Associated with John Drinkle - one of the city's first, and one of the youngest, real estate operators • Symbolic of the boom and bust experienced by the city in its early years
127	Priel Block	-	214 21 st Street East	Central Business District	<ul style="list-style-type: none"> • Architectural style • Associated with James Priel, a local builder
128	Zenith Building	-	128 2 nd Avenue North	Central Business District	<ul style="list-style-type: none"> • Architectural style • A modern masonry building

					for its time
129	City Greenhouses	-	1101 Avenue P North	Hudson Bay Park	<ul style="list-style-type: none"> • Representative of greenhouses and nursery's built in the post-WWII era • Cultural landmark
130	Assumption of the Virgin Mary Greek Orthodox Church	-	1020 Dufferin Avenue	Nutana	<ul style="list-style-type: none"> • Architectural style • Religious and Cultural landmark
131	Exhibition Grounds - Log Cabin	-	Ruth/Herman Avenue	Exhibition	<ul style="list-style-type: none"> • Arts and Crafts architectural style • Constructed to commemorate early pioneers
132	Kinsmen Park	-	Spadina Crescent East	City Park	<ul style="list-style-type: none"> • Cultural landmark
133	Louise Grounds	-	411 11 th St E.	Nutana	<ul style="list-style-type: none"> • Cultural and historic landmark
134	First Baptist Church	-	401 4 th Avenue North	City Park	<ul style="list-style-type: none"> • Architectural style • Associated with architectural firm, Webster and Gilbert • Religious and cultural landmark
135	First Synagogue	-	136 Avenue F South	Riversdale	<ul style="list-style-type: none"> • Classical Revival architectural style • Associated with architect, Frank P. Martin • Religious and cultural landmark

136	Grace Gospel Hall	-	120 25 th Street West	Caswell Hill	<ul style="list-style-type: none"> • Old South architectural style • Religious and cultural landmark
137	Walter Lock House		1038 University Drive	Varsity View	<ul style="list-style-type: none"> • Architectural style • Contributes to University Drive's streetscape
138	St. Andrew's Presbyterian Church	-	436 Spadina Crescent East	Central Business District	<ul style="list-style-type: none"> • Gothic architectural style • Associated with architectural firm, Webster and Gilbert • Religions and cultural landmark
139	Henry Leher House	-	1034 University Drive	Varsity View	<ul style="list-style-type: none"> • Tudor architectural style
140	St. James Anglican Church	-	607 Dufferin Avenue	Nutana	<ul style="list-style-type: none"> • Romanesque Revival architectural style • Religious and cultural landmark
141	St. Mary's Church	-	217 Avenue O South	Pleasant Hill	<ul style="list-style-type: none"> • Gothic architectural style • Religious and cultural landmark
142	St. Paul's Cathedral	-	720 Spadina Crescent East	Central Business District	<ul style="list-style-type: none"> • Architectural style (Romanesque and Gothic influences) • Religious and cultural landmark
143	St. Thomas Wesley-United Church	-	808 20 th Street West	Riversdale	<ul style="list-style-type: none"> • Religious and cultural Landmark

144	St. Vincent of Lerins Orthodox Church	-	224 25 th Street West	Caswell Hill	<ul style="list-style-type: none"> Religious and cultural landmark
145	Ernest Bricker House	-	1004 University Drive	Varsity View	<ul style="list-style-type: none"> Dutch Colonial architectural style Associated with Ernest Bricker, proprietor
146	McKim Block	-	416 23 rd Street East	Central Business District	<ul style="list-style-type: none"> Associated with realtor W. McKim
147	The Original Bulk Cheese Warehouse	-	732 Broadway Avenue	Nutana	<ul style="list-style-type: none"> Contributes to the Broadway streetscape
148	W.G Watson House	-	1066 Spadina Crescent East	City Park	<ul style="list-style-type: none"> Architectural style (English and Craftsman influences) Associated with Saskatoon jeweller W. G. Watson
149	Western Rooms Building	-	120 20 th Street West	Riversdale	<ul style="list-style-type: none"> One of the last remaining examples of boom time buildings on 20th Street West
150	Rook and Raven	-	154 2 nd Avenue South	Central Business District	<ul style="list-style-type: none"> Mark of progress Contributes to 2nd Avenue streetscape
151	Buckwold Building	-	75 24 th Street East	Central Business District	<ul style="list-style-type: none"> Architectural style (Chicago influences) Association with the Buckwold Family, including former mayor Sid Buckwold

152	Butler Block	-	239 20 th Street West	Riversdale	<ul style="list-style-type: none"> • Reminiscent of the early boom years of Riversdale
153	Past and Present Building (Chinese National Building)	-	327 21 st Street West	Riversdale	<ul style="list-style-type: none"> • Symbolizes the efforts of Saskatoon citizens to establish a democratic nation within China
154	Fred Delf House	-	1035 University Drive	Varsity View	<ul style="list-style-type: none"> • Craftsman architectural style • Associated with Robert McQueen, prominent western Canadian educator and banker
155	Bill's House of Flowers (Handmade House)	-	712 Broadway Avenue	Nutana	<ul style="list-style-type: none"> • Example of commercial architecture during the Depression period • Contributes to the Broadway streetscape
156	Hearn Block	-	269 3rd Avenue South	Central Business District	<ul style="list-style-type: none"> • Art Moderne architectural style
157	Hudson's Bay Building	-	203 2 nd Avenue North	Central Business District	<ul style="list-style-type: none"> • Representative of retail buildings constructed during the post-WWII era. • Association with the Hudson's Bay Company • Landmark building in Saskatoon's downtown
158	Joe's Cycle	-	220 20 th Street West	Riversdale	<ul style="list-style-type: none"> • Representative of commercial buildings during the pre-WWI era • Contributes to 20th Street

					streetscape
159	Lee Wing Laundry	-	118 20 th Street West	Riversdale	<ul style="list-style-type: none"> • One of the last remaining buildings from Pre WWI time period on 20th Street • Contributes to the 20th Street streetscape
160	Mayfair Drugs	-	504 33 rd Street West	Mayfair	<ul style="list-style-type: none"> • Spanish architectural style
161	McGowan Building	-	122 2 nd Avenue North	Central Business District	<ul style="list-style-type: none"> • Italianate architectural style • Associated with James Wilson, early Saskatoon Councillor and Mayor • Iconic building along 2nd Avenue North
162	McKay Block	-	223 2 nd Avenue South	Central Business District	<ul style="list-style-type: none"> • Represents the historic 2nd Avenue downtown streetscape • Associated with W. McKay, a physician who ran an office at this location during the height of the typhoid epidemic
163	McQuarries Tea and Coffee Merchants	-	708 Broadway Avenue	Nutana	<ul style="list-style-type: none"> • Representative of a commercial building built during WWII • Adds character to Broadway Avenue
164	O.K. Economy Store	-	702 14 th Street East	Nutana	<ul style="list-style-type: none"> • Spanish architectural style • Neighbourhood landmark based on its history as a local corner store.

165	Northeast Swale	-	Northeast Swale	-	<ul style="list-style-type: none"> • Ancient river channel that is now one of the largest pieces of unbroken prairie, riparian forest and wetlands in the Saskatoon region
166	Woolworth F.W. Store	-	220 21 st Street East	Central Business District	<ul style="list-style-type: none"> • Associated with the F.W. Woolworth Store • Contributes to the 21st Street streetscape
167	York Building	-	158 2 nd Avenue North	Central Business District	<ul style="list-style-type: none"> • Chicago architectural style • Association with trade unions and professional associations • Landmark building
168	Five Corners	-	Broadway Avenue	Nutana	<ul style="list-style-type: none"> • Focal intersection throughout Saskatoon's history
169	Long Hill	-	400 – 500 Block Saskatchewan Crescent East	Nutana	<ul style="list-style-type: none"> • Historic roadway located on the East bank of the River
170	Short Hill	-	500 – 600 Block Victoria Avenue	Nutana	<ul style="list-style-type: none"> • Historic roadway located on the East bank of the River
171	Granite Curling Club	-	480 1 st Avenue North	City Park	<ul style="list-style-type: none"> • Unique interior • Associated with Saskatoon's curling history
172	Sutherland Memorial Hall	-	1112 Central Avenue	Sutherland	<ul style="list-style-type: none"> • Reminiscent and symbolic of the original Sutherland community

173	19th Street Subway	-	100 Block 19 th Street East	Central Business District	<ul style="list-style-type: none"> • Only remaining subway in downtown Saskatoon
174	Empyrean Building	-	616 10 th Street East	Nutana	<ul style="list-style-type: none"> • False front architectural style • Symbolic of Saskatoon's early lumber industry • Adds charm and character to the Broadway area
175	Great Western Brewing Company	-	510 2nd Avenue North	Central Business District	<ul style="list-style-type: none"> • Historic brewery
176	Heinze Institute	-	247 1 st Avenue North	Central Business District	<ul style="list-style-type: none"> • Architectural style (Beaux Arts influences)
177	John Deere Warehouse	-	330 Ontario Avenue	Central Industrial	<ul style="list-style-type: none"> • Representative of warehouses built during the pre-WWI era • One of the prominent agricultural machinery warehouses in the early 1900's.
178	Robin Hood Mill	-	95 33 rd Street East	Central Industrial	<ul style="list-style-type: none"> • Symbolizes the importance of flour mills in the development of agriculture in Saskatchewan
179	Sterling Home Furnishing's/DeFehr's	-	261 1 st Avenue North	Central Business District	<ul style="list-style-type: none"> • Architectural style (Beaux Arts influences) • Contributes to the 1st Ave streetscape
180	Walter's Cycle	-	225 20 th Street West	Riversdale	<ul style="list-style-type: none"> • Association with George Norman, early newspaper

					<p>publisher, Councillor, Mayor and Provincial politician</p> <ul style="list-style-type: none"> • Association with Jack Walters, a local bicycle retailer
181	A.L. Cole Pumphouse	-	145 Spadina Crescent West	Riversdale	<ul style="list-style-type: none"> • Only surviving remnant of the original Saskatoon Power Plant
182	Andrew Boyd House	Designated	803 9 th Avenue North	City Park	<ul style="list-style-type: none"> • Tudor architectural style • Association with Andrew N. Boyd, an auto dealership owner • Association with Dr. Lorne McConnell, a pioneer in the field of neurosurgery in Western Canada
183	Joe Duquette / Oskayak High School	-	919 Broadway Avenue	Nutana	<ul style="list-style-type: none"> • Important educational institution for First Nations students.
184	Mayfair School	-	510 34 th Street West	Mayfair	<ul style="list-style-type: none"> • Representative of the importance of education in Saskatoon • Neighbourhood landmark
185	The Weir	-	1100 Block Spadina Crescent East	City Park	<ul style="list-style-type: none"> • Historic Landmark
186	Moose Jaw Trail	-	Mark Thompson Park (122 Rempel Manor)	Stonebridge	<ul style="list-style-type: none"> • Honors the original wagon cart trail which brought settlers from Moose Jaw to

					Saskatoon
187	Sion Academy	-	830 Idylwyld Drive North	Central Industrial	<ul style="list-style-type: none"> Religious institution
188	Ukrainian War Veterans Hall	-	228 Avenue G South	Riversdale	<ul style="list-style-type: none"> War veterans tribute
189	Gabriel Dumont Statue	-	Friendship Park (325 Spadina Crescent East)	Central Business District	<ul style="list-style-type: none"> Tribute to Metis leader Gabriel Dumont
190	International Peace Plaza	-	Rotary Park (225 Saskatchewan Crescent East)	Nutana	<ul style="list-style-type: none"> Symbol of community peace
191	Patterson Garden Arboretum	-	Preston Avenue North (University of Saskatchewan)	U of S Management Area	<ul style="list-style-type: none"> Representative of prairie plant and tree species
192	Victoria Park	-	810 Spadina Crescent West	King George	<ul style="list-style-type: none"> Cultural landmark



STANDING POLICY COMMITTEE ON PLANNING, DEVELOPMENT AND COMMUNITY SERVICES

City of Saskatoon's Response to Syrian Refugee Crisis

Recommendation of the Committee

1. That the information be received;
2. That the initiatives identified on page 3 of the October 5, 2015 report of the General Manager, Community Services, with no cost implications, including A, C, D, F, G, and H, be supported;
3. That the Administration report further with respect to any cost implications of items B and E on page 3 of the above report; and
4. That the Administration consult with the provincial and federal government on what they are doing in the areas identified in the above report, including the issues of transit services and issues around the identified options, and check further with organizations in the community, including faith-based organizations and those supporting refugees, on any updated information for further reporting to the Standing Policy Committee on Planning, Development and Community Services.

History

At the October 5, 2015 meeting of the Standing Policy Committee on Planning, Development and Community Services, a report of the General Manager, Community Services Department, dated October 5, 2015, was considered.

Attachment

October 5, 2015 report of the General Manager, Community Services Department.

City of Saskatoon's Response to Syrian Refugee Crisis

Recommendation

That the information be received.

Topic and Purpose

This report provides an overview of the current programs and practices the City of Saskatoon (City) already has in place to assist in the support of refugees in Saskatoon. In light of the present Syrian refugee crisis, this report also identifies potential new initiatives the City can consider to enhance supports for refugees coming to Saskatoon.

Report Highlights

1. The City and refugee-serving organizations presently have a number of initiatives underway that assist in welcoming, help to settle, and integrate, refugees and immigrants to the community.
2. In recognition of the current Syrian refugee crisis, the Administration has gathered input from key stakeholders in the community, and identified several possible initiatives to improve and enhance already existing services, as well as offering new ideas of what the City could do regarding refugee settlement and integration.
3. There are a couple of options to consider related to Saskatoon Transit providing either discounted or no-cost bus passes for the first year of refugee settlement in Saskatoon.

Strategic Goal

Under the Strategic Goal of Quality of Life, the information presented in this report supports the four-year priority to implement the Immigration Action Plan.

Background

During its September 8, 2015 meeting, the Standing Policy Committee (SPC) on Planning Development and Community Services (PDCS) resolved:

“That the Administration be requested to report on the practices, programs and policies regarding provision for support of refugees settled in Saskatoon, including funding available for assisting organization such as the Mennonite Central Committee, Global Gathering Place and the Saskatoon Open Door Society to secure private sponsorship of Syrian refugees and the possibility of Saskatoon Transit providing no cost transit passes for the first year of refugee settlement in Saskatoon to assist them in finding employment in the city.”

Report

In the past few months, the Syrian refugee crisis has become increasingly apparent. The situation has actually been going on for nearly five years. Not only is the plight of refugees very complex, but it is also a complex process for refugees to come to Canada. Many people in the community would like to help and are asking what they can do, how they can sponsor, and who they can talk to. The will is there, but the information on a website or in a pamphlet is sometimes too complex to understand. Much good work has been done in the Saskatoon community to provide a safe place for refugees, and more needs to be done, especially now during this time of a humanitarian crisis. Attachment 1 provides background information on categories of refugees and ways in which refugees are sponsored to come to Canada.

Present City of Saskatoon Initiatives for Refugees and Immigrants Living in Saskatoon

Over the past number of years, through the implementation of the Immigration Action Plan, the City's primary roles, related to supporting immigrants and refugees, have been in the areas of education and awareness, program funding, community connections, and ensuring the City's programs and services are accessible and welcoming to the needs of immigrants and refugees. Below is a summary of some of the specific initiatives the City currently has underway or could consider undertaking (for further details on these initiatives please see Attachment 2):

- a. Funding supports are provided to a number of the settlement agencies through various city-wide grants.
- b. The Leisure Access Program and subsidized bus passes for all low-income residents.
- c. The Administration has already implemented a process for Government-Assisted Refugees (GAR) to allow immediate access to the Leisure Access Program.
- d. The Immigration, Diversity and Inclusion Consultant (IDIC) has been meeting with the Saskatoon Refugee Coalition since 2006 to keep informed about the situation of refugees in Saskatoon. The IDIC assists with the coordination of events such as World Refugee Day, is on the Advisory Committee for the Canadian Council of Refugees National Conference in Saskatoon in June 2016, and is currently working with the University of Saskatchewan, Saskatoon Open Door Society, and Station 20 to deliver a project called "Moving Stories" to help refugees and indigenous peoples tell their stories.
- e. Since early September 2015, the IDIC has been in contact with people in the settlement community as well as the Chairperson of the Saskatoon Refugee Coalition, and on September 11 and 16, assisted in hosting meetings with key stakeholders to begin a larger community conversation on how, as a community, we address this refugee crisis situation.
- f. On September 22, the IDIC participated in the Mennonite Central Committee community information session about refugee sponsorship.

New, Continued, and/or Renewed Initiatives

- a. Continue working with the refugee-serving organizations to provide learning opportunities on both the processes of refugees coming to Canada, as well as the integration and settlement needs of refugees.
- b. Host community meetings to help create broader community awareness and understanding of the current refugee situation and how individuals, organizations, and groups can best help.
- c. Connect with the media to assist in expanding the message and call to action for individuals and the community to support refugees.
- d. On the City website, include information about private sponsorship of refugees and the contact information for existing Sponsorship Agreement Holders in Saskatoon.
- e. Expand the Leisure Access and Discounted Bus Pass Programs to include privately sponsored refugees.
- f. Encourage residents to consider privately sponsoring a refugee/refugee family or donating to support the needs of refugees arriving in Saskatoon.
- g. Work with partners in Immigration at the provincial and federal governments to help provide a coordinated approach in Saskatoon.
- h. City Council could consider advocating on behalf of refugees by writing to the Federal Government and could proclaim Saskatoon as a city welcoming refugees.

Transit Support for Refugees

Saskatoon Transit was asked to consider a lower-cost or no-cost bus pass for all refugees as transition support for their first year living in Saskatoon, to assist them in finding employment.

Currently, all GAR adult refugees receive \$81 per month from Citizenship and Immigration Canada (CIC) to purchase a bus pass. Therefore, this consideration for transit support would be targeted to the other categories of refugees (i.e. privately sponsored, inland refugee claimants, and refugee dependants).

There are a number of options that could be considered, ranging from a percentage discount to a low-rate subsidized bus pass to a free bus pass. These options, and some background on each of these particular options, can be found in Attachment 3 of this report.

Options to the Recommendation

1. The Administration could be directed to expand efforts in supporting refugees and refugee-serving agencies in Saskatoon, as identified earlier in this report.
2. City Council could direct Saskatoon Transit to implement one or more of the transit support options highlighted in Attachment 3.

Public and/or Stakeholder Involvement

The community held a rally on September 6, 2015. Two of the organizers from the rally have been consulted with regards to this report. On September 11 and 16, the IDIC organized meetings of refugee-serving organizations and individuals to share information on the state of the crisis and to get input on what the City can do to help.

Communication Plan

Continue working with community stakeholders involved with refugee settlement and support to share information and assist in planning and hosting community information sessions.

Financial Implications

Option 1 – Most of the initiatives do not have a financial impact, except as more community meetings are planned and with expansion of the communications plan, the refugee-serving organizations are looking for some potential assistance with costs for the community information meetings and public education or awareness materials.

A potential funding source is the Community Contingency Fund, which is part of the Cash Grant Social funding, and each year \$4,800 is set aside for City Council's discretion. Also, any funding not allocated from year to year is put into a reserve for future expenditures. There is currently a balance of \$11,600 in the Community Initiatives Reserve, and another \$4,800 in the 2015 Operating Budget, that has not yet been allocated. There is \$16,400 that City Council has at its discretion to allocate.

Option 2 – See Attachment 3 for a complete summary of the high-level financial implications for each of the transit support options to consider.

Other Considerations/Implications

There are no policy, environmental, privacy, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

The follow-up on a number of the no-cost initiatives is already underway or could be implemented in the immediate future. If the SPC on PDCS identifies an option for Transit support that requires an identified funding source, the Administration will provide a further report for consideration during the 2016 Budget Review meetings.

Public Notice

Public notice, pursuant to Section 3 of Public Notice Policy No. C01-021, is not required.

Attachments

1. Background on Refugees in Canada or Ways to Come to Canada
2. Present and Potential New Initiatives by the City of Saskatoon
3. Transit Support for Refugees – Potential Options

Report Approval

Written by: April Sora, Immigration, Diversity and Inclusion Consultant and
Shannon Hanson, Social Development Manager

Reviewed by: Lynne Lacroix, Director of Community Development

Approved by: Randy Grauer, General Manager, Community Services Department

S/Reports/CD/PDCS – City of Saskatoon's Response to Syrian Refugee Crisis/lc

Background on Refugees in Canada or Ways to Come to Canada

Background Information about Refugees in Canada or Coming to Canada

The Numbers

Canada - On average, for the past few years, Canada has received about 25,000 refugees per year. It is important to note that Canada has, over its history, responded in times of world crisis by receiving significantly higher numbers of refugees in a given year. For example: in the 1940s during and after World War II, approximately 48,000 war brides and their 22,000 children arrived in Canada; in 1956/57 – 37,500 Hungarian refugees; in 1968/69 - Canada took in 11,000 Czechoslovakian refugees; in 1979/80 - approximately 60,000 Laotians, Vietnamese, and Cambodians; early 1990's - Canada received 53,000 refugees.

Saskatchewan - On average, for the past few years, Saskatchewan has received approximately 600 refugees per year.

Saskatoon - On average, in the recent past, Saskatoon has received anywhere from 100 to a maximum of 300 refugees per year.

Categories of Refugees

1. Government-Assisted Refugees (GAR) – Convention Refugees Abroad whose initial resettlement in Canada is entirely supported by the Government of Canada. On average, about 33% of refugees to Canada are GARs.
2. Privately Sponsored Refugees – refugees coming to Canada through one of a number of the sponsorship categories (see further details below). On average, about 20% of refugees in Canada are privately sponsored.
3. Refugees Landed in Canada – commonly referred to as inland claimants or those individuals who claim refugee protection from inside Canada. People already in Canada who fear persecution or who would be in danger if they had to leave. Some dangers they may face include torture, a risk to their life, or a risk of cruel and unusual treatment or punishment. If they feel they could face one of these risks if they go back to their home country or the country where they normally live, they may be able to seek protection in Canada as a refugee. On average, 32% of refugees in Canada are refugee claimants.
4. Refugee Dependents – dependents of refugees sponsored to come to Canada or of refugees landed in Canada claiming refugee protection. On average, 15% of refugees in Canada are refugee dependents.

Ways in Which Refugees are Sponsored to Come to Canada

- 1. Government-Assisted Refugees (GAR)** are Convention Refugees Abroad whose initial resettlement in Canada is entirely supported by the Government of Canada or Quebec. This support is delivered by Citizenship and Immigration Canada (CIC) supported non-governmental agencies. Support can last up to one year from the date of arrival in Canada, or until the refugee is able to support himself or herself, whichever happens first. It may include:
 - accommodation;
 - clothing;
 - food;
 - help in finding employment and becoming self-supporting; and
 - other resettlement assistance.

Organizations offering resettlement services to newcomers to Canada are called service provider organizations. There are various service provider organizations located throughout Canada working within the Resettlement Assistance Program. The Canadian Government offers financial assistance for GARs and loans for all resettled refugees.

- 2. Sponsorship Agreement Holders**

A number of organizations across the country have signed agreements with the Government of Canada to help support refugees from abroad when they resettle in Canada. They are known as sponsorship agreement holders. They can sponsor refugees themselves or work with others in the community to sponsor refugees.

Most sponsorship agreement holders are religious, ethnic, community, or service organizations.

- 3. Blended Visa Office-Referred Program**

The Blended Visa Office-Referred (VOR) Program matches refugees identified for resettlement by the United Nations High Commissioner for Refugees (UNHCR) with private sponsors in Canada.

- 4. Groups of Five**

A Group of five (G5) is five or more Canadian citizens or permanent residents who have arranged to sponsor a refugee living abroad to come to Canada. All of the group members must be at least 18 years of age and live or have representatives in the area where the refugee will settle. The group must agree to give emotional and financial support to the refugee(s) for the full sponsorship period - usually one year.

- 5. Community Sponsors**

Some community organizations can sponsor refugees to come to Canada. A community sponsor can be:

- an organization;
- an association; or
- a corporation.

The organization, association, or corporation does not have to be incorporated under federal or provincial law.

A community sponsor must make an emotional and financial commitment to support the refugees for the duration of the sponsorship. Sponsorships generally last for one year. The amount of money needed varies, depending on the number of refugees sponsored. A community sponsor must be located in the community where the refugees are expected to settle.

Special Note: To help more Canadians and permanent residents privately sponsor Syrian and Iraqi refugees, the CIC has waived the requirement for Groups of Five and Community Sponsors to provide proof that the sponsored person is a refugee.

6. Co-Sponsors

A community sponsor can form a partnership with an individual or another organization. These are known as co-sponsors. The co-sponsor must agree to help deliver settlement assistance and share the responsibility for supporting the sponsored refugee.

7. Joint Assistance Program – Sponsoring Refugees with Special Needs

CIC sometimes partners with organizations to resettle refugees with special needs. This program is called Joint Assistance Sponsorship (JAS). Refugees with special needs may need more support than other refugees in order to settle in Canada. Special needs may arise due to:

- trauma from violence or torture;
- medical disabilities;
- the effects of systemic discrimination; or
- a large number of family members.

Under the JAS program, refugees receive support from the government and a private sponsor for up to 24 months, depending on the case. In a few cases, the private sponsor may provide support for up to 36 months. These refugees get income support from the Government of Canada for food, shelter, clothing, and basic household goods. They are also matched with a private sponsorship group. The sponsorship group helps refugees adjust to life in Canada by providing settlement help and emotional support.

Present and Potential New Initiatives by the City of Saskatoon

Present City of Saskatoon (City) Initiatives for Refugees and Immigrants Living in Saskatoon

- a) Funding supports are provided to a number of the settlement agencies through various city-wide grants, such as:
 - i) Cash Grant Social funding;
 - ii) Community Grant funding; and
 - iii) Sports Participation Grant funding.

(Note: it is recognized that not all funding is targeted specifically to refugees).
- b) The Leisure Access Program and subsidized bus passes are provided for social assistance clients as well as the Discounted Bus Pass Program for all low-income residents.
- c) The Administration has already implemented a process for Government-Assisted Refugees (GAR) to allow immediate access to the Leisure Access Program. Of note, each GAR adult refugee receives \$81 per month from Citizenship and Immigration Canada (CIC) to go towards a bus pass.
- d) The Immigration, Diversity and Inclusion Consultant (IDIC) has been meeting with the Saskatoon Refugee Coalition since 2006 to keep informed about the situation of refugees in Saskatoon and when and how the City can help. In addition, the IDIC assists with the coordination of events such as World Refugee Day.
- e) The IDIC is working with the University of Saskatchewan, Saskatoon Open Door Society, Station 20, and other partners to deliver a project called "Moving Stories" to help refugees and indigenous peoples tell their stories.
- f) The IDIC is on the Advisory Committee for the Canadian Council of Refugees National Conference being held in Saskatoon on June 2 to 4, 2016, at the Hilton Garden.
- g) Since the breaking news of the Syrian refugee crisis in early September, 2015, the IDIC has been in contact with people in the settlement community as well as the Chairperson of the Saskatoon Refugee Coalition and will continue to stay connected with the refugee-serving organizations and assisting with community information meetings.
- h) On September 11 and 16, the IDIC assisted in hosting meetings with key stakeholders to begin a larger community conversation on how, as a community, we address this refugee crisis situation.
- i) On September 22, the IDIC attended the Mennonite Central Committee community information session on what is refugee sponsorship, the emotional impact to the community, and general question and answer time.

New, Continued, and/or Renewed Initiatives

- a) Continue working with the community-based organizations to provide learning opportunities for all residents on both the processes of refugees coming to Canada, as well as the integration and settlement needs of refugees. This would also include sharing information on existing City program that can help support the needs of refugees such as the Affordable Housing Program, the Housing Handbook, the Leisure Access Program, and the Discounted Bus Pass Program.
- b) Host community meetings to help create broader community awareness and understanding of the current refugee situation and how individuals, organizations, and groups can best help. A community meeting is already planned for early October, 2015, to begin this broader community conversation, and on October 23 and 24, there are plans to host a Refugee Sponsorship Training Program.
- c) Connect with the media to assist in expanding the message and call to action for individuals and the community to support the current and future situations with refugees.
- d) On the City website's Immigration webpage, the Administration could include information about private sponsorship of refugees and the contact information for existing Sponsorship Agreement Holders in Saskatoon.
- e) Expand the Leisure Access and Discounted Bus Programs to include privately sponsored refugees (related to expediting the early access and process for applying for the program).
- f) Encourage the community, included members of City Council, and City staff to consider privately sponsoring a refugee/refugee family or donating to support the needs of refugees arriving in Saskatoon.
- g) The City to work with our partners in Immigration at the Provincial and Federal Governments to provide a coordinated approach to ensure the resources and services are reaching those who are most vulnerable.
- h) City Council could consider advocating on behalf of refugees by writing to the Federal Government asking for a stronger and more immediate response to supporting the Syrian refugees or could proclaim Saskatoon as a City welcoming refugees and supportive of refugee-serving agencies.
- i) Consider a low- or no-cost one-year bus pass as transition support for all refugees for their first year living in Saskatoon.

Transit Support for Refugees – Potential Options

Consideration for transit support would be targeted to the following categories of refugees: privately sponsored refugees, inland refugee claimants, and refugee dependants. As noted in the report, Government-Assisted Refugees (GARs) do receive funding from the Federal Government to cover the cost of their monthly bus pass.

The privately sponsored refugees do not receive any government funding for transportation costs; the private sponsor must provide money to cover these costs. The refugees who land in Canada (often referred to as inland claimants) have no support from the other levels of government and are not supported through a private sponsor. There are a number of options that could be considered, ranging from a percentage discount, to a low-rate subsidized bus pass to a free bus pass.

Some of the possible options that could be considered are noted below along with a high-level approximate financial implication for each option. **Please note**, that the details for the full-cost implications and/or reduced revenue potential would need to be further explored should one of these options be selected. The financial implications have been based on a range from the low end (100 refugees per year) to the upper end (300 refugees per year) and reflects revenue that would otherwise be collected if the passes were sold at full price. If the Standing Policy Committee on Planning, Development & Community Services identifies an option for transit support that could have an impact on 2016 projected revenues or requires an identified funding source, the Administration will provide a further report for consideration during the 2016 Budget Review meetings.

1. Discounted Bus Pass Program

The Administration could implement a process for screening and allow for early access to the Discounted Bus Pass Program, (without refugees needing to submit an Income Tax Notice of Assessment).

This Discounted Bus Pass Program is currently connected to the Leisure Access Program and allows each eligible family member that has received a one-year free LeisureCard to also purchase a bus pass at a rate of 20% savings on the purchase of each monthly bus pass. In order to qualify under the current process, all applicants must complete an application form, and submit an Income Tax Notice of Assessment to verify their annual household income. The annual household income must be at or below the established low-income levels

Potential Financial Implications of this Option

100 annual adult passes discounted at 20% equalling approximately \$35,000 lower revenue than full price passes to a maximum of 300 annual passes discounted at 20% equalling approximately \$104,000 less revenue than full price passes.

2. Subsidized/Discounted Bus Pass Program

Expand the existing Subsidized Bus Pass Program to include all refugees. This program is currently only available to people on social assistance. The current Ministry of Social Assistance Funding Agreement has the purpose of assisting with

the provision of public transit at a reduced cost for recipients of the Saskatchewan Social Assistance Program, the Saskatchewan Assured Income for Disability, the Transitional Employment Allowance, Saskatchewan Employment Supplement, and the Provincial Training Allowance.

With this program, for an adult monthly bus pass only, (not applicable to an annual pass rate), the individual pays approximately one third of the cost or \$27, the Ministry's Funding Agreement with the City of Saskatoon (City), provides \$25 to the City and the balance of the cost of the bus pass (\$29), is subsidized through the Saskatoon Transit's operating budget. The Funding Agreement with the City of Saskatoon for the 2015 to 16 fiscal year is \$888,000. This equates to a \$25 subsidy on approximately 35,500 monthly passes, or the equivalent of approximately 3,000 annual bus passes.

If a similar program were offered for refugees who are not eligible to receive social assistance, where the refugee would be required to pay the total of \$27 for a one-month bus pass, then a funding source to support the full \$54 discount per month, would need to be identified and/or accounted for within the Saskatoon Transit cost recovery targets.

Potential Financial Implications of this Option

For adult one-month bus passes for 100 refugees for a full year, with a \$54 discount on each monthly pass would represent approximately \$64,800 less revenue.

Note: If this calculation were done using the annual pass rate with the refugee paying \$27 per month, the amount would be \$56,700.

For adult one-month bus passes for 300 refugees for a full year, with a \$54 subsidy on each monthly pass, would represent approximately \$194,400 less revenue.

Note: If this calculation were done using the annual pass rate with the refugee paying \$27 per month, the amount would be \$170,100.

3. Free One-Year Bus Pass for Adult Refugees

To assist adult refugees with the opportunity to find employment in Saskatoon, offer a free one-year bus pass to the adult refugees that are privately sponsored or are inland claimant refugees.

Potential Financial Implications of this Option

Free one-year bus pass for adult refugees (based on approximately 80% of the refugees being adults), at the low end of 80 adult passes at the current annual rate of \$891, would require a subsidy or funding source of approximately \$71,300, and the upper end of 240 annual adult bus passes would require a subsidy or funding source of approximately \$213,800.

4. Free One-Year Bus Pass to all Refugees

Offer a free one-year bus pass to all refugees regardless of age (other than GARs). This would include all privately sponsored refugees and dependants, refugees landed in Canada (inland claimants), and all dependants of refugees.

Potential Financial Implications of this Option

A maximum of 300 annual adult passes would require a subsidy or funding source of approximately \$267,300. The final total would be dependent on the total number of refugees arriving in Saskatoon annually and would depend on the mix of adults, youth, and seniors.



STANDING POLICY COMMITTEE ON PLANNING, DEVELOPMENT AND COMMUNITY SERVICES

Communications to Council – Jesse Todd – Request for Bylaw – Disposal of Asbestos Material

Recommendation of the Committee

1. That the report of the General Manager, Community Services Department, dated October 5, 2015, be received as information; and
2. That the Administration report back on the matter at a later date on other improvements that might be occurring in Saskatoon.

History

At the October 5, 2015 meeting of the Standing Policy Committee on Planning, Development and Community Services, a report of the General Manager, Community Services Department, dated October 5, 2015, was considered.

A presentation was also received from Mr. Jesse Todd, expressing appreciation for the work done in asbestos awareness to date and offering suggestions for further improvements.

Attachment

October 5, 2015 report of the General Manager, Community Services Department.

Communications to Council – Jesse Todd – Request for Bylaw – Disposal of Asbestos Material

Recommendation

That the report of the General Manager, Community Services Department, dated October 5, 2015, be forwarded to City Council for information.

Topic and Purpose

The purpose of this report is to provide information on current procedures regarding the disposal of asbestos material.

Report Highlights

1. The monitoring, testing, and inspection of asbestos abatement projects falls within the jurisdiction of the provincial *Occupational Health & Safety (OH&S) Regulations*.
2. The City of Saskatoon's (City) Environmental and Corporate Initiatives Division has a Declaration of Intent form that is completed by demolition contractors at the building permit application stage to disclose the contractor's intent on the disposal of demolition materials.
3. The Building Standards Division has created a form and process that specifically deals with the removal of asbestos.
4. The City currently does not accept asbestos at its landfill site.

Strategic Goal

This report supports the City's Strategic Goal of Continuous Improvement by having processes in place that promote health and safety for employees and citizens.

Background

At its March 31, 2014 meeting, City Council considered a letter and presentation from Jesse Todd, Chairman, Saskatchewan Asbestos Disease Awareness Organization, requesting a bylaw regarding the disposal of asbestos material (see Attachment 1). City Council requested that the Administration report back on the issue.

This report focuses on the type of information received at the permit application stage for demolitions and the process the City landfill uses for handling construction materials.

Report

Monitoring, Removal, and Testing of Asbestos Projects

Regulations regarding the monitoring, removal, testing, and inspection of asbestos abatement processes are contained in the provincial *OH&S Regulations*. These regulations are enforced by the provincial OH&S Division. The province of

Saskatchewan has developed a document entitled Guidelines for Managing Asbestos in Buildings, which details asbestos abatement and disposal requirements and procedures.

Disposal of Materials

Building permits are required for the demolition of buildings. As part of the current application process for a demolition, the applicant must complete a Declaration of Intent form that is collected on behalf of the Environmental and Corporate Initiatives Division (see Attachment 2). This form requires the applicant to declare if there are any hazardous materials involved in the demolition and where these materials will be taken. The information on this form has two purposes:

- 1) The Building Standards Division uses it to ensure that inspection staff are appropriately informed and aware of asbestos or other hazards; and
- 2) The Environmental and Corporate Initiatives Division collects data from the forms to support existing and future environmental program initiatives.

Form for Removal of Asbestos

On January 1, 2015, the Building Standards Division introduced the “Asbestos Removal Notification Form” that building permit applicants are required to complete before a building or demolition permit will be issued (see Attachment 3). By completing this form, it can be determined if an asbestos abatement process is planned for a demolition or renovation project. By disclosing this information at the building permit application stage, the building inspector may be made aware of the potential hazard.

No Acceptance of Asbestos at City Landfill

The City currently does not accept asbestos materials at its landfill site, and all reported asbestos waste is currently diverted to the Northern Landfill. If any asbestos material is discovered at the City landfill, it is immediately buried by landfill operators. Over the last year, there have been no reported incidents of asbestos at the City’s landfill.

Public and/or Stakeholder Involvement

There is no public or stakeholder involvement.

Other Considerations/Implications

There are no options, policy, financial, environmental, privacy, or CPTED implications or considerations. A communication plan is not required at this time.

Due Date for Follow-up and/or Project Completion

There is no follow-up report planned.

Public Notice

Public notice, pursuant to Section 3 of Public Notice Policy No. C01-021, is not required.

Attachments

1. Copy of Communications to Council – Jesse Todd
2. Declaration of Intent
3. Asbestos Removal Notification Form

Report Approval

Written by: Kara Fagnou, Director of Building Standards

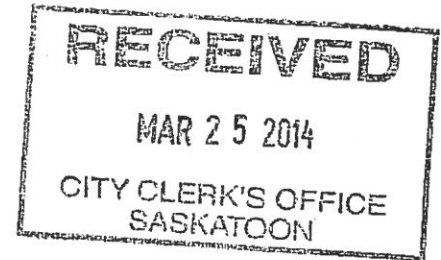
Reviewed by: Pat Hyde, Director of Public Works

Approved by: Randy Grauer, General Manager, Community Services Department

S:\Reports\DS\2015\PDCS – Communications to Council – Jesse Todd – Request for Bylaw – Disposal of Asbestos Material\ks
BF 39-14

Copy of Communications to Council - Jesse Todd

Jesse Todd
Saskatchewan Asbestos Disease Awareness Organization
R.R. 2, Stn Main
Saskatoon, SK
S7K 3J5



March 25, 2014

Office of the City Clerk
City of Saskatoon

To whom it may concern,

I would like to speak to Saskatoon City Council at the March 31 meeting on the matter of introducing a new bylaw that would require contractors to notify building inspectors of beginning work on buildings constructed prior to 1983 in order to prevent worker exposure to asbestos containing material.

Saskatoon City Council may be aware that the first week of April has been proclaimed by the Saskatchewan Government as "Asbestos Awareness Week." The Saskatchewan Asbestos Disease Awareness Organization is dedicated to raising awareness with the end goal of preventing exposure to asbestos on the job, at home, at school or in any public building. As a way to move toward our goal and in the spirit of Asbestos Awareness Week, we would like to bring forward a proposal for city council.

The councillors of Saskatoon may be aware of a bylaw that has been passed in Calgary recently. This bylaw addresses the demolition, renovation or construction that may affect any building that was built prior to 1983. It has been determined that many building constructed prior to this date have the potential to contain asbestos. The intent of this bylaw is to trigger a notification process when a contractor intends to work on a building that was constructed prior to 1983. This will require contractors to notify the city building inspectors office prior to beginning work on the intended building. A contractor would be required to demonstrate to the inspectors office that they have taken the appropriate samples and have demonstrated due diligence prior to beginning work. If asbestos is identified in the samples, the contractor is required to take the appropriate precautions for the abatement process. This notification process will allow inspectors to become aware of asbestos processes that are going on within the city in order to monitor the progress. If inspectors are aware of the presence of asbestos in buildings where work is happening, it will also give them the opportunity to take the appropriate precautions to protect themselves from exposure to asbestos containing material.

We are asking that Saskatoon City council consider adopting a bylaw of a similar nature in order to ensure the protection of construction workers, city employees and the general public.

I have enclosed a copy of the City of Calgary Notice of Motion as a reference.

Sincerely,

Jesse Todd

Chairman, Saskatchewan Asbestos Disease Awareness Organization



RECEIVED

NOTICE OF MOTION 2014 JAN -2 A 7:15
CC 561 (R2009-05)

NM2014-January 2

THE CITY OF CALGARY
CITY CLERK'S

RE: ASBESTOS AND DEMOLITION

COUNCILLOR CARRA

WHEREAS construction and demolition activities are considered fundamental to the creation, maintenance and revitalization of new and established communities in Calgary;

AND WHEREAS buildings existing or constructed before 1983 have the potential to contain harmful asbestos fibres in the building materials;

AND WHEREAS in buildings to be renovated or demolished, materials having the potential for releasing asbestos fibres must be removed prior to renovation or demolition;

AND WHEREAS specific procedures are outlined by Occupational Health and Safety (OHS) for workers to ensure that any airborne contaminants are contained and anyone exposed is protected;

AND WHEREAS if particulates get released into the air during demolition and the public could be affected it becomes a matter under the jurisdiction of Alberta Health Services (AHS);

AND WHEREAS The City has no direct jurisdiction pertaining to asbestos abatement, the Alberta Building Code has indirect authority by requiring that the safety of the public is maintained;

NOW THEREFORE BE IT RESOLVED that Administration provide notice to contractors and/or owners that, as part of the building permit process and in accordance with the Alberta Building Code, contractors or owners shall provide notification to Building Regulations prior to the commencement of demolition activity, thereby enabling City building inspectors to monitor whether the abatement procedures are occurring in the interest of public safety.

Signature of Member(s) of Council

DECLARATION OF INTENT

Property owner:

name: _____

address: _____

Demolition Contractor:

name: _____

address: _____

phone number: _____

Type of building being demolished: _____

Date(s) of demolition: _____

Address of the demolition site: _____ in the City of Saskatoon.

With respect to the demolition that will be taking place at the above referenced site it is my intent to dispose of the demolition material in the following manner:

1. Clean material: Estimated Amount: _____

- Not Applicable
- Overpass Farms clean fill site
- City of Saskatoon Landfill
- Northern Landfill

2. Mixed material: Estimated Amount: _____

- Not Applicable
- City of Saskatoon Landfill
- Northern Landfill

3. Material separated for recycling: Estimated Amount: _____

- Not Applicable
- Other: _____

4. Hazardous Material: * Please attach abatement plan as indicated in instructions.

- Not Applicable
- Other: _____

Dated: _____

Signed by: _____
owner or contractor

**ASBESTOS REMOVAL
NOTIFICATION FORM**

January 1, 2015

This form must be completed and submitted to the City of Saskatoon, Building Standards Division prior to the issuance of a Building Permit for the demolition, alteration or renovation of a building that was constructed before 1983 or that is known to contain asbestos products.

Project Name: _____

Project Address: _____

(Check applicable box below.)

- Materials having the potential for releasing asbestos fibres have been removed from the project area to be altered or demolished in accordance with Saskatchewan Occupational Health and Safety (OH&S) Regulations.
OR
- Asbestos removal is planned as part of this renovation or demolition project. Attach details.
OR
- Asbestos containing materials will not be disturbed or removed as part of this project.

Name (print): _____

Signature: _____

Representing (firm): _____

Address: _____

Phone: _____

Date: _____

Frequently Asked Questions:*Do I need a Building Permit to perform asbestos abatement work?*

No. A Building Permit is not a permit to perform asbestos abatement work. Rather, this form is required as part of your Building Permit application and serves as notification to ensure the safety of building inspectors. Limited demolition or construction is expected during asbestos abatement work and jurisdiction for this work is with the Government of Saskatchewan. Provincial OH&S may be contacted at 306-933-5052.

What happens if I find asbestos after I already have a Building Permit?

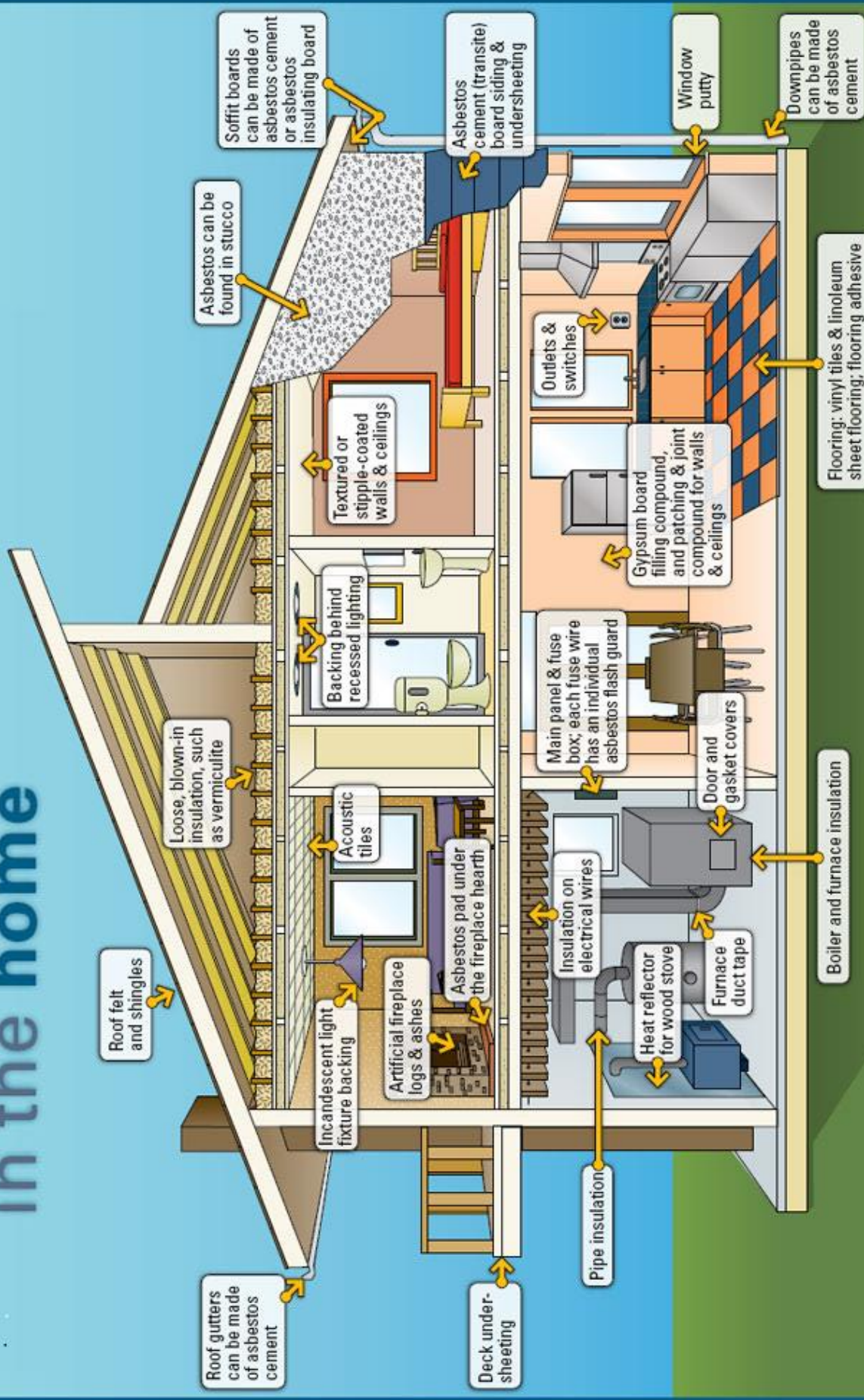
In the event that asbestos is discovered, you are expected to:

1. Immediately stop work;
2. Contact the Building Inspections Phone at 306-975-7924 to notify the Building Inspector; and
3. Address the asbestos that has been encountered by providing notification as required by OH&S and ensuring asbestos will be dealt with according to OH&S requirements.

Sources of Asbestos Containing Materials (ACM) in buildings may be found at the following:

- <http://www.saskatchewan.ca/work/safety-in-the-workplace/safety-standards-and-alerts/asbestos>
- Drawing of an older home depicted on the reverse side of this form.

Potential sources of asbestos in the home





STANDING POLICY COMMITTEE ON PLANNING, DEVELOPMENT AND COMMUNITY SERVICES

After Hours Barking Dog Complaints

Recommendation of the Committee

That the report of the General Manager, Community Services Department, dated October 5, 2015, be received as information.

History

At the October 5, 2015 meeting of the Standing Policy Committee on Planning, Development and Community Services, a report of the General Manager, Community Services Department, dated October 5, 2015, was considered.

Attachment

October 5, 2015 report of the General Manager, Community Services Department.

After Hours Barking Dog Complaints

Recommendation

That the report of the General Manager, Community Services Department, dated October 5, 2015, be forwarded to City Council for information.

Topic and Purpose

This report provides information on an Administrative review of the number of after hour animal complaints received by Saskatoon Police Service (SPS) and whether changing the hours of operation of the Saskatoon Animal Control Agency (SACA) would affect the responsiveness to handling barking dog complaint calls in the evening.

Report Highlights

1. From January 1, 2012, to August 31, 2015, SPS received a total of 47 calls regarding barking dog complaints between the hours of 8 p.m. and 8 a.m. This volume represents, on average, less than two calls per month.
2. To increase the service level to respond to after hour barking dog complaints would require SACA to schedule two additional animal control officers (for safety reasons) at a cost of \$3,200 per month. Animal Services does not have the funds within its 2015 operating budget to cover this additional expense and increase in service.

Strategic Goal

Under the Strategic Goal of Continuous Improvement, reviewing the effectiveness and efficiency of the process for handling barking dog complaints will support the long-term strategy of providing a coordinated approach to customer service, with quick and accurate responses.

Background

A report was submitted to City Council on April 27, 2015, with recommendations to consider proposed amendments to Animal Control Bylaw No. 7860 (Bylaw). In response to these recommendations, City Council directed the Administration to complete a further review of complaints received by SPS. City Council requested information on the feasibility of extending the hours for SACA and if it would have an effect on the responsiveness for handling barking dog complaint calls in the evening.

The current process used by SACA for handling barking dog complaints is as follows:

- i. From **April to October**, SACA provides one on-call staff member for after hour animal response between the hours of **10 p.m. to 7 a.m.**;
- ii. From **November to March**, SACA provides one on-call staff member for after hour animal response between the hours of **8 p.m. to 8 a.m.**;

After Hours Barking Dog Complaints

- iii. After hours, the public can phone SACA and leave a message. The on-call staff member checks for messages regularly but only responds if it is an emergency (i.e. dangerous animal at large or injured animal). They do not respond to barking dog complaints after hours;
- iv. Barking dog complaints are documented and followed up the next day;
- v. SACA currently requires documentation of nuisance barking (barking log) prior to officers attending an address where the animal is allegedly barking; and
- vi. SPS enforces Noise Bylaw No. 8244 and can be contacted at any time to respond to barking dog complaints.

Report

After Hour Calls to SPS

Table 1, shown below, illustrates the reported number of after hour barking dog complaint calls to SPS from January 1, 2012, to August 31, 2015 (8 p.m. to 8 a.m.).

Table 1

Year	Total Dog Complaint Calls to SPS	Dog Complaints Average Calls per Month	Barking Dog Calls After Hours	Barking Dog Average After Hour Calls per Month
2012	93	7.7	9	<1.0
2013	97	8.0	20	<2.0
2014	70	5.8	11	<1.0
2015	49	6.1	7	<1.0
Totals	309	6.9	47	<1.3

A total number of 47 after hour calls between 8 p.m. and 8 a.m. were received by SPS between January 1, 2012, and August 31, 2015. This represents, on average, less than two calls per month. The number of calls for 2015 is on track to be the same volume as in the previous three years.

Extending SACA Hours

To increase the hours of operation to respond to after hour barking dog complaints would require SACA to schedule additional animal control officers. To provide an additional 60 hours of coverage each month will cost approximately \$3,200. To provide this additional coverage during the peak periods from May 1 to October 31, from 10 p.m. to 12 p.m., will cost approximately \$19,000. Additional coverage during the winter months (November 1 to April 30), from 8 p.m. to 10 p.m., will cost approximately \$19,000. The Recreation and Sport Division Animal Services operating budget would require additional funding to provide this service level increase. At this time, the volume

After Hours Barking Dog Complaints

of barking dog complaints after hours may not warrant the cost to increase this service level.

Financial Implications

An increase in the service level to provide an additional two hours per day for a total of 60 hours per month is estimated at \$3,200 per month. There are insufficient funds in the 2015 Animal Services operating budget to cover the costs associated with the increase in service level. The 2016 Operating Budget has not included any additional funding to provide an increase in service.

Options to the Recommendation

The Committee may consider the option to recommend that City Council consider an increase to the 2016 Animal Services operating budget to include an additional \$19,000 to respond to after hours barking dog complaints during peak periods. The Administration is of the view that the low volume of after hour complaints may not warrant an increase to the 2016 Animal Services operating budget.

Other Considerations/Implications

There are no policy, environmental, privacy, or CPTED implications or considerations. No communication plan or stakeholder involvement is required.

Due Date for Follow-up and/or Project Completion

No further follow-up is required at this time.

Public Notice

Public notice, pursuant to Section 3 of Public Notice Policy No. C01-021, is not required.

Report Approval

Written by: Chelsie E. Schafer, Open Space Consultant, Recreation and Sport
Reviewed by: Andrew Roberts, Acting Director of Recreation and Sport
Approved by: Randy Grauer, General Manager, Community Services Department

S:\Reports\RS\2015\PDCS – After Hours Barking Dog Complaints\ks
BF 044-15



STANDING POLICY COMMITTEE ON PLANNING, DEVELOPMENT AND COMMUNITY SERVICES

Meewasin Valley Authority Northeast Swale Master Plan

Recommendation of the Committee

1. That the Meewasin Northeast Swale Master Plan be approved, in principle;
2. That the Administration be directed to work with the Meewasin Valley Authority to develop a funding strategy and communication plan, consider potential regulations, and assist with additional implementation planning, reporting back at the appropriate time; and
3. That the City Solicitor be directed to amend the "Master Agreement" (2007) between the City of Saskatoon and the Meewasin Valley Authority to include the Northeast Swale of this report to City Council, for review and approval.

History

At the October 5, 2015 meeting of the Standing Policy Committee on Planning, Development and Community Services, a report of the General Manager, Community Services Department, dated October 5, 2015, was considered.

Your Committee heard from various speakers regarding the Master Plan and highlighting further environmental measures needed to protect the swale, wildlife movements and plant species within the swale.

Attachments

October 5, 2015 report of the General Manager, Community Services Department.
Communications from:

- Candace Savage, dated October 1, 2015;
- Jan Norris, dated October 1, 2015;
- Rick Huziak and Louise Jones, Swale Watchers, dated October 1, 2015;
- Andrew McKinlay, Axon Development Corporation, dated October 1, 2015; and
- Joanne Blythe, dated October 4, 2015
- Honor Keever, dated October 5, 2015

Meewasin Valley Authority Northeast Swale Master Plan

Recommendation

That the Standing Policy Committee on Planning, Development and Community Services recommend to City Council:

1. That the Meewasin Northeast Swale Master Plan be approved, in principle;
2. That the Administration be directed to work with the Meewasin Valley Authority to develop a funding strategy and communication plan and assist with additional implementation planning, reporting back at the appropriate time; and
3. That the City Solicitor be directed to amend the “Master Agreement” (2007) between the City of Saskatoon and the Meewasin Valley Authority to include the Northeast Swale.

Topic and Purpose

This report presents the Meewasin Valley Authority’s (Meewasin) Northeast Swale Master Plan (Plan) for approval, in principle, by City Council. Following approval, the Administration will work with Meewasin on the necessary agreements, implementation plans, and funding strategies.

Report Highlights

1. Meewasin has prepared a Plan for the Northeast Swale (Swale) within city limits to support its objectives of conservation, education, and passive recreation.
2. The Plan includes a significant number of proposed amenities: the Greenway (trail and transition area adjacent to urban development), trails, boardwalks, parking, facilities (benches, picnic areas, waste receptacles, etc.), signage, access control, and lighting.
3. Development of the Plan has been a collaborative effort between Meewasin, the City of Saskatoon (City), key stakeholders, and the public.
4. Meewasin has estimated the total capital cost to implement the Plan at \$14 million, split between the swale area proper (\$6.1 million) and the Greenway (\$8.0 million). An implementation plan and funding strategy will be developed that will also consider annual operating impact (estimated at \$186,000 for the City and \$192,500 for Meewasin).

Strategic Goals

Meewasin’s vision document, Naturally Beautiful, Uniquely Ours: A Vision for the Meewasin Valley 2014-2024, aligns very closely with the City’s Strategic Plan 2013-2023. By aligning the Plan with these strategy/vision documents, Meewasin has worked to ensure that both its and the City’s goals are achieved through the conservation and appropriate development of the Swale.

Meewasin Valley Authority Northeast Swale Master Plan

In particular, the Plan supports the Strategic Goals of Environmental Leadership, Quality of Life, and Sustainable Growth by protecting, enhancing, and providing access to an important natural asset. The Plan describes in detail how it supports the City's Strategic Plan 2013-2023 (see Attachment 1, Page 12).

Background

The Swale is a significant natural feature in the northeast quadrant of Meewasin's Conservation Zone (Zone) that has been protected from development by the City (through the University Heights Sector Plan) since 2007.

At its October 7, 2013 meeting, City Council approved the amended University Heights Sector Plan which updated the Northeast Swale Development Guidelines and established the parameters for development adjacent to and within the Swale. The Northeast Swale Development Guidelines were also adopted by Meewasin.

At its June 9, 2015 meeting, the Meewasin Board approved an updated Northeast Policy, which had previously been endorsed by City Council. In addition to providing guidance to Meewasin's decision making and planning in the northeast quadrant of its Zone, this policy contains a recommendation for the development of a "Master Plan" to address recreation, education, and interpretation in the Swale. It also includes a recommendation for the future extension of the Zone to include the whole Swale within the city limits.

Report

The Plan

Meewasin has prepared a Plan for the conservation and sensitive development of the Swale within the city. This comprises approximately 300 hectares, all owned by the City, most of which is in the Zone. The City and Meewasin are working to extend Meewasin's jurisdiction to include the entire Swale within the city.

Meewasin and the City acknowledge that increased human activity and existing and planned urban growth within the University Heights development area will have long-term impacts on the Swale. This includes neighbourhood development (Evergreen, Aspen Ridge, and the unnamed "University Heights 3"), roadway development (North Commuter Parkway and the provincial perimeter highway), and infrastructure (the stormwater management within the Swale). The Plan and a complementary Northeast Swale Resource Management Plan (2013) provide the necessary guidance for the mitigation of these impacts in support of conservation, while also facilitating appropriate access and use of the Swale for education and passive recreation.

Plan Components

The Plan provides a conceptual design for the physical space within the Swale including pathways and amenities. For the purposes of resource management and site planning, the area within the Swale has been divided into four zones, each with a specific use and function (see Attachment 1, Figure A-8):

Meewasin Valley Authority Northeast Swale Master Plan

- Greenway;
- Ecological Core;
- Stormwater Ponds; and
- Recreation Zone.

The Plan proposes a concept for the design of trails and amenities for each of these areas in order to meet its intended function. Further, the Plan proposes that certain uses, such as biking, picnicking, and on-leash dogs be restricted to specific locations in order to minimize impacts on important ecological areas. Other uses, such as motorized vehicles, and off-leash dogs are proposed to be prohibited within the Swale.

Proposed amenities include:

- pathway lighting;
- over 16 km of trails;
- boardwalks;
- wildlife-friendly fencing;
- benches;
- waste receptacles;
- wayfinding maps;
- picnic tables and shelters;
- interpretive signage;
- parking;
- washrooms; and
- outdoor education staging area.

Implementation

The Plan is intended to provide a conceptual design and recommendations to guide the development of the Swale. As development proceeds, more detailed design will be required. The Plan describes in detail the next steps, following Meewasin Board adoption of the Plan (see Attachment 1; Pages 46-47). As a partner in Meewasin and the landowner of the Swale, the City will be actively involved in many of these activities.

Upon City Council's approval in principle of the Plan, as recommended in this report, the City should begin the process to add the Swale to the maintenance agreement. This agreement outlines responsibilities of Meewasin and the City to build and maintain park space, trails, and facilities in Meewasin jurisdiction.

Options to the Recommendation

The option exists to not approve the Plan. This option is not recommended as it would lead to significant uncertainty for the future conservation and use of the Swale. There are few options for alternative uses of the area and none that would be consistent with the City's Strategic Goals and Meewasin's mandate. In the absence of a plan, the City and Meewasin will be challenged to manage the Swale and mitigate the impacts of increased human use and development in the area.

Public and/or Stakeholder Involvement

Development of the Plan has been a collaborative effort between Meewasin, the City, key stakeholders, and the public. In early 2014, an internal Meewasin project team was established, along with two committees – Steering, and Technical Advisory – comprised of Meewasin and City representatives. A design charrette (charette) initiated the design process by bringing over 45 experts, key stakeholders, and students together to work in a group format generating design concepts and ideas. Following the charrette, design options were refined, and a report was developed. Consultations with the project team, committees, public, and key stakeholders have been ongoing throughout this process. A list of stakeholders can be found in Attachment 1, Pages 4-5.

Communication Plan

As part of the Plan, Meewasin has developed a communications plan framework (see Attachment 1, Pages 44-45). It highlights several important considerations for the future development of a communication plan. City participation in the development of a communication plan will be required.

Policy Implications

The conservation and development of the Swale, as proposed in the Plan, is consistent with Official Community Plan Bylaw No. 8769 and Wetland Policy No.C09-041.

Financial Implications

There are no financial implications for 2016. Meewasin has estimated the total capital cost to implement the Plan at \$14 million, split between the Swale area proper (\$6.1 million) and the Greenway (\$8.0 million) (see Attachment 1, Figure B-6; note that the total estimate does not include the Outdoor Education Staging Area). Meewasin estimates its operating costs at approximately \$192,500 per year, averaged over the first five years. The City has estimated its annual operating costs at \$186,000 per year, upon full implementation of the Plan (see Attachment 2, all estimates in 2015 dollars). Annual operating costs for the City will be lower initially as the Plan will be implemented in phases. A funding strategy, addressing both capital and operating costs, will be developed as part of the next stage of implementation. Currently, all costs (City and Meewasin) do not have a funding source identified.

Environmental Implications

The Plan is expected to mitigate anticipated impacts from urban development adjacent to the Swale. It also has positive implications associated with the quality of storm water and wetlands.

The Plan is expected to have positive greenhouse gas emission implications. It will ensure that the Swale is protected and continues to function as a natural carbon sink. The Plan will also have a positive impact on the Swale by providing the general public with information on the value of conserving important natural areas.

Safety/Crime Prevention Through Environmental Design (CPTED)

A CPTED Review of the plans for the Northeast Swale Master Plan was undertaken by the CPTED Review Committee on March 5, 2015. The Plan addresses all of the CPTED Review Committee's recommendations that can be at this stage of the process. The remaining recommendations relate to later stages of development and the ongoing operation and management of the Swale.

Other Considerations/Implications

There are no privacy implications.

Due Date for Follow-up and/or Project Completion

Depending on funding, Meewasin reports that construction for Phase 1 is expected to start with fencing as early as 2016. Construction of trails, etc. could start in 2017. An implementation plan and funding strategy will be brought forward in 2016

Public Notice

Public notice, pursuant to Section 3 of Public Notice Policy No. C01-021, is not required.

Attachments

1. Meewasin Northeast Swale Master Plan
2. Northeast Swale Estimated Operating Impact

Report Approval

Written by: Christopher Schulz, Senior Planner II
Reviewed by: Alan Wallace, Director of Planning and Development
Approved by: Randy Grauer, General Manager, Community Services Department

S/Reports/CP/2015/PDCS – MVA Northeast Sale Master Plan/gs

Meewasin Northeast Swale Master Plan

August 5, 2015



A prized natural feature within Saskatoon’s urban landscape: a refuge for wildlife and people alike, where disturbance mimics natural patterns, biodiversity is high, native species flourish and residents or visitors of Saskatoon can learn from and enjoy wilderness right outside their own backyard.

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Photo: Shannon Dyck (Northeast Swale - Nature's swimming hole, 2013).

Quote: the *Northeast Swale Resource Management Plan* (Meewasin Valley Authority, 2013, p. iv).

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Executive Summary

Extending north from Peturrson's Ravine in Saskatoon to the Rural Municipality of Aberdeen, the Meewasin Northeast Swale is a channel scar that is approximately 26 kilometres long and covers 2,800 hectares. It is the result of the post-glacial South Saskatchewan River carving one of many paths weaving through the landscape over 15,000 years ago, evolving as the waters subsided into a unique geological, hydrological and ecological area. This distinctive and rugged landscape has existed for many thousands of years and remained relatively intact in that time. It provides significant ecological services as a diverse, connected network of rare native prairie, riparian areas and wetlands, all within reach of the Saskatoon area.

The Swale is unique, not only locally, but within the context of the greater prairie region. It has intrinsic value as a natural system, as urban ecological infrastructure and as a significant neighbourhood, city and provincial amenity. Ongoing protection, selective development, resource management, education and conservation will support and enhance these values.

From a jurisdiction perspective, the Meewasin Northeast Swale consists of two parts: lands within the City of Saskatoon (the Swale), and lands within the Rural Municipality of Corman Park and Aberdeen (the Greater Swale). The Swale, within Saskatoon city limits (primarily owned by the City of Saskatoon), is approximately 300 hectares in size and 5 kilometres in length (roughly 10% of the Greater Swale area).

The **Meewasin Northeast Swale Master Plan** was developed for the Swale within the City of Saskatoon, to support and foster **Conservation, Education, and passive Recreation**. It aligns with the Strategic Plans of both the Meewasin Valley Authority and the City of Saskatoon (Meewasin Valley Authority, 2014; City of Saskatoon, 2013), and it builds on important previous work including the *Northeast Swale Resource Management Plan* (Meewasin Valley Authority, 2013) and the *Northeast Swale Development Guidelines (2012)* (Stantec Consulting Ltd., 2012).

Human activity and urban growth within city limits will have long term impacts on the Swale. Currently this activity includes the planning and development of residential neighbourhoods (Evergreen, Aspen Ridge and University Heights 3), the City's North Commuter Parkway and the Provincial Perimeter Highway. The Swale is also a significant piece of ecological urban infrastructure, providing a site for stormwater management systems for adjacent developments. Given all of these impacts, comprehensive planning and ongoing resource management are necessary to conserve this valuable resource.

The design ideas and recommendations within this Master Plan are intended to be conceptual, and will require further detailed design as development proceeds. It will be the basis for decision making and provide direction relating to the ongoing development of the Swale and adjacent lands. The Master Plan's design ideas and recommendations may also be applied to similar sites such as the adjacent Small Swale and the Greater Swale.

The Master Plan process began in early 2014 with the establishment of an internal project team and two committees: Steering and Technical Advisory. A Design Charrette initiated the design process by bringing over 45 experts, key stakeholders and students together to work in a group

format, generating design concepts and ideas. Following the Charrette, design options were refined, and a report was developed. Consultations with the project team, committees, public and key stakeholders have been ongoing throughout this process.

The previously completed *Northeast Swale Resource Management Plan* (Meewasin Valley Authority, 2013) divided the site into a number of zones that provided a base layer of information for the design. These four zones include the Ecological Core, Recreational Zone, Greenway and Stormwater Pond.

A design program was developed that identifies the intended outcomes of the Master Plan. These have guided the development of the Plan and include key ideas such as **conserving biodiversity** (including the accommodation of ongoing resource management), **supporting passive recreation activities**, **accommodating educational programming**, **interpreting cultural and natural history** and **supporting a communications plan**. Other critical design considerations include minimizing site fragmentation, accommodating wildlife corridors and connectivity, maintaining nocturnal light levels, providing for pedestrian connections to adjacent areas, and providing for public safety, comfort and accessibility.

To meet the goals of the design program, proposed amenities include: the Greenway, trails, boardwalks, parking, facilities and amenities, signage, fencing, access control and lighting. Proposed amenities include over 16 kilometres of trails (the Greenway and internal trails), interpretive features, over 7 kilometres of fencing and an Outdoor Education Staging Area.

The cost opinion to implement the Master Plan totals approximately \$6.1 million dollars for work within the Swale or \$20,260 per hectare (\$8,213 per acre), not including the Outdoor Education Staging Area or the Greenway. There is an additional option for post construction, one year maintenance totaling approximately \$250,000. The cost to develop the Greenway totals approximately \$8 million dollars. The Resource Management and Communication Plan costs, based on a five year work plan, total approximately \$778,000 and \$75,000 respectively. The Education Plan costs are approximately \$110,000. These totals are based on 2014 dollars, without an inflation factor.

Ongoing collaboration with Meewasin's funding partners will be necessary to determine a sound funding strategy for implementation, operation and life cycle replacement. The completed Master Plan provides baseline costing information and proposes a number of sequential development phases with an unknown overall timeframe (the work would be synchronized with adjacent development). The cost opinion for each phase is provided as well as data that will assist in determining ongoing operating costs.

The work completed to date has been collaborative, with significant investments in time and consultation from Meewasin, the City of Saskatoon, key stakeholders and the public. This collaborative approach must continue to ensure the successful implementation of the Master Plan and conservation of this valuable resource. Once endorsed by the Meewasin Valley Authority and the City of Saskatoon, this document will be a mutually accepted plan that outlines the framework for ongoing planning and development in the Swale.

Background

The Meewasin Northeast Swale (the Swale) is unique in that it exemplifies the prairies. Native grasslands are one of the most imperiled ecosystems on the planet (Gauthier & Riemer, 2003), where only 20% of native prairie remains in Saskatchewan (Bailey, McCartney, & Schellenberg, 2010, p. iv). The Swale represents the Saskatchewan prairie landscape that is now considered endangered (Trottier, 1992).



Figure 1
Meewasin Northeast Swale (City of Saskatoon, 2014)

Value of the Meewasin Northeast Swale

The Swale holds significant value locally and regionally, as a relatively intact natural system. It has intrinsic value as a natural system, as urban ecological infrastructure and as a significant neighbourhood, city and provincial amenity. Protection, selective development, resource management, education programming and conservation will support these values over time.

From a value perspective, the Swale provides the following:

- Conservation of a remnant prairie grassland ecosystem with high levels of biodiversity and natural capital;
- A carbon sink, helping with greenhouse gas emissions;

- Exposure to historic, culturally and environmentally significant areas that provide a 'sense of place' that is unique to Saskatoon and the region. These benefits help make Saskatoon a more desirable region to live and work;
- Increased property values adjacent to the Swale, and therefore increased tax based revenue. The relationship between access and proximity to open space and property value has been studied extensively, and is referred to as the Proximate Principle. Research indicates that there may be up to a 25% increase in property values adjacent to open spaces, depending on a number of variables (and sometimes higher in certain circumstances) (Alberta Real Estate Foundation, 2007);
- Surface stormwater utility management;
- Active transportation corridors and routes;
- Contributing factor to healthy lifestyles, improved health and well-being;
- A tourist attraction;
- Outdoor and environmental education and research opportunities (allowing new and existing partnerships to flourish); and
- Valuable and accessible passive recreation opportunities within easy reach of numerous neighbourhoods.

Project Objective

The objective of the *Meewasin Northeast Swale Master Plan* is to design the physical space, including pathways and amenities in the Meewasin Northeast Swale (the Swale), within the city limits such that it supports Conservation (resource management), Education and Development (passive recreational uses) consistent with *The Meewasin Valley Project* (Raymond Moriyama Architects and Planners, 1978), *An Inventory of Natural Areas Remaining in the Vicinity of Saskatoon* (Johnson and Weichel, 1992), the *Vegetation and Wildlife Survey of the Northeast Swale Near Saskatoon* (Meewasin Valley Authority, 2001), the *Northeast Swale Development Guidelines (2012)* (Stantec Consulting Ltd., 2012), the *Northeast Swale Resource Management Plan* (Meewasin Valley Authority, 2013), the *University Heights Sector Plan* (City of Saskatoon, 2013), Meewasin's Strategic Plan, *A World-Class Corridor, a Vision for the Meewasin Valley 2014-2024* (Meewasin Valley Authority, 2014) and the *Northeast Policy* (Meewasin Valley Authority, 2015). The Master Plan also aligns with the City of Saskatoon's *Strategic Plan 2013 – 2023* (City of Saskatoon, 2013), specifically with the goals of Environmental Leadership, Sustainable Growth, Moving Around and Quality of Life.

As a Master Plan, all design ideas and recommendations are conceptual, and will require further detailed design as development proceeds. The Master Plan will be the basis for decision making and provide direction relating to the ongoing development of the Swale and adjacent lands. The Master Plan's design ideas and recommendations have been developed with the ability to be scaled and applied to similar sites such as the adjacent Small Swale and the greater Swale, currently outside of city limits.

The Meewasin Valley Project – 100 Year Conceptual Master Plan (1978)

The Meewasin Valley Authority's (Meewasin) foundation document is *The Meewasin Valley Project* (Raymond Moriyama Architects and Planners, 1978) and guides Meewasin's overall development plan today. Meewasin initiatives strive toward the outcomes of health, fit, balance and vibrancy. The Swale is identified as a natural area and corridor in the 100 year plan.

An Inventory of Natural Areas Remaining in the Vicinity of Saskatoon (1992)

An Inventory of Natural Areas Remaining in the Vicinity of Saskatoon (Johnson and Weichel, 1992) was a report which compiled the remaining natural areas of priority in Saskatoon and area for land planning and management decisions. The Meewasin Northeast Swale was identified in this report as a priority for protection. The Swale was also recognized as the "largest natural area in the vicinity of the city" (Johnson and Weichel, 1992, p. 10).

Vegetation and Wildlife Survey of the Northeast Swale Near Saskatoon (2001)

The *Vegetation and Wildlife Survey of the Northeast Swale Near Saskatoon* (Meewasin Valley Authority, 2001) was one of the first opportunities Meewasin had to gather a detailed survey of species in the Meewasin Northeast Swale, and surrounding area. This survey captured large numbers of plants and bird species (206 plant species and 181 bird species) in addition to a high relative cover of fescue grass (13%) (Meewasin Valley Authority, 2001, p. 2).

The Northeast Swale Development Guidelines (2012)

The *Northeast Swale Development Guidelines (2012)* (Stantec Consulting Ltd., 2012, p. 1.1) provides direction for the minimization of disturbance to the Swale, while also meeting transportation, utility, stormwater management and other community needs. The document replaces the original 2002 guidelines as it contains additional details on the Swale boundary, road crossings and stormwater management, and introduces the concept of the Greenway.

The Northeast Swale Resource Management Plan (2013)

The *Northeast Swale Resource Management Plan* (Meewasin Valley Authority, 2013) envisions the site as a prized natural feature within Saskatoon's urban landscape: a refuge for wildlife and people alike, where disturbance mimics natural patterns, biodiversity is high, native species flourish, and residents or visitors of Saskatoon can learn from and enjoy the wilderness right outside their own backyard. The plan outlines a management approach to protect and restore biodiversity, unique landscape characteristics and heritage features inherent in the Swale. It prescribes burning, grazing, invasive species control and an adaptive management approach.

The *Northeast Swale Resource Management Plan* also recommends that Meewasin develop a Recreation Plan for the Swale that:

- Integrates landscape planning for the Swale;
- Ensures passive recreation uses only (e.g. no off-leash dogs and on-leash dogs limited to certain areas);
- Considers varying intensity and activity types for different zones in the Swale, including the Greenway, Ecological Core, Recreation Zone and Stormwater Ponds; and
- Incorporates interpretive opportunities for both individuals and groups.

University Heights Sector Plan (2013)

The *University Heights Sector Plan* (City of Saskatoon, 2013) established a new Suburban Development Area for Saskatoon, established a layout for preparation of future Neighbourhood Concept Plans and identified key land uses, transportation and servicing. This plan has direct implications for the Meewasin Northeast Swale as it falls within the Sector boundaries. Considerations such as transportation, stormwater management and alternative transportation corridors affect the Swale.

The Meewasin Valley Authority's Strategic Plan, A World-Class Corridor, a Vision for the Meewasin Valley 2014-2024

A World-Class Corridor, a Vision for the Meewasin Valley 2014-2024 (Meewasin Valley Authority, 2014) is a document that will guide the work of Meewasin to 2024. The document presents a vision for the Meewasin Valley articulated through 3 major themes:

- Healthy and Vibrant;
- Sense of Community; and
- Protecting the Legacy.

The Meewasin Valley is a unique and world-class feature that must be protected and supported. This document details the actions required to achieve the goals set and identifies key directions necessary to achieve the vision and respond to growth. The Master Plan aligns with the Strategic Plan in the following areas:

- Healthy and Vibrant
 - A healthy, natural river corridor supporting the wellbeing of present and future generations (2014, p. 8);
 - Extend the Meewasin Trail and amenities to better serve a community that is growing quickly (2014, p. 8); and
 - Promote participation and appreciation by planning for and supporting activities and programs in all seasons (2014, p. 8).
- Sense of Community
 - Foster community participation and meaningful contribution to conservation and development projects (2014, p. 12).
- Protecting the Legacy
 - Renew the Meewasin Valley Interpretive Plan with an aim to enhance experiential learning opportunities (2014, p. 16).

The Northeast Policy (2015)

The Meewasin Valley Authority adopted a Northeast Policy as part of the Development Plan in 1987 to guide its decision making and planning in the northeast quadrant. At that time the available information on the Meewasin Northeast Swale was limited and the policy recommended further research. A fully revised *Northeast Policy* was adopted by the Authority in 2015 in conjunction with the City of Saskatoon and University of Saskatchewan (Meewasin Valley Authority, 2015). Based on research during the intervening years, the Meewasin Northeast Swale was identified as a key natural area in the quadrant. The policy recommended the development of a Master Plan to address recreation, education and interpretation.

The City of Saskatoon's Strategic Plan 2013 - 2023

The *Strategic Plan 2013 – 2023* (City of Saskatoon, 2013) for the City of Saskatoon contains priorities that align with the Master Plan as follows:

- Environmental Leadership
 - Supporting biodiversity and the Meewasin Valley (2013, p. 31);
 - Committing to healthy and growing urban and grassland parks which are natural assets to be protected, enhanced and linked (2013, p. 31);
 - Improving the quality and reducing quantity of stormwater run-off entering the river (2013, p. 32);
 - Improving access to ecological systems and spaces, both natural and naturalized (2013, p. 32); and
 - Communicating the financial benefit of environmental initiatives (2013, p. 32).
- Sustainable Growth
 - Planning collaboratively with partners and stakeholders (2013, p. 36); and
 - Measuring residents' perception of the quality of their neighbourhood (2013, p. 37).
- Moving Around
 - Working on the North Commuter Parkway Project (2013, p. 40); and
 - Measuring length and quality of walking and cycling network (2013, p. 41).
- Quality of Life
 - Providing access to citizens to facilities and programs which promote active living, and enjoying the natural beauty and benefits of parks, trails and the Meewasin Valley which brings people together (2013, p. 27);
 - Finding new and creative ways to showcase Saskatoon's natural and cultural heritage (2013, p. 27);
 - Working collaboratively through conversation to move forward as a community (2013, p. 27);
 - Ensuring existing and future recreational facilities are physically and financially accessible and meet community needs (2013, p. 28);
 - Providing opportunities for activities in a winter city (2013, p. 28); and
 - Measuring public perceptions of quality of life (2013, p. 29).

Planning Principles

Planning undertaken by Meewasin is guided by five fundamental planning principles:

- Valley's resources are accessible to everyone;
- Conserve natural and heritage resources;
- Recreation and development balanced with conservation;
- Diverse activities for a varied and changing demographic; and
- Public participation in decision making.

Process

The *Northeast Swale Resource Management Plan* recommended plans for a Recreation, Education and Interpretation as well as a Communications Strategy for the Swale (Meewasin Valley Authority, 2013, p. viii). *A World-Class Corridor, a Vision for the Meewasin Valley 2014-*

2024 refers to the development of an interpretive plan for the Meewasin Northeast Swale, coordinated with trail planning (Meewasin Valley Authority, 2014, p. 19). This prompted work to begin on the Master Plan with the intention of incorporating recreation, education, interpretation and communications. Meewasin created an internal project team to draft a Terms of Reference for the Master Plan. This included establishing a Steering Committee and a Technical Advisory Committee with the Terms of Reference describing the duties and expectations of the project team and each of the Committees (see Appendix C).

Design work was initiated with a Design Charrette held in April 2014, which provided a one-day interdisciplinary, creative design exercise to kick-start the process. It involved over 45 experts, key stakeholders and students. The full design charrette report, *Design Charrette Northeast Swale Master Plan (2014)* can be found on Meewasin's website.

Following the Design Charrette, options were developed and refined and the draft report was compiled. This work has been guided by the Steering Committee and the Technical Advisory Committee throughout 2014 and 2015. The draft concept plan was released in fall 2014 and Meewasin hosted an open house in October 2014 to obtain feedback and input from members of the public.

Prior to release of the final report, additional consultation included meetings with groups, such as the Swale Watchers and Wanuskewin Heritage Park and meetings within the City of Saskatoon including their committees: Standing Policy Committee on Planning, Development and Community Services, Municipal Heritage Advisory Committee (MHAC), Saskatoon Environmental Advisory Committee (SEAC) and Crime Prevention Through Environmental Design (CPTED). Meewasin advisory committees (Development Review, Conservation, Education, and Design) have also reviewed the Master Plan.

After completion, the report must be presented to the Meewasin Valley Authority Board of Directors and City of Saskatoon City Council for approval.

Site Description

The Swale is a channel scar, the result of an ancient river carving a path on the landscape, creating unique geological, hydrological and ecological conditions. The wetlands within the Swale are sustained by the Forestry Farm aquifer, where the ground water is not really affected by surface topography (BBT Geotechnical Consultants Ltd., 1986; Meewasin Valley Authority, 2013, p. 10) . The water table for this site is high and defined as a zone of unserviceable land (Stantec Consulting Ltd., 2012; Meewasin Valley Authority, 2013, p. 10).

The Swale has two distinct parts: lands within the City of Saskatoon and lands within the Rural Municipality of Corman Park and Aberdeen (i.e. greater Swale). See Appendix A Figure A-1 for a map of the estimated Swale extent. The estimated extent of the complete Swale is approximately 2,800 hectares (ha) and 26 kilometres (km) in length. The complete Swale begins within Saskatoon at Peturrson's Ravine and extends northeast about 26 km through the Rural

Municipality of Corman Park and reaches the South Saskatchewan River at a ravine near Columbus Bosco Homes, near Clark's Crossing and again within the Rural Municipality of Aberdeen. The scope of the Master Plan refers to the Swale within Saskatoon city limits, which is owned primarily by the City of Saskatoon. The Swale within the City of Saskatoon is approximately 300 ha and 5 km in length (roughly 10% of the greater Swale area). Unless otherwise stated, the Swale referenced throughout the Master Plan is the section located within the City of Saskatoon.



Figure 2
Meewasin Northeast Swale and adjacent neighbourhoods
(City of Saskatoon, 2014)

Location

The Swale is located in the northeastern corner of the City of Saskatoon, in the University Heights Suburban Development Area. See Appendix A Figure A-2 for a location map of the Swale. The majority of the Swale is within Meewasin's jurisdiction and Conservation Zone. See Appendix A Figure A-3 for a map illustrating the current Meewasin jurisdiction for the Swale. The *Northeast Policy (2015)* recommends that the remaining Swale lands and Small Swale lands be added to Meewasin's jurisdiction (Meewasin Valley Authority, 2015).

Ecological Importance

The Swale contains a variety of environments including steep rocky ridges, rolling prairie, lush valleys, treed areas and ephemeral and semi-permanent wetlands. These environments provide significant ecological services, and contain over 200 documented plant species, 103 avian species and a variety of mammals within the 300 ha of Swale in Saskatoon alone (Meewasin Valley Authority, 2013). Some of the rare or endangered species in the Swale include plants (Crowfoot Violet, Western Red Lily, Narrow-leaved Water Plantain, Sweet Grass), birds (Sprague's Pipit, Barn Swallow, Loggerhead Shrike, Horned Grebe, Short-eared Owl, Common Nighthawk) and amphibians (Northern Leopard Frog) (Meewasin Valley Authority, 2013). Maintaining biodiversity is integral to the health, success and future of this ecosystem.

Cultural History

There are many intact and visible heritage features found at the Swale. Remnants are still visible from the Moose Woods-Batoche Trail, which provides insight and interpretation opportunities into transportation systems and routes used at various time periods and by different groups in the area such as First Nations, Métis and early Saskatoon-era settlers

(Meewasin Valley Authority, 2013, p. 16). As a result of the glacial rivers that formed in the Swale, large limestone boulders were deposited on the site. These boulders were used for making fertilizer, mortar and to clad buildings. The Swale contains limestone quarries, lime kilns and a pinned rock, which are all evidence that the limestones found on site were being used for construction, including some early University of Saskatchewan construction projects (Meewasin Valley Authority, 2013, p. 17).

The Swale lands entered Meewasin's jurisdiction through ownership by the University of Saskatchewan. In addition to the geomorphology of the site, it is through the University's ownership that the Swale was protected from large disturbances. Ownership of the Swale has since transferred to the City of Saskatoon. The Swale is on Treaty 6 lands (Office of the Treaty Commissioner, 2015).

Adjacent Natural Areas and Connections

The Meewasin Trail runs along both sides of the Meewasin Valley and currently extends to both Crocus Prairie and Saskatoon Natural Grasslands. The proximity of these sites allows for a convenient connection to the existing Meewasin Trail system. Other potential future connections to the Meewasin Trail system can be seen on Appendix A Figure A-4, including connections to Saskatoon Natural Grasslands, Crocus Prairie, Peggy McKercher Conservation Area and the Saskatoon Forestry Farm Park & Zoo.

The Swale is part of a larger natural areas corridor in the northeast of Saskatoon. Due to suburban development, some of these natural areas have been separated, limiting connectivity. It is critical that the Swale maintains connectivity to these adjacent natural areas.

Crocus Prairie is a riverside site directly connected to Peturrson's Ravine and geographically located next to the Regional Psychiatric Centre. Crocus Prairie is part of the continuous link of core areas of native habitat with other remnant natural areas (Meewasin Valley Authority, 2013, p. 1). The Meewasin Trail runs through Crocus Prairie.

Peggy McKercher Conservation Area, previously known as Maryville, is a riverside site with cultural significance. The site used to be owned by the Episcopal Corporation of Saskatoon who converted the site to a summer residence for Catholic nuns in the Saskatoon Diocese (Meewasin Valley Authority, 2009). Peggy McKercher Conservation Area contains a section of the Meewasin Trail and provides connectivity from the Small Swale to the South Saskatchewan River.

Peturrson's Ravine connects the Swale to the South Saskatchewan River. It contains a marl bog which is a unique ecosystem due to its soil and moisture properties and unique floral community (Golder Associates Ltd., 1995, p. 19). To protect Peturrson's Ravine, it should be hydrologically isolated from potentially contaminated surface run-off from nearby subdivisions (Stantec Consulting Ltd., 2012, p. 3.14).

Riddell Paleontological Site is an important site where vertebrate fossils, ocherous wood and shells have been found. Nineteen taxa of large mammals from the late Rancholabrean time were also uncovered at this site (City of Saskatoon, 2013, p. 14). It is located north of the Swale along Central Avenue.

The Small Swale is similar to the Swale in its topography, although it is not connected to the Forestry Farm aquifer (Meewasin Valley Authority, 2013, p. 10). The Small Swale connects to the South Saskatchewan River at two locations and is composed of grasslands and wetlands (Stantec Consulting Ltd., 2013, p. 31). It is critical to restore this natural area and maintain the connection to adjacent natural areas in order to protect the biodiversity and health of this natural system. The Master Plan recommends maintaining a linear park connection and trail between the Small Swale and the Meewasin Northeast Swale.

Saskatoon Forestry Farm Park & Zoo is a National Historic Site started as a tree nursery hosting millions of saplings which now remain a cornerstone of ecology for the prairies (City of Saskatoon, 2015). The aquifer for this area is the same one which recharges the Swale. The Meewasin Trail currently connects to the Saskatoon Forestry Farm Park & Zoo.

Saskatoon Natural Grasslands aims to preserve the ecological integrity of this remnant prairie while providing a living laboratory for environmental education and interpretation (Delcan Western Ltd., 1994, p. 1). This area also provides a corridor for wildlife to the river and the Swale. The Meewasin Trail currently connects to Saskatoon Natural Grasslands.

The University of Saskatchewan Reclamation Site is an area directly south of the Swale along Central Avenue, providing habitat and connectivity to Peturrson's Ravine and the South Saskatchewan River. Although this area is not ecologically sensitive, it provides connectivity to other adjacent natural areas. This is especially important as the Swale narrows along Central Avenue and could pose a challenge for wildlife movement and connectivity.

The Master Plan

The Master Plan outlines a comprehensive site plan addressing the following elements:

- Design of the Greenway, Ecological Core, Buffer Zone and Recreation Zone;
- Greenway and trail design including pathways, site access and connectivity to adjacent land uses, adjacent neighborhoods and crossing features;
- Amenity design based on permissible uses, safety and comfort, including site furniture and signage; and
- Site design to support resource management, interpretation and programming.

The Conceptual Design can be found in Appendix A Figure A-4. A simplified version of the Conceptual Design can be found in Appendix A Figure A-5.



Figure 3
Northern Leopard Frog at the Swale (Dyck, 2013)

The Design Program

The intended outcomes of Swale development and the Site Design Program are organized based on the categories of activities that are supported. These categories include conserving biodiversity, supporting passive recreation, supporting education, interpreting natural and

cultural history and supporting a communications plan. The categories have guided the creation and development of the Master Plan. See Appendix A Figure A-6 for the Site Design Program and Appendix A Figure A-7 for a matrix evaluating the Master Plan and how it intends to meet the Design Program goals.

Site Considerations

The Swale is a unique feature. As such, it requires different treatment than a conventional or naturalized park. Specifically, this site focuses on connectivity and ecology, which requires identification and management of permissible, restricted and prohibited uses. All activities at the Swale should be passive recreation, not active recreation.

The Swale is not currently publically accessible, permission must be granted to use the site. It is not until certain phases of the Master Plan are implemented that the site will become publicly accessible. In the future, it is the goal of Meewasin that the Swale will be open for public use, but for the time being is accessible with permission.

Connectivity & Wildlife

The Swale is an important wildlife connection leading to the South Saskatchewan River. The *Northeast Swale Resource Management Plan* describes the wildlife found within the Swale that relies on this connection, including birds, mammals, amphibians, reptiles and insect (Meewasin Valley Authority, 2013). The Swale also provides physical connection to the South Saskatchewan River in three locations (Saskatoon, near Clark's Crossing and the RM of Aberdeen) and connects wildlife to surrounding natural areas and greenspace such as Saskatoon Natural Grasslands, Saskatoon Forestry Farm Park & Zoo, Peturrson's Ravine, the University of Saskatchewan Reclamation Site and Crocus Prairie. Connectivity between these areas and to the South Saskatchewan River is critical to maintain natural corridors where possible. Minimizing fragmentation must be a priority when designing features within the Swale.

Human Impacts

The Swale has remained virtually untouched as the water table is high and makes the land unserviceable. As a result, the Swale had been used to graze animals. Parts of this area have been overgrazed but have also demonstrated resiliency. The Swale's proximity to new and growing neighbourhoods has created a critical window of opportunity. Current and future suburban growth will claim land up to the edge of this resource. While recognizing that the Swale will change, human encroachment could result in unprecedented and non-reversible impacts to the site. These damaging impacts could be caused by increased noise and light levels, construction activity, illegal dumping, foot and vehicle traffic, off road recreation activities, littering, pets at large, vandalism, bonfires, theft of plants, camping, horseback riding; and other recreation activities such as model airplanes (Meewasin Valley Authority, 2013, pp. 50-51). Proactive monitoring could help mitigate or reduce the type of changes that could happen in the Swale.

Permissible Uses

Although there is a concern about some activity in the Swale, there are also a number of activities that will be supported and encouraged. The Swale contains a number of sensitive

areas and the following suggested uses will allow appreciation, education and conservation of the site:

- Walking;
- Hiking;
- Wildlife watching;
- Appreciation of flora and fauna;
- Photography;
- Appreciation of the scenic vistas;
- Inspiring interpretive site exploration opportunities for individuals and guided tours (such as school groups);
- Research; and
- Geo-caching.

Restricted Uses

Activities in the Ecological Core are subject to controlled use due to its sensitive nature. A number of activities will be permitted at the Swale, but restricted to certain areas:

- Biking
 - Permitted on primary, secondary, Greenway and boardwalk trails only.
- Cross-country skiing
 - Potential for development in Recreation Zone; and
 - Potential for guided use in Ecological Core.
- Snowshoeing
 - Potential for development in Recreation Zone; and
 - Potential for guided use in Ecological Core.
- Picnicking
 - Restricted to the Recreation Zone.
- On-leash dogs
 - Restricted to the Recreation Zone.

Prohibited Uses

It is recommended that the following uses be prohibited in all areas of the Swale:

- Off-leash dogs (Bylaw No. 7860);
- ATVs (Bylaw No. 7767);
- Snowmobiles (Bylaw No. 7767);
- Power driven vessel;
- Other motorized vehicles (except motorized devices for the mobility impaired and emergency and service vehicles) (Bylaw No. 7767);
- Picking wildflowers (Bylaw No. 7767);
- Dumping (Bylaw No. 5713); and
- Littering (Bylaw No. 7767).

With development proceeding on all sides of the Swale, there is increasing public demand for dog parks in the area. As the Swale is not suitable for off-leash use, it will be important to

determine appropriate location(s) nearby for this activity that can accommodate demand and reduce the risk of off-leash activity within the Swale.



Figure 4
Meewasin Northeast Swale (City of Saskatoon, 2011)

Design Components

This section includes a full detailed description of the design elements and amenities included within the *Meewasin Northeast Swale Master Plan*. The following components are based on the Conceptual Design, which can be found in Appendix A Figure A-4. Detailed design work will be undertaken as phased site development is initiated.

Site Zones

For the purpose of both resource management and site planning, the area within the Swale has been divided into different zones (Meewasin Valley Authority, 2013). Each zone has a different intended use and function. The four zones are the Greenway, the Ecological Core, the Stormwater Ponds and the Recreation Zone. See Appendix A Figure A-8 for a map illustrating the four zones.

The Greenway

The *Northeast Swale Development Guidelines (2012)* (Stantec Consulting Ltd., 2012) established the concept of the Greenway. It is a multipurpose corridor along the perimeter of the

Swale that includes an ecological buffer, trail zone and transition zone. It supports active transportation, stormwater management and conservation of sensitive areas (Stantec Consulting Ltd., 2012).

Location

The Greenway follows the Swale edge where possible and frames the entirety of the northern side of the Swale and the boundary of the Aspen Ridge neighbourhood on the south edge. The Greenway will serve as a transition between suburban development (typically single family residences) and the Swale. Residential backyards will back on to the Greenway.

Along Fedoruk Drive and Central Avenue, the Swale is bordered by roadways and therefore does not include the Greenway. These areas will differ from the Greenway in that they will not include all the same elements; the cycling pathways are included in the roadway development. A pedestrian pathway and ecological buffer is planned along the roadways.

Cross Section

The Greenway consists of three components: the Ecological Buffer, the Trail Zone and the Transition Zone.

- The Ecological Buffer is located next to the Swale edge and is planted with native species. It is a minimum of 15 meters wide that is widened to a 20 meter minimum if the slope towards the Swale is 5% or greater;
- The Trail Zone is located adjacent to the buffer. It is 3-4 meters wide and will feature a shared use path; and
- The Transition Zone separates adjacent land uses and supports stormwater management. It ranges from 3 – 5 meters in width; 5 meters minimum width if adjacent to residential development or used for stormwater management or 3 meters minimum width if adjacent to roads and other land uses.

Neighbourhood development may include a fence at the back of the lot line, or fencing may be installed by the homeowner as part of the yard landscaping (Saskatoon Land, 2014). See Figure 4 for a cross section of the Greenway.

Ecological Buffer

The function of the Ecological Buffer is to protect the Swale from invasive and exotic species encroachment. The *Northeast Swale Development Guidelines (2012)* (Stantec Consulting Ltd., 2012) suggests that this area should be planted with a mix of native species, and will require active, ongoing management to prevent and minimize invasive species encroachment.

The Swale side of the Ecological Buffer will be delineated with a fence to protect the Swale. This fencing will also provide access control along the perimeter of the Ecological Core. Delineating the edge of the Swale will help to signify the importance of this natural area.

Areas currently within the Ecological Buffer contain a mixture of native and exotic species. These areas will require efforts for invasive species management, seeding and/or natural

regeneration in order to control invasive species and reintroduce native species (Stantec Consulting Ltd., 2012, p. 3.4).

Trail Zone

The Trail Zone will support a shared use path for active transportation. The initial phase will include construction of a fully accessible asphalt trail. It is anticipated that this trail will be cleared for four-season use.

Transition Zone

The Transition Zone protects the Swale from encroaching invasive and exotic species from adjacent land uses. This zone has the potential to be a tool in the City of Saskatoon's stormwater management system and integrated with low impact stormwater management practises.

Suggested low impact stormwater management practices include a grass swale, bioswales, rain gardens or linear retention ponds. Vegetation in the Transition Zone should include only native vegetation, predominantly grass species.

Ecological Core

The Ecological Core is the heart of the Swale where resource management and biodiversity are the priority. The Ecological Core is intended to have controlled access and support passive recreation, research and education.

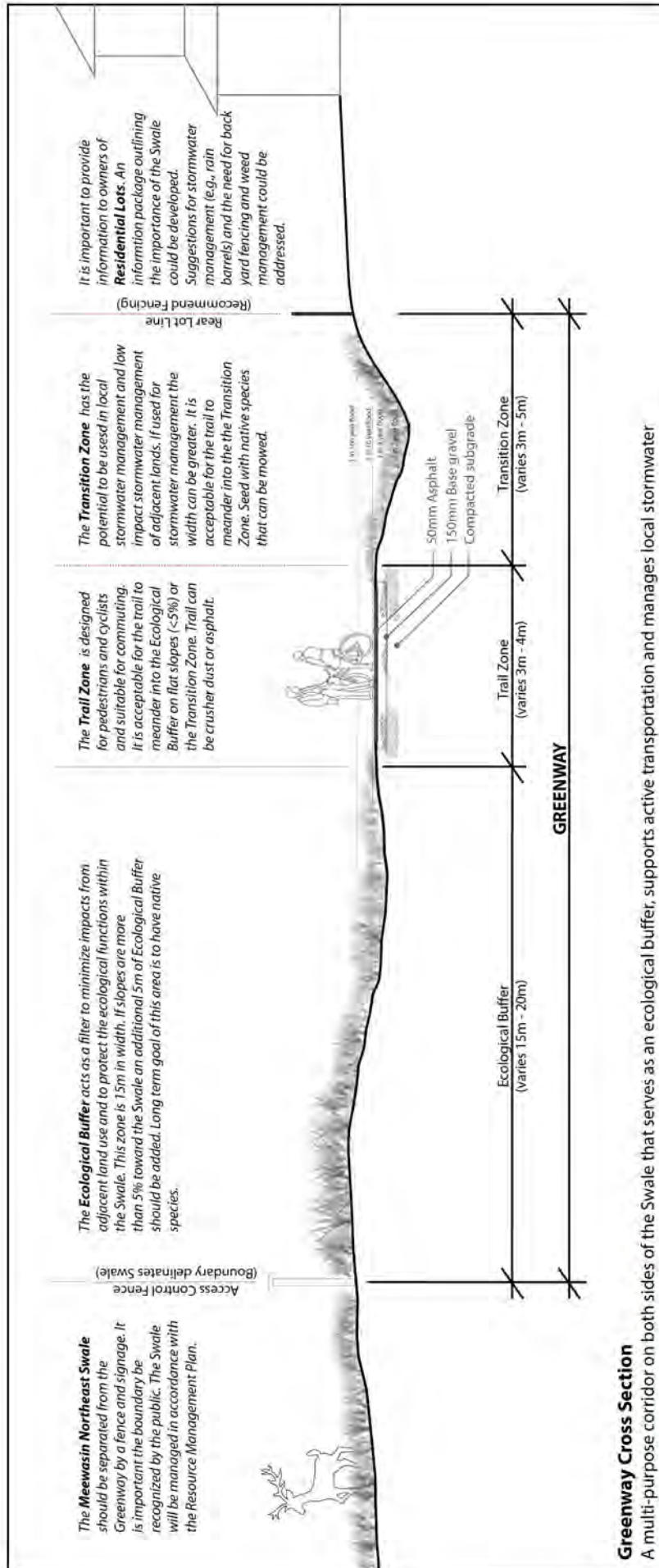
Access to this area will be controlled using fencing and access control points on its perimeter. Fencing will be used to delineate the area, manage grazing animals within the Swale and restrict prohibited access such as motorized vehicles. To encourage the use of designated trails, access points will open to secondary trails. Amenities will consist of low-impact trails whose surfaces are mowed, crusher dust, mulching or boardwalks. Resource management tools such as grazing, burning and adaptive invasive species management will be focused in this zone. In addition to active research, educational guided tours and interpretation are planned.

Stormwater Pond

The Stormwater Pond and surrounding area supports higher intensity use for recreation, education and development of trails. This area requires some restoration.

Recreation Zone

The Recreation Zone supports higher intensity use for recreation, education and development of trails. This area is adjacent to highly disturbed areas and can accommodate higher levels of human use.



Greenway Cross Section
A multi-purpose corridor on both sides of the Swale that serves as an ecological buffer, supports active transportation and manages local stormwater

Figure 5 – Greenway Cross Section

Road Alignment

The *Northeast Swale Development Guidelines (2012)* delineate the three planned road crossings through the Swale within the current City limits: the North Commuter Parkway, Central Avenue and Range Road 3050 (Stantec Consulting Ltd., 2012). In addition to these three roadways the Province also has plans to construct a controlled access highway through the Swale, referred to as the Perimeter Highway. All roadways crossing the Swale, with the exception of the Perimeter Highway, will have a posted speed limit of 50 km/h to reduce the risk of wildlife interactions (Stantec Consulting Ltd., 2012, p. 3.9).

North Commuter Parkway

The development of the North Commuter Parkway is subject to the *Northeast Swale Development Guidelines (2012)* (Stantec Consulting Ltd., 2012). The conceptual design has been approved by the Meewasin Valley Authority Board of Directors, with conditions.

Wildlife Considerations

As part of the conditions of approval, the design for the North Commuter Parkway must include culverts which will allow the natural flow between east and west, while providing consideration for small mammals, fish, reptiles and amphibian movement. Rocks, small shrubs and/or taller vegetation are recommended at the entry and exit to provide cover for the animals using the culverts (Stantec Consulting Ltd., 2012, p. 3.9). In addition, to minimize disturbance to mammals crossing the roadways, it is recommended that the culverts be designed to minimize grade changes (Stantec Consulting Ltd., 2012, p. 3.10).

Central Avenue

The widening and realignment of Central Avenue is part of the North Commuter Parkway project, and is subject to the *Northeast Swale Development Guidelines (2012)* (Stantec Consulting Ltd.).

Range Road 3050

The current north-south alignment for Range Road 3050, also known as Lowe Road, will be maintained as a potential road crossing. In accordance with the *Northeast Swale Development Guidelines (2012)* this collector roadway/utility crossing will lie along the existing centreline with a right-of-way 27 m wide and will connect Evergreen with the University Heights 3 neighbourhood (Stantec Consulting Ltd., 2012, p. 3.7).

Perimeter Highway

The location of the Perimeter Highway on the Conceptual Design (see Appendix A Figure A-4) is preliminary and subject to change. Based on consultation and review, Meewasin recommends that the alignment currently noted remain as the final configuration. Discussions between the Saskatchewan Department of Highways and Transportation, Meewasin and the City of Saskatoon are ongoing regarding this project. It is important to continue the dialogue as the Perimeter Highway progresses due to its potential impacts on the natural systems within the Swale and the Small Swale.

Decommissioned Roadways

Range Road 3045 currently runs through the Swale but will be decommissioned for vehicular access upon the construction of the North Commuter Parkway and Perimeter Highway. There is potential to use the existing road bed for trails and lookouts. The possibility of removing the roadbed and reconnecting the wetland crossed by this roadway is recommended.

Agra Road intersects Central Avenue and forms a portion of the northern boundary of the Swale. The plans for the North Commuter Parkway suggest there may be a need for a road at the intersection of Agra Road and Central Avenue, which will be determined by the City of Saskatoon as part of University Heights 3 development. Agra Road will be decommissioned if it is not necessary for this neighbourhood. Agra Road represents the boundary for the Greenway and if decommissioned, the existing infrastructure can be utilized for Greenway development.

The *Northeast Swale Development Guidelines (2012)* provides direction on decommissioning roadways in the Swale (Stantec Consulting Ltd., 2012).

Internal Trails

Secondary Trails in the Recreation Zone

Secondary trails within the Recreation Zone facilitate connections from major entry points to internal amenities such as the Outdoor Education Staging Area, picnic sites and interpretive nodes. The location of the trail in one section also follows the alignment of the historic Moose Woods-Batoche Trail. These trails will be designed to be fully accessible.

Future detailed design must consider the volume of trail users. The installation of trail counters at major entrances will assist in determining if the trail will be cleared in winter months; however, it is recommended that at least one north-south and east-west connection should be maintained during winter months.

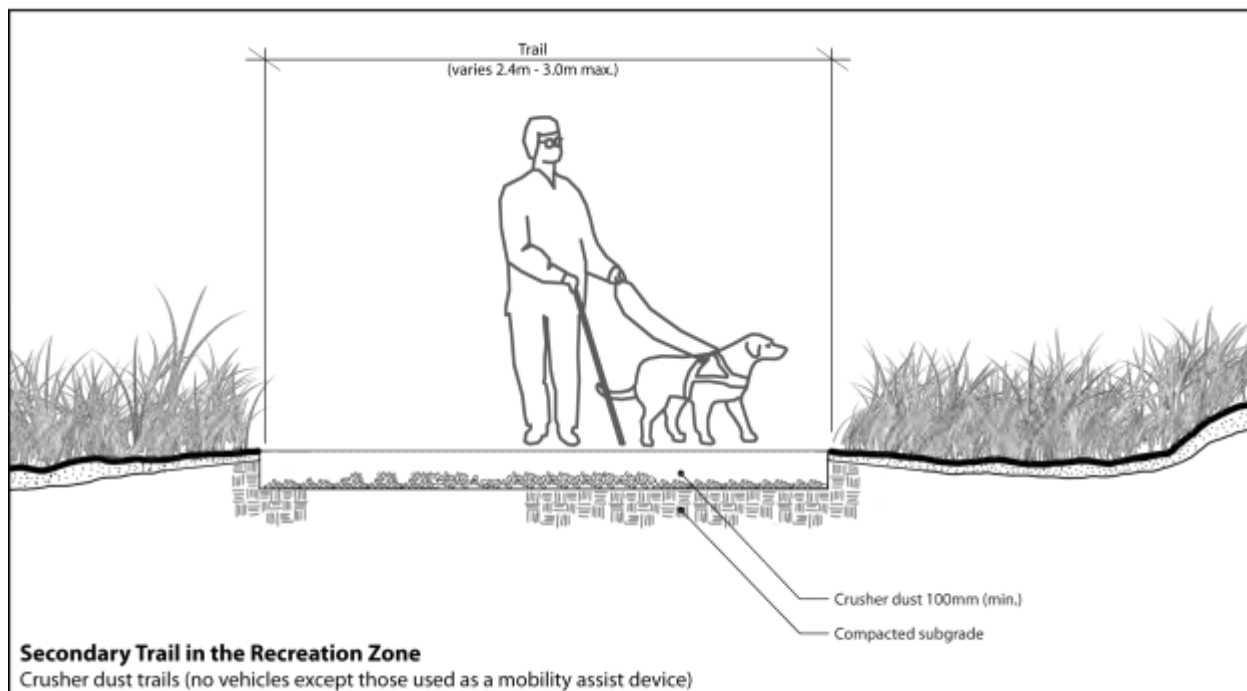


Figure 6 - Cross Section of a Secondary Trail in the Recreation Zone

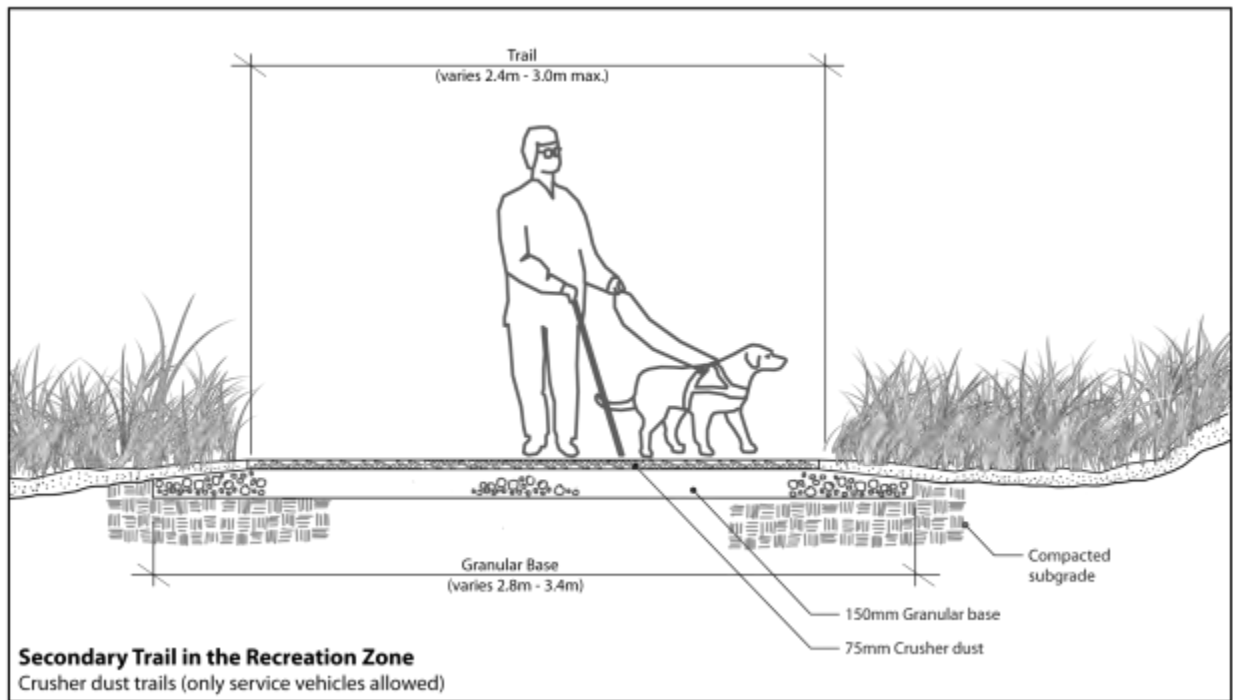


Figure 7 - Cross Section of a Secondary Trail in the Recreation Zone. Only service vehicles allowed.

Secondary Trails in the Ecological Core

The internal trail system within the Ecological Core features a trail that allows pedestrians to cross through the Swale. This trail also has a branch heading west that provides a connection to the Outdoor Education Staging Area. The alignment of this trail provides access to the major interpretive nodes in the Ecological Core.

This alignment was selected to minimize the impacts on highly sensitive and biodiverse areas, and to provide minimal grade changes for users.

This trail will be constructed on grade to minimize ground disturbances (no excavation of the existing ground). It will follow a similar model used at Saskatoon Natural Grasslands, which has performed very well and minimizes the introduction of additional invasive species to the site. The south terminus location of this trail will align with a proposed linear park connecting to the park system in Aspen Ridge, providing a strong connection back into the neighbourhood. The branch of the trail that connects west to the Outdoor Education Staging Area and Recreation Zone was aligned to make use of existing site disturbances (former farm access road).



Figure 8
Meewasin Trail - crusher dust at Crocus Prairie

There are also two trails located on the existing road bed of Range Road 3045 to facilitate interpretation and wildlife watching.

It should be determined in the future which trails will be cleared in winter months. Off-leash dogs and motorized vehicles will be prohibited and cycling will be discouraged. The primary use will be hiking and interpretive tours.

Future detailed design must consider the volume of trail users. The installation of trail counters at major entrances will assist in determining how and if the trail will be cleared in winter months.

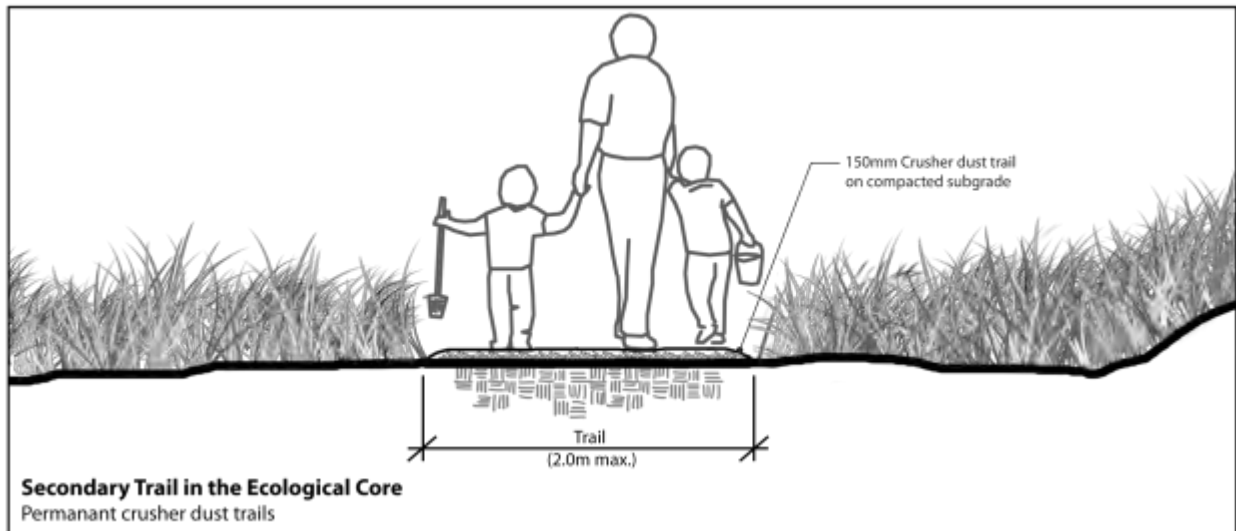


Figure 9 - Cross Section of a Secondary Trail in the Ecological Core

Tertiary Trails in the Ecological Core

The secondary crusher dust trail linkage will be supported by a tertiary trails system. Tertiary trails are mowed, semi-permanent trails and can be relocated occasionally to reduce long term impacts. The placement of these trails will depend on the resource management activities occurring in the Swale. These trails will not likely be visible in winter months.

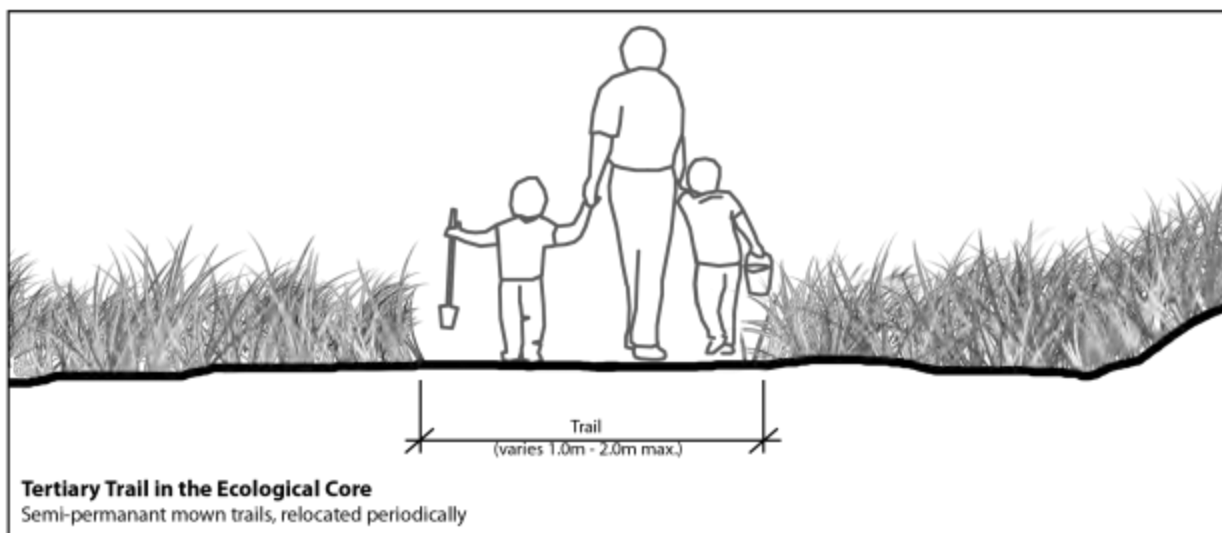


Figure 10 - Cross Section of a Tertiary Trail in the Ecological Core

Boardwalks

Boardwalks will be used to traverse wetlands or ecologically sensitive areas. The boardwalks will support bird watching activities and overall Swale viewing. A short trail and feature lookout is located off the North Commuter Parkway crossing, mid-way through the Swale. This trail will provide a vantage point to the Ecological Core, including some of the wetlands and provides an opportunity for interpretive signage.

The northern wetlands are the most active bird watching sites located in the Swale. There is potential for a boardwalk to be incorporated for wildlife viewing and pedestrian movement around the Aspen Ridge stormwater management system.



Figure 11
Example of boardwalk (Gone Hikin', 2011)

The boardwalks will not be accessible year round. Railings will be installed where necessary.

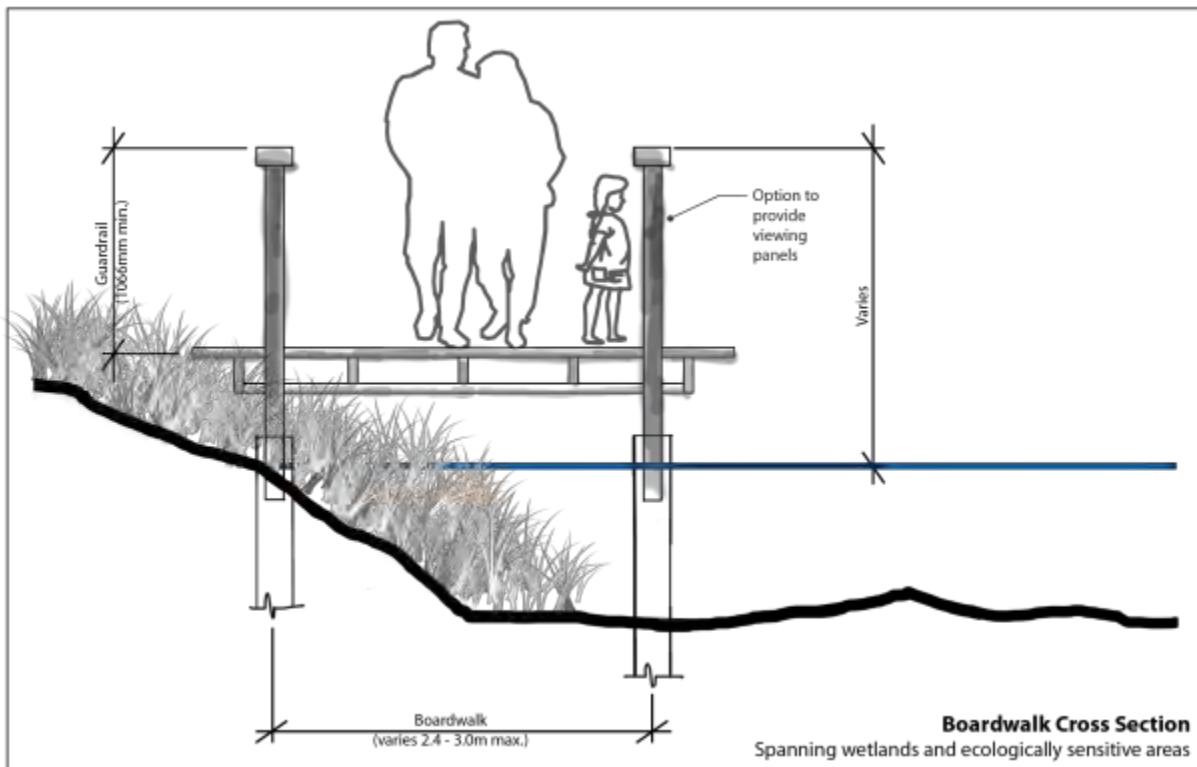


Figure 12 - Boardwalk Cross Section

External Trails

The trails outlined in this section are intended to inform future planning by identifying important linkages for shared use paths surrounding the Swale. These trails connect to surrounding natural areas and support both natural and human corridors.

Meewasin Trail

The most significant external trail linkage is to the Meewasin Trail. Currently, the existing trail terminates at Crocus Prairie, which is the south boundary of Peturrson's Ravine. The Master Plan recommends further trail development south along Central Avenue, connecting the Meewasin Trail to the Swale and continuing further north from this point. Trail development is also recommended along the top of bank through Peturrson's Ravine, further through private land holdings, with an eventual connection to the Peggy McKercher Conservation Area. Further detailed planning of trails in the northeast is anticipated as development proceeds in these areas.

Moose Woods-Batoche Trail

North of the Swale, the Master Plan illustrates a potential trail that could continue to follow the alignment of the historic Moose Woods-Batoche Trail. This trail has important cultural value that will be interpreted in the Swale. Including this trail into future neighbourhood design would reinforce the importance of this feature.

Linear Park and Trail to the Small Swale

The Master Plan proposes a trail through the Swale that connects with the linear park trail system in Aspen Ridge. A continuation of that trail is suggested to head north through University Heights 3 neighbourhood and across Central Avenue that would link in with potential future trails on the south boundary of the Small Swale. This would provide a strong linear pedestrian connection between the two swales and through the two neighbourhoods' park systems.

Amenities

Outdoor Education Staging Area

The central gathering point for visitors to the Swale will be the Outdoor Education Staging Area located in the Recreation Zone. It will provide good access to both the Recreation Zone and Ecological Core. The detailed design of the staging area remains to be undertaken, but the use of environmental sustainability technology may be suitable for the area. The Staging Area is intended to be an accessible public area, open year round.

The Staging Area will support all season classroom activities and group gatherings by providing visitor and activity information, shelter and a washroom facility. It will also have the ability to store materials required to host interpretive activities for large groups.

Further detailed design of the Outdoor Education Staging Area will require additional study and design. The roadway and parking lot design to provide access to the Staging Area will also need to be considered further.

Nodes

Major nodes are located at primary entry points into the Swale in both the Ecological Core and the Recreation Zone. Each major node will include benches, waste receptacles, and signage which may include interpretive, wayfinding and regulatory information.

Minor nodes are located along the Greenway and within the Recreation Zone, interspersed between

major nodes. Each minor node will include benches and possibly waste receptacles.

Interpretive nodes are located throughout the Swale. Each node will have seating, a viewpoint and interpretive panels.

Picnic Sites

Picnic sites will be located within the Recreation Zone and adjacent to the parking and washroom located at the Outdoor Education Staging Area.

It is anticipated that the Recreation Zone will support a number of picnic sites, subject to detailed design. Sites will include tables and waste receptacles. Some sites may also be designed to support larger groups with multiple tables. Shelters and barbecues may also be a potential option for these sites.

Washrooms

The primary washrooms will be located at the Outdoor Education Staging Area. There is potential for this site to have composting toilets as a low impact demonstration feature.

Additional washrooms will be located at the north parking lot. It is anticipated that the construction of this lot and the associated washrooms will be included in a later phase of construction. Since the northern portion of the Swale is not yet surrounded with residents, access to this portion of the Swale remains a lower priority.

Parking

Outdoor Education Staging Area

The primary parking lot within the Swale will be located at the Outdoor Education Staging Area within the Recreation Zone. This lot will be accessed from Fedoruk Drive via an access roadway. This parking lot will accommodate visitors to the Outdoor Education Staging Area, the



Figure 13
Example of minor seating node at
Conservation Easement



Figure 14
Example of picnic area
(Surrey Wildlife Trust)

picnicking sites and internal trails, and allow for the pickup and drop-off of school groups. The size of the parking lot remains to be determined. It is anticipated that this parking lot will be gravel and cleared for winter use. Range Road 3050 may also support some on street parking.

Central Avenue

A gravel parking lot exists on the west side of Central Avenue, at Crocus Prairie and Peturrson's Ravine. Visitors can access both the Meewasin Trail and the Swale from this lot. Pedestrians can cross Central Avenue at Fedoruk Drive to access the trails within the Recreation Zone. This location has approximately 16 parking stalls.

North Lot

A smaller parking lot in the north portion of the Ecological Core will improve access for trail users. This north lot is located at the north end of the Swale, near the Commuter Parkway.

The feasibility of creating an off-site parking lot (outside of the Swale) should be considered to minimize impacts. Parking within the Greenway or University Heights 3 may be a possible alternative. Disturbance to the Swale should be minimized.

Temporary Parking

Additional temporary parking for special events may be accommodated as needed in summer months by mowing non-sensitive areas. Temporary parking locations should be limited to areas where disturbances have already occurred, and planned in conjunction with Meewasin's Planning and Conservation Department.

Access

The Master Plan illustrates locations where the Ecological Core and Recreation Zone can be accessed by the public. The Recreation Zone is meant to accommodate the largest amount of public access. The *Northeast Swale Resource Management Plan* (Meewasin Valley Authority, 2013) recommends controlled access to the Ecological Core to help manage domestic grazing animals while providing a physical barrier to define the site. This will be a physical reminder of the importance of this area.

Greenway/Primary Trail Access

The Master Plan illustrates access points to the Greenway. Silverspring and Evergreen have pre-existing connections to Fedoruk Drive, where crosswalks will assist pedestrians in safely crossing the street to access the Recreation Zone. There is an existing berm along Fedoruk Drive that limits access to the Swale. In Aspen Ridge, proposed access points are based on planned connections to the Greenway through linear parks and pedestrian linkages.

The access points along the north Greenway, into the future University Heights 3 neighbourhood, have been located at similar intervals to those in Aspen Ridge. They may need to be adjusted based on future neighbourhood design.

Access to the Greenway does not necessarily provide access to the Swale; access to the Swale is limited to trail connection points (and illustrated with a yellow arrow on Appendix A Figure A-4). Access to the Ecological Core will be restricted by fencing and gates. The Recreation Zone will need controlled access defined as this area is actively being accessed for prohibited uses such as ATVs and off-leash dogs. See the fencing section on page 31 for further details on access to the Swale.

Pedestrian Crossings

The design of the roadways surrounding the Swale ensures that all entry points are located where pedestrians can cross to access the site. All of these crossing are at intersections, creating no midblock crossing points.

The North Commuter Parkway road design will include at least two mid-block pedestrian crossings within the Swale (Stantec Consulting Ltd., 2012, p. 3.9). See Appendix A Figure A-4 for the location of these light actuated crossings.

The Outdoor Education Staging Area will feature a parking lot for bus loading and unloading. Students will need to cross Range Road 3050 to get to the Ecological Core for guided tours. A crosswalk at the intersection of Range Road 3050 and Fedoruk Drive is anticipated.

Crosswalks are desirable for safe pedestrian connectivity at all road intersections along the perimeter of the Swale, including at Central Avenue, Konihowski Road, and Zary Road, as well as any future planned streets in University Heights 3. Pedestrian crossings for the Perimeter Highway will be subject to further discussion with the Department of Highways.

Resource Management

Resource management activities in the Swale are based on adaptive techniques that will include, but are not limited to, grazing animals, controlled burns and weed removal, as proposed in the *Northeast Swale Resource Management Plan* (Meewasin Valley Authority, 2013).

There are two proposed resource management access points and staging areas. The first is located north of Fedoruk Drive, just west of Range Road 3050. The second is located at the north parking lot. These staging areas will be accessed by a locked gate and not intended for public use or access.

Other features required to support resource management such as water supply, salt licks and corrals will be determined through Meewasin's Planning and Conservation Department as part of ongoing resource management activities.



Figure 15
Example of a resource management access gate (Moyfab Engineering Limited, 2012)

Emergency Services

There are proposed points along the perimeter fencing that allow for emergency vehicle access. A portion of the internal secondary trail within the Recreation Zone will be built to support emergency services' needs. The resource management access points will also allow emergency service access. In addition, due to the size of the Swale, signage and wayfinding will be developed to help orientate emergency service providers and users to their location within the site. For example "mile markers" could be installed along the Greenway with a site number that corresponds with latitudinal and longitudinal coordinates (see Figure 16). The naming of internal trails will also help with this wayfinding.

Signage

Signage is an important way to delineate the significance and value of the Swale. Wildlife crossing, pedestrian crossing and no stopping signage is recommended in the *Northeast Swale Development Guidelines (2012)* (Stantec Consulting Ltd., 2012, p. 3.9). All signage should follow Meewasin's existing brand and visual identity and be consistent with the Communication and Education Plans.

Interpretive Signage

Specifics of the interpretive signage will be subject to further research and detailed design. However, the Conceptual Design (Appendix A Figure A-4) offers suggestions for interpretation which include: limestone quarries, Moose Woods-Batoche Trail, stormwater management, wetland restoration, resource management, grassland ecology, rubbing stone, pinned rock, hydrology, geomorphology, grassland ecology, wetland ecology and riparian ecology.

Wayfinding Signage

Site wayfinding signage has not yet been designed. It must emphasize the importance of the Swale while providing location and directional guidance for people throughout and into the site and be consistent with the current brand.

Naming the internal trail within the Swale should be considered. Trails should be named to be consistent with the branding of the site. Trail names could potentially incorporate First Nations and Métis, ecological, cultural or heritage history.



Figure 16
Example of a "mile marker"
(Corbin Design)



Figure 17
Example of interpretive signage at
Saskatoon Natural Grasslands

Regulatory Signage

To protect the Swale from unwanted uses, additional regulatory signage is required. The existing signs (see Figure 17) are situated at visible entrance points and fulfill a minimum requirement. Additional regulatory signage must be installed on site as soon as adjacent development occurs. Signage describing the importance of the site and the reason for its protection will support the regulatory signage and should be installed simultaneously. The Communications Plan currently underway will determine further signage requirements.



Figure 18
Existing predevelopment signage at the Swale (to be updated through the Communications Plan)

Fencing

It is critical to delineate the extent of the ecologically sensitive areas of the Swale as this will increase the understanding, emphasis and importance of the area. The *Northeast Swale Development Guidelines (2012)* supports this delineation by fencing and/or signage (Stantec Consulting Ltd., p. 3.4). It is imperative for visitors to understand the uniqueness and importance of the site and to foster a sense of stewardship within the community.

Fencing is the first component of the Master Plan. Signage providing information on the importance of the Swale and its permissible, restricted and prohibited uses should be installed in conjunction with the fencing.

The remainder of the Swale, including the Recreation Zone and section of the Ecological Core east of the North Commuter Parkway should include measures for access control and protection of the site.

Ecological Core

Wildlife-friendly fencing will be used along the complete perimeter of the Ecological Core. Although fencing is only delineated south of the North Commuter Parkway on the Conceptual Design (see Appendix A Figure A-4), it is recommended that it continue to the north as well.

This fencing serves to control pedestrian access, ensuring that entry into the Swale occurs only at the designated trailheads. This fencing also provides control for animals grazing as part of resource management practices in the Ecological Core.



Figure 19
Example of access control gates (Centrewire)

The fence will allow movement for wildlife travelling through the Swale. Access to the internal trails will be determined upon detailed design but may include gates, removable fence and panels. In addition, openings at regular intervals in the fence along the roadway sections to allow for improved wildlife movement should be considered. These openings can be blocked off as needed with temporary fencing to allow for grazing.

Additional internal fencing may be required to manage grazing activities within the Swale. This fencing will be temporary in nature and will allow different parts of the Swale to be chosen for animal grazing. This fencing is part of the resource management operations within the Swale.

Recreation Zone

The Recreation Zone will not have the same controlled access as the Ecological Core. To protect this area from prohibited activities (i.e. ATV and off-leash dog use) it is recommended that it also have a physical delineation. Further work must be done to determine the best method of delineation but options include bollards and fencing.

Stormwater Management

The Swale provides many ecological services, including stormwater management. There are both natural and constructed stormwater systems present.

The *Northeast Swale Resource Management Plan* notes two stormwater management systems, one for Aspen Ridge and another for University Heights 3, north of the Swale. Existing

stormwater management ponds (dry and wet) exist in the Recreation Zone (Meewasin Valley Authority, 2013). Information for stormwater management can be found in the Aspen Ridge Neighbourhood Concept Plan (Saskatoon Land, 2014) and the Evergreen Neighbourhood Concept Plan (Land Branch, 2009).

An additional stormwater management system is being proposed on the north side of the Swale, (for University Heights 3) between the North Commuter Parkway and Perimeter Highway. Given the potential negative impact this could have on animal connectivity and natural systems at this location, further study and exploration of options are necessary. It is recommended that additional land be added to the Swale boundary to compensate for the fragmentation of the site between the North Commuter Parkway and Perimeter Highway. See Figure 20 for the suggested area of land. Although land has not been dedicated for Swale boundary alterations, the dotted future Greenway line shows a potential area to be considered in the future. This portion of land is unserviceable, due to its high water table and grades. Although the vegetation is predominantly non-native, it can be restored and incorporated into the Swale.



Figure 20
Area of additional potential land for the Swale north of Commuter Parkway. See the red shaded area.

Environmental Design Considerations

Design components found in this plan reflect potential changes to the Swale landscape. These changes can have positive, negative or neutral effects. These effects should be considered and if negative should be mitigated where possible.

Lighting

Lighting will be limited to reduce impacts to wildlife in the Swale. The site is intended to be predominantly unlit, as light pollution can have a negative effect on flora and fauna. Effects of increased light levels include altering the natural patterns and behaviours of wildlife and the flowering and dormancy period of plants (Dick, 2013, p. 2). If any lighting is needed it must be dark sky compliant. *The Meewasin Trail Study* contains recommendations and considerations for lighting with the Meewasin Valley (Meewasin Valley Authority, 2014).

Nocturnal Preserve

A nocturnal preserve designation through the Royal Astronomical Society of Canada (RASC) can help preserve darkness on site that will help maintain natural ecosystems and processes.

According to the RASC, a nocturnal preserve “is an area accessible to the public in which all lighting fixtures within its borders minimize their contribution to light pollution: glare, light trespass and sky glow” (Dick, 2013, p. 2). This designation will also assist to educate users about the value of darkness. Examples of nocturnal or dark-sky preserves in Canada include Cypress Hills Interprovincial Park Dark-sky Preserve (Saskatchewan), Grasslands National Park Dark-sky Preserve (Saskatchewan) and Rouge Park (Toronto, Ontario) (Huziak, 2015). It is recommended that the feasibility of creating a nocturnal preserve on site be researched further.

Roadways

There will be four roadways crossing the Swale, Central Avenue, Range Road 3050, the North Commuter Parkway and Perimeter Highway. The *Northeast Swale Development Guidelines (2012)* require street lighting at all crossings to be dark sky compliant (Stantec Consulting Ltd., p. 3.9). The Perimeter Highway will follow guidelines from the Ministry of Highways.

Greenway

If the Greenway is lit, lighting will be designed to support a safe active transportation corridor with a link from the surrounding neighbourhoods to the Meewasin Valley and downtown in all seasons. The asphalt cycling lane along Fedoruk Drive will be lit with roadway lighting.

Further discussion is needed to determine what measures can be put in place to best manage lighting within the Greenway. One option may be to time the lighting to switch off from midnight to 5:00 am when trail usage is minimal.

Outdoor Education Staging Area

If lighting is needed for the Outdoor Education Staging Area, it should reflect nocturnal preserve guidelines. Methods should be considered to reduce light use outside of regular hours of operation. External lighting for the staging area should only be used for programming requirements.

Views & Vistas

Viewsheds within the Swale should be maintained where possible. Nodes (major, minor and interpretative) can be used in these places. The Swale should be consistent with the view protection plan for the Meewasin Valley suggested in *The Meewasin Trail Study* (Meewasin Valley Authority, 2014). The experience of a site is important, and as such protecting views should be a priority. As mentioned in *The Meewasin Trail Study* “identification, establishment, re-establishment and protection of these areas are an important component to the Meewasin Valley” (Meewasin Valley Authority, 2014, p. 23).

Soundscapes

As the Swale and its surroundings change, there should be consideration for the soundscape. The Swale currently has species that rely on low levels of noise for breeding and habitat. There are also residents and visitors who come to the Swale to experience and listen to nature. As development proceeds, the Swale may change. This change may include the soundscape, as there may be increased levels of vehicle traffic and human presence.

Accessibility

Part of the unique aspect of the Swale is its topography and physical characteristics. The landscape varies between “steep rocky ridges, rolling prairie, lush valleys, treed areas and ephemeral wetlands” (Meewasin Valley Authority, 2013, p. 1). Due to the varying topography, the Swale’s Ecological Core may require assisted accessibility and will be determined as detailed design proceeds. It is ideal to provide as much accessible trail and infrastructure as possible.

Trails

Trails within the Recreation Zone will be designed and constructed to Meewasin’s standard trail detail with a maximum grade of 5%. The feasibility of meeting the 5% maximum grade within the Ecological Core secondary internal trails will be determined during detailed design. Maintaining the ecological integrity of the Ecological Core will be a special consideration during the detailed design.

Interpretation

The interpretation features throughout the Swale, including signage and the Outdoor Education Staging Area should consider visual and hearing impairments. The washrooms located at the Outdoor Education Staging Area will be designed to meet accessibility standards.

Public Art

The Swale has many interesting interpretation opportunities including human heritage, ecology and glaciation. These histories may be interpreted through on site public art that will also enhance the cultural experience and provide another means for interpretation and education of the unique characteristics in the area. Similar installations have been completed recently at Mark Thompson Park located in the Stonebridge neighbourhood where art was developed to help interpret the remnant Moose Jaw Trail on that site.

Discussions have begun between Meewasin and the City of Saskatoon to incorporate public art on site at the Swale.

Implementation

As a master planning document, all design ideas and recommendations are intended to provide a conceptual level of detail and will require further detailed design. The Master Plan is the basis for decision making and provides direction relating to the development of the Meewasin Northeast Swale and adjacent lands.



Figure 21
Sheep Grazing at the Meewasin Northeast Swale with Jared Epp

Funding Strategy

Ongoing collaboration with Meewasin's partners will be necessary to determine a sound strategy for implementation and annual operating costs. The completed Master Plan provides baseline information to help in this regard. The collaborative approach undertaken for the Master Plan must continue to ensure its successful implementation and conservation of the Swale itself.

Capital Development and Funding

Meewasin has compiled a cost opinion for the Master Plan which includes approximately \$6.1 million dollars for development work within the Swale (not including the Outdoor Education Staging Area or the Greenway). There is an additional option for post construction, one year maintenance totaling approximately \$250,000. The cost to develop the Greenway totals \$8 million dollars. The Resource Management and Communication Plan costs, based on a five year work plan to start but would be ongoing afterwards, totals approximately \$778,000 and \$75,000 respectively. The Education Plan costs are approximately \$110,000. These totals are based on 2014 dollars and do not consider inflation. The cost remains undetermined for the Outdoor Education Staging Area until further planning and design have been undertaken. See the Section on Construction (page 41) for additional information on costing and phasing.

The Meewasin Northeast Swale

The size of the Swale within City limits is approximately 300 ha (740 ac) and translates to an average cost opinion of approximately \$20,260 per ha (approximately \$8,213 per ac). This is a relatively low unit cost when compared with standard neighbourhood park development, or typical residential neighbourhood development. This low unit cost is due to the overall type and method of development that is intended to minimize adverse effects on the existing natural systems and ecological services.

Greenway

The Greenway protects the Swale and public access. It is multi-functional to the Swale, neighbourhoods, City and region. One aspect of functionality is being a component of adjacent neighbourhood infrastructure (overland drainage and temporary storage). The cost opinion noted of \$8 million is based on a per acre cost to develop linear parks (City of Saskatoon Parks Branch values). Further analysis will be necessary to verify these costs.

Funding Requirements

The completed Master Plan provides baseline information to help determine the appropriate budgets and phasing for full implementation and any restoration required. It also provides quantitative data that will assist in determining ongoing operating costs pertaining to standard park and facility maintenance. Ongoing monitoring, resource management, education and programming costs must also be considered, as well as necessary life cycle replacement costs.

Ongoing Investments

The work completed to date, including the *Northeast Swale Development Guidelines (2012)* (Stantec Consulting Ltd., 2012), the *Northeast Swale Resource Management Plan* (Meewasin Valley Authority, 2013) and now the *Meewasin Northeast Swale Master Plan*, have all been collaborative projects with significant investments in time and consultation. To follow through with the recommendations and next steps, formulated in these reports and summarized on page 48, Meewasin will continue to invest significant resources in the Swale and collaborate with the City and other key stakeholders in the process.

Operating Impacts

Development within the Swale will result in operating costs. The phases noted in the Master Plan indicate preliminary quantities necessary to calculate operating impacts. These costs include operation and maintenance for standard park infrastructure and facilities. Other site work related to ongoing resource management can be completed by Meewasin, assuming appropriate funding is in place to complete this work.

Construction

Construction and development of the Master Plan is best done in conjunction with adjacent development. As such, the conceptual design has been separated into 5 phases of construction.

Construction is dependent on adjacent development and funding opportunities, therefore dates have not been assigned. There is flexibility for each phase to be completed in smaller stages. The development of the Swale may be a gradual process but is still meant to be proactive in its implementation. The Swale will be surrounded by new residential development and will face increased pressure on its ecosystem. It is important to be proactive and delineate the importance of this site as soon as possible.

Monitoring

In accordance with the *Northeast Swale Development Guidelines (2012)* (Stantec Consulting Ltd., 2012), onsite and adjacent development monitoring is recommended. During construction the land owner must ensure that an independent environmental monitor is on site for regular inspections (e.g. every second day) (Stantec Consulting Ltd., 2012, p. 3.10). The monitoring of reclamation success post construction should continue until the right-of-way and disturbed lands are vegetated (Stantec Consulting Ltd., 2012, p. 3.12).

Phases of Construction

Item	Description (*see the accompanying phasing drawings in Appendix B)
Phase 1	<ul style="list-style-type: none"> • Fencing the Ecological Core, east of the stormwater management system to the North Commuter Parkway. • Delineate the Recreation Zone through bollards, signage or fencing. • Trail along south edge of the Swale from Central Avenue to Aspen Ridge neighbourhood, adjacent to Fedoruk Drive. • Trail along Central Avenue, south of Fedoruk Drive to Silverspring neighbourhood. • The east-west trail and the south trails within the Recreation Zone up to the stormwater management system (pond). • Resource Management access points (north and south). • Emergency services access points. • Interpretive nodes in the south portion of the Recreation Zone. • Seating nodes (major/minor) in the south portion of the Recreation Zone. • Seating nodes (major/minor) in the Ecological Core adjacent to Fedoruk Drive.
Phase 2	<ul style="list-style-type: none"> • Southern Greenway along Aspen Ridge neighbourhood. • Establish a mown edge for the north Greenway bordering University Heights 3. • North-south internal secondary trail in the Ecological Core from Aspen Ridge to

	<p>University Heights 3.</p> <ul style="list-style-type: none"> • East-west internal secondary trail in the Ecological Core. • Interpretive nodes in the Ecological Core along the secondary trails (east-west/north-south). • Major seating nodes adjacent to the secondary trails (east-west/north-south) in the Ecological Core. • Minor Seating nodes adjacent to Aspen Ridge neighbourhood.
Phase 3	<ul style="list-style-type: none"> • Northern Greenway along University Heights 3 to North Commuter Parkway. • North trails in the Recreation Zone and the trails east of the stormwater management system (pond). • Internal tertiary trails in the Ecological Core. • Interpretive nodes between Central Avenue and North Commuter Parkway. • Seating nodes (major/minor) between Central Avenue and North Commuter Parkway. • Parking for Outdoor Education Staging Area. • Trail along Central Avenue, north of Fedoruk Drive to University Heights 3. • Picnic areas.
Phase 4	<ul style="list-style-type: none"> • Fencing between the North Commuter Parkway and Perimeter Highway. • Greenway between the North Commuter Parkway and Perimeter Highway. • Feature lookout from the North Commuter Parkway in the Ecological Core. • Trail along Range Road 3045, between the North Commuter Parkway and Perimeter Highway. • Interpretive nodes along the feature lookout and internal trails. • Seating nodes (major/minor) along the feature lookout, North Commuter Parkway and the Greenway. • North washroom. • North parking lot.
Phase 5	<ul style="list-style-type: none"> • Fencing from Perimeter Highway to City limits. • Greenway from Perimeter Highway to City limits. • Boardwalk in Recreation Zone. • Outdoor Education Staging Area. • Trail along Range Road 3045, between Perimeter Highway and City limits. • Interpretive node between Perimeter Highway and City limits. • Seating nodes (minor) along Perimeter Highway.

Phasing Notes

Phase 5 contains the Outdoor Education Staging Area, which like all other design components is subject to available funding, but it is not subject to adjacent development. As such, the Outdoor Education Staging Area may be built at any time.

Preparing construction specifications for the Greenway will include sections on native seed mixes, seed rates and construction maintenance practices. As adjacent and internal

development occurs, any native vegetation, such as the fescue located in University Heights 3, should be transplanted and conserved.

See Appendix C for more detailed information regarding phases of construction, including a construction phase quantity matrix and figures of each phase.

Cost Opinion

A cost opinion has been prepared for the design components of the Master Plan. This cost opinion is based on 2014 pricing and does not include inflation. In addition, the cost for the Greenway has been separated and based on the City of Saskatoon's per acre costs for developing linear parks. The Outdoor Education Staging Area has not been considered in the cost opinion as the cost is highly variable depending on the programming and detailed design.

See Appendix B for more detailed information regarding the cost opinion, including phasing maps and matrices.

Effectiveness

For the purpose of future effectiveness reporting, the Design Site Program will serve as Meewasin's objectives for the Meewasin Northeast Swale (Appendix A Figure A-6). It defines elements of the program that articulate the outcomes of a successful design. Some of these program objectives will be qualitative and some can be measured quantitatively.



Figure 22
Monitoring at the Swale (Christensen, 2011)

Environmental Monitoring

It is recommended that the Swale be monitored for nighttime levels of noise, light and movement immediately, during adjacent development and continuing upon the completion of development. Nocturnal light and ambient sound levels directly impact flora and fauna. Monitoring these levels as adjacent areas are developed can provide valuable information that will assist in understanding the impact of adjacent development on the natural systems. This monitoring can be undertaken by Meewasin in conjunction with community partners.

Monitoring the Swale should also continue in conjunction with the *Northeast Swale Resource Management Plan* (Meewasin Valley Authority, 2013) to measure biodiversity, health of the habitat, wildlife and flora and fauna.

Site Users

As development proceeds, it is important to understand usage of the Swale. Trail counters can provide reliable data on types of users (cyclist or pedestrians) and use patterns. This information will be helpful in future site planning.

Going Forward

The following section provides recommendations for work that is not yet completed or requires a funding source.



Figure 23
Pinned Rock (DAFT Media, 2011)

Communication Plan

A Communication Plan is required, which “provides the framework for establishing a Communication Strategy that will: celebrate the Swale as a treasured and valuable resource to Saskatoon and region; [and] foster a sense of stewardship and understanding for those living near and visiting the [Meewasin] Northeast Swale” (Meewasin Valley Authority, 2013, p. 7).

Naming the Swale

The current working title for the site is the Meewasin Northeast Swale. The Communications Plan will provide further insight into site naming.

Adjacent Neighbourhoods and Residents

According to the Aspen Ridge Neighbourhood Concept Plan, “information about the Greenway, the Swale, and the native plants existing in both, will be provided to all those who purchase a lot backing onto the Swale. This will include information on the value of the Swale, the function of

the Greenway, and how it relates to adjacent development. It will also identify the differences between a naturalized area like the Greenway and standard, irrigated linear and pocket parks that will exist throughout Aspen Ridge” (Saskatoon Land, 2014, p. 7). This information should also be extended to all neighbourhoods bordering the Swale, including Silverspring, Evergreen, Aspen Ridge and the future University Heights 3 neighbourhood north of the Swale. This distribution of information should be coordinated between the City of Saskatoon and Meewasin and must include an explanation of natural areas, nocturnal preserves, and suggestions on where to purchase lights that are dark sky compliant. A strategy for relaying this information to new home owners who did not originally purchase the lot should be identified as well.

Marketing and Visual Identification

A concise and consistent message is required for the Swale. The Communication Plan will cover the design and messaging of on-site signage, marketing, branding and social media awareness.

External Communication

With multiple and often overlapping civic projects underway (and ongoing changes in relevant personnel) it can be difficult to determine whom to contact as issues arise. Improvements in communication protocols would be beneficial. Possible options include establishing one City contact for all projects in the area, or establishing a development committee or working group that meets regularly. Improving communications relating to work activities within the Swale is beneficial to the success and conservation of the overall site.

Central Information Depository

Through research, curriculum and educational information, there is a large amount of data regarding the Swale. For ease of access, this information should be publically available through a central information depository. The location for this depository is yet to be determined.

Education Plan

The opportunity for education in the Swale is expansive and important, covering areas such as human history, natural history, glaciation and ecology. Further site interpretation must also be developed in conjunction with the educational goals set out in the plan.

Components of the Education Plan could potentially include an educational guide, similar to the one that was created for *Saskatoon Natural Grasslands Ecological Guide* (Meewasin Valley Authority, 2005). Information could be accessed through hard copies or digital/online resources. The curricula should include lesson plans for all grade levels, with a strong focus on the Saskatchewan Ministry of Education’s Grade 6-8 curricular expectations.

Educational themes will include ecological theories and concepts, natural and geological history, biodiversity, sustainability and conservation, symbiosis and connections and civilian science. The activities should promote curiosity and enthusiasm for nature through hands-on learning while promoting scientific inquiry; and culturally an understanding of what has taken place in the past to better understand the future.

The goal of the Education Plan should be that youth experiencing education programs in the Swale, and Meewasin Valley, will become stewards in promoting the importance of wetland conservation, in addition to the conservation of all natural and cultural resources. It is important to provide connections to nature and engage youth as what they learn and understand now will have an influence on their future decisions.

Next Steps

Upon the completion of the Master Plan, the following steps must be completed to implement the design concepts. These steps will build on the collaboration between Meewasin and key stakeholders.

1. **Develop a funding strategy for capital construction.** A number of options have been presented regarding capital funding. For the Master Plan to be implemented effectively in concert with adjacent development it is imperative that sufficient capital resources be allocated at the appropriate time.
2. **As funding becomes available, proceed with Phase 1 implementation of the Master Plan.** Install fencing and signage for the Ecological Core, as shown on Phase 1 of the Master Plan. It is important to delineate the sensitive areas within the Swale and help protect the Swale's biodiversity from unwanted use and activities. Fencing will also allow for grazing as part of ongoing resource management. Continue with phased development in concert with adjacent development. Detailed design work and tendering of the initial phase of work can proceed as early as 2015, subject to staff capacity and funding. Details such as mowing the internal secondary trails within the Ecological Core in 2015/2016 should be considered. This would allow for users to test the trail routes before construction to allow for further refinement for improved accessibility and minimization of negative impacts.
3. **Detailed design and project management to implement phased development within the Swale, according to the section on Phases of Construction.**
4. **Develop operating impacts for standard maintenance.** Preliminary quantities have been determined as part of the Master Plan, organized per implementation phase. As projects are developed and completed, accurate detailed design and as-built data will be determined.
5. **Plan for Meewasin Trail development for the remainder of the northeast sector.** *The Meewasin Trail Study* (Meewasin Valley Authority, 2014) was recently completed, providing information regarding current and projected usage and gaps within the current trail network. The northeast sector has been a consideration of *The Meewasin Trail Study*, but will require further detailed design upon incorporation. In addition, Meewasin will be preparing a Trails Master Plan which will inform the expansion of the Meewasin Trail within the northeast sector.
6. **Develop a program plan and conceptual design for the Outdoor Education Staging Area.** The proposed facility is intended to support site recreation activities, educational programming, resource management and research. More detailed program development

and conceptual design will be required to proceed further. It will allow for a better understanding of development costs, operational costs, needs and responsibilities.

7. **Amend the maintenance agreement between Meewasin and the City of Saskatoon to reflect the development of the Swale.** Updates to the agreement will be necessary as the Master Plan is implemented and regular maintenance and resource management operations are initiated.
8. **Pursue opportunities to showcase and raise awareness of the Master Plan and other Swale initiatives.** There are a number of potential methods that could include applying for professional awards, presenting or speaking about the Swale at events or conventions, pursuing media coverage, social media initiatives, publishing site research and hosting events on site.
9. **Add the remaining City Swale lands to Meewasin's jurisdiction.** As part of the implementation of the Northeast Policy, Meewasin has signed a memorandum of understanding with the City of Saskatoon regarding alteration of the Meewasin jurisdiction. Part of this includes adding those portions of the Swale, which are now within the City limits as a result of post-1979 boundary alterations, to Meewasin's Conservation Zone. This will enable Meewasin to expand its work in the Swale.
10. **Ensure connectivity remains a priority within the Swale.** An area of concern in regards to connectivity is in between the North Commuter Parkway and Perimeter Highway. This area contains Aspen Ridge's forebay, semi-permanent wetlands, two large roadways and a future stormwater management system for University Heights 3.
11. **Apply Master Plan concepts to similar sites.** Future planning for similar work in the Small Swale and ongoing discussion with all relevant organizations regarding the Greater Swale. The ideas within the Master Plan are scalable for use at the Small Swale and the Greater Swale.
12. **Continue research and collaboration with community partners.**
13. **Continue discussions pertaining to the Perimeter Highway.** Relevant discussions include the crossings of the Meewasin Northeast Swale, Small Swale and the Meewasin Valley (South Saskatchewan River).
14. **Define legal parcel.** The Swale is a unique landscape and site within Saskatoon and differs greatly from manicured parks. Legal designation of Swale lands is recommended to ensure long term protection in terms of public access and interest. This could include an Environmental Reserve designation and/or a conservation easement. In addition to the legal definition of the land, a Special Use Park definition best describes the site using current City of Saskatoon park definitions as noted in the Park Development Guidelines.

Summary of Recommendations

The following recommendations are supplemental to the core elements proposed in the Meewasin Northeast Swale Master Plan. These recommendations are not numbered in terms of priority.

Site Considerations

1. **Plan for dog parks in adjacent areas.** As adjacent neighbourhood development proceeds, there will be increasing pressure for dog parks. It is important to determine appropriate location(s) nearby as the Swale is not suitable for off-leash activities.

Design Components

2. **Investigate the feasibility of winter recreational activities in the Greenway.** This includes but is not limited to cross country-skiing and snowshoeing.
3. **Explore options to aid in maintaining natural systems and connectivity crossing all planned roads that intersect the Swale.** Connectivity is of integral importance in the conservation of natural systems. Road crossings in the Swale significantly impact the ability of wildlife to move freely through the Swale and to other areas. Mitigating the impacts of road crossings and site fragmentation, which can be a significant barrier to connectivity, is of the utmost importance.
4. **Install trail counters at major entrances.** Future detailed design must consider the volume of trail users and data gathered through trail counters can help inform this decision and will provide a cross section of the type of users.
5. **Explore opportunities to promote environmental initiatives at the Outdoor Education Staging Area.** The Outdoor Education Staging Area should be developed as a sustainable, resilient, or high efficiency building that provides an opportunity for education and ways of reducing the human ecological footprint.
6. **Use native plant species to re-vegetate the Stormwater Management Systems (ponds) and other disturbed areas.** Disturbed areas such as the stormwater management ponds provide a significant source of non-native, noxious and invasive vegetation. This significant disturbance facilitates the spread of unwanted vegetation to other more pristine areas and reduces the site's overall biodiversity.
7. **Research the feasibility of creating a nocturnal preserve.** A nocturnal preserve designation through the Royal Astronomical Society of Canada can help preserve darkness that will assist in maintaining the natural ecosystems and processes. It will also help educate users about the value of darkness.

8. **Pursue opportunities for the introduction of public art on site.** Public art can be used to enhance the cultural experience and provide another means for interpretation and education regarding the unique characteristics of the area.

Funding Strategy

9. **Explore the use of existing donor programs and develop new programs for the Swale to support development and ongoing resource management costs.**

Construction

10. **Prepare construction specifications for the Greenway.** This will include sections on native seed mixes, seed rates and construction maintenance practices.
11. **Determine a cost estimate upon detailed design.** The cost currently shown is a cost opinion based on 2014 numbers without inflation.
12. **Develop annual standard site operational costs.** This program must include pre and post development.
13. **Monitor site and adjacent construction.** It is critical that the Swale maintain its ecological integrity during construction. To effectively monitor all activity, environmental monitors must be on site, in accordance with the *Northeast Swale Development Guidelines (2012)* (Stantec Consulting Ltd., 2012, p. 3.10).
14. **Transplant any native vegetation from adjacent and internal development.** As development occurs, any native vegetation, such as the fescue located in University Heights 3, should be transplanted and used to re-vegetate disturbed areas.

Effectiveness

15. **Develop a program for ecological monitoring within the Swale.** This program should include monitoring biodiversity, nighttime noise and light levels. Monitoring should be done in accordance with the *Northeast Swale Resource Management Plan* (Meewasin Valley Authority, 2013).
16. **Ensure bylaws are in place for protection of the Swale and the enforcement of prohibited and restricted uses.**
17. **Develop and maintain strong, active partnerships with volunteers and support groups.** Active and ongoing conservation and education are critical for the Swale's long-term survival. Besides participation from provincial, municipal, conservation and education partners, a strong volunteer component is important and must engage the community's sense of ownership. Volunteers could include, for example, local community associations, service groups, clubs and non-profit organizations.

Going Forward

18. Communication Plan

- a. **Engagement with adjacent neighbours and residents.** Provide education materials to neighbouring residents to encourage stewardship and foster an understanding of the importance of the Swale.
- b. **Marketing and Visual Identification.** Create a concise and consistent message for the Swale.
- c. **Coordinate external communication.** Improve communication protocol.
- d. **Develop a central depository for information.** A central body to distribute information and data collected on the Swale for public use.

19. Education Plan

- a. **Curriculum development.**
- b. **Further develop site interpretation.**

20. **Explore the potential for designation of the Swale as a Municipal Heritage site.** *The Heritage Property Act* provides for the preservation, interpretation, and development of heritage resources in Saskatchewan. Any building, structure, or site that is significant for its cultural, historical, architectural, environmental, archaeological, paleontological, scientific, or aesthetic value may be designated as heritage property. A heritage designation would provide protection of the site and bring attention and awareness to the value of the Swale and its unique attributes.

21. **Explore the potential for scaling the ideas within the *Meewasin Northeast Swale Master Plan* to current and future Meewasin sites.** Meewasin's jurisdiction includes many natural areas that would benefit from similar planning and management.

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Appendix A – Figures and Maps

Figure A-1. The Swale Extent (from the *Northeast Swale Resource Management Plan, 2013*)

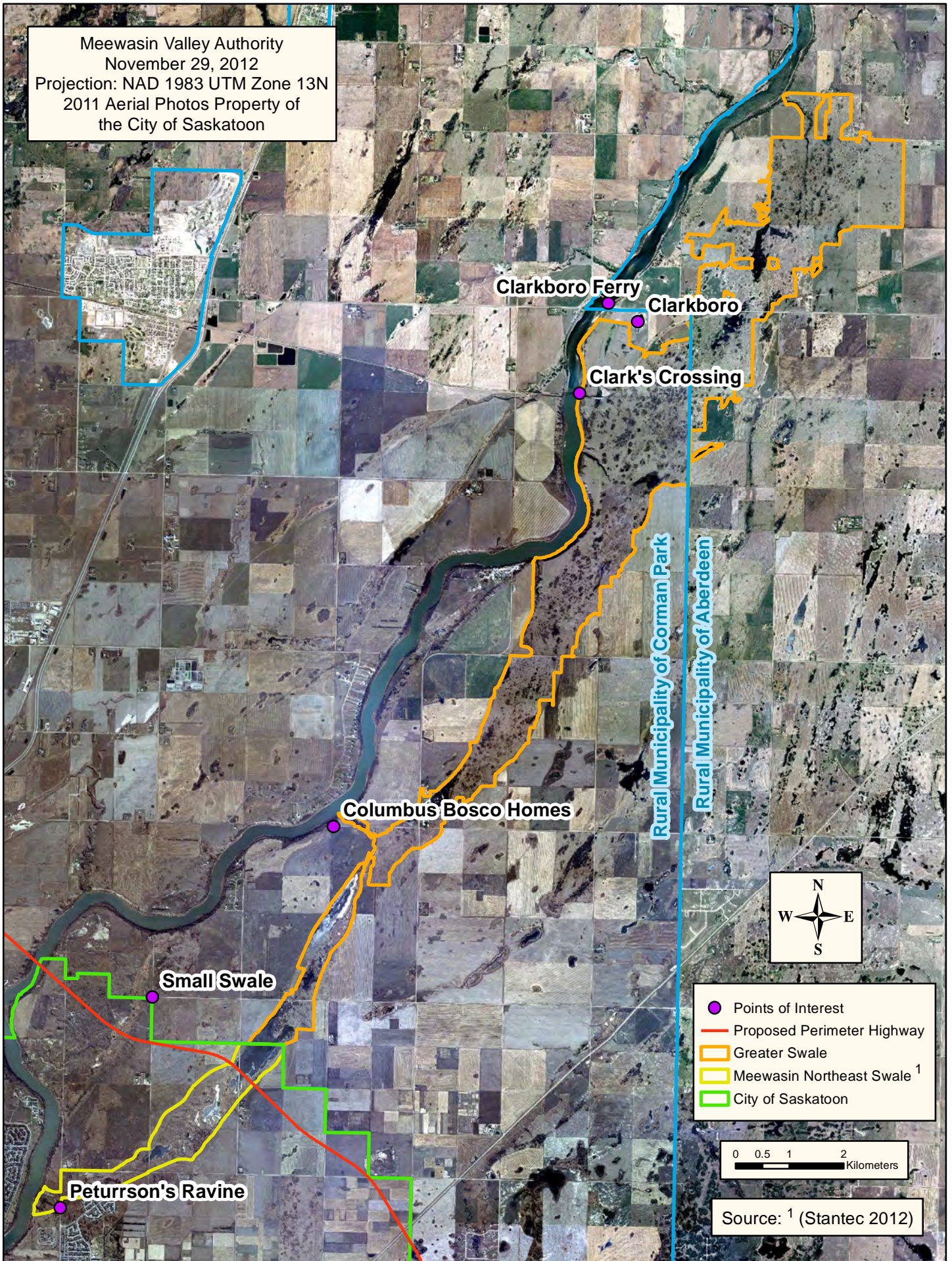
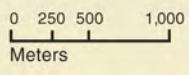


Figure A-2. Meewasin Northeast Swale Location Map



1. Peturrson's Ravine
2. Crocus Prairie
3. University of Saskatchewan Reclamation Site
4. Saskatoon Natural Grasslands



Aerial imagery courtesy of the City of Saskatoon and the Saskatchewan Geospatial Imagery Collaborative



Figure A-3. Meewasin Valley Authority Jurisdictional Boundaries (from the *Northeast Swale Resource Management Plan, 2013*)

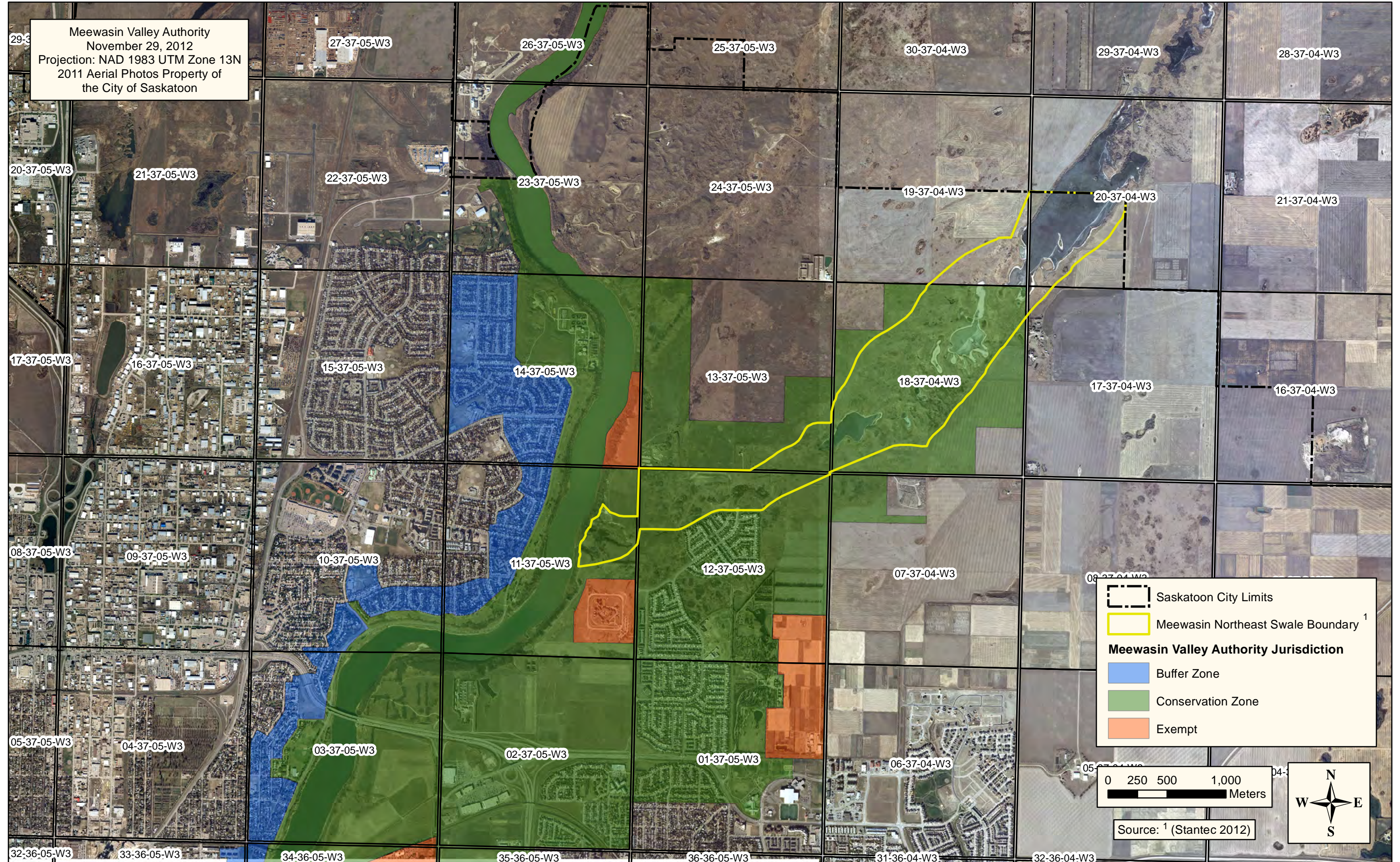
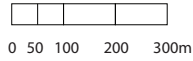


Figure A-4. Conceptual Design

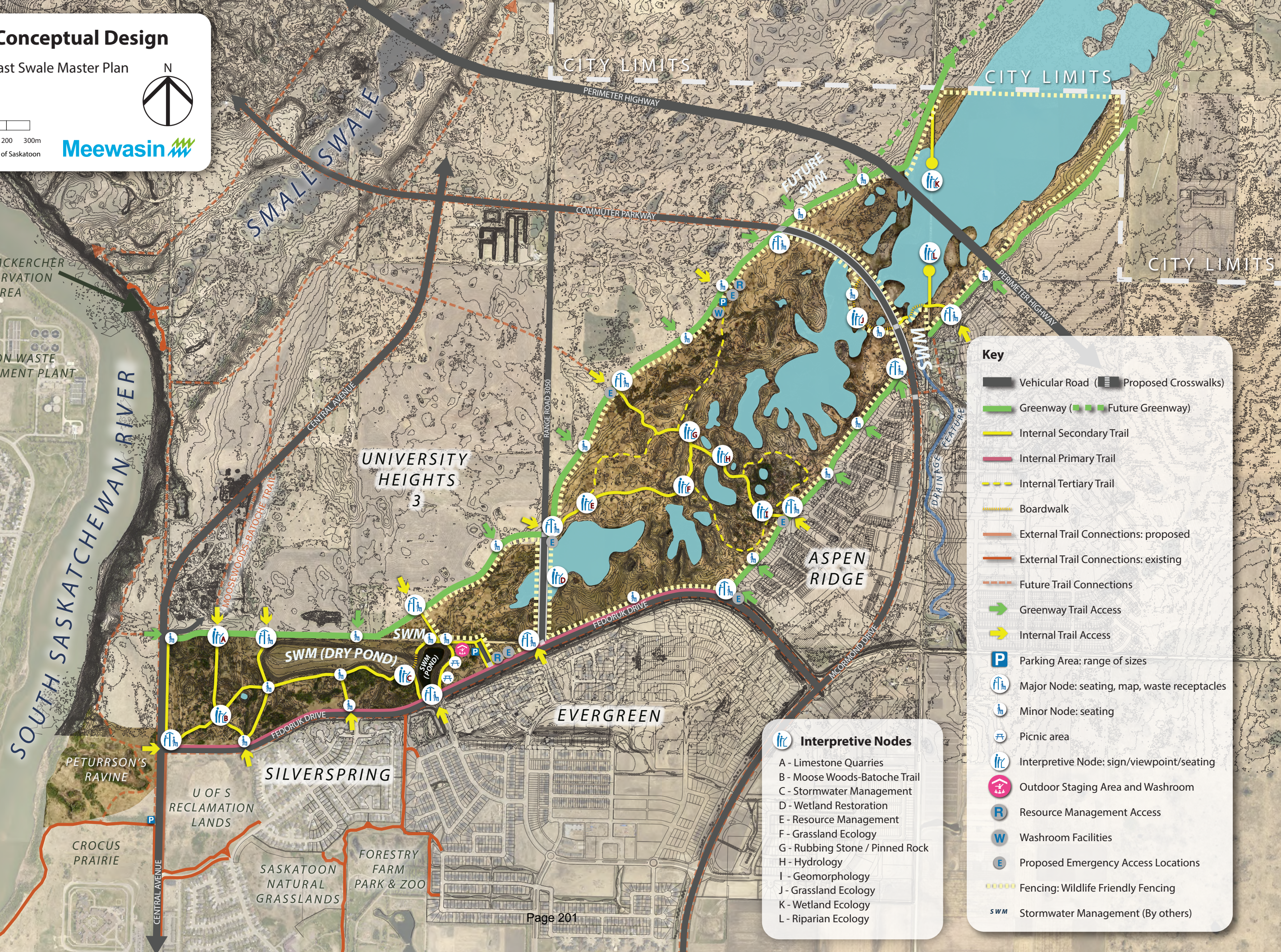
Meewasin Northeast Swale Master Plan
July 28, 2015

Contour interval: 0.5 m

Scale: 1:7500



2013 Aerial Imagery Courtesy of City of Saskatoon



Key

- Vehicular Road (Proposed Crosswalks)
- Greenway (Future Greenway)
- Internal Secondary Trail
- Internal Primary Trail
- Internal Tertiary Trail
- Boardwalk
- External Trail Connections: proposed
- External Trail Connections: existing
- Future Trail Connections
- Greenway Trail Access
- Internal Trail Access
- Parking Area: range of sizes
- Major Node: seating, map, waste receptacles
- Minor Node: seating
- Picnic area
- Interpretive Node: sign/viewpoint/seating
- Outdoor Staging Area and Washroom
- Resource Management Access
- Washroom Facilities
- Proposed Emergency Access Locations
- Fencing: Wildlife Friendly Fencing
- Stormwater Management (By others)

Interpretive Nodes

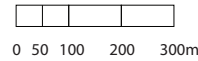
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- E - Resource Management
- F - Grassland Ecology
- G - Rubbing Stone / Pinned Rock
- H - Hydrology
- I - Geomorphology
- J - Grassland Ecology
- K - Wetland Ecology
- L - Riparian Ecology

Figure A-5. Simplified Concept

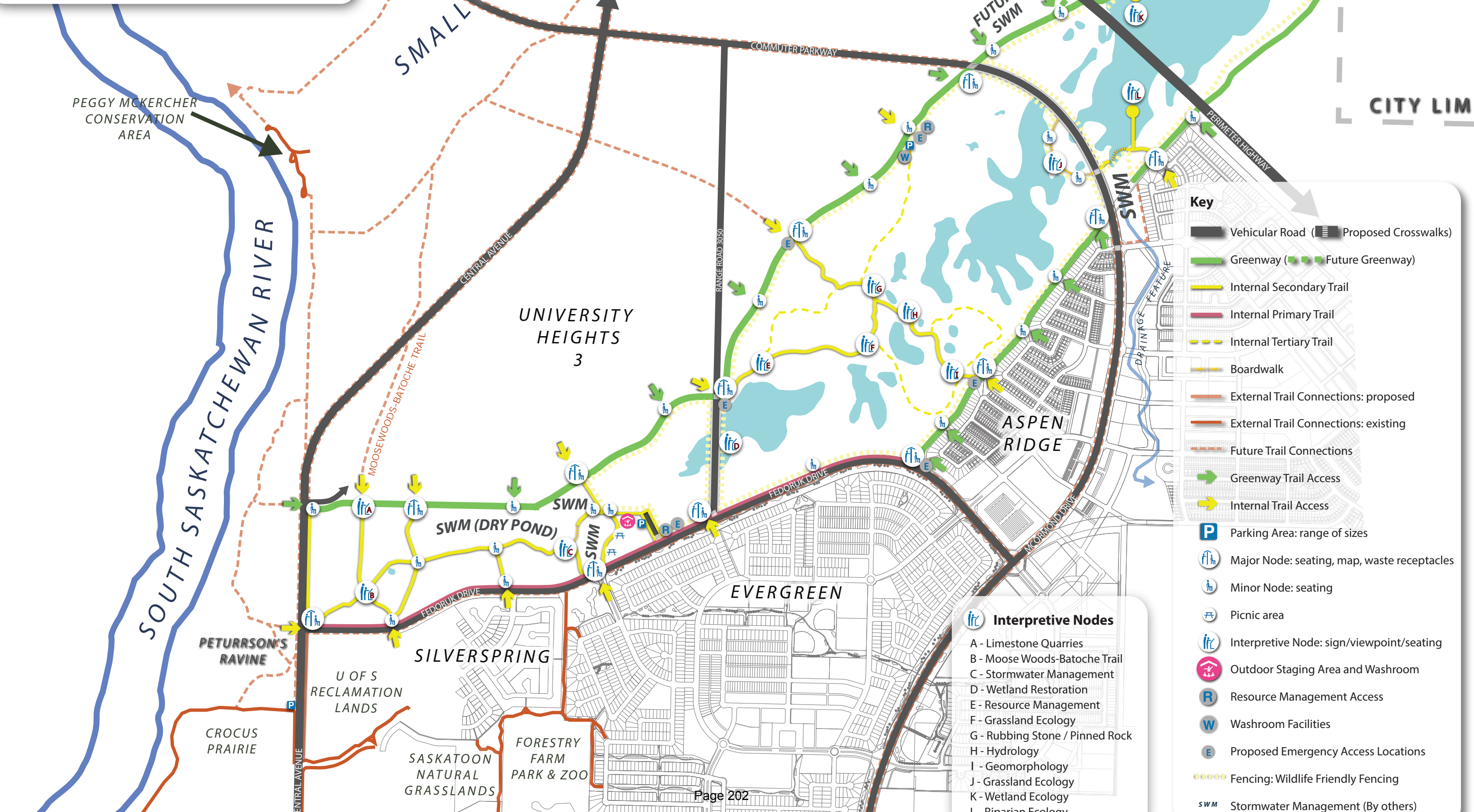
Meewasin Northeast Swale Master Plan
July 28, 2015

Contour interval: 0.5 m

Scale: 1:7500



(Simplified Conceptual Design)



Key

- Vehicular Road
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- Greenway
- Future Greenway
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Figure A-6. Meewasin Northeast Swale Master Plan Site Design Program

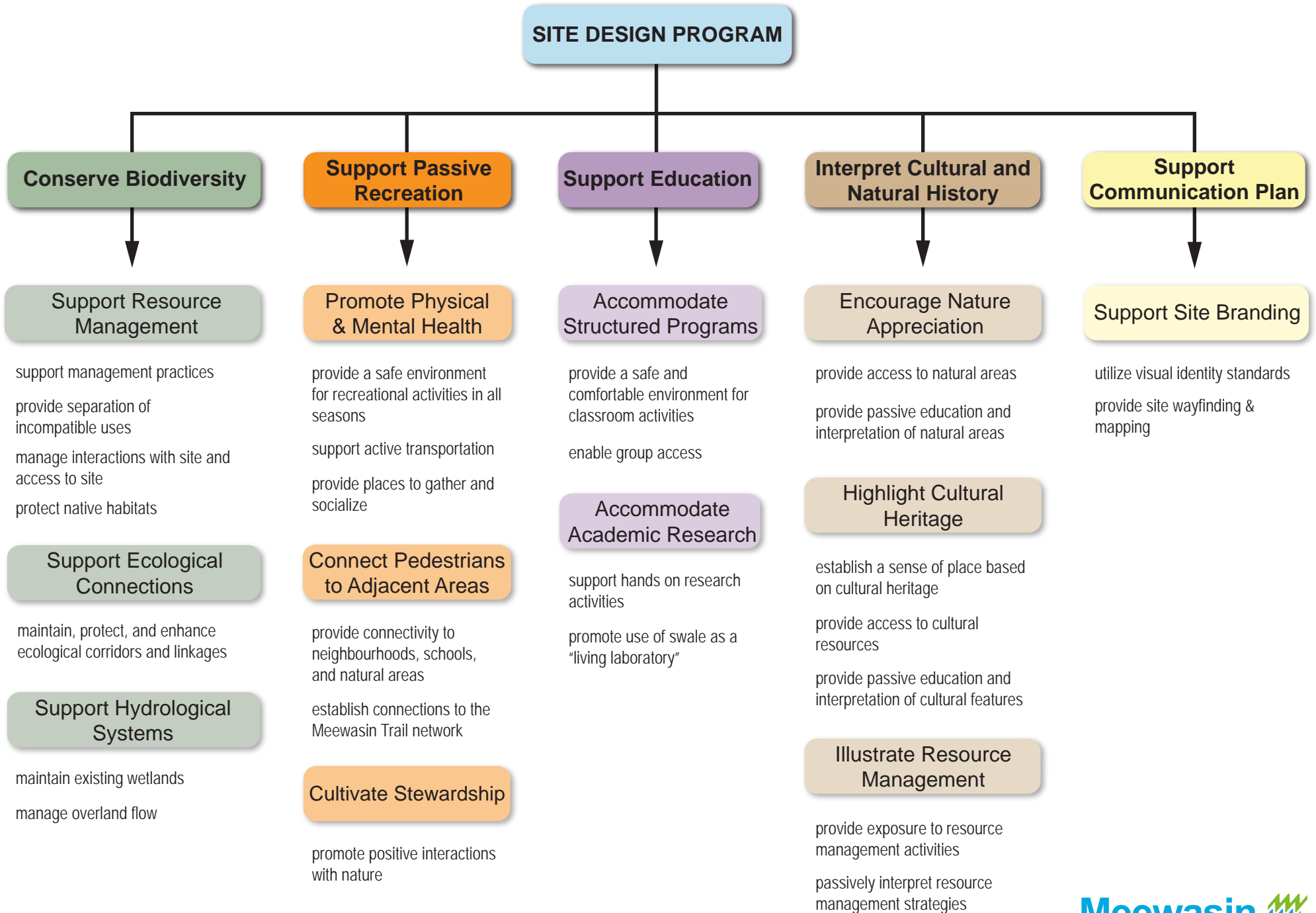
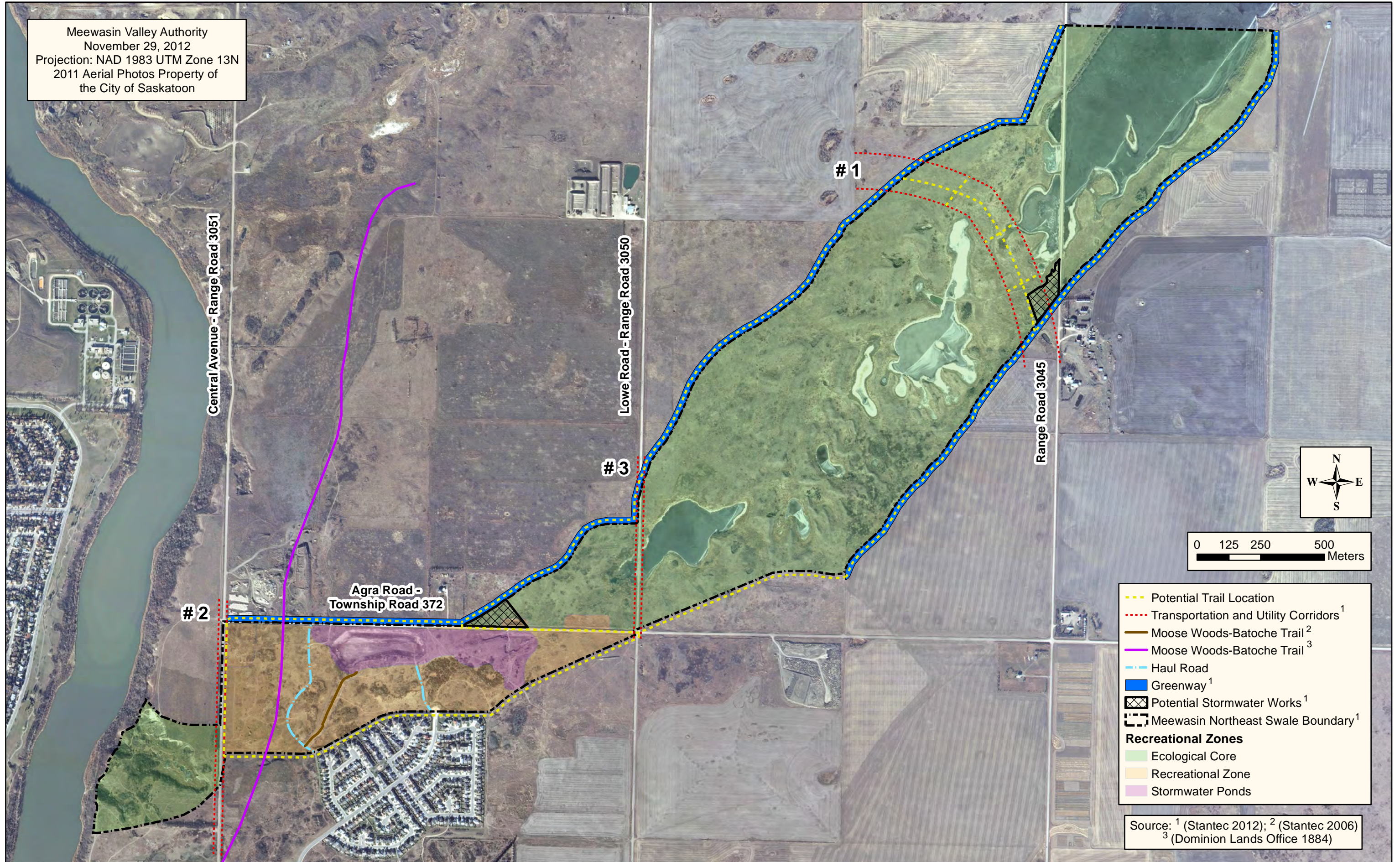


Figure A-7. The Meewasin Northeast Swale Master Plan Design Program Matrix

Feature	Conserve Biodiversity						Support Passive Recreation						Support Education				Interpret Cultural and Natural History						Support Communications Plan					
	Support Resource Management			Support Ecological Connections			Support Hydrological Systems			Promote Physical and Mental Health			Connect Pedestrians to Adjacent Areas			Cultivate Stewardship	Accommodate Structured Programs		Accommodate Academic Research		Encourage Nature Appreciation		Highlight Cultural Heritage			Illustrate Resource Management		Support Site Branding
	support management practices	provide separation of incompatible uses	manage site access	protect native habitats	maintain, protect and enhance ecological corridors and linkages	maintain wetlands	manage overland flow	provide a safe environment for recreational activities in all seasons	support active transportation	provide gathering places	provide connectivity to neighbourhoods, schools and natural areas	establish connections to Meewasin Trail network	promote positive interactions with nature	provide a safe and comfortable environment for classroom activities	enable group access	support hands on research activities	promote use of Swale as a "living laboratory"	access to natural areas	passive education and interpretation of natural areas	establish a sense of place based on cultural heritage	access to cultural resources	passive education and interpretation of cultural features	exposure to resource management activities	passively interpret resource management strategies	utilize visual identity standards	site wayfinding and mapping		
Greenway		X	X	X	X		X	X	X	X	X	X	X	X			X	X		X	X		X	X	X	X		
Greenway Access Points							X	X		X		X		X			X	X		X			X	X	X	X		
Fencing - Wildlife Friendly	X	X	X	X	X		X					X						X				X	X					
Fencing - Access Control Gates	X	X	X	X								X					X	X					X	X	X	X		
Fencing - Resource Management Gates	X	X	X	X								X				X							X	X				
Fencing - Emergency Service Access	X	X	X	X								X											X	X	X	X		
Outdoor Education Staging Area with Washroom	X						X	X	X			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X		
Parking Lots			X				X		X				X	X	X	X	X	X		X						X		
Secondary Washroom	X			X		X	X	X	X				X	X	X	X	X	X								X		
Picnic Areas							X		X			X	X	X			X	X								X		
Trail - Greenway		X	X				X	X	X	X	X	X	X	X			X	X		X	X	X	X					
Trails - In Recreation Zone / Fedoruk			X	X			X	X	X	X		X	X				X	X	X	X	X	X	X					
Trails - In Ecological Core (Secondary)		X	X	X			X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X					
Trails - In Ecological core (Tertiary)				X			X		X	X		X	X	X	X	X	X	X					X	X				
Trails - External Connections							X	X	X	X	X	X	X	X			X	X		X						X		
Boardwalks			X	X		X	X	X	X			X	X	X	X	X	X	X								X		
Node - Minor Seating							X	X	X			X	X				X	X					X	X	X			
Node - Interpretive							X	X	X			X	X				X	X	X	X	X	X	X	X	X	X		
Node - Major							X	X	X			X	X				X	X					X	X	X	X		
Ecological Core	X			X	X	X	X	X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X		X		
Recreation Zone	X			X	X	X	X	X	X	X	X	X	X		X	X	X	X	X	X	X	X	X			X		

An X indicates when a feature supports a site design program goal (yes or no - not weighted)
 The matrix assumes the full programming potential has been realised on the site.

Figure A-8. Guidelines for Development of the Meewasin Northeast Swale (from the *Northeast Swale Resource Management Plan, 2013*)



Meewasin Valley Authority
 November 29, 2012
 Projection: NAD 1983 UTM Zone 13N
 2011 Aerial Photos Property of
 the City of Saskatoon

0 125 250 500
 Meters

- Potential Trail Location
- Transportation and Utility Corridors¹
- Moose Woods-Batoche Trail²
- Moose Woods-Batoche Trail³
- Haul Road
- Greenway¹
- ▣ Potential Stormwater Works¹
- ▣ Meewasin Northeast Swale Boundary¹
- Recreational Zones**
- Ecological Core
- Recreational Zone
- Stormwater Ponds

Source: ¹ (Stantec 2012); ² (Stantec 2006)
³ (Dominion Lands Office 1884)

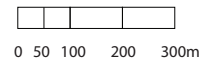
Appendix B – Construction, Costing and Phasing

Figure B-1. Phase 1

Meewasin Northeast Swale Master Plan
July 28, 2015

Contour interval: 0.5 m

Scale: 1:7500



2013 Aerial Imagery Courtesy of City of Saskatoon



PEGGY MCKERCHER
CONSERVATION
AREA

SASKATOON WASTE
WATER TREATMENT PLANT

SOUTH SASKATCHEWAN RIVER

SMALL SWALE

UNIVERSITY
HEIGHTS
3

ASPEN
RIDGE

EVERGREEN

SILVERSPRING

PETURRSON'S
RAVINE

U OF S
RECLAMATION
LANDS

CROCUS
PRAIRIE

SASKATOON
NATURAL
GRASSLANDS

FORESTRY
FARM
PARK & ZOO

CITY LIMITS

CITY LIMITS

CITY LIMITS

Key

- Vehicular Road
- Proposed Crosswalks
- Greenway (Future Greenway)
- Internal Secondary Trail
- Internal Primary Trail
- Internal Tertiary Trail
- Boardwalk
- External Trail Connections: proposed
- External Trail Connections: existing
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- Washroom Facilities
- Proposed Emergency Access Locations
- Fencing: Wildlife Friendly Fencing
- Stormwater Management (By others)

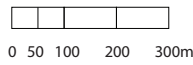
Interpretive Nodes

- A - Limestone Quarries
- B - Moose Woods-Batoche Trail
- C - Stormwater Management
- D - Wetland Restoration
- E - Resource Management
- F - Grassland Ecology
- G - Rubbing Stone / Pinned Rock
- H - Hydrology
- I - Geomorphology
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- L - Riparian Ecology

Figure B-2. Phase 2

Meewasin Northeast Swale Master Plan
July 28, 2015

Contour interval: 0.5 m
Scale: 1:7500



2013 Aerial Imagery Courtesy of City of Saskatoon



PEGGY MCKERCHER CONSERVATION AREA
SASKATOON WASTE WATER TREATMENT PLANT

SOUTH SASKATCHEWAN RIVER

SMALL SWALE

UNIVERSITY HEIGHTS 3

ASPEN RIDGE

EVERGREEN

SILVERSPRING

PETURRSON'S RAVINE

U OF S RECLAMATION LANDS

CROCUS PRAIRIE

SASKATOON NATURAL GRASSLANDS

FORESTRY FARM PARK & ZOO

CITY LIMITS

CITY LIMITS

CITY LIMITS

PERIMETER HIGHWAY

COMMUTER PARKWAY

CENTRAL AVENUE

RANGE ROAD 3050

FEDORUK DRIVE

SWM (DRY POND)

FEDORUK DRIVE

MCORMOND DRIVE

SWM

SWM

SWM

SWM

SWM

Key

- Vehicular Road
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Interpretive Nodes

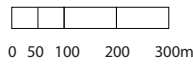
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Figure B-3. Phase 3

Meewasin Northeast Swale Master Plan
July 28, 2015

Contour interval: 0.5 m

Scale: 1:7500



2013 Aerial Imagery Courtesy of City of Saskatoon



PEGGY MCKERCHER
CONSERVATION
AREA

SASKATOON WASTE
WATER TREATMENT PLANT

SOUTH SASKATCHEWAN RIVER

SMALL SWALE

UNIVERSITY
HEIGHTS
3

ASPEN
RIDGE

EVERGREEN

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CITY LIMITS

CITY LIMITS

CITY LIMITS

PERIMETER HIGHWAY

COMMUTER PARKWAY

CENTRAL AVENUE

RANGE ROAD 3050

FEDORUK DRIVE

MCORMOND DRIVE

SWM (DRY POND)

SWM (POND)

Interpretive Nodes

- A - Limestone Quarries
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Key

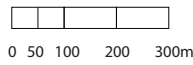
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- Fencing: Wildlife Friendly Fencing
- Stormwater Management (By others)

Figure B-4. Phase 4

Meewasin Northeast Swale Master Plan
July 28, 2015

Contour interval: 0.5 m

Scale: 1:7500



2013 Aerial Imagery Courtesy of City of Saskatoon



PEGGY MCKERCHER CONSERVATION AREA
SASKATOON WASTE WATER TREATMENT PLANT

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SMALL SWALE

UNIVERSITY HEIGHTS 3

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CITY LIMITS

CITY LIMITS

CITY LIMITS

PERIMETER HIGHWAY

COMMUTER PARKWAY

CENTRAL AVENUE

RANGE ROAD 3050

FEDORUK DRIVE

SWM (DRY POND)

SWM (POND)

FEDORUK DRIVE

MCORMOND DRIVE

DRAINAGE FEATURE

Key

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Interpretive Nodes

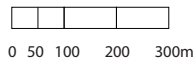
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Figure B-5. Phase 5

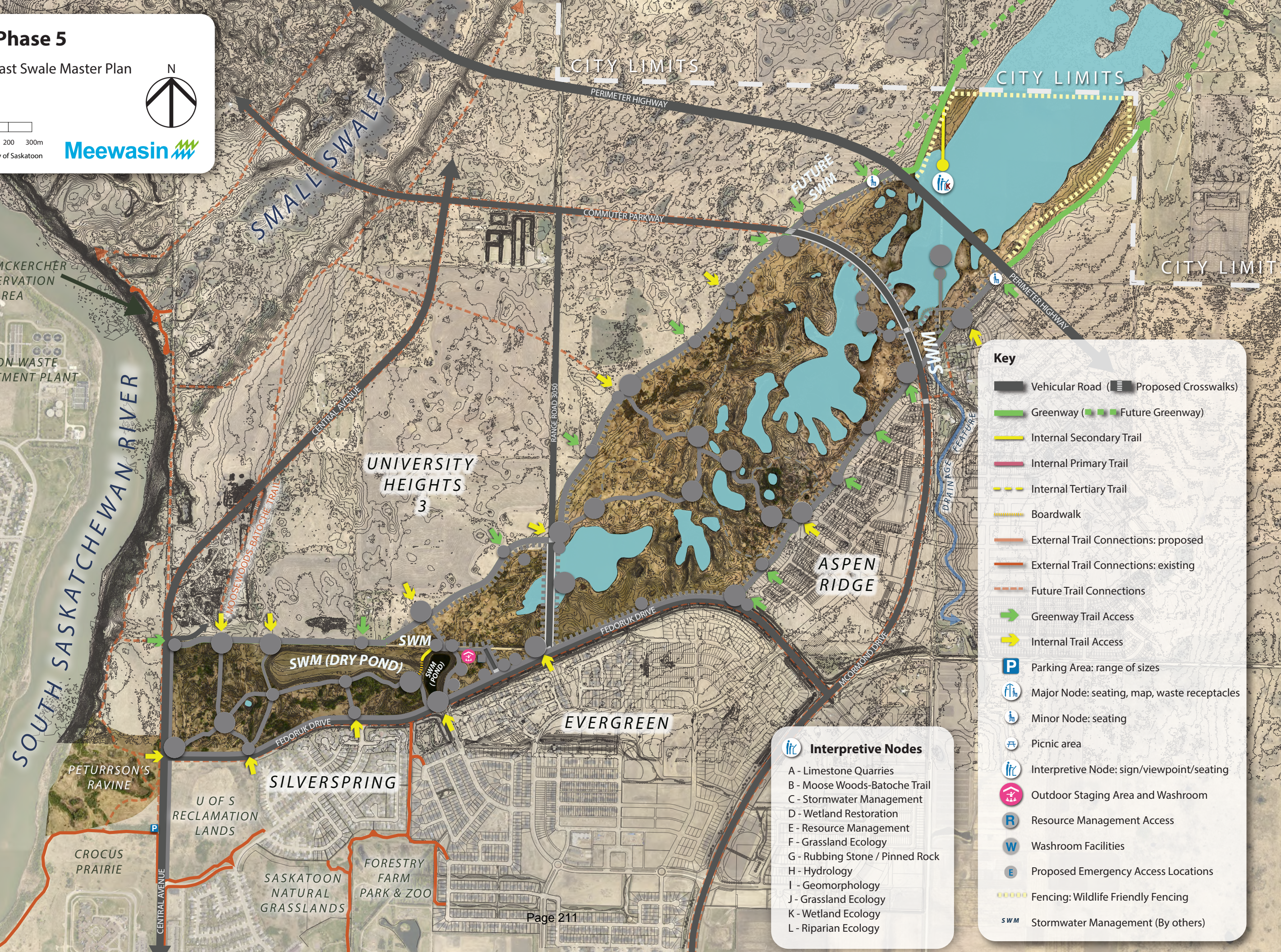
Meewasin Northeast Swale Master Plan
July 28, 2015

Contour interval: 0.5 m

Scale: 1:7500



2013 Aerial Imagery Courtesy of City of Saskatoon



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Figure B-6 Meewasin Northeast Swale Master Plan - Overall Capital Cost Opinion

*Phasing	Description (See Figures B-1 to B-5 for more detail)	Capital (Swale)	Capital (Greenway)	**Optional Maintenance (1YR Beyond FAC)
Phase1	<ul style="list-style-type: none"> Fence the Ecological Core Trail along south edge of the Swale adjacent to Fedoruk Drive Trail connection along Central Avenue Meewasin Trail Trails within the Recreation Zone Resource Management and emergency services access points (north and south) Interpretive and seating (major/minor) nodes in the the Recreation Zone and Ecological Core 	\$ 2,434,079.70	No Greenway Development Phase 1	\$ 50,000.00
Phase2	<ul style="list-style-type: none"> South Greenway along Aspen Ridge neighbourhood Mow north Greenway bordering University Heights 3 neighbourhood Internal secondary trail in the Ecological Core from Interpretive nodes in the Ecological Core along the secondary trails (east-west / north-south) Major seating nodes adjacent to the secondary trails (east-west / north-south) in the Ecological Core Minor Seating nodes adjacent to Aspen Ridge neighbourhood 	\$ 522,007.20	\$ 1,459,200.00	\$ 50,000.00
Phase3	<ul style="list-style-type: none"> North Greenway Trails in Recreation Zone Internal tertiary trails in the Ecological Core Interpretive and seating (major/minor) nodes between Central Avenue and North Commuter Parkway Parking for Outdoor Education Staging Area Trail along Central Avenue, north of Fedoruk Drive to University Heights 3 Picnic areas 	\$ 1,205,907.75	\$ 3,964,800.00	\$ 50,000.00
Phase4	<ul style="list-style-type: none"> Fencing between the North Commuter Parkway and Perimeter Highway Greenway between the North Commuter Parkway and Perimeter Highway Feature lookout from the North Commuter Parkway in the Ecological Core Trail development Interpretive and seating (major/minor) nodes along the feature lookout and internal trails North washroom North parking lot 	\$ 1,118,650.50	\$ 1,190,400.00	\$ 50,000.00
Phase5	<ul style="list-style-type: none"> Fencing from Perimeter Highway to City limits Greenway from Perimeter Highway to City limits Boardwalk in Recreation Zone Outdoor Education Staging Area Trail along Range Road 3045, between Perimeter Highway and City limits Interpretive node between Perimeter Highway and City limits Seating nodes (minor) along Perimeter Highway 	\$ 797,502.24	\$ 1,344,000.00	\$ 50,000.00
Total		\$ 6,078,147.39	\$ 7,958,400.00	\$ 250,000.00

* Currently there is no scheduled timeline for the start of construction, and therefore work is set out in phases rather than distributed over years.

** It is recommended that construction maintenance be continued one year post FAC after which time the City would assume maintenance operations

Note: this is based on the concept plan and is subject to change during detail design

Figure B-6 Meewasin Northeast Swale Master Plan - Ongoing Operational Costs

*Phasing	*Resource Management	Communication Plan	Education Plan	Operating Budget (By others)
Year 1	\$ 186,443.00	\$ 15,000.00	\$ 50,000.00	
Year 2	\$ 134,400.00	\$ 15,000.00	\$ 60,000.00	
Year 3	\$ 144,240.00	\$ 15,000.00		
Year 4	\$ 147,543.00	\$ 15,000.00		
Year 5	\$ 165,125.00	\$ 15,000.00		
Total	\$ 777,751.00	\$ 75,000.00	\$ 110,000.00	\$ -

* Resource management costs are not affected by construction, and therefore this work can begin as early as 2016. These are annual costs for a five year period.

Note: this is based on the concept plan and is subject to change during detail design

Figure B-7. Meewasin Northeast Swale Construction Phases

Quantity Matrix

Features	Greenway (acres)	Greenway Lighting (each)	Primary Trails (l.m.) (Asphalt)	Secondary Trails (l.m.) (Crusher Dust)	Tertiary Trails (l.m.) (Mown)	Boardwalk with railing (l.m.)	Wildlife Friendly Fencing (l.m.)	Resource Management Access (each)	Emergency Services Access (each)	Benches (each)	Trash Unit (each)	Trail Wayfinding Maps (each)	Secondary Trail Wayfinding Maps (each)	Picnic Tables (each)	Picnic Shelter (each)	Interpretive Sign (each)	Parking Lot (sq m)	Bollards (Timber /Knock Down)	Washroom	Outdoor Education Staging Area
Phase 1																				
Greenway (Storm water / trails / planting / lighting)																				
Recreation Zone				2566														5		
Ecological Core							7550	2	6											
Trail Widening (Fedoruk Road)			2570																	
Trails outside the Swale			475																	
Major Seating Nodes (4)										8	4	3	1							
Minor Seating Nodes (5)										10			3							
Interpretive Nodes (2)										2						2				
Subtotal			3045	2566			7550	2	6	20	4	3	4			2				
Phase 2																				
Greenway (Storm water / trails / planting / lighting)	8	42	1250		1250															
Recreation Zone																				
Ecological Core				2080																
Major Seating Nodes (3)										6	2	2								
Minor Seating Nodes (3)										6			3							
Interpretive Nodes (5)										5						5				
Subtotal	8	42	1250	2080						17	2	2	3			5				
Phase 3																				
Greenway (Storm water / trails / planting / lighting)	21	116	3500																	
Recreation Zone			485	1450													1600	80		
Ecological Core					1275															
Major Seating Nodes (2)										4	2	2								
Minor Seating Nodes (8)										16	5		3							
Interpretive Nodes (2)										2						2				
Picnic Nodes (2)											2			4	1					
Subtotal	21	116	3985	1450	1275					22	9	2	3	4	1	2	1600			
Phase 4																				
Greenway (Storm water / trails / planting / lighting)	6	35	1055																	
Recreation Zone																				
Ecological Core				150		450	1600		1								800	40	1	
Major Seating Nodes (3)										6	3	3								
Minor Seating Nodes (3)										5	2									
Interpretive Nodes (2)										2						2				
Subtotal	6	35	1055	150		450	1600		1	13	5	3	0			2	800		1	
Phase 5																				
Greenway (Storm water / trails / planting / lighting)	7	38	1140																	
Recreation Zone				74		100														
Ecological Core						250	2090	1											1	1
Major Seating Nodes																				
Minor Seating Nodes (2)										4	2		2							
Interpretive Nodes (1)										1						1				
Facility																				
Subtotal	7	38	1140	74		350	2090			5			2						1	1
TOTALS (ALL PHASES COMBINED)	42	231	10475	6320	1275	800	11240	2	7	77	20	10	12	4	1	11	2400		2	1

* Note: this is based on the concept plan and is subject to change during detail design

**Appendix C – Terms of Reference for the Meewasin Northeast Swale
Master Plan**

Terms of Reference for

Meewasin Northeast Swale Master Plan
Meewasin Valley Authority

Meewasin Valley Authority
April 23, 2014

TERMS OF REFERENCE

1. OVERVIEW

Meewasin is proposing to prepare a Master Plan for the Northeast Swale to plan and design the physical space, including pathways and amenities, within the Northeast Swale. The plan will support resource management, education and recreation uses consistent with the *Northeast Swale Resource Management Plan* and the *Northeast Swale Development Guidelines* (Stantec, 2012).

2. BACKGROUND

Saskatoon's Northeast Swale is an ancient river channel. It begins at Peturrson's Ravine and carves a 26 km long path adjacent to the South Saskatchewan River. It is one of the largest pieces of unbroken prairie, riparian forest and wetlands in the Saskatoon region with patches of rare fescue grassland. The swale is home to several rare and endangered and culturally significant plant and animal species.

The Northeast Swale and the Greater Swale has signs of human habitation and use over the past several centuries, including a remnant section of the Moose Woods-Batoche Trail, Middleton's staging camp on the on the trek to the Battle of Batoche, the site of the telegraph line that linked North America to Europe by way of Russia, and tipi rings from the encampments of the original residents of the Saskatoon area. More recent archeological remains are the lime kilns near the swale and a hole left by the movement of large limestone boulders used to build parts of the University of Saskatchewan.

In 2012, Meewasin collaborated with the City of Saskatoon and a wide variety of stakeholders throughout the community to develop a plan to integrate this sensitive natural area into an urban context. The *Northeast Swale Resource Management Plan* includes appropriate passive recreational activities, education and research opportunities. It also describes management techniques appropriate to a natural area including burning and grazing to manage invasive species and protect and enhance biodiversity.

The *Northeast Swale Development Guidelines (2012)*, a companion document, is intended to ensure minimal impact from development within and adjacent to it. Meewasin played a key role in its development working with the City of Saskatoon, a consultant and experts from the community. The Guidelines have been adopted by both Meewasin Board of Directors and Saskatoon City Council.

3. SCOPE OF WORK

Meewasin requires the following work to be completed as part of this project:

- Establishment of Steering and Technical Committees and planning for stakeholder involvement to support the development of the plan;
- Coordination and facilitation of 8 meetings with the Technical Advisory Committee, 4 meetings with the Steering Committee and multiple meetings with stakeholder groups as required.
- Planning and execution of full day design charrette, resulting in the development of document summarizing the results of the charrette. A presentation of the design options resulting from the charrette will be given to the Technical Advisory Committee to provide guidance for finalization.
- Development of a comprehensive site plan for the Northeast Swale. The plan must outline improvements in the ecological core and the greenway and include site design drawings. It must consider adjacent uses and access management and describe the rationale and intent of design features.
- Presentation of a draft site plan to the Meewasin Management Team, Meewasin Advisory Committees, and to the Meewasin Board of Directors.
- Development of communications and branding strategy for the Northeast Swale to effectively communicate importance of the swale and the activities taking place there. The strategy will also include a proposed name for the site. An approach to include stakeholder input will be incorporated.
- Development of an implementation appendix that includes cost estimates, funding strategy options for capital construction, an approach for phased construction, and a description of ongoing site maintenance requirements.
- Development of a presentation on the comprehensive site plan, the communications and branding strategy, and the implementation appendix. The presentation will be given to Meewasin committees, the Meewasin Board of Directors, the Planning and Operations Committee at the City of Saskatoon and City council for input and approval.
- The final report and presentation must be completed by February, 2015

4. DELIVERABLES

Report, Drawings and Presentation Material defining the following:

Comprehensive Site Plan:

- Overall site plan and proposed improvements in the ecological core, buffer zones, and recreation zones;
- Greenway and trail design including pathways, site access and connectivity to adjacent land uses adjacent, neighborhoods and crossing features;
- Site grading, drainage and storm water management;
- Amenity design for safety and comfort, including site furniture and signage;
- Site design to support resource management, interpretation and programming;

Education and Communication Strategy:

- Education, interpretation and programming opportunities;
- Communication strategy including naming and branding;

Implementation Appendix:

- Cost estimate for capital construction including quantities and unit costs;
- Proposed funding strategy options for capital construction;
- Implementation plan including proposed phasing;
- Site maintenance plan and recommendations.

5. ROLES

Project Team:	Project Manager	Alan Otterbein
	Project Manager	Mike Velonas
	Project Designer	Nola Stein
	Design Assistant	Jonathan Behnke
	GIS Specialist	Sarina Gersher
	Project Coordinator	Erin Akins
	Interpretive Advisor	Doug Porteous
	Special Advisor	Colleen Yates

Steering Committee

City of Saskatoon:	Planning and Development	Alan Wallace
	Recreation and Sport	Cary Humphrey
	Parks	Darren Crilly
	Enviro & Corporate Initiatives	Brenda Wallace
	Saskatoon Land	Frank Long
Meewasin:	Design and Development	Allan Otterbein
	Planning and Conservation	Mike Velonas
	Community Development	Doug Porteous

Technical Advisory Committee

City of Saskatoon:	Long Range Planning	Terry Fusco
	Education and Enviro Performance	Amber Jones

	Land Development	Tyson McShane
	Storm Water Management	Andrew Hildebrandt
	Safe Growth/CPTED	Elisabeth Miller
	Recreation and Sport	Elan Ballantyne
	Parks	Gerald Rees
Meewasin:	Design and Development Manager	Alan Otterbein
	Project Designer	Nola Stein
	Design Assistant	Jonathan Behnke
	Project Coordinator	Erin Akins
	Conservation Officer	Luc Delanoy
	GIS Specialist	Sarina Gersher
	Interpretive Lead	Doug Porteous
	Special Advisor	Colleen Yates
	Conservation Advisory Committee	Peter Goode
Other:	Swale Watcher	Stan Shadick
	Swale Watcher	Barb Hanbridge
	Soil and hydrology specialist	Les Henry
	Metis cultural interpretation	Fiji Robinson
	Saskatoon Heritage Society	Claire Bullaro
	Archeological specialist	Butch Amundson

6. RESPONSIBILITIES – PROJECT TEAM

Project Managers are accountable for:

- 1) Achievement of project milestone and deliverables
- 2) Budgeting and allocation of project resources
- 3) Managing risks and resolving conflicts
- 4) Providing direction to project team and committees
- 5) Obtaining project approval by Meewasin Board and Saskatoon City Council
- 6) Handling all project related media relations

The Project Team is accountable for:

- 1) Coordinating Steering and Technical Committees
- 2) Researching and summarizing relevant information
- 3) Managing project communications and materials
- 4) Planning and implementing stakeholder engagement
- 5) Technical support including GIS, mapping, and design
- 6) Report writing including, content and editing
- 7) Development of the communications and branding strategy
- 8) Being responsive to committee input and direction

7. RESPONSIBILITIES – STEERING COMMITTEE

The Steering Committee will:

- 1) Provide advice and direction to the Project Team on the Master Plan;
 - i. represent their respective organization’s views, ensuring that the plan progresses in a manner consistent with the interests of their organization.
 - ii. Serve as a link to the communities of interest which they represent and ensure that these communities are made aware of the progress on the Master Plan;
 - iii. provide direction with the intention of meeting the goals outlined in the *Northeast Swale Resource Management Plan (2013)* and the *Northeast Swale Development Guidelines (Stantec, 2012)*.
- 2) Undertake initial reviews of major accomplishments and reports prepared by the Meewasin Valley Authority and the Technical Advisory Committee;
- 3) Provide guidance on stakeholder engagement throughout the project;
- 4) Meet on a periodic basis (approximately 4 times), face-to-face, in Saskatoon.

A tentative meeting schedule is proposed as follows but could be revised depending on progress of the project:

Meeting #1	Apr 17, 1:30-3:00 pm
Meeting #2	Jul 15, 1:30-3:00 pm
Meeting #3	Nov 18, 1:30-3:00 pm
Meeting #4	Jan 20, 1:30-3:00 pm

8. RESPONSIBILITIES – TECHNICAL ADVISORY COMMITTEE

The Technical Advisory Committee will:

- 1) Serve as a link to the communities of interest or organizations which they represent and ensure that these communities are made aware of the progress on the Master Plan;
- 2) Bring forward any issues or recommendations to the Project Team and to the Steering Committee based on professional knowledge and expertise;
- 3) Undertake reviews of major reports once they have been vetted by the Steering Committee and provide advice to the Project Team
- 4) Meet on a monthly basis, face-to-face, in Saskatoon.

A tentative meeting dates schedule is proposed as follows but could be revised depending on progress of the project:

Meeting #1	April 16, 1:30-3:00 pm
Meeting #2	June 5, 1:30-3:00 pm
Meeting #3	July 10, 1:30 -3:00 pm
Meeting #4	Aug 28, 1:30-3:00 pm
Meeting #5	Sept 25, 1:30- 3:00 pm
Meeting #6	Oct 30, 1:30-3:00 pm

Meeting #7 Nov 27, 1:30-3:00 pm
 Meeting #8 Jan 15, 1:30- 3:00pm

9. PROJECT MILESTONES

Week of	2014
Mar 17	Confirm committee representation
April 16	Technical Advisory Committee meeting #1
April 17	Steering Committee meeting #1
May- June	Meetings with key stakeholders
June 5	Technical Advisory Committee meeting #2
July 10	Technical Advisory Committee meeting #3
July 15	Steering Committee meeting #2 (Review Preliminary Draft)
Aug 28	Technical Advisory Committee meeting #4
Sept 25	Technical Advisory Committee Meeting #5
Oct 30	Technical Advisory Committee Meeting #6
Oct	Stakeholder meetings and public open house
Nov 18	Steering Committee Meeting #3 (Review 75% draft)
Nov 20	Presentation to Development Review
Nov 27	Technical Advisory Committee meeting #7
Week of	2015
Jan 15	Technical Advisory Committee meeting #8
Jan 20	Steering Committee Meeting #4 (Review Final Draft)
Feb 16	Presentation to Meewasin Development Review Committee Presentation to City of Saskatoon CPTED Committee
Mar 2	Presentation to Meewasin Board for Approval
April	Presentation to City of Saskatoon P&O Committee
April	Presentation to City Council

10. STAKEHOLDER ENGAGEMENT

Phase 1 Engagement (Input): May 1-28

- Workshop sessions with key stakeholders groups

Phase 2 Engagement (Review): October 6-20

- Stakeholder meetings to review 75% draft
- Public open house to seek input on 75% draft
- Draft document is posted online for comment

Stakeholder Groups:

- General Public

- City of Saskatoon:
 - Leisure Facilities (Recreation and Sport)
 - Facilities Maintenance (for park sites)
 - Parks - Maintenance (Naturalization)
 - Parks - Open Space Design
 - Strategic Services (Storm water)
 - Community Development
 - Neighbourhood Services

- RM Corman Park
- Provincial Government – Perimeter Highway Planning

- Environmental: Swale Watchers, Saskatoon Nature Society, Native Plant Society of Saskatchewan, Saskatchewan Environmental Society, Ducks Unlimited, Saskatoon Wildlife Federation
- Recreation: Saskatoon Cycles, Nordic Ski Club, Saskatoon Road Runners Association; Trail Rastas, off-leash dogs and ATV or Snowmobile groups
- Aboriginal: First Nations and Metis
- Education: School boards, University of Saskatchewan, SIAST
- Community Associations: Silverspring, Evergreen, Aspen Ridge, University Heights 3 (UH3)

11. CONTEXT DOCUMENTS

Plans:

The Northeast Swale Resource Management Plan, 2013
The Northeast Swale Development Guidelines, 2012
Meewasin Valley Trail System Plan -1990
Meewasin Valley Interpretive Concept - 1984
The Meewasin Valley Project – 100 Year Conceptual Master Plan, 1981

Policy, legislation, standards and regulations:

The Planning and Development Act, 2007
The Subdivision Regulations
The Statements of Provincial Interest Regulations, 2003
The Weed Control Act
The Environmental Management and Protection Act

The Highways and Transportation Act, 1997
The Meewasin Valley Authority Act

The City of Saskatoon Official Community Plan
The City of Saskatoon Zoning Bylaw
The City of Saskatoon's proposed wetland policy

The Saskatoon Planning District Official Community Plan
The Saskatoon Planning District Zoning Bylaw

New Neighborhood Design and Development Standards Manual, City of Saskatoon
City of Saskatoon Park Development Guidelines and Construction Specifications
University Heights Sector Plan

Meewasin Valley Authority - Northeast Policy

12. BUDGET

Costs associated with developing the Master Plan and design will be assumed by Meewasin.

13. CONFIDENTIALITY

As part of discussions supporting this project, proprietary information may be disclosed. Any unauthorized release or carelessness in the handling of confidential or proprietary information is considered a breach of the duty to maintain confidentiality.

NORTHEAST SWALE ESTIMATED OPERATING IMPACT

PROJECT #: Northeast Swale

ITEM DESCRIPTION	Phase 1	Phase 2	Phase 2 Cumulative	Phase 3	Phase 3 Cumulative	Phase 4	Phase 4 Cumulative	Phase 5	Phase 5 Cumulative
Saskatoon Light & Power (Greenway Lighting)	\$ -	\$ 7,137	\$ 7,137	\$ 19,711	\$ 26,847	\$ 5,947	\$ 32,795	\$ 6,457	\$ 39,252
Facilities	\$ 15,525	\$ 5,300	\$ 20,825	\$ 18,000	\$ 38,825	\$ 29,910	\$ 68,735	\$ 32,885	\$ 93,620
Parks	\$ 19,034	\$ 13,940	\$ 32,974	\$ 13,400	\$ 46,374	\$ 6,706	\$ 53,080	\$ 23	\$ 53,103
TOTAL	\$ 34,559	\$ 26,376	\$ 60,935	\$ 51,111	\$ 112,046	\$ 42,563	\$ 154,610	\$ 39,365	\$ 185,975

4131-5, x 4205-5

From: candace savage <candace.savage@sasktel.net>
Sent: October 01, 2015 9:23 AM
To: Web E-mail - City Clerks
Subject: Request to speak to Standing Policy Committee on Planning, Development, & Community Services
Attachments: Future of Swale.pdf

I would like to speak to the Committee on Planning, Development and Community Services on Monday, October 5, with reference to Item 7.2.7 Meewasin Valley Authority Northeast Swale Master Plan.

I am attaching a document for circulation to the committee.

Candace Savage
302 Albert St
S. 400m, SK
S7N 1G1

Pause to Consider Future of Swale

By Candace Savage

Monday is decision time for Saskatoon's signature natural area, the Northeast Swale. The Swale, in case you don't know much about it, is a 26-kilometer-long expanse of wind-swept prairie and species-rich wetlands that begins at the river, just north of Sutherland on Central Avenue, and then sweeps east across city limits into the adjoining countryside. In its entirety, the Swale is one of the largest surviving remnants of ancient grassland in central Saskatchewan, making it a critical refuge for the prairie's many rare and threatened species.

Within the 300 hectares (740 acres) of this resource that lie within city limits, the Swale provides habitat for an astonishing diversity of life, including more than 100 kinds of birds, over 200 different species of plants, and dozens of animals large and small. There are badgers out there, and coyotes and rabbits, and a herd of fifty deer. It's the go-to place if you want to see the endangered crowfoot violet, with its intense blue blossoms and feathery leaves, or to hear the weird wet-balloon-on-a-wall creaking of the endangered northern leopard frog.

The Northeast Swale is a biodiversity hot spot, Saskatoon's answer to Vancouver's Stanley Park, Calgary's Nose Hill and Toronto's Rouge Valley.

So how can it be that this ecologically important landscape — a place that everyone agrees is a priority for conservation -- is soon to be bisected by two major traffic arteries within a stone's throw of each other? One of these projects (being undertaken by the City) was formerly known as the North Commuter Bridge but has recently been redubbed the North Commuter Parkway, as if a shift in terminology

could mitigate the damage caused by routing an arterial roadway through a natural treasure. The second major roadway (being built by the Province) will cross the Swale just *one kilometer* further east, as part of a perimeter highway around the city. This pile-up is the result of delay, conflicting agendas and a breakdown of communication.

The price tag on the Parkway is \$211.4 million; the bypass comes in at a cool \$2 billion. The cost of this extravagant duplication to the ecological health of the Swale -- # of animals lost as road kill, # of invasive species introduced, % of peace and quiet lost – cannot be tallied in dollars and cents.

Although design details for the perimeter highway have yet to be released, plans for the Parkway include speed limits and other minor refinements to reduce collisions between wildlife and cars. Apart from providing underground passages for small critters to hop and slither through, the idea of separated crossings for wildlife was dismissed early on, as unduly expensive.

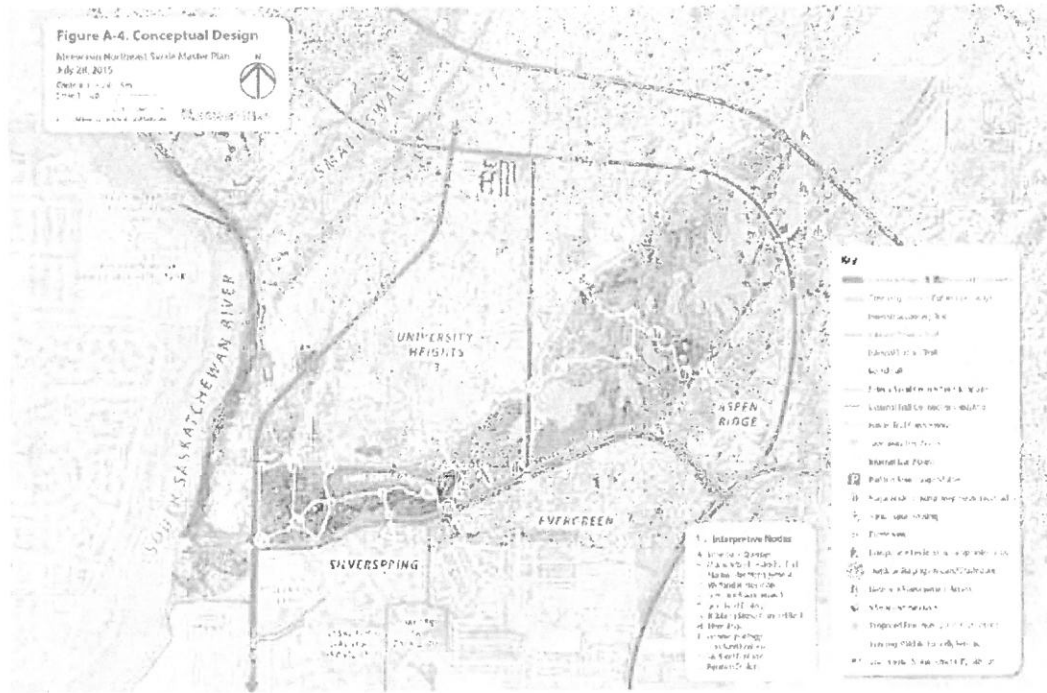
The organization that is responsible for protecting the Northeast Swale is the Meewasin Valley Authority, or MVA, “a conservation agency” that, in the words of its website, is “dedicated to conserving the cultural and natural resources of the South Saskatchewan River Valley.” Since the Swale is an ancient channel of the river, this sounds like a perfect fit. But as its website goes on to explain, the MVA is the creature of its founding partners: the university, the city and the province. When its sponsors tighten their purse strings, the MVA feels the pinch. (The MVA lacks both the budget and the mandate to monitor road works and other developments during the construction phase.) And when its parent bodies make decisions without

consultation, the MVA lacks the autonomy to intervene or to bring the parties together to talk sensibly.

It pains me to air this criticism in public because, in so many ways, the MVA is a gem. It is staffed by skilled and enthusiastic people, and it does many important jobs well – education, stewardship and trail-building, among them. But even our proudest institutions have their limitations. The fact is that plans to conserve the unique values of the Northeast Swale are grossly deficient and almost certain to fail.

On Monday, the Meewasin Northeast Swale Master Plan will be presented to a committee of city council for approval in principal, before going to the full council later in the month. At a minimum, our civic leaders need to hit “pause” and give themselves time to consult widely with city residents. They also need to initiate urgent negotiations with the province to get the Perimeter Highway re-routed further east, both to ensure wise use of public resources and to give the Northeast

Swale room to breathe.



From: Jan Norris <jannorri@gmail.com>
Sent: October 01, 2015 9:34 AM
To: City Council
Subject: Form submission from: Write a Letter to Council

Submitted on Thursday, October 1, 2015 - 09:33
Submitted by anonymous user: 70.64.84.163
Submitted values are:

Date: Thursday, October 01, 2015
To: His Worship the Mayor and Members of City Council
First Name: Jan
Last Name: Norris
Address: 422 10th St. E
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0C9
Email: jannorri@gmail.com

Comments:

I would like to attend and speak at the meeting this Monday October 5th of the Standing Committee on Planning Development and Community Services. This is in regard to the MVA report on the plan for the NE Swale.
Thank you.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/43279>

4131-5, x4205-5

From: Tim Jones <tandljones@sasktel.net>
Sent: October 01, 2015 12:51 PM
To: Web E-mail - City Clerks
Subject: submission to Oct 5th meeting of the Standing Committee on Planning, Development and Community Services
Attachments: Revised PRESENTATION TO MVA BOARD December 5.rtf

Hello,

The Northeast Swalewatchers would like to speak to item 7.2.7 Meewasin Valley Northeast Swale Master Plan. We have been active as a stakeholder in the consultations leading up to the preparation of the Plan and are pleased that there will be funding to ensure the implementation of such an important plan. As background information for the Committee, I am attaching a submission made by our group to the MVA board on December 5, 2014.

Both Rick Huziak and I would like to speak to this topic.

Thank you,

Louise Jones

(306)374-6257

206 111th St

Saskatoon SK S7N 1T2

PRESENTATION by SWALE WATCHERS
to the BOARD of the MEEWASIN VALLEY AUTHORITY
December 5, 2014

As Swale Watchers we appreciate the opportunities we have had to work with you and the City to plan for the protection of the Northeast Swale. Since 2011 we have expressed our concerns about maintaining the area's ecological integrity - the characteristics of the natural area and the composition and abundance of native species and biological communities they support. Planning has come a long way, but some of our key concerns remain. While we focus today on the Master Plan for the Northeast Swale, many of our concerns and suggestions pertain also to the Small Swale because both are important, remaining natural areas in the South Saskatchewan River watershed.

We urge you to:

Monitor Development

Environmental monitoring is essential to protect the ecological integrity of the Swale while development takes place adjacent to, and in, this area.

- a. Environmental monitoring means preventing, minimizing or mitigating adverse impacts to the natural environment (plants, wildlife, soils, water, air, cultural sites) through on-site supervision and data collection by qualified environmental monitors prior to, during and after disturbance events. Environmental monitors have become the industry standard in Saskatchewan to demonstrate environmental stewardship and due diligence, and can reduce costly reclamation and sometimes irreparable damage due to negligence
- b. Environmental monitoring must be undertaken by an independent third party whose qualifications match those set out by the province in terms of education, experience and skills. The monitor must be empowered by the City to halt construction, if necessary, until compliance with environmental protection measures is fully achieved.
- c. The developer must pay for environmental monitoring.

Put Nature First

Protection of the native species - plants and wildlife – and the supporting ecosystem must be the governing principle if ecological integrity is to be maintained. All work in and adjacent to the Swale, and future public access, must be planned and designed to enable native species to survive and thrive.

- a. Minimize the disturbance of and intrusion into the natural habitats of species living in the Swale, by limiting the number of constructed trails within the Swale and reducing the number of entrance points along its perimeter. Maybe there has to

be one constructed connecting path across the centre of the Swale; any other paths should be defined only by mowing.

- b. Design road crossings with wildlife corridors, keeping ecological connectivity in mind. Corridors, with appropriately sized underpasses, must allow animals of all sizes to navigate the length of the Swale all the way to the riverbank without having to cross dangerous roads, where practical. Reduced speeds must be maintained.
- c. Apply the Saskatoon Dark-Sky Policy throughout and adjacent to the Swale. Because light is spilled from adjacent properties and roadways, a lighting-control buffer zone for lands abutting the Swale needs to be established or the required darkness levels for healthy faunal behaviour cannot be maintained.
- d. Monitor native species in the Swale at least annually - both by day and at night - to track impacts of development on the health of populations of species of all sizes, from amphibians to moose.

Protect in Perpetuity

Legally binding long-term protection for this important natural area must be provided.

- a. Revise Zoning Bylaw 8770 to create a "special area" including neighbouring residential and commercial areas, to protect the Swale from invasion of alien species, light pollution, and polluted run-off within the watershed.
- b. Ensure that the legal status of the Swale as a natural protected area is unassailable, either through enhancement of the MVA conservation area status or through strengthening the Meewasin Valley Authority Act.

Educate the Public

The ecological integrity of the Swale will depend on the understanding and attitude of those who build infrastructure in and around it, those who manage it, and those who live and work nearby.

- a. Assemble and use a technical oversight group to help the MVA protect the area through all phases of development and in management in the long term.
- b. Provide opportunities for nearby residents to help protect the Swale, through citizen science programs: such as monitoring water quality and biodiversity and health of species, or monitoring road-kill, or projects such as helping to control invasive species or reporting problems or threats.
- c. Work with adjacent neighbourhood community associations and schools to develop a program of nature stewardship.

- d. The Swale Watchers, through Ducks Unlimited Canada, can provide some funding for educational programs and materials, and our members and member groups can contribute skill and knowledge. Let's work together!

Thank you.

For further information:

Swale Watchers could provide a more comprehensive interpretive document expanding on the ideas expressed in this presentation. See also:

On Ecological Integrity:

- <http://ottawa.ca/en/official-plan-0/24-maintaining-environmental-integrity>
- Fall Report of the Commissioner of the Environment and Sustainable Development, 2013

On Environmental Monitoring:

- <http://www.agriculture.gov.sk.ca/Default.aspx?DN=6756c791-2954-4885-b533-a63d76b5cb3f>
- <http://environment.alberta.ca/01528.html>

On Environmental Monitor Qualifications:

- http://www.environment.gov.sk.ca/adx/asp/adxGetMedia.aspx?DocID=1240,1167,1162,1161,240,94,88,Documents&MediaID=601&Filename=Qualification_Requirements_for_Environmental_Monitors.pdf

On Wildlife Corridors:

- www.cflhd.gov/programs/techDevelopment/wildlife
- Parks Canada Conservation Results in Canada's Mountain National Parks: see www.pc.gc.ca/eng/pn-np/mtn/conservation.aspx

On Dark-Sky:

- Saskatoon Comprehensive and Integrated Dark-Sky Policy (2007)

On Community Nature Stewardship:

- www.portals.iucn.org/library/efiles/documents/PAG-022.pdf

Swale Watchers is made up of concerned members of the public as well as representatives of the following organizations: Saskatoon Nature Society, Ducks Unlimited Canada, Saskatchewan Environmental Society, Native Plant Society of Saskatchewan Inc, Saskatchewan Light Pollution Abatement Committee, c/o RASC Saskatoon Centre Inc, Wild About Saskatoon, Saskatoon Wildlife Federation, South Saskatchewan River Watershed Stewards, University of Saskatchewan Ecological Education Program

From: Andrew McKinlay <apmckinlay@gmail.com>
Sent: October 01, 2015 4:05 PM
To: Web E-mail - City Clerks
Subject: for Committee on Planning, Development and Community Services

Please circulate to the committee re. MVA plan for the northeast swale, item 7.2.7

To whom it may concern,

My name is Andrew McKinlay. I am a local business person, one of the owners of Axon Development Corporation, a software company with offices at Innovation Place, employing approximately 50 people.

As a concerned citizen, I am also one of the founders of EcoFriendlySask.ca which publishes local environmental news and gives out small grants for related local projects.

I am writing today to add my voice in defence of Saskatoon's natural areas, in particular the northeast swale.

Many people are keen to see growth and economic development in Saskatoon. I just hope that we don't pursue this at the expense of what has made our city a great place to live and work. Unless people want to live here, no amount of roads will help our economy.

I have lived in Saskatoon all my life, but I have also had the privilege of travelling all over the world, giving me some perspective on what makes a city attractive. Few people admire cities for their freeways. Talking to people who visit Saskatoon, what they find attractive are our riverbank and other natural areas. Imagine if we had run a freeway along the riverbank (as other cities have done) instead of leaving it as a park.

Running two major roadways across the swale in close proximity, at large expense, seems not only unnecessary but also wasteful. In addition, it is clear that it would have adverse effects on the swale. I would hope the city and the province can cooperate to plan a single roadway, or to relocate one of them outside the swale.

Lastly, we need to protect natural areas not just for the benefit of people, but for their own sake. Just as in the past we grew our definition of "us" to include other races, today we need to expand "us" to include the whole natural world, which we are inescapably a part of.

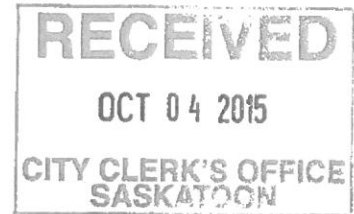
I urge you to choose a path forward that does more to minimize the impact on our valuable natural areas such as the swale. We should remember that building a roadway across a natural area is not something that can be undone later.

Respectfully,

Andrew McKinlay
apmckinlay@gmail.com
1722 Alexandra Avenue
Saskatoon SK S7K 3C5

4131-5
x4205-5

From: Joanne Blythe <jblythe@sasktel.net>
Sent: Sunday, October 04, 2015 9:25 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Sunday, October 4, 2015 - 21:24
Submitted by anonymous user: 216.197.139.244
Submitted values are:

Date: Sunday, October 04, 2015
To: His Worship the Mayor and Members of City Council
First Name: Joanne
Last Name: Blythe
Address: 1209 Munroe Avenue S.
City: Saskatoon
Province: Saskatchewan
Postal Code: S7H 2G3
Email: jblythe@sasktel.net
Comments:

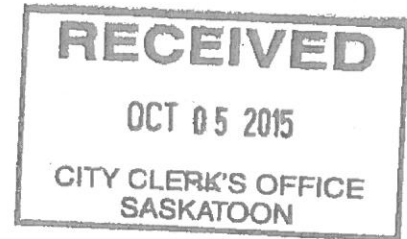
I am writing regarding the plans for the northeast swale to be discussed at the City of Saskatoon's Standing Policy Committee on Planning, Development and Community Services on the morning of Monday, October 5. As you know the swale is one of the last surviving remnants of ancient prairie and is home to many rare and threatened species. It is a living treasure and as such should be preserved in its entirety. It's conservation should take precedence over any human built developments. At a minimum, the current plans for the northeast swale should be paused and our civic leaders should give themselves time to consult widely with city residents. They could also begin negotiations with the province to move the perimeter highways further east to ensure the protection of the swale. The City has the opportunity here to preserve and care for something far more valuable in the long run than another bridge or parkway. Future generations, as well as current, one will thank you for your foresight and wisdom in preserving this unique and diverse ecosystem. You have the opportunity here to preserve an area as important to Saskatoon as is Stanley Park to Vancouver or the Rouge Valley to Toronto.

Sincerely,
Joanne Blythe

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/43852>

4/31-5

From: Honor Kever <honorkever@sasktel.net>
Sent: October 05, 2015 10:47 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Monday, October 5, 2015 - 10:46
Submitted by anonymous user: 71.17.151.82
Submitted values are:

Date: Monday, October 05, 2015
To: His Worship the Mayor and Members of City Council
First Name: Honor
Last Name: Kever
Address: 607 Temperance St
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0M4
Email: honorkever@sasktel.net

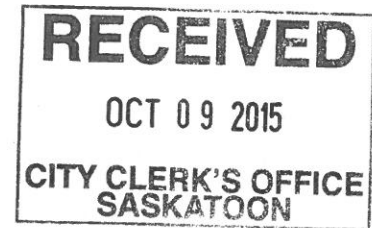
Comments:

The City plan to build a commuter expressway through the NE Swale, and the provincial plan to build a perimeter highway to also cross the swale within a km east of that expressway, make me really mad. This area needs to be protected and no amount of culverts or other dubious plans to "protect" wildlife is acceptable. I cannot believe these plans are even being considered. We are becoming a city where anything will be sacrificed to protect the right to drive a car wherever one wants. Is City Council not aware that in city after city, wherever a freeway, expressway, perimeter highway is built, traffic increases? We own a car, we drive our car, but we are not slaves to our car. We want to live in a progressive city where natural spaces are preserved for all to enjoy. Having two highways cross the swale is not conducive to a meaningful encounter with nature for humans or a sustainable environment for wildlife.

Respectfully,
Honor Kever

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/43931>

From: Christine Varnam <cjvarnam@gmail.com>
Sent: Thursday, October 08, 2015 8:41 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Thursday, October 8, 2015 - 20:41
Submitted by anonymous user: 10.191.225.106
Submitted values are:

Date: Thursday, October 08, 2015
To: His Worship the Mayor and Members of City Council
First Name: Christine
Last Name: Varnam
Address: 810 11th St East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0G6
Email: cjvarnam@gmail.com

Comments: Please halt roadway through the NorthEast Swale. The landscape is too rare and too valuable as wetlands and natural flood control and habitat for plants and animals and respite for humans to be disputed for roads, even if disguised as "Parkway". Thank you.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/44671>



STANDING POLICY COMMITTEE ON PLANNING, DEVELOPMENT AND COMMUNITY SERVICES

Proposed Adult Services Licensing Bylaw Amendments

Recommendation of the Committee

- 1) That the proposed amendments to Adult Services Licensing Bylaw, 2012 No. 9011, as outlined in the report of the General Manager, Community Services Department dated October 5, 2015, be approved; and
- 2) That City Council consider Bylaw No. 9321.

History

At the October 5, 2015 meeting of the Standing Policy Committee on Planning, Development and Community Services, a report of the General Manager, Community Services Department, dated October 5, 2015, was considered.

Attachments

October 5, 2015 report of the General Manager, Community Services Department and Proposed Bylaw No. 9321

Proposed Adult Services Licensing Bylaw Amendments

Recommendation

That the Standing Policy Committee on Planning, Development and Community Services recommend to City Council:

- 1) That the proposed amendments to Adult Services Licensing Bylaw, 2012 No. 9011, as outlined in this report, be approved; and
- 2) That the bylaw, prepared by the City Solicitor to address the proposed amendments to Adult Services Licensing Bylaw, 2012 No. 9011, be included with the submission of this report to City Council, for review and approval.

Topic and Purpose

The purpose of this report is to recommend amendments to Adult Services Licensing Bylaw, 2012 No. 9011 (Bylaw) to enhance its effectiveness. The proposed amendments will facilitate the licensing process for transient performers, clarify licensing categories for non-resident applicants, and provide Saskatoon Police Service (SPS) with ticketing options to address infractions of the Bylaw.

Report Highlights

1. The proposed extension of the timeline for license issuance will facilitate the licensing process for non-residents.
2. A new definition for “Transient Independent Agency” is proposed to clarify licensing categories for non-resident applicants.
3. Provision for the issuance of Notice of Violations will provide more flexible enforcement options.

Strategic Goal

This report supports the City of Saskatoon’s (City) Strategic Goal of Continuous Improvement through continued monitoring and updating of City bylaws.

Background

The Bylaw has been in effect for approximately three years. Business License and SPS staff maintain regular communication to discuss any potential issues related to the administration of the Bylaw or enforcement concerns. A summary table providing an overview of licensing, which has occurred since the Bylaw was implemented in 2012, is provided (see Attachment 1).

Report

The proposed amendments to the Adult Services Licensing Bylaw enhance the process for issuance of licenses, ensure the Bylaw is responsive to the needs of the community, and provide SPS with the necessary tools for effective enforcement.

Extend Timeline for License Issuance Requirements

Subsection 9(1) of the Bylaw establishes a timeframe of 21 days for staff to issue an Adult Service License, upon receipt of a completed application. The 21-day window does not always provide sufficient time to complete the review and approval process, which includes the issuance of photo identification by SPS. This is an issue for non-resident (transient) applicants who typically submit an application for review in advance of arriving in Saskatoon.

It is recommended that the timeframe for license issuance be increased from 21 days to 60 days.

Clarification for Self-Employed Non-Residents

It is recommended that a new “Transient Independent Agency” definition be created to clarify and distinguish licensing categories for transient applicants. Transient applicants may be employed by, or affiliated with, a local Adult Service Agency, or alternatively, independently provide adult services in Saskatoon.

Provision to Issue a Notice of Violation for First-Time Offenders will Provide Increased Flexibility in Enforcement

The Bylaw currently requires all offences to be prosecuted under *The Summary Offences Procedure Act, 1999*, which requires the offender (or a representative) to appear in court to respond to the charges. This places a strain on the resources of multiple agencies involved with the court process and can be problematic for transients who are only in the city for a short period of time. SPS has recommended adding a subsection, which would provide the option to issue a Notice of Violation (ticket) to the offender. The issuance of a Notice of Violation, rather than a Summary Offence charge, would only be offered to first-time offenders found to be operating without an Adult Service License, at the discretion of the police officer managing the file.

Options to the Recommendation

City Council may consider the option to reject the proposed amendments. In this case, further direction would be required.

Public and/or Stakeholder Involvement

The proposed Bylaw amendments were prepared with input from SPS and the City Solicitor’s Office.

Communication Plan

Information pertaining to the amendments will be provided to Adult Service Agencies.

Other Considerations/Implications

There are no policy, financial, privacy, environmental, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

In order to expedite the amendment process, approval is sought to include the proposed Bylaw amendment with this report to City Council. The proposed Bylaw amendments will become effective upon City Council approval.

To address recent provincial legislation, which now prohibits Adult Entertainment Venues, a further report proposing amendments to the Bylaw and Zoning Bylaw No. 8770 will be submitted for consideration in the coming months.

The Administration will continue to monitor Adult Service regulations and provide further reports if required.

Public Notice

Public notice, pursuant to Section 3 of Public Notice Policy No. C01-021, is not required.

Attachment

1. Adult Services Licensing Summary

Report Approval

Written by: Wes Holowachuk, Planner, Business License Program
Reviewed by: Andrew Hildebrandt, Director of Community Standards
Approved by: Randy Grauer, General Manager, Community Services Department

S/Reports/CS/2015/PDCS – Proposed Adult Services Licensing Amendments/ks

Adult Services Licensing Summary

License Type	No. of New Licenses			No. of Renewed Licenses			No. of Closed Licenses			ASL Bylaw Charges Issued*		
	2012	2013	2014	2012	2013	2014	2012	2013	2014	2012	2013	2014
Adult Service Agency	3	2	2	0	1	3	0	1	3	0	6	0
Independent Adult Service Agency	5	7	14	0	2	4	1	1	6	4	22	50
Adult Service Performer (local/transient)	6	55	51	0	4	11	0	9	51	0	30	88
Adult Service Worker	2	10	10	0	1	3	0	1	9	0	3	1
TOTAL	16	74	77	0	8	21	1	12	69	4	61	139

*Data for "No. of Summary Offence Charges Issued" provided by Saskatoon Police Services

Definitions:

An Adult Service Agency is an agency in which the owner employs one or more performers.

An Independent Adult Service Agency is defined as any adult service business which is owned, operated, and serviced by one adult service performer.

An Adult Service Performer is a person who is employed by, affiliated with, or contracted by, an Adult Service Agency to perform an adult service on behalf of the agency, or is paid to perform an adult service in an adult entertainment venue.

An Adult Service Worker is a person who is employed by an Adult Service Agency in a capacity other than as an Adult Service Performer.

A Transient Adult Service Performer means an Adult Service Performer who does not reside or maintain a place of business in the city of Saskatoon.

BYLAW NO. 9321

The Adult Services Licensing Amendment Bylaw, 2015 (No. 2)

The Council of The City of Saskatoon enacts as follows:

Short Title

1. This Bylaw may be cited as *The Adult Services Licensing Amendment Bylaw, 2015 (No. 2)*.

Purpose

2. The purpose of this Bylaw is to amend *The Adult Services Licensing Bylaw, 2012* to reflect changes to the City's adult services licensing scheme to:
 - (a) extend the time frame within which the City shall either issue or deny a licence after receipt of an application;
 - (b) clarify the licensing categories for transient applicants; and
 - (c) expand enforcement options to include an ability to issue a Notice of Violation for first time offenders.

Bylaw No. 9011 Amended

3. *The Adult Services Licensing Bylaw, 2012* is amended in the manner set forth in this Bylaw.

Section 2 Amended

4. Section 2 is amended:
 - (a) in clause 2(l) by striking out "and" after "later date;"
 - (b) in clause 2(m) by adding "and" after "in the City;" and

(c) by adding the following clause after clause 2(m):

“(n) ‘transient independent adult service agency’ means an adult service agency which is owned, operated and serviced by one person that does not maintain a permanent residence or place of business in the City.”

Section 3 Amended

5. Subsection 3(1) is amended by adding “a transient independent adult service agency,” after “independent adult service agency,”.

Section 5.1 Added

6. The following section is added after section 5:

“Licensing Requirements – Transient Independent Adult Service Agency

- 5.1 (1) Before any person may operate as a transient independent adult service agency, a written application must be made to the City for a transient independent adult service agency licence.
- (2) A transient independent adult service agency licence may not be issued to:
- (a) an individual under the age of 18 years;
 - (b) a partnership; or
 - (c) a corporation with any shareholder, director or officer under the age of 18 years.
- (3) A transient independent adult service agency licence may only be issued to an individual if that individual is the only person performing adult services on behalf of that agency and that person does not maintain a permanent residence or place of business in the City.
- (4) A transient independent adult service agency licence may only be issued to a corporation if the only person performing adult services on behalf of that agency is the sole shareholder, director and officer of that corporation and that person does not maintain a permanent residence or place of business in the City.

- (5) Every application for the issue or renewal of a transient independent adult service agency licence must be in a form prescribed by the City and must contain at least the following information regarding the applicant:
- (a) if the applicant is an individual:
 - (i) family name and all given names;
 - (ii) date of birth;
 - (iii) current address and telephone number;
 - (iv) acceptable written proof of the applicant's identity and age (e.g. Canadian citizenship card, Canadian or foreign passport, or driver's licence). The identification must:
 - (A) include a photograph;
 - (B) be government issued;
 - (C) be original, not a copy of the document;
 - (D) indicate a unique identification number; and
 - (E) be valid (i.e., current). An expired licence or passport is not acceptable;
 - (v) written proof of Canadian citizenship or residency status; and
 - (vi) a criminal record check;
 - (b) if the applicant is a corporation:
 - (i) the legal name of the corporation;
 - (ii) the corporate entity number;
 - (iii) the family name, all given names and the date of birth for the sole shareholder, director and officer;
 - (iv) acceptable written proof of the identity and age of the sole shareholder, director and officer (e.g. Canadian

citizenship card, Canadian or foreign passport, or driver's licence). The identification must:

- (A) include a photograph;
 - (B) be government issued;
 - (C) be original, not a copy of the document;
 - (D) indicate a unique identification number; and
 - (E) be valid (i.e., current). An expired licence or passport is not acceptable;
- (v) written proof of Canadian citizenship or residency status of the sole shareholder, director and officer; and
 - (vi) a criminal record check of the sole shareholder, director and officer;
- (c) for all applicants:
- (i) the complete legal name and current licence number of each adult service worker working for the transient independent adult service agency whether by direct employment, independent contract, or otherwise;
 - (ii) the mailing address of the agency;
 - (iii) the business telephone numbers used by the agency including, but not limited to, cellular telephones, digital telephones, pagers and facsimile lines;
 - (iv) all names used by the agency;
 - (v) all electronic mail addresses used by the agency; and
 - (vi) all internet computer addresses used by the agency.
- (6) Every application for the issue or renewal of a transient independent adult service agency licence must be personally signed by:
- (a) the individual named in the application; or

- (b) the sole shareholder, director and officer of the corporation,
as the case may be.”

Section 9 Amended

- 7. Section 9 is amended:
 - (a) in subsection (1) by striking out “21” and substituting “60”;
 - (b) in clause (2)(c) by adding “5.1,” after “5,”;
 - (c) in subsection (4) by:
 - (i) adding “or an independent adult service agency” before “, the address of the business”; and
 - (ii) adding “or independent adult service agency” before “licence shall be posted” and “so that it may be easily read”; and
 - (d) in subsection (5) by adding “a transient independent adult service agency,” after “independent adult service agency,”.

Section 12 Amended

- 8. Subsection 12(1) is amended by adding “a transient independent adult service agency,” after “independent adult service agency licence,”.

Section 13 Amended

- 9. Subsection 13(2) is amended by adding “5.1,” after “5,”.

Section 14 Amended

- 10. Clause 14(1)(c) is amended by adding “or the independent adult service agency” after “the adult service agency”.

Section 15 Amended

11. Section 15 is amended:
 - (a) in subsection (1) by adding “, independent adult service agency or transient independent adult service agency” after “adult service agency”; and
 - (b) in subsection (2) by adding “transient independent adult service agency,” after “independent adult service agency,”.

Section 17 Amended

12. Subsection 17(2) is amended by adding “or transient independent adult service agency” after “independent adult service agency” wherever it appears.

Section 23.1 Added

13. The following section is added after section 23:
 - “23.1 (1) Notwithstanding section 23, in the case of a person who contravenes a provision of this Bylaw for the first time, a peace officer may issue a Notice of Violation to that person.
 - (2) The Notice of Violation shall provide that, if the person pays the City the sum of \$250.00 within 14 calendar days of the date of the Notice of Violation, the person shall not be prosecuted for the contravention.
 - (3) The fine may be paid:
 - (a) in person, during regular office hours, to the cashier located at City Hall, Saskatoon, Saskatchewan;
 - (b) by deposit, at the depository located at the main entrance to City Hall, Saskatoon, Saskatchewan; or
 - (c) by mail addressed to the Office of the City Treasurer, City Hall, 222 – 3rd Avenue North, Saskatoon, Saskatchewan, S7K 0J5.”

Schedule “B” Amended

- 14. Schedule “B” is repealed and the schedule marked as Schedule “A” to this Bylaw is substituted.

Coming into Force

- 15. This Bylaw comes into force on the day of its final passing.

Read a first time this _____ day of _____, 2015.
Read a second time this _____ day of _____, 2015.
Read a third time and passed this _____ day of _____, 2015.

Mayor

City Clerk

Schedule "A" to Bylaw No. 9321

Schedule "B"

Fees		
	Adult Service Agency Licence	Fee
(a)	Initial Licence	\$500.00
(b)	Annual Licence Renewal	\$200.00
	Independent Adult Service Agency Licence	
(c)	Initial Licence	\$250.00
(d)	Annual Licence Renewal	\$100.00
	Transient Independent Adult Service Agency Licence	
(e)	Initial Licence	\$250.00
(f)	Annual Licence Renewal	\$100.00
	Adult Service Performer (i.e. Employed, Affiliated, Contracted, Transient)	
(g)	Initial Licence	\$250.00
(h)	Annual Licence Renewal	\$100.00
	Adult Service Worker Licence	
(i)	Initial Licence	\$250.00
(j)	Annual Licence Renewal	\$100.00



STANDING POLICY COMMITTEE ON FINANCE

Amendments to Policy Nos. C02-030 Purchase of Goods, Services and Work, and A02-027 Corporate Purchasing Procedure

Recommendation of the Committee

1. That the amendments to reporting requirements contained in Council Policy No. C02-030 and Administrative Policy No. A02-027 be adopted;
2. That the City Clerk be requested to update the policies as reflected in the October 5, 2014 report of the CFO/General Manager, Asset and Financial Management Department; and
3. That the City Solicitor be instructed to amend Bylaw No. 8174, The City Administration Bylaw, 2003, to reflect the change in Reporting to Council.

History

At the October 5, 2015 Standing Policy Committee on Finance meeting, a report of the CFO/General Manager, Asset & Financial Management Department dated October 5, 2015 was considered.

Attachment

October 5, 2015 Report of the CFO/General Manager, Asset & Financial Management.

Amendments to Policy Nos. C02-030 Purchase of Goods, Services and Work, and A02-027 Corporate Purchasing Procedure

Recommendation

That the Standing Policy Committee on Finance recommend to City Council:

1. That the amendments to reporting requirements contained in Council Policy No. C02-030 and Administrative Policy No. A02-027 be adopted; and
2. That the City Clerk be requested to update the policies as reflected in this report; and
3. That the City Solicitor be instructed to amend Bylaw No. 8174, The City Administration Bylaw, 2003, to reflect the change in Reporting to Council.

Topic and Purpose

The purpose of this report is to obtain City Council's approval to amend reporting requirements pertaining to contract awards. The proposed change is to allow contract award reports to be a delegated authority matter of the Standing Policy Committee on Finance.

Report Highlights

1. Council Policy No. C02-030 and Administrative Policy No. A02-027 requires the Administration to provide a contract award report of all contracts that are between \$50,000 and \$75,000, with the exception of public tenders, three times a year for the information of City Council.
2. The requested amendments will not eliminate the requirement to report contract awards between \$50,000 and \$75,000, but rather transfer the delegated authority to the Standing Policy Committee on Finance.

Strategic Goal

This report supports the long-term strategy of increasing productivity by being more efficient in the way we do business under the Strategic Goal of Continuous Improvement.

Background

Council Policy No. C02-030 was adopted by City Council on September 10, 2001. In July 2012, the Council Policy was amended to reflect changes to spending thresholds as imposed by the New West Partnership Trade Agreement which governs Saskatchewan, Alberta and British Columbia.

Report

Contract Award Report Reporting Requirements

The New West Partnership Trade Agreement (NWPTA) obligates municipal governments to provide open and non-discriminatory access to procurements where the

Amendments to Policies C02-030 Purchase of Goods, Services and Work, and A02-027 Corporate Purchasing Procedure

procurement value is (i) \$75,000 or greater for goods; (ii) \$75,000 or greater for services; or (iii) \$200,000 or greater for construction. The NWPTA does not require reporting to City Council.

Although the NWPTA allows for construction tenders below \$200,000 to not be publicly tendered, in 2012 the Administration decided to publicly tender all goods, services, and construction where the procurement value was \$75,000 or greater. Once the NWPTA came into effect, Policy Nos. C02-030 and A02-027 changed contract award report levels from between \$50,000 and \$100,000 to between \$50,000 and \$75,000.

The reporting requirement for contract award reports was established in 2001 in order to ensure transparent disclosure of the award of contracts. In today's market, purchases valued between \$50,000 and \$75,000 are relatively low. In a comparison of publicly tendered projects in 2001 to 2014, there were 94 publicly advertised projects in 2001, and in 2014, there were 259 publicly posted projects.

Policy Updates to Transfer Delegated Authority

The Administration is recommending amendments to Council Policy No. C02-030 and Administrative Policy No. A02-027 (Attachment 1) to transfer the delegated authority of the receipt and final consideration of contract award reports to the Standing Policy Committee on Finance. If approved, the obligation of transparency will still be fulfilled, improve efficiencies, and ensure the City of Saskatoon's policies reflect the modern day era.

It should be noted that the Administration will be presenting a report in 2016 with further, extensive recommended amendments to both policies.

Options to the Recommendation

The option is to keep both Council Policy No. C02-030 and Administrative Policy No. A02-027 as they currently exist.

Policy Implications

If the recommendations in the report are approved, Council Policy No. C02-030 and Administrative Policy No. A02-027 will be amended to reflect the change.

Other Considerations/Implications

There are no financial, environmental, privacy, or CPTED implications or considerations, and neither public and/or stakeholder involvement nor a communication plan is required.

Due Date for Follow-up and/or Project Completion

There is no due date for follow-up and/or project completion required.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Amendments to Policies C02-030 Purchase of Goods, Services and Work, and A02-027 Corporate Purchasing Procedure

Attachment

1. Amendments - Excerpts from Council Policy No. C02-030 and Administrative Policy No. A02-027.

Report Approval

Written by: Linda Leedahl, Purchasing Services Manager
Reviewed by: Linda Rauckman, Director of Materials Management
Approved by: Kerry Tarasoff, CFO/General Manager, Asset & Financial Management Department

Amendments_Policies C02-030 and A02-027.docx

**Amendments – Excerpts from Council Policy No. C02-030 and
Administrative Policy No. A02-027**

Council Policy C02-030, Purchase of Goods, Services and Work, Section 6.2:

- 6.2 All contracts, with the exception of public tenders, that are between \$50,000.00 and \$75,000.00 must be reported three times per year to ~~City Council~~ **the Standing Policy Committee on Finance** for the information of ~~Council~~ **Committee**. Consulting contracts that are issued by departments shall be included in the information reports to City Council. Applicable information regarding consulting contracts shall be forwarded to the Purchasing Services Section for inclusion in the reports.

Administrative Policy A02-027, Corporate Purchasing Procedure, Section 23:

23. The Purchasing Services section shall provide ~~City Council~~ **the Standing Policy Committee on Finance** with reports three times a year on the award of contracts and requests for proposals that are between \$50,000 and \$75,000. In the case of consulting contracts, departments are responsible to provide the following information to Purchasing Services for inclusion in the reports: project title, date of issue, name of contractor/consultant, indication of whether sole source or competitive, and dollar amount of contract. Reports will be provided to ~~City Council~~ **the Standing Policy Committee on Finance** in January, May and September for the previous four month period.



STANDING POLICY COMMITTEE ON FINANCE

Contract Award Report – May 1 to August 31, 2015 Contracts Between \$50,000 and \$75,000

Recommendation of the Committee

That the report of the CFO/General Manager, Asset and Financial Management Department, dated October 5, 2015, be received as information.

History

At the October 5, 2015 Standing Policy Committee on Finance meeting, a report of the CFO/General Manager, Asset & Financial Management Department dated October 5, 2015 was considered.

Attachment

October 5, 2015 Report of the CFO/General Manager, Asset & Financial Management.

Contract Award Report – May 1 to August 31, 2015 Contracts Between \$50,000 and \$75,000

Recommendation

That the report of the CFO/General Manager, Asset and Financial Management Department, dated October 5, 2015, be forwarded to City Council for information.

Strategic Goal

This report supports the Strategic Goal of Asset and Financial Sustainability through the open, accountable, and transparent disclosure of the award of contracts.

Report

In accordance with Council Policy No. C02-030, Purchase of Goods, Services and Work, the Administration is required to report three times a year on the award of contracts and requests for proposals between \$50,000 and \$75,000. Attachment 1 is a detailed list of the competitive and sole source contract awards for the period May 1, 2015 to August 31, 2015.

Other Considerations/Implications

There are no policy, financial, environmental, privacy, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

The next contract award report will be tabled in February 2016, outlining the award of contracts and request for proposals for the period September 1, 2015 to December 31, 2015.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Attachment

1. Competitive and Sole Source Contracts Between May 1, 2015 to August 31, 2015

Report Approval

Written by: Linda Leedahl, Purchasing Services Manager
Reviewed by: Linda Rauckman, Director, Materials Management
Approved by: Kerry Tarasoff, CFO/General Manager, Asset & Financial Management Department

Contract Award Report May to Aug 2015.docx

**Competitive Contracts Between \$50,000 - \$75,000
For the Period May 1, 2015 to August 31, 2015**

<u>Date</u>	<u>Project Title</u>	<u>Department</u>	<u>Contractor/Supplier</u>	<u>Contract Amt</u>
15/07/08	Crane Inspections & Service - WTP	Transportation & Utilities	Kristian Electric Ltd.	\$ 50,000.00
15/07/21	Hydrovac Truck Services	Transportation & Utilities	Klark's Trenching Ltd.	\$ 51,456.56
15/07/21	Hydrovac Truck Services	Transportation & Utilities	Badger Daylighting LP	\$ 51,737.50
15/06/17	Lease to Own Roller	Transportation & Utilities	Redhead Equipment	\$ 52,800.00
15/06/22	Street Light Extensions & Arms	Transportation & Utilities	Valmont West Coat Engineering Group	\$ 53,545.80
15/07/07	Dust Suppression Services	Transportation & Utilities	McGills Industrial Services	\$ 53,674.22
15/08/12	A1 License Training	Transportation & Utilities	Trans Industrial Safety Training	\$ 53,900.00
15/07/31	Cisco Nexus Switches	Corporate Performance	SaskTel CMR	\$ 54,335.32
15/08/11	Window Cleaning B.O.	Asset & Financial Mgmt.	Beaming Clean Windows	\$ 55,000.00
15/08/11	Cold Mix Asphalt	Transportation & Utilities	ASL Paving Ltd.	\$ 55,000.00
15/07/09	Microsoft System Centre Service Manager (SCSM)	Corporate Performance	Softlanding Network Solutions Inc.	\$ 64,163.00
15/06/19	Network Cables	Transportation & Utilities	Texcan	\$ 64,225.80
15/07/07	2015 Wooden Playground Replacements	Asset & Financial Mgmt.	1 Stop Playgrounds Ltd.	\$ 64,659.00
15/07/29	Confederation Park Tree Maintenance	Community Services	Arbour Crest Tree Services Ltd.	\$ 67,110.75
15/06/09	Arena Capacitor Banks - Kinsmen, Archibald & Lions Arenas	Asset & Financial Mgmt.	Shaughnessy Electric	\$ 67,515.00
15/06/17	Bike Racks, Table Tops, Retractable Seats	Community Services	Metalshapes Manufacturing Saskatoon	\$ 67,528.13
15/08/24	Marquis Industrial Water Main Chlorination	Transportation & Utilities	Garnett Industries Ltd.	\$ 71,267.28
15/06/09	2015 Manhole Inspection	Transportation & Utilities	Superior City Contracting	\$ 73,500.00
15/06/25	Fareboxes, Parts & Displays B.O.	Transportation & Utilities	BEA Transit Solutions Inc.	\$ 75,000.00

**Sole Source Contracts Between \$50,000 - \$75,000
For the Period May 1, 2015 to August 31, 2015**

<u>Date</u>	<u>Project Title</u>	<u>Department</u>	<u>Contractor/Supplier</u>	<u>Contract Amt</u>
15/07/16	Motorola Parts & Materials	Asset & Financial Mgmt.	Motorola Solutions Canada Inc.	\$ 50,000.00
Policy Section 4.3(b): Standardization/compatibility. This equipment is compatible with the corporate trunked radio system in place until at least June 2019.				
15/06/08	Primary Basin Parts	Transportation & Utilities	Evoqua Water Technologies	\$ 59,701.00
Policy Section 4.3(b): Standardization/compatibility. This equipment is compatible with the existing chain system used in the Primary Basin. Evoqua designed the system and is the sole distributor.				



STANDING POLICY COMMITTEE ON FINANCE

Proposed Internal Audit Administration Process and Amendments to Council Policy No. C02-032

Recommendation of the Committee

That the amendments to Council Policy No. C02-032, Internal Audit Charter, be approved.

History

At the October 5, 2015 Standing Policy Committee on Finance meeting, a report of the CFO/General Manager, Asset & Financial Management Department dated October 5, 2015 was considered.

The Committee has approved the administrative process for Internal Audit work as proposed in the submitted report.

Attachment

October 5, 2015 Report of the CFO/General Manager, Asset & Financial Management.

Proposed Internal Audit Administration Process and Amendments to Council Policy No. C02-032

Recommendation

1. That the proposed administrative process for Internal Audit work be approved; and
2. That the Administration submit a report to City Council recommending amendments to Council Policy No. C02-032, Internal Audit Charter.

Topic and Purpose

This report delivers an outline of the process for the administration of the Internal Audit system, including handling of terms of reference for the scope of work of each audit, handling of audit reports, updates on the audit plan and risk assessments, and additional audit or consulting work over and above that included in the annual audit plan. In addition, this report recommends minor amendments to Council Policy No. C02-032 for consideration.

Report Highlights

1. This report proposes an administrative process for:
 - a. updating the key risks list by the Standing Policy Committee on Finance and aligning the audit plan with any changes to the key risks list or rankings annually;
 - b. developing and implementing the terms of reference for each particular audit, including any additional audits performed;
 - c. reporting and using audit results and how the Administration will respond to audit recommendations; and
 - d. requesting additional audits or consulting work.
2. Council Policy No. C02-032, Internal Audit Charter, requires minor amendments.

Strategic Goal

Being efficient and effective through performance of internal audits supports the long-term strategy of being more efficient in the way we do business under the Strategic Goal of Continuous Improvement.

Background

The Internal Audit services contract recently awarded to PricewaterhouseCoopers (PwC) included helping the City of Saskatoon (City) identify critical and strategic risks, and was intended to help focus the audit plan on the City's most important risks. The contract, which runs from January 1, 2015 to December 31, 2019 calls for PwC to perform 1,400 hours of work each year for an annual fee of \$224,650 in the first year, with 2% incremental increases in each subsequent year of the agreement. The City's

internal audit budget for 2015 is set at an amount (\$427,000) in excess of the contract amount to allow for additional audit or advisory work as may be necessary or desired.

The initial risk identification and ranking process is now complete, and the five-year internal audit plan was approved by the Standing Policy Committee on Finance (Committee) at its meeting on August 17, 2015. PwC has embarked on the first of the audits as outlined in the plan. It is important to provide the Committee with proposals for the administration of the Internal Audit program to assure the Committee that the best possible use of audit resources will be made.

Report

Audit Plan

The five-year audit plan is based on the key risks rankings provided by the Executive Committee. The key risks list and rankings will be reviewed and updated annually, as required by Section 5.9 of Council Policy No. C02-032, Internal Audit Charter (Attachment 1). Key risks and rankings will likely change over a period of time due to a number of reasons, including successes or changes in risk mitigation, personnel changes, financial aspects, or changes and developments in the overall internal or external business environment. If changes to the key risks and/or rankings are made, it would be necessary to realign the audit priorities to the new risk priorities.

In order to ensure ongoing alignment of the audit plan to the list of key risks, the Administration will submit an annual report to the Committee outlining proposed changes to the list of key risks and rankings. The Administration will also work with PwC to bring forward desired realignments of the audit plan for consideration of the Committee. These updates will occur in advance of, or concurrent with, the annual budget process so that any necessary alignments with the budget process may be coordinated. Once approved, the updated list of risks and audit plan will guide the work of Internal Audit and the risk management efforts of the Administration for the next year, including development of detailed audit Terms of Reference (TOR) for each audit to be performed in any one year.

Audit Terms of Reference (TOR)

Each audit proposed in the initial or amended audit plan will require a detailed TOR. The TOR will provide information on the precise nature, focus and scope of the audit work, and cost information to the Administration. This will ensure that the audit meets the needs of the City and will help establish an expectation on cost of work prior to it being undertaken. The draft TOR document will be developed by Internal Audit in consultation with the Administration and then submitted to the Committee (In Camera) for comment and feedback, and later tabled with the Committee at its next public meeting.

Audit Reports

As per Council Policy No. C02-032, Internal Audit will provide detailed draft reports to the Administration on all completed audits for review and comment, and to the Committee (In Camera) for review, comment and feedback. Upon finalization, an

executive summary of the report will be submitted to the Committee for approval (public agenda).

All audit reports presented to the Committee will incorporate comments from the Administration highlighting their perspective and/or specific actions that are planned to be undertaken to implement the recommendations of the Internal Audit team. The Administration will also provide a report as necessary to the Committee highlighting the status of the implementation of action plans agreed within different audit reports on a go forward basis.

Additional Audit and Consulting Work

Council Policy No. C02-032 provides that the Committee, or the City Manager, may request additional audits and/or consulting work above and beyond those outlined in the five-year audit plan where necessary or desirable. Where the budget allows, any such additional audit or consulting work requests will be handled according to the administrative process as outlined in this report, and subject to the requirements of the process and Council Policy No. C02-032. Where the annual budgeted amount had been allocated, and additional work is deemed necessary, a report to the Committee will set out the terms of reference and costs, and seek approval for the work and any necessary additional funds.

Where a report is submitted to the Committee in respect of any of the above, the Committee, as a matter of course, will have an opportunity to discuss the report privately with the Internal Auditor.

Amendments to Council Policy No. C02-032

The Administration is recommending amendments to Council Policy No. C02-032, as shown in Attachment 1, in order to reflect the current committee structure and refocus the internal audit plan on risk based audit methodology. If approved, the Administration will submit a report to City Council for its consideration.

Options to the Recommendation

The Committee could elect to set any other process in accordance with Council Policy No. C02-032.

Communication Plan

The Administration will develop an audit information communications strategy with staff from the Communications Division.

Other Considerations/Implications

There are no policy, financial, environmental, privacy, CPTED or other implications, and public and/or stakeholder involvement is not required.

Due Date for Follow-up and/or Project Completion

There is no due date for follow-up and/or project completion as this project is ongoing.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Attachment

1. Council Policy No. C02-032

Report Approval

Written by: Wayne Bischoff, Director of Corporate Risk

Approved by: Kerry Tarasoff, CFO/General Manager, Asset & Financial
Management Department

Internal Audit Admin Process.docx

CITY OF SASKATOON COUNCIL POLICY

NUMBER

C02-032

POLICY TITLE <i>Internal Audit Charter</i>	ADOPTED BY: <i>City Council</i>	EFFECTIVE DATE <i>January 19, 2004</i>
		UPDATED TO <i>April 16, 2012</i>
ORIGIN/AUTHORITY <i>Clause 2, Report No. 1-2004 of the Audit Committee; and Clause 2, Report No. 1-2012 of the Audit Committee</i>	CITY FILE NO. <i>CK. 1600-1</i>	PAGE NUMBER <i>1 of 10</i>

1. PREFACE

- 1.1 Section 1000 of the IIA Standards for the Professional Practice of Internal Auditing require that the purpose, authority and responsibility of the Internal Audit Function be formally defined in a Charter, consistent with the Standards, and approved by the governing body.
- 1.2 This Charter formalizes the Internal Audit Function, defines the scope and responsibilities of the Internal Auditor, and provides the Internal Auditor with the necessary authority to fulfill his/her responsibilities.
- 1.3 This document draws extensively on material published by the Institute of Internal Auditors including, but not necessarily limited to, the International Standards for the Professional Practice of Internal Auditing (hereafter referred to as the IIA Standards) and related Practice Advisories.

2. POLICY OBJECTIVES

- 2.1 To ensure the independence and objectivity of the Internal Audit Function.
- 2.2 To ensure the Internal Audit Function adheres to the Standards for the Professional Practice of Internal Auditing.

3. DEFINITIONS

- 3.1 Assurance Audit – An objective examination of evidence for the purpose of providing an independent assessment on risk management, control, and governance processes for City Council. Examples of assurance audits are provided in section 5.8 of this policy.

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- 3.2 Compliance – The ability to reasonably ensure conformity and adherence to legislation, regulations, contracts, agreements, policies, plans, and procedures.

- 3.3 Consulting Services – Advisory and related client service activities, the nature and scope of which are agreed upon with City Council, the ~~Audit Committee~~ **Standing Policy Committee on Finance**, or the City Manager and which are intended to add value and improve the City’s operations. Examples include counsel, advice, facilitation, process design, and training.

- 3.4 Fraud – Any illegal acts characterized by deceit, concealment, or violation of trust. These acts are not dependent upon the application of threat of violence or of physical force. Frauds are perpetrated by individuals and organizations to obtain money, property, or services; to avoid payment or loss of service; or to secure personal or business advantage.

- 3.5 Governance Processes – The procedures utilized by City Council and its Committees, Boards, and Commissions to provide oversight of risk and control processes administered by management.

- 3.6 IIA Standards – Institute of Internal Auditors Standards for the Professional Practice of Internal Auditing.

- 3.7 Internal Auditing – Independent, objective assurance and consulting activity designed to add value and improve an organization’s operations. It helps an organization accomplish its objectives by bringing a systematic, disciplined approach to evaluate and improve the effectiveness of risk management, control, and governance processes.

- 3.8 Risk – The uncertainty of an event occurring that could have an impact on the achievement of objectives. Risk is measured in terms of consequences and likelihood.

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4. POLICY STATEMENT

- 4.1 City Council shall retain an Internal Auditor, independent of management, to provide internal assurance audit and consulting services designed to add value and improve the City's operations.
- 4.2 The mission of the Internal Audit Function is to help the City accomplish its objectives by bringing a systematic, disciplined approach to evaluate and improve the efficiency and effectiveness of risk management, control, and governance processes.

5. POLICY REQUIREMENTS

5.1 Standards

- a) The Internal Auditor shall adhere to the *Standards for the Professional Practice of Internal Auditing* and the *Code of Ethics* of the Institute of Internal Auditors.

5.2 Reporting Relationship

- a) The Internal Auditor shall report to and be accountable to City Council through the ~~Audit Committee~~ **Standing Policy Committee on Finance** of City Council.
- b) Appointment and dismissal of the Internal Auditor shall be based on recommendations from the ~~Audit Committee~~ **Standing Policy Committee on Finance** and shall require the approval of a majority of City Council.

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5.3 Authority

- a) The Internal Auditor shall have authority to audit all aspects of the City's operations including governance processes, departments, programs, business processes, systems, functions, activities, policies, etc.
- b) In accordance with IIA Standard 1110, the Internal Auditor shall be free from interference in determining the scope of internal auditing, performing work, and communicating results. Subject to complying with the requirements of IIA Standard 1110, the Internal Auditor shall consult with management and the ~~Audit Committee~~ **Standing Policy Committee on Finance** for input into the lines of enquiry to be pursued in each audit.
- c) The Internal Auditor shall have unrestricted access to all records, personnel and physical property relevant to the performance of audits.

5.4 Scope of Internal Audit Activities

- a) The Internal Auditor shall **utilize a methodology that links internal audit practices to the City's risk management systems, so as to provide assurance to Council that risk management practices and processes are managing risk effectively. The Internal Auditor shall** determine and report on the extent to which the City's network of risk management, control, and governance processes, as designed and represented by management and City Council, is adequate and functioning in a manner to ensure:
 - i) With respect to financial systems, that risks are appropriately identified and are effectively managed;
 - ii) In accordance with an audit plan, that significant financial, managerial, and operating information is accurate, reliable, and timely;
 - iii) In accordance with an audit plan, due diligence is exercised in the development and implementation of computer systems and system changes;

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- iv) With respect to financial systems, employees' actions are in compliance with policies, standards, procedures, and applicable laws and regulations;
- v) In accordance with an audit plan, that resources are acquired economically, used efficiently, and adequately protected; and
- vi) With respect to financial systems, that quality and continuous improvement are fostered in the organization's control process.

5.5 Role of Internal Audit in Providing Consulting Services

- a) The Internal Auditor shall have authority to perform consulting services where such services do not represent a conflict of interest or detract from the Internal Auditor's assurance services obligations.
- b) The performance of consulting services by the Internal Auditor shall be governed by the IIA Standards.

5.6 Role of Internal Audit in the Risk Management Process

- a) Risk management is a key management responsibility. This responsibility includes identifying, assessing, mitigating, and monitoring risks on a continuous basis.
- b) Consistent with IIA Standard 2110, the Internal Auditor shall assist the organization by identifying and evaluating significant exposures to risk and contributing to the improvement of risk management and control systems.

5.7 Role of Internal Audit in Fraud Investigation

- a) Management is responsible for establishing and maintaining controls to discourage the perpetuation of fraud and limit the exposure if fraud does occur.
- b) The Internal Auditor is responsible for assisting in the deterrence of fraud by examining and evaluating the adequacy and the effectiveness of the system of internal control, with due consideration to the extent of the potential exposure/risk in the various segments of the City's operations.

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- c) The Internal Auditor shall be notified of all fraud allegations or potential conflict of interest situations. The Internal Auditor may provide assistance in investigating and documenting the nature, extent, and effect of any allegations, ensuring sensitivity and confidentiality throughout the process.

5.8 Types of Assurance Audits

- a) The types of assurance audits to be undertaken by the Internal Auditor shall include, but not necessarily be limited to:
 - i) Financial control audits – to determine if adequate systems, practices, and controls are in place to safeguard the City’s financial resources;
 - ii) Fraud investigations – to determine whether fraud has occurred, identify the perpetrator(s), identify the extent of the fraud, and determine the techniques used and cause of the fraud;
 - iii) Follow-up audits – to assess the actions taken by management and City Council in response to significant audit observations and recommendations.
 - iv) **Value for Money Audits – to assess the efficiency, effectiveness and economy of operations, activities or programs.**

5.9 Corporate Audit Plan

- a) The Internal Auditor shall establish a long-range plan for assurance audits. The input of management and the ~~Audit Committee~~ **Standing Policy Committee on Finance** shall be considered in this process.
- b) The long-range plan shall be based on a risk assessment and shall be updated at least annually. The actual cycle for specific departments, programs, or functions appearing in the long-range plan shall be based on weightings assigned to various risk factors including, but not necessarily limited to, materiality (e.g. the size of the budget), staff levels, type of organization (e.g. Board vs. department), findings from previous audit, extent of public exposure, funding source, relevant legislative requirements, and complexity/diversity.

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- c) The Plan shall be subject to City Council approval.

5.10 Coordination of Internal Audit with External Parties

- a) The Internal Auditor shall meet with the City's External Auditor on at least a semi-annual basis to share information and coordinate activities for the purpose of ensuring proper audit coverage and minimizing duplication of effort.

5.11 Reporting Results of Internal Audits

- a) The Internal Auditor shall document the results of each assurance audit in a formal report. The report shall be discussed with management, shall incorporate management responses and target dates for implementation of recommendations, and shall be tabled with and presented to the ~~Audit Committee~~ **Standing Policy Committee on Finance**.
- b) All information systems and cash handling audit reports shall remain in-camera, shall not be tabled with City Council, and shall not be made available to the public.
- c) The City's External Auditor shall have access to all information systems and cash handling audit reports after they have been tabled with, presented to, and cleared through the ~~Audit Committee~~ **Standing Policy Committee on Finance**. Distribution of information systems and cash handling audit reports to any and all other external parties shall be subject to the prior approval of the ~~Audit Committee~~ **Standing Policy Committee on Finance**.
- d) Notwithstanding (d) above, the Internal Auditor shall have authority to release copies of any information systems or cash handling audit report to other municipal Internal Auditors on the understanding that recipients respect the confidentiality of these documents. The Internal Auditor shall inform the ~~Audit Committee~~ **Standing Policy Committee on Finance** of any such requests that have been received and reports released.

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5.12 Management Response to Assurance Audit Findings

- a) After being provided with draft audit observations, the manager with authority to and responsibility for taking corrective action shall, in a timely manner, provide a written response to the Internal Auditor on the recommendations contained in the report.
- b) Management shall, in a timely manner, following receipt of the audit report, provide the Internal Auditor with a plan of action, including target dates, for implementing the audit recommendations.
- c) The Internal Auditor shall have authority to set the timelines for the receipt of a response or action plan, with respect to (a) and (b) above, when, in the opinion of the Internal Auditor, it is necessary to set the timelines, and may grant any such extensions to the same.
- d) Management shall provide the ~~Audit Committee~~ **Standing Policy Committee on Finance** with progress reports, until action has been taken on all audit recommendations.

5.13 Follow-up

- a) The Internal Auditor shall include all follow-up audits in the Corporate Audit Plan.
- b) The follow-up audit procedure and reporting shall be performed in accordance with the process approved by the ~~Audit Committee~~ **Standing Policy Committee on Finance**.

5.14 Audit Working Papers

- a) Audit working paper files are the property of the Internal Auditor. They shall remain under the control of, and shall be accessible only to personnel authorized by, the Internal Auditor.
- b) Access to working paper files shall be governed by the IIA Standards for the Professional Practice of Internal Auditing and related Practice Advisories.

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5.15 Consulting Legal Counsel & Labor Relations Expertise

- a) The Internal Auditor shall consult with the City Solicitor before including results and issuing opinions in audit reports regarding legal or regulatory violations or other legal issues.
- b) The Internal Auditor shall consult with the City’s Human Resources ~~Branch~~ **Division** before including results and issuing opinions in assurance engagement reports that could have labor relations implications.

5.16 Accountability Reporting Requirements

- a) The Internal Auditor shall provide the ~~Audit Committee~~ **Standing Policy Committee on Finance** with periodic reports (no less than quarterly) on performance and progress relative to the Corporate Audit Plan.
- b) The Internal Auditor shall disclose to the ~~Audit Committee~~ **Standing Policy Committee on Finance**, the nature and extent of all consulting engagements undertaken at the request of the City Manager.

5.17 Periodic Assessment of the Charter

- a) The Internal Auditor shall periodically assess whether the purpose, authority, and responsibility of the internal audit function, as defined in the Charter, continue to be adequate to enable the internal audit function to accomplish its objectives. The Internal Auditor shall communicate the results of the assessments to the ~~Audit Committee~~ **Standing Policy Committee on Finance**.

5.18 Communications with and Access to the Internal Auditor

- a) The Chair of the ~~Audit Committee~~ **Standing Policy Committee on Finance** may consult and/or meet with the Internal Auditor prior to any scheduled ~~Audit Committee~~ **Standing Policy Committee on Finance** meeting for the purpose of preparing the ~~Audit Committee~~ **Standing Policy Committee on Finance** agenda.
- b) Individual members of City Council shall have unrestricted authority to meet at any time with the Internal Auditor but shall have no authority to direct the Internal Auditor to undertake any audit work.

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- c) Only the ~~Audit Committee~~ **Standing Policy Committee on Finance**, or the City Manager, shall have authority to engage the Internal Auditor to perform audits pursuant to this policy. Any requests by individual Councillors to engage the services of the Internal Auditor must be referred to the ~~Audit Committee~~ **Standing Policy Committee on Finance** for decision. This does not preclude the opportunity for the applicable Councillor to consult and/or meet in confidence with the Internal Auditor prior to the matter being brought before the Committee.

6. ENQUIRIES

All enquiries concerning this Charter should be referred to the Internal Auditor and/or the Chair of the ~~Audit Committee~~ **Standing Policy Committee on Finance**.



STANDING POLICY COMMITTEE ON FINANCE

Request to Sell City-Owned Property – Aspen Ridge Neighbourhood

Recommendation of the Committee

1. That the Director of Saskatoon Land be authorized to sell 134 single-family lots with legal descriptions of Lots 1 to 38, Block 701, Plan No. to be registered; Lots 1 to 13, Block 702, Plan No. to be registered; Lots 1 to 25, Block 703, Plan No. to be registered; Lots 1 to 16, Block 704, Plan No. to be registered; Lots 1 to 28, Block 705, Plan No. to be registered; and Lots 1 to 14, Block 706, Plan No. to be registered on Fast Crescent, Court, Way and Land through a lot draw process;
2. That if the parcels are not sold through the lot draw process, they will be placed for sale over-the-counter on a first-come, first-served basis;
3. That the Director of Saskatoon Land be authorized to sell Lots 1 and 2, Block 701 to the highest bidder through a public tender process for the intended use of developing Type II Residential Care Homes, Child Care Centres or Pre-Schools with tender conditions and reserve bid prices plus applicable taxes;
4. That any of the pre-designated Type II Residential Care Homes, Child Care Centres or Pre-School lots which are not sold through the public tender process be placed for sale over-the-counter on a first-come, first-served basis for the same intended purpose for a period of one year with conditions specified in the Agreement for Sale;
5. That the Director of Saskatoon Land be authorized to make minor adjustments to the approved pricing that may be necessary to account for changes in servicing costs and lots being returned;
6. That the Director of Saskatoon Land be authorized to administer development controls for the 134 single-family lots; and
7. That the City Solicitor be requested to prepare the Agreements for Sale and that His Worship the Mayor and the City Clerk be authorized to execute the agreements under the Corporate Seal.



STANDING POLICY COMMITTEE ON FINANCE

History

At the October 5, 2015 Standing Policy Committee on Finance meeting, a report of the CFO/General Manager, Asset & Financial Management Department dated October 5, 2015 was considered.

Attachment

October 5, 2015 Report of the CFO/General Manager, Asset & Financial Management Department.

Request to Sell City-Owned Property – Aspen Ridge Neighbourhood

Recommendation

That the Standing Policy Committee on Finance recommend to City Council:

1. That the Director of Saskatoon Land be authorized to sell 134 single-family lots with legal descriptions of Lots 1 to 38, Block 701, Plan No. to be registered; Lots 1 to 13, Block 702, Plan No. to be registered; Lots 1 to 25, Block 703, Plan No. to be registered; Lots 1 to 16, Block 704, Plan No. to be registered; Lots 1 to 28, Block 705, Plan No. to be registered; and Lots 1 to 14, Block 706, Plan No. to be registered on Fast Crescent, Court, Way and Lane through a lot draw process;
2. That if the parcels are not sold through the lot draw process, they will be placed for sale over-the-counter on a first-come, first-served basis;
3. That the Director of Saskatoon Land be authorized to sell Lots 1 and 2, Block 701 to the highest bidder through a public tender process for the intended use of developing Type II Residential Care Homes, Child Care Centres or Pre-Schools with tender conditions and reserve bid prices plus applicable taxes;
4. That any of the pre-designated Type II Residential Care Home, Child Care Centre or Pre-School lots which are not sold through the public tender process be placed for sale over-the-counter on a first-come, first-served basis for the same intended purpose for a period of one year with conditions specified in the Agreement for Sale;
5. That the Director of Saskatoon Land be authorized to make minor adjustments to the approved pricing that may be necessary to account for changes in servicing costs and lots being returned;
6. That the Director of Saskatoon Land be authorized to administer development controls for the 134 single-family lots; and
7. That the City Solicitor be requested to prepare the Agreements for Sale and that his Worship the Mayor and the City Clerk be authorized to execute the agreements under the Corporate Seal.

Topic and Purpose

The purpose of this report is to obtain approval to sell and administer development controls for 134 single-family lots in the Aspen Ridge neighbourhood through a lot draw process.

Report Highlights

1. Proposed prices in this lot draw range from \$101,500 to \$235,800.
2. Two Type II Residential Care Home, Child Care Centre or Pre-School lots are proposed to be sold by public tender.
3. Development controls for the 134 single-family lots are being proposed in order to enhance the character within the neighbourhood, assist in strengthening the streetscape and harmonize the interface between various housing forms.

Strategic Goal

This report supports the long-term strategy of increasing revenue sources and reducing reliance on residential property taxes under the Strategic Goal of Asset and Financial Sustainability.

Background

City Council, at its meeting on June 23, 2014, approved the Aspen Ridge Neighbourhood Concept Plan (Attachment 1). The Aspen Ridge neighbourhood is approximately 640 acres in area and will accommodate and estimated population of 12,544 persons living within 5,702 dwelling units. The City of Saskatoon (City) owns and will develop 71% of Aspen Ridge development. Topsoil stripping began in fall of 2013. Construction tenders for grading, deep services and roadways for this first phase were initiated in 2014 and will be completed this fall.

Saskatoon Land is requesting approval to set prices for and sell the first 134 single-family lots, including two Type II Residential Care Home, Child Care Centre or Pre-School lots.

Report

Lot Pricing

This first phase of Aspen Ridge is 134 lots, which includes a variety of lot sizes and types. Lot sizes range from 9.14 metres (30 feet) to 14.82 metres (49 feet). The majority of lots are between 9.14 metres (30 feet) and 13.42 metres (44 feet) in width.

The general breakdown and unique features of the lots are as follows:

- 46 detached garage lots with lanes;
- 23 attached garage standard lots with lanes;
- 18 attached garage standard lots;
- 14 attached garage standard lots that back on to a linear park and feature decorative rear-yard fencing;
- 8 attached garage standard lots that back on a Municipal Buffer;
- 23 attached garage walk-out basement lots that back on to the Northeast Swale and feature decorative rear-yard fencing; and
- 2 Type II Residential Care Home, Child Care Centre or Pre-School lots.

A variety of lot price points will be available in this phase that will accommodate affordable homes, mid-range homes and higher-end homes. Lot prices have been

determined based on an examination of current lot prices for comparable properties in the Saskatoon market.

A base unit price of \$11,500 per front metre was used to calculate the lot prices. Adjustments were made to the base price to account for lot location and lot specific characteristics, including spectacular views of the Northeast Swale. The lot prices range from \$101,500 to \$235,800 and the average lot price for this area is \$146,766. A list of individual lot prices is attached (Attachment 2). If the lots are not completely serviced at the time of lot selection, they will be sold with a delayed possession date.

Lots to be Sold by Public Tender

Lots 1 and 2 of Block 701, Plan to be registered, are pre-designated Type II Residential Care Home, Child Care Centre or Pre-School lots which are proposed to be sold by public tender (Attachment 3). If these lots are not sold through the public tender process, they will be placed for sale over-the-counter on a first-come, first-served basis for the same intended purpose for a period of one year. If not sold during that year, Saskatoon Land will place the lots for sale over-the-counter on a first-come, first-served basis for any purpose allowed within the R1A zone.

Development Controls

Development Controls (Attachment 4) are being proposed for the single-family lots in this phase of development in order to enhance the character within the neighbourhood, to strengthen the streetscape, and to harmonize the interface between various housing forms.

These Development Controls are structured to encourage the construction of houses with more exterior architectural detailing and with the use of a variety of exterior building materials. The noteworthy changes from previous development controls are the following:

- No vinyl siding will be permitted on the 23 walk-out basement lots that back on to the Northeast Swale.
- Attached garages shall not protrude more than 2.4 metres (8 ft) from the façade of any habitable floor area of the dwelling.
- Rather than prescribing stone, all dwellings will require a minimum of two different exterior building materials on the front elevation.
- For 107 lots, the minimum roof pitch is 6-in-12 except when it can be demonstrated that a lesser sloped roof pitch is integral to the intended architectural style and consistent with the goal of harmonizing the interface between housing forms. Twenty seven lots, that are grouped together, will have no roof pitch requirement.
- Saskatoon Land will receive and evaluate each house plan for compliance as it gets submitted for a building permit.
- Saskatoon Land's interpretation and decision will be final.

Options to the Recommendation

The only other option would be not to proceed with the sale of land at this time.

Public and/or Stakeholder Involvement

The single-family lots being offered for sale are consistent with the approved Aspen Ridge Concept Plan. Public and Stakeholder consultation occurred in 2014.

Communication Plan

Notice of the intent to allocate these lots to builders and individuals by lot draw and public tender will be advertised in The StarPhoenix a minimum of two Saturdays prior to the draw and public tender, pursuant to City Council Policy No. C09-006, Residential Lot Sales – General Policy, and will be posted on the City's website.

Financial Implications

Proceeds from the sale of these lots will be deposited in the Aspen Ridge Neighbourhood Land Development fund.

Safety/Crime Prevention Through Environmental Design (CPTED)

The subdivision and sale of these lots are in conformance with the approved Aspen Ridge Neighbourhood Concept Plan. A CPTED review is not required.

Other Considerations/Implications

There are no policy or privacy implications or considerations.

Due Date for Follow-up and/or Project Completion

There is no due date for follow-up and/or project completion required.

Public Notice

Public Notice, pursuant to Section 3 of the Public Notice Policy No. C01-021 is not required.

Attachments

1. Aspen Ridge Neighbourhood Land Use Concept Plan
2. Aspen Ridge Proposed Price List
3. Aspen Ridge Phase I Sales Map
4. Aspen Ridge Development Controls

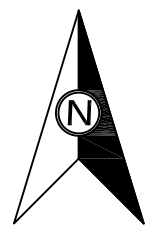
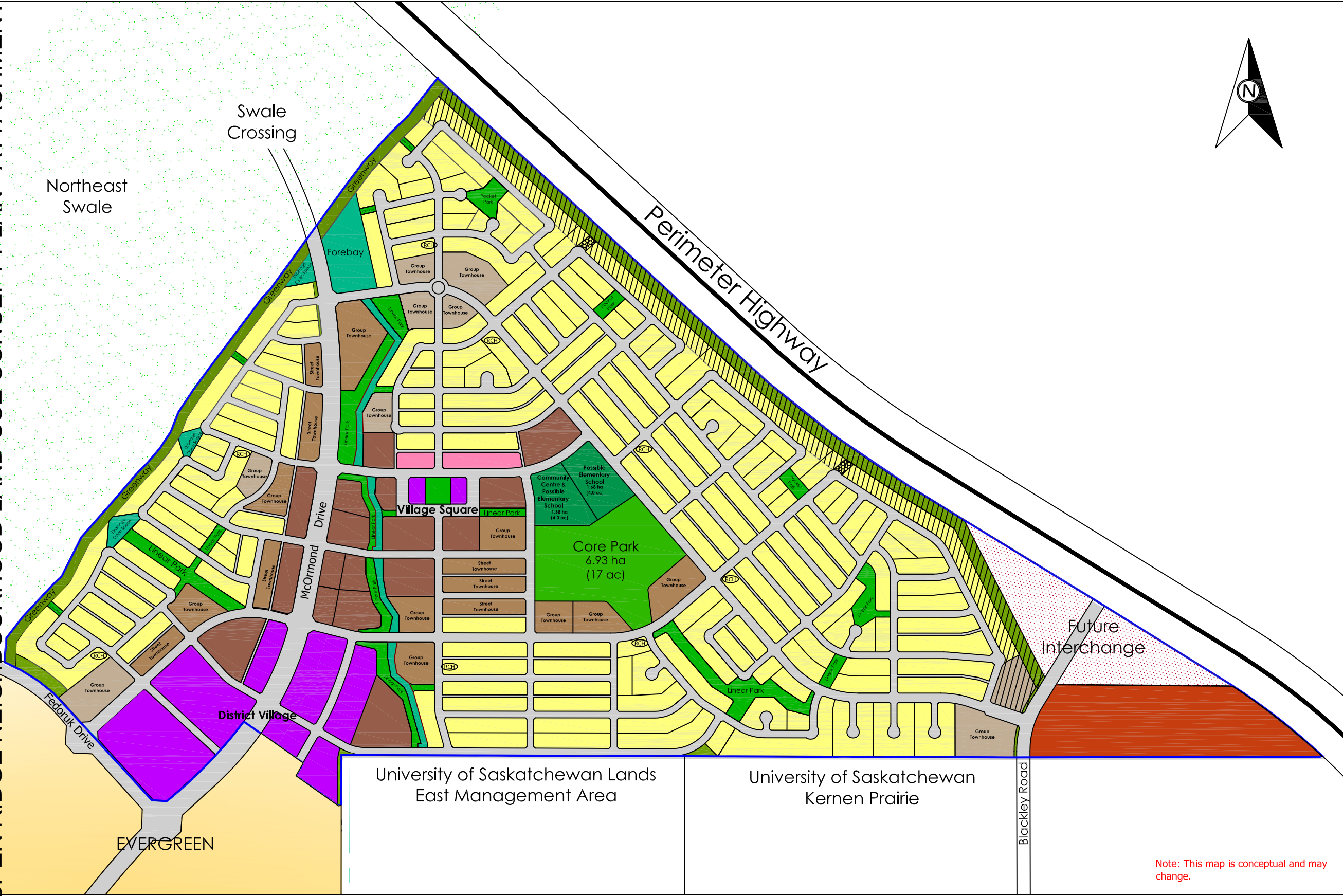
Report Approval

Written by: Derek Thompson, Development Project Manager, Saskatoon Land
Reviewed by: Frank Long, Director of Saskatoon Land
Approved by: Kerry Tarasoff, CFO/General Manager, Asset & Financial Management Department

Land Use Concept Plan

Aspen Ridge Neighbourhood Concept Plan

ASPEN RIDGE NEIGHBOURHOOD LAND USE CONCEPT PLAN ATTACHMENT 1



Legend

- Single-Unit Detached
- Low-Density Multi Unit
- Low/Med Density Multi Unit
- Medium-Density Multi Unit
- Medium-Density Mixed-Use
- Low-Density Mixed-Use
- Park
- Landscaped Drainage Open Space
- Greenway
- Buffer
- Elementary Schools/Community Centre
- Urban Holding Area
- Northeast Swale Preserved Land
- Possible Roadway Extension
- Neighbourhood Boundary
- RCH Possible Residential Care Home Site
- 📶 Possible Cell Tower Location
- Final configuration to be determined once the Perimeter Highway alignment is confirmed

University of Saskatchewan Lands
East Management Area

University of Saskatchewan
Kernen Prairie

EVERGREEN

Note: This map is conceptual and may change.



Aspen Ridge Proposed Price List

Aspen Ridge (2015) - Fast

NOTE: Plan for all lots to be registered

Lot	Block	Price
1	701	\$185,100
2	701	\$181,000
3	701	\$133,300
4	701	\$133,300
5	701	\$139,900
6	701	\$159,100
7	701	\$164,400
8	701	\$172,600
9	701	\$167,600
10	701	\$154,300
11	701	\$154,300
12	701	\$154,300
13	701	\$151,800
14	701	\$172,800
15	701	\$184,900
16	701	\$217,600
17	701	\$214,300
18	701	\$208,100
19	701	\$193,000
20	701	\$193,000
21	701	\$193,000
22	701	\$193,000
23	701	\$193,000
24	701	\$193,000
25	701	\$193,000
26	701	\$193,000
27	701	\$193,000
28	701	\$193,000
29	701	\$183,700
30	701	\$183,400
31	701	\$183,500
32	701	\$184,400
33	701	\$184,400
34	701	\$193,000
35	701	\$193,000
36	701	\$190,200
37	701	\$231,300
38	701	\$235,800
1	702	\$145,100
2	702	\$147,500
3	702	\$138,700
4	702	\$138,700
5	702	\$138,700
6	702	\$138,700
7	702	\$138,800
8	702	\$138,800
9	702	\$138,800

Lot	Block	Price
10	702	\$138,800
11	702	\$131,600
12	702	\$130,000
13	702	\$127,900
1	703	\$163,800
2	703	\$165,000
3	703	\$154,500
4	703	\$154,500
5	703	\$154,500
6	703	\$154,500
7	703	\$154,500
8	703	\$154,500
9	703	\$153,400
10	703	\$152,700
11	703	\$101,500
12	703	\$101,800
13	703	\$101,800
14	703	\$110,100
15	703	\$110,100
16	703	\$110,100
17	703	\$110,100
18	703	\$110,100
19	703	\$110,100
20	703	\$110,100
21	703	\$110,100
22	703	\$114,900
23	703	\$114,300
24	703	\$113,400
25	703	\$112,100
1	704	\$105,100
2	704	\$113,100
3	704	\$113,300
4	704	\$110,100
5	704	\$110,100
6	704	\$110,100
7	704	\$110,100
8	704	\$110,100
9	704	\$110,100
10	704	\$110,100
11	704	\$110,100
12	704	\$110,100
13	704	\$110,100
14	704	\$110,100
15	704	\$110,300
16	704	\$105,000
1	705	\$117,500
2	705	\$117,500

Lot	Block	Price
3	705	\$117,500
4	705	\$110,100
5	705	\$110,100
6	705	\$110,100
7	705	\$110,100
8	705	\$110,100
9	705	\$110,100
10	705	\$110,100
11	705	\$110,100
12	705	\$110,100
13	705	\$117,500
14	705	\$119,700
15	705	\$119,700
16	705	\$159,900
17	705	\$160,900
18	705	\$154,500
19	705	\$154,500
20	705	\$154,500
21	705	\$154,500
22	705	\$147,000
23	705	\$147,000
24	705	\$147,000
25	705	\$147,000
26	705	\$158,700
27	705	\$158,700
28	705	\$149,100
1	706	\$179,600
2	706	\$166,000
3	706	\$166,000
4	706	\$158,000
5	706	\$158,000
6	706	\$158,000
7	706	\$158,000
8	706	\$158,000
9	706	\$158,000
10	706	\$158,000
11	706	\$158,000
12	706	\$158,000
13	706	\$166,000
14	706	\$179,600



Aspen Ridge Phase 1 Sales Map



Legend:

Single Family		Easement 2m		Concrete Pad	
Single Family No Roof Slope Requirement		Easement 3m		Drainage	
Multi-Family Parcels Low/Medium Density		Easement 5m		Park	
Multi-Family Parcels Low Density		Mail Boxes		Buffer	
Mixed Use Commercial		Proposed Bus Stop		Residential Care Home	
Lot Number	12	Corner Garage Location		Fire Hydrant	
Decorative Fence					



Note: The Saskatoon Land does not guarantee the accuracy of this plan. Lot dimensions and the location of other features are compiled from available information and are subject to change without notice. Park and buffer easements are for illustrative purposes only and does not represent what will be constructed. To ensure accuracy, please refer to the Registered Plan of Survey. Distances are in meters unless shown otherwise. Do not scale.



Aspen Ridge

Phase 1 Architectural Controls



ARCHITECTURAL CONTROLS

To enhance the street appeal of Aspen Ridge, Saskatoon Land has selected a variety of architectural controls aimed at creating an aesthetically pleasing streetscape and encouraging a variety of different exterior building materials.

The architectural controls selected by Saskatoon Land are an integral component in strengthening neighbourhood streetscapes and fostering diversity amongst housing choices, creating a warm and inviting environment.

INDEX

- 3 Architectural Controls by Lot Type**
- 3 Standard Lot - Attached Garage**
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ARCHITECTURAL CONTROLS BY LOT TYPE

Phase 1 of Aspen Ridge features three lot types: Standard Lot Attached Garage, Executive Walk-Out Basement Lot Attached Garage and Laned Lot Detached Garage. The architectural controls for each type are noted below:

Standard Lot - Attached Garage

a) No dwelling shall be constructed on any of the lots which has an above-grade floor area (excluding attached decks, patios and garages) less than:

- i. 1,000 square feet in the case of a bungalow, bi-level or split-level dwelling.
- ii. 1,200 square feet in the case of a two-storey dwelling.

Standard Lot Attached Garage

- Lots 1 to 15, Block 701
- Lots 1 to 13, Block 702
- Lots 1 to 10, Block 703
- Lots 16 to 28, Block 705
- Lots 1 to 14, Block 706

b) All dwellings must be constructed with a minimum single-wide attached garage. The garage must be constructed at the same time as the dwelling is built. Minimum inside dimensions shall be 3.5 metres wide and 6.0 metres long.

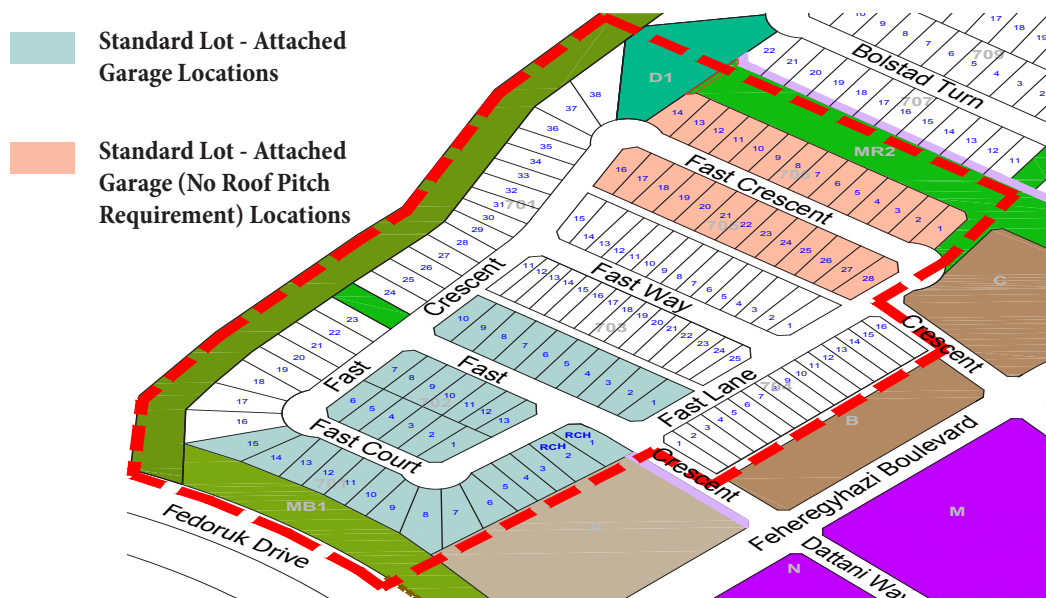
c) The roof of the principal dwelling shall have a minimum 6-in-12 pitch except when it can be demonstrated that a lesser sloped roof pitch is integral to any architectural style and consistent with the intent of strengthening the streetscape and harmonizing the interface between housing forms.

No roof pitch requirement exists for Lots 16 to 28, Block 705 and Lots 1 to 14, Block 706.

d) Attached garages shall not protrude more than 2.4 metres (8 feet) from the facade of any habitable floor area of the dwelling.

e) All attached garages require architecturally significant garage doors consistent with the architectural style of the dwelling.

f) All dwellings require a minimum of two different exterior building materials on the front elevation with each material being no less than 50 square feet in area or a minimum of three exterior building materials with each material being no less than 25 square feet in area.



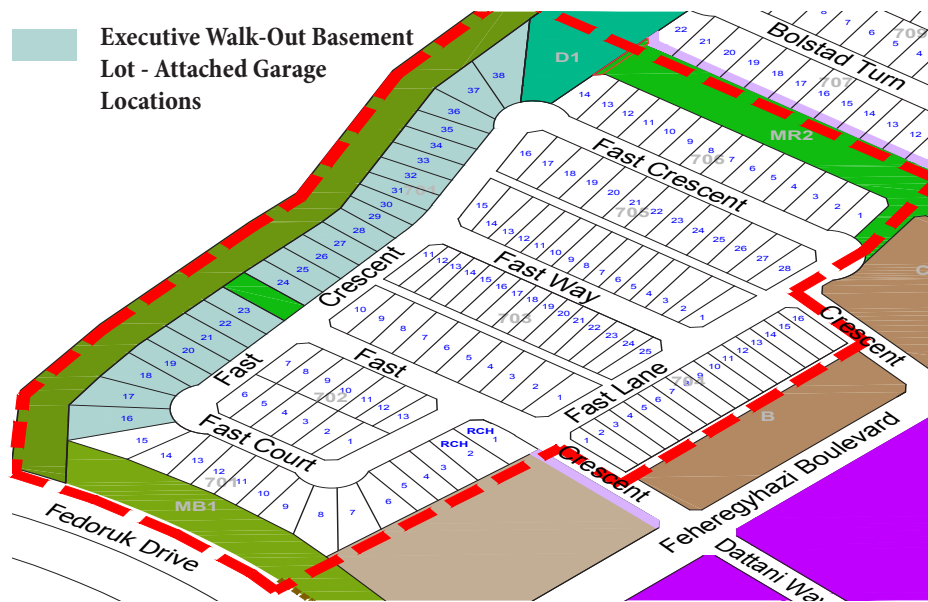


Executive Walk-Out Basement Lot - Attached Garage

Executive Walk-Out Basement
Lot - Attached Garage

- Lots 16 to 38, Block 701

- a) No dwelling shall be constructed on any of the lots which has an above-grade floor area (excluding attached decks, patios and garages) less than:
- 1,200 square feet in the case of a bungalow, bi-level or split-level dwelling.
 - 1,500 square feet in the case of a two-storey dwelling.
- b) All dwellings must be constructed with a minimum double-wide attached garage. The garage must be constructed at the same time as the dwelling is built. Minimum inside dimensions shall be 5.4 metres wide and 6.0 metres long.
- c) The roof of the principal dwelling shall have a minimum 6-in-12 pitch except when it can be demonstrated that a lesser sloped roof pitch is integral to any architectural style and consistent with the intent of strengthening the streetscape and harmonizing the interface between housing forms.
- d) Attached garages shall not protrude more than 2.4 metres (8 feet) from the facade of any habitable floor area of the dwelling.
- e) All attached garages require architecturally significant garage doors consistent with the architectural style of the dwelling.
- f) All dwellings require a minimum of two different exterior building materials on the front elevation with each material being no less than 75 square feet in area or a minimum of three exterior building materials with each material being no less than 40 square feet in area. **No vinyl siding shall be permitted on any building elevation.**
- g) All dwellings shall be constructed with direct access from the basement level to the backyard (walk-out units).



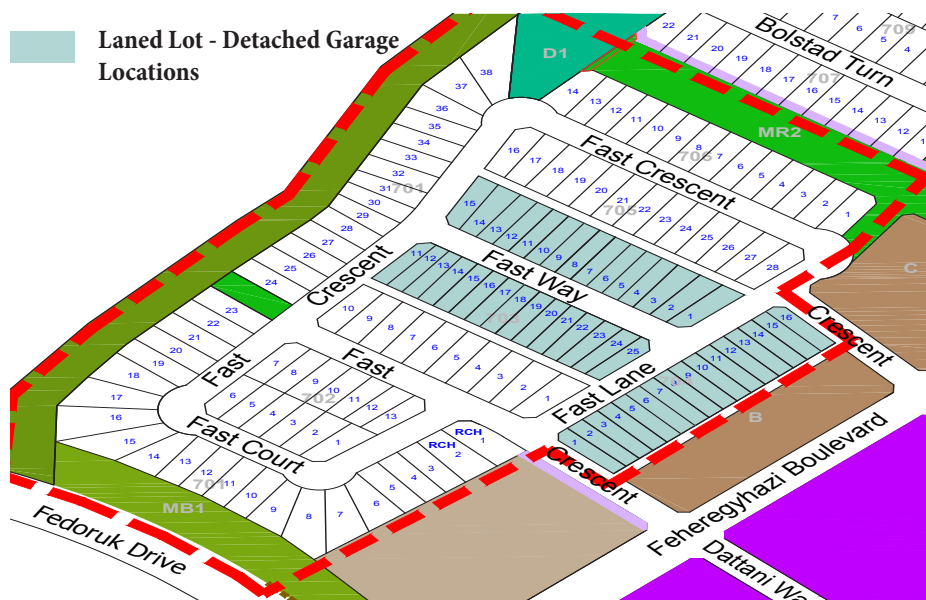


Laned Lot - Detached Garage

- a) No dwelling shall be constructed on any of the lots which has an above-grade floor area (excluding attached decks, patios and garages) less than:
 - i. 1,000 square feet in the case of a bungalow or bi-level.
 - ii. 1,200 square feet in the case of a two-storey dwelling.
- b) All dwelling units shall be bungalows, raised bungalows, bi-levels, or two-storeys. Split-level dwellings are not permitted.
- c) All dwellings shall be constructed with an architecturally significant entry feature which may include a covered veranda, a covered entry feature or a modern contemporary interpretation of a pergola. In the event that columns are used, column styles should be consistent with the overall architectural style of the dwelling.
- d) The roof of the principal dwelling shall have a minimum 6-in-12 pitch except when it can be demonstrated that a lesser sloped roof pitch is integral to any architectural style and consistent with the intent of strengthening the streetscape and harmonizing the interface between housing forms.
- e) All dwellings must be constructed with a concrete garage pad with access from the rear lane only. The concrete garage pad must be constructed at the same time the dwelling is built with a minimum dimension of 6 metres wide and 6 metres long. The concrete pad shall be located at a minimum of 1.2 metres from the rear property line, and include a paved apron that connects it to the rear property line.
- f) All dwellings require a minimum of two different exterior building materials on the front elevation with each material being no less than 50 square feet in area or a minimum of three exterior building materials with each material being no less than 25 square feet in area.

Narrow Lots - Detached Garage

- Lots 11 to 25, Block 703
- Lots 1 to 16, Block 704
- Lots 1 to 15, Block 705



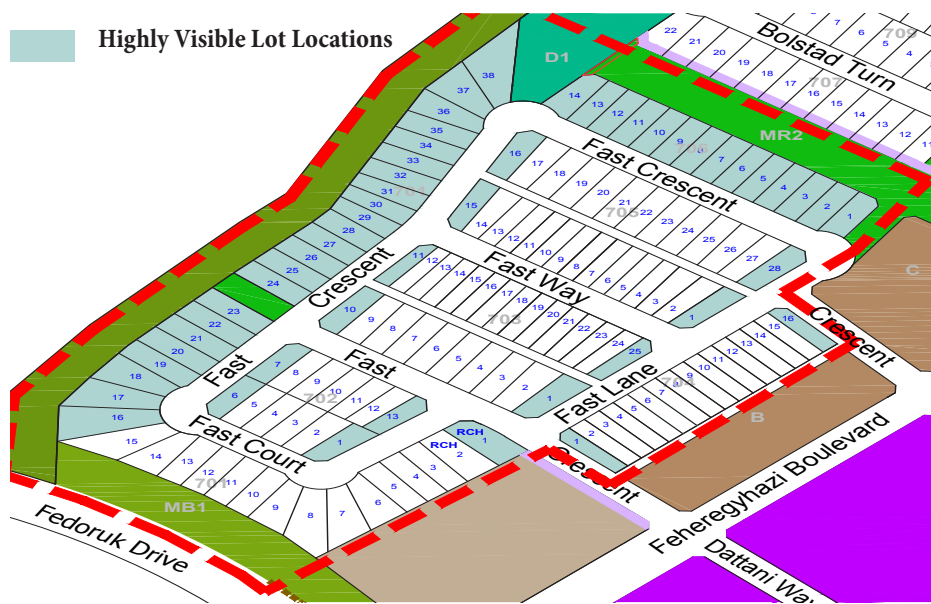


Highly Visible Lots

In addition to the controls noted in the preceding sections, any dwelling elevation facing a street or park also requires the following:

- a) Other than a front facade which has other requirements as noted in this document, any building elevation which flanks a street or faces a park requires a secondary exterior building material no less than 40 square feet in the area.
- b) Other than a front facade which has other requirements as noted in this document, any building elevation which flanks a street or faces a park requires the placement of windows and/or a door that occupy no less than 5 percent of the area of the subject building elevation.

- Highly Visible Lots**
- Lots 1, 16 to 38, Block 701
 - Lots 1, 6, 7, and 13, Block 702
 - Lots 1, 10, 11, and 25, Block 703
 - Lots 1 and 16, Block 704
 - Lots 1, 15, 16 and 28, Block 705
 - Lots 1 to 14, Block 706





EXTERIOR BUILDING MATERIAL REQUIREMENTS

Standard Attached Garage Lots and Laned Detached Garage Lots

All dwellings require a minimum of two different exterior building materials on the front elevation with each material being no less than 50 square feet in area or a minimum of three exterior building materials with each material being no less than 25 square feet in area.

Executive Walk-out Basement Lots

All dwellings require a minimum of two different exterior building materials on the front elevation with each material being no less than 75 square feet in area or a minimum of three exterior building materials with each material being no less than 40 square feet in area. No vinyl siding is permitted on any building elevation.

Note: Material types and areas must be clearly identified on the drawings submitted in support of your Building Permit Application. Failure to do so may result in delays to the approval of your Building Permit Application.



This home utilizes cedar shakes along the roof line and stone accent features near the garage edges, with the balance of the facade consisting of cement fiber siding.



This home features board and batten siding along the dormers, stone accents along the veranda columns, with the balance of the front facade consisting of cement fiber siding.

- *Trim features will not be counted as a material. In order to be included towards the required material amount, the material must be visible from the front street. Material wrapped around a column or architectural feature, facing a side or rear yard will not count towards the required material total.*
- *The materials being used need to be distinctly different and discernible from each other. Contrast with the trim materials and exterior building materials being used is also encouraged.*



EXTERIOR BUILDING MATERIAL REQUIREMENTS HIGHLY VISIBLE LOTS - PARK & STREET FLANKING ELEVATIONS

Other than a front facade which has other requirements as noted in this document, any building elevation which flanks a street or faces a park requires a secondary exterior building material no less than 40 square feet in area.

Other than a front facade which has other requirements as noted in this document, any building elevation which flanks a street or faces a park requires the placement of windows and/or a door that occupy no less than 5 percent of the area of the subject building elevation. Illustrations of building facade requirements are shown below.



50 square feet of a cedar accent material is provided along the rear elevation of the dwelling which faces a linear park. The rear elevation has an area of 1,100 square feet, requiring the placement of a minimum of 55 square feet of window and/or door area. In this example, 124 square feet of window and door area has been provided.



80 square feet of cedar plank is provided along the side building elevation which faces the flanking street. The side elevation facing the flanking street has an area of 840 square feet, requiring the placement of a minimum of 42 square feet of window and/or door area. Five windows are provided resulting in a total window area of 75 square feet.



GARAGE PLACEMENT

For lots requiring an attached garage, one further requirement will also apply to all of the lots that indicates which side of the lot the garage must be placed.

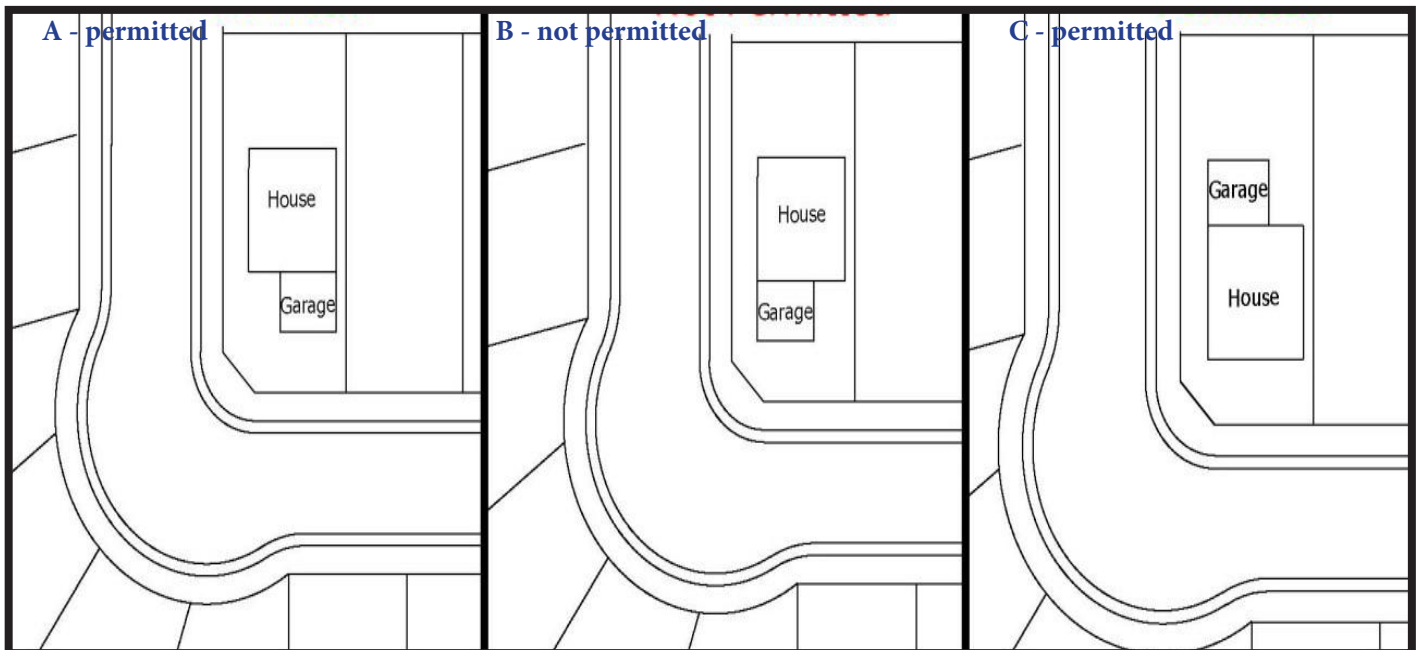
Garage sides on a given block are paired together against a common property line in order to provide a better streetscape appearance.

Corner Lots

For corner attached garage lots, the garage must be located on the side of the property that is furthest from the intersection.

Figure 1 demonstrates the permitted options (A & C) for the placement of an attached garage on a corner lot, attached to the front of the house furthest from the street corner or attached to the rear of the house.

Figure 1 - Corner Lot Garage Placement





ARCHITECTURALLY SIGNIFICANT GARAGE DOORS

The street appeal of a home contributes to the general appeal of any residential area. Garage doors make up a significant portion of the facade of any attached garage home. Any home with a front or rear attached garage requires garage door styles that complement the home's exterior. The garage door must include some architectural features. Examples of garage door types, design elements and corresponding architectural styles are outlined below:

- Coped edges;
- Decorative glass;
- Designed inserts;
- Wide trim pieces;
- Natural woods; and
- Aluminum and glass.

Garage door supplier and model information must be submitted along with your building permit application. Garage doors will be reviewed and approved.



Aluminum composite garage door, suitable for modern contemporary architectural style.



Aluminum composite garage door with left side window placements suitable for modern contemporary architectural style.



Craftsman style garage door featuring wide trim pieces and decorative glass windows.



Garage door with dark finish and coped edges, suitable for traditional architectural style.



ATTACHED GARAGE INTEGRATION

For lots which require an attached garage, the attached garage shall not protrude more than 2.4 metres (8 feet) from the facade of any habitable floor area of the dwelling.

Examples showing acceptable garage projections are illustrated below.



The attached garage projects 2.0 metres from the second storey of the dwelling.



The attached garage projects 2.4 metres from the second storey of the dwelling.



The attached garage projects 2.4 metres from the second storey bonus room.



COLUMNS & ENTRY FEATURES

For lots which require an architecturally significant entry feature, features should be consistent with the overall architectural style of the dwelling.

This may include the use of covered verandas, modern contemporary interpretations of pergolas or covered entry features.

In the event that columns are used, column styles should be consistent with the overall architectural style of the dwelling. This might include the use of wide-based, tapered columns with stone detailing at the column base for craftsman style homes, cedar clad columns for modern contemporary style homes, or columns with masonry cladding along the full length of the column for traditional style homes.



Modern interpretation of a pergola entry.



Covered entry feature, with supporting column.



Covered entry feature, with supporting column.



Covered veranda with substantial columns.



Wide-based, tapered columns, with stone detailing.



Wide-based columns, with stone detailing.



ROOF PITCH

The roof of the principal dwelling shall have a minimum 6-in-12 pitch except in instances where it can be demonstrated that a lesser sloped pitch contributes towards a consistent architectural style or enhanced level of street appeal.

Approval of roof pitches less than 6-in-12 will be at the discretion of Saskatoon Land. Some examples that outline the acceptable use of roof pitches less than 6-in-12 are identified below.



The use of a low-pitched gabled roof for a craftsman style home.



The use of a flat roof or low-sloped roof in a modern contemporary style home.



The use of a low-pitched hip roof, with wide overhanging eaves in a prairie style home.



GENERAL DESIGN CONSIDERATIONS - ALL LOT TYPES

Massing and Relief

All building volumes must incorporate intermittent variances in plan and elevation to encourage shadow lines on facades and visible elevations which will enhance the streetscape by introducing interesting architecture. This includes all building elevations that are adjacent to or visible from public streets, public parks, and adjacent developments.



Repetition

The same elevations should not be repeated on adjacent properties or directly across the street. For adjacent properties, a change in architectural detailing is required.

Examples of acceptable changes are, but not limited to, the following: roof and dormer orientation and slope, front attached garage roof style or orientation, the size and location of windows and doors, colours and finish materials.



Front Door Placement

It is strongly encouraged that the front door of a dwelling be highly visible on the front facade by directly facing the fronting street.

If the door does not face the street, architectural elements such as foyer windows, additional accent materials or other architectural elements must be added to enhance the house entrance.





BUILDING PERMIT APPLICATION - PLAN SUBMITTAL REQUIREMENTS

As a summary to the requirements outlined in this document, please ensure the following information is included in your building permit application:

Standard Lot Attached Garage & Executive Walk-Out Basement Lot Attached Garage Types

- Exterior building material types and areas along the front facade of the dwelling;
- For highly visible lots, exterior building material areas, and window and door areas for building elevations that flank a street or face a park; and
- Garage door supplier and model information.

Laned Lot Detached Garage

- Exterior building material types and areas along the front facade of the dwelling; and
- For highly visible lots, exterior building material areas, and window and door areas for building elevations that flank a street or face a park.

Failure to include this information may result in delays in the processing of your building permit application. Building permit applications must be submitted to the Building Standards Division.

Interpretation and application of the Aspen Ridge Architectural Controls are at the sole discretion of Saskatoon Land. Saskatoon Land's decision is final. Failure to comply with these controls may result in removal from Saskatoon Land's eligible contractor list.

For any questions on the requirements and guidelines included in this document, please contact Saskatoon Land at (306)-975-3278 or by email at land@saskatoon.ca.





STANDING POLICY COMMITTEE ON TRANSPORTATION

2016 Neighbourhood Traffic Management Reviews

Recommendation of the Committee

That the eight neighbourhoods selected for 2016 traffic reviews, as part of the Neighbourhood Traffic Management Program, include Stonebridge, Willowgrove, Hampton Village, Silverspring, Grosvenor Park, Lakeridge, Sutherland, and Parkridge.

History

At the October 13, 2015 Standing Policy Committee on Transportation meeting, a report of the General Manager, Transportation & Utilities Department, dated October 13, 2015 was considered.

Attachment

October 13, 2015 report of the General Manager, Transportation & Utilities Department.

2016 Neighbourhood Traffic Management Reviews

Recommendation

That the Standing Policy Committee on Transportation recommend to City Council:
That the eight neighbourhoods selected for 2016 traffic reviews, as part of the Neighbourhood Traffic Management Program, include Stonebridge, Willowgrove, Hampton Village, Silverspring, Grosvenor Park, Lakeridge, Sutherland, and Parkridge.

Topic and Purpose

This report identifies the eight neighbourhoods selected for traffic reviews in 2016. The traffic reviews are intended to address local traffic concerns such as speeding, shortcutting, pedestrian accommodation, and parking.

Report Highlights

The eight neighbourhoods selected for traffic reviews include Stonebridge, Willowgrove, Hampton Village, Silverspring, Grosvenor Park, Lakeridge, Sutherland, and Parkridge based on Councillor input, collision history, number of concerns received and number of existing temporary traffic calming devices.

Strategic Goal

This report supports the Strategic Goal of Moving Around as it improves the safety of all road users (pedestrians, cyclists, and drivers), and helps provide a great place to live, work and raise a family.

Background

City Council, at its meeting held on August 14, 2013, approved a new process within the Neighbourhood Traffic Management Program. This process includes a strategy to review concerns on a neighbourhood-wide basis by engaging the community and stakeholders in first identifying specific traffic issues, and secondly, developing joint recommendations that address the issues.

In 2014, Neighbourhood Traffic Plans were developed for the following neighbourhoods: Varsity View, Westmount, Brevoort Park, Holliston, Haultain, Hudson Bay Park, Caswell Hill, City Park, Mayfair, and Nutana.

In 2015, Neighbourhood Traffic Plans are being developed for the following neighbourhoods: Mount Royal, Adelaide-Churchill, Lakeview, Meadowgreen, Montgomery Place, Confederation Park, Avalon, and Greystone Heights.

In addition, City Council, at its meeting held on April 27, 2015, reviewed the report on parking issues on Avenue M South and Traffic Safety Concerns in the West Industrial Area and resolved, in part:

- “3. That the Administration report back when the King George neighbourhood traffic review would fall in to the program for scheduling.”

Report

Neighbourhoods were prioritized based on the following criteria:

- Councillor input (3 points per selection);
- Collisions (0 points for low, 1 point for medium, 2 points for high);
- Number of outstanding concerns (1 point per concern); and
- Number of temporary traffic calming devices in place (1 point per device).

This process results in the following neighbourhoods selected for traffic reviews in 2016:

- Stonebridge (Ward 7);
- Willowgrove (Ward 10);
- Hampton Village (Ward 4);
- Silverspring (Ward 10);
- Grosvenor Park (Ward 6);
- Lakeridge (Ward 9);
- Sutherland (Ward 1); and
- Parkridge (Ward 3).

Speeding concerns in other neighbourhoods will be addressed through the Speed Management Program through use of speed radar signs, and educational/awareness campaigns.

The prioritization of the neighbourhoods is illustrated in Attachment 1. Based on the prioritization criteria, the King George neighbourhood is anticipated to be reviewed in 2017.

The neighbourhoods reviewed since this process began, and distribution citywide is shown in Attachment 2.

Public and/or Stakeholder Involvement

Public meetings will be held for each of the eight neighbourhoods, including an initial meeting with residents and stakeholders to identify specific traffic concerns and potential improvements, and a second meeting to present a neighbourhood draft traffic plan for discussion. A third meeting may be required if significant changes to the traffic plan are required.

Residents and business owners who cannot attend the meetings will be able to provide feedback via the City of Saskatoon's online neighbourhood traffic concerns form, Shaping Saskatoon.ca website, or by phone, email, or mail.

The initial meetings will be held in spring 2016, and the second meetings in fall 2016. It is expected that traffic patterns will change after the completion of the interchange at Victor Road and Highway 11 in Stonebridge.

Depending on the date of completion of the interchange, consultation for the Stonebridge traffic review may carry over into 2017.

The City of Saskatoon's internal departments will have an opportunity to provide input on the plan pertaining to the impact on their operations.

Communication Plan

Residents and stakeholders in each neighbourhood will be invited to attend both meetings. The meeting invitations will be provided as follows:

- A flyer delivered to each residence in the neighbourhood;
- Through the Shaping Saskatoon.ca website;
- Through requesting the neighbourhood community associations to post the information on their website or Facebook page; and
- By notifying the appropriate Councillor.

The collection of issues and potential improvements will be completed through the following:

- The Shaping Saskatoon.ca website;
- Written submissions at the meetings;
- Written notes taken by the Administration at the meetings; and
- Written, verbal, and e-mail submission to the Administration.

Financial Implications

The resources required to undertake the neighbourhood traffic reviews outlined in this report is estimated at \$350,000, and will be submitted for approval as part of the 2016 Corporate Business Plan and Detailed Budget under Capital Project #1512 – Neighbourhood Traffic Management funded from the Traffic Safety Reserve.

Improvements identified in the traffic plans are funded through the Traffic Safety Reserve. The purpose of the Traffic Safety Reserve is to provide funding for vehicular traffic, pedestrian and safety related projects including traffic calming. It is funded from the City's share of the fine revenue generated from red light cameras and automated speed enforcement (ASE). It is estimated that in 2016, there will be approximately \$1 Million available from the red light camera program. At this time, no projections have been made for revenues from the ASE pilot program due to the lack of historical trending data. Any revenues generated from the ASE pilot program in 2015 or 2016 will be allocated for 2017 projects. This funding is typically used to address neighbourhood traffic and pedestrian crossing concerns, rail crossing improvements, or traffic safety improvements.

In recent years, less than \$100,000 per year has been available for neighbourhood-level improvements. In 2016, this amount is expected to increase to \$310,000.

Environmental Implications

Neighbourhood traffic reviews are expected to have positive greenhouse gas emissions implications, as the tendency is to reduce total vehicle mileage in an area by reducing speeds and improving conditions for walking, cycling and transit use.

Other Considerations/Implications

There are no options, policy, privacy, or CPTED considerations or implications.

Due Date for Follow-up and/or Project Completion

A report presenting the traffic plan will be presented for each neighbourhood and an annual report outlining the following years' selections will be presented to City Council.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Attachments

1. Neighbourhood Prioritization List
2. Neighbourhood Selections

Report Approval

Written by: Jay Magus, Engineering Section Manager, Transportation
Justine Nyen, Transportation Engineer, Transportation

Reviewed by: Angela Gardiner, Director of Transportation

Approved by: Jeff Jorgenson, General Manager, Transportation & Utilities
Department

TRANS JN – 2016 Neighbourhood Traffic Management Reviews.docx

Neighbourhood Prioritization List

Attachment 1

Neighbourhood	# of Concerns	Temporary Traffic Calming	Collisions	Councillor Selection	TOTAL SCORE	Year of Review	Ward	Councillor
Stonebridge	36		1	3	40		7	Loewen
Willowgrove	32		1		33		10	Jeffries
Hampton Village	26		1	3	30		4	Davies
Sutherland	14	1	1	3	19		1	Hill
Silverspring	12	1	0	3	16		10	Jeffries
Grosvenor Park	12		0	3	15		6	Clark
Lakeridge	7	2	0	3	12		9	Paulsen
Parkridge	8	1	0	3	12		3	Iwanchuk
Silverwood Heights	6	1	1	3	11		5	Donauer
Pleasant Hill	6	0	2	3	11		2	Lorje
Dundonald	8	1	0		9		4	Davies
College Park	5		1	3	9		8	Olauson
Riversdale	1	5	2		8		2	Lorje
Pacific Heights	7		0		7		3	Iwanchuk
Buena Vista	7		0		7		6	Clark
Exhibition	6		1		7		7	Loewen
Westview	5	1	0		6		4	Davies
Wildwood	4		2		6		9	Paulsen
King George	4	1	0		5		2	Lorje
Briarwood	4		1		5		8	Olauson
North Park	3	1	0		4		1	Hill
Fairhaven	3		1		4		3	Iwanchuk
Massey Place	3	1	0		4		4	Davies
River Heights	3		1		4		5	Donauer
Eastview	3	1	0		4		7	Loewen
Queen Elizabeth	3		1		4		7	Loewen
Forest Grove	3		0		3		1	Hill
Lakewood SC	2		1		3		9	Paulsen
Rosewood	3		0		3		9	Paulsen
Erindale	1	2	0		3		10	Jeffries
Evergreen	2		1		3		10	Jeffries
Richmond Heights	2		0		2		1	Hill
Holiday Park	2		0		2		2	Lorje
Lawson Heights	2		0		2		5	Donauer
Nutana SC	0		2		2		7	Loewen
Arbor Creek	2		0		2		10	Jeffries
Blairmore	1		0		1		3	Iwanchuk
College Park East	1		0		1		8	Olauson
Nutana Park	0		0		0		7	Loewen
The Willows	0		0		0		7	Loewen
Brevoort Park						2014	8	Olauson
Caswell Hill						2014	2	Lorje
City Park						2014	1	Hill
Haultain						2014	2	Lorje
Holliston						2014	6	Clark
Hudson Bay Park						2014	1	Hill
Kelsey-Woodlawn						2014	1	Hill
Mayfair						2014	1	Hill
Nutana						2014	6	Clark
Varsity View						2014	6	Clark
Westmount						2014	4	Davies

Confederation Park						2015	3	Iwanchuk
Montgomery Place						2015	2	Lorje
Greystone Heights						2015	8	Olauson
Avalon						2015	7	Loewen
Lakeview						2015	9	Paulsen
Meadowgreen						2015	2	Lorje
Mount Royal						2015	4	Davies
Adelaide-Churchill						2015	7	Loewen

Neighbourhood Selections

Ward	Councillor	2014	2015	2016	TOTAL
1	Hill	4	0	1	5
2	Lorje	2	2		4
3	Iwanchuk	0	1	1	2
4	Davies	1	1	1	3
5	Donauer	0	0		0
6	Clark	3	0	1	4
7	Loewen	0	2	1	3
8	Olauson	1	1		2
9	Paulsen	0	1	1	2
10	Jeffries	0	0	2	2



STANDING POLICY COMMITTEE ON TRANSPORTATION

33rd Street – Boulevard Removal – Parking and Traffic Operations

Recommendation of the Committee

1. That the 33rd Street Corridor Study be adopted as the framework for future traffic improvements along 33rd Street, to be undertaken as funding is made available through the annual budget process;
2. That the Administration continue consultations with the 33rd Street Business Improvement District and all abutting Community Associations, regarding the future of traffic improvements along 33rd Street; and
3. That the Administration continue discussions with stakeholders regarding pedestrian safety measures (pedestrian bulbing) and the number of lanes of traffic between Avenues B and G.

History

At the October 13, 2015 Standing Policy Committee on Transportation meeting, a report of the General Manager, Transportation & Utilities Department, dated October 13, 2015 was considered.

Attachment

October 13, 2015 report of the General Manager, Transportation & Utilities Department.

33rd Street – Boulevard Removal – Parking and Traffic Operations

Recommendation

That the Standing Policy Committee on Transportation recommend to City Council:
That the 33rd Street Corridor Study be adopted as the framework for future traffic improvements along 33rd Street, to be undertaken as funding is made available through the annual budget process.

Topic and Purpose

The purpose of this report is to provide information on the 33rd Street Corridor Study which addresses concerns pertaining to future traffic improvements to improve efficiency, parking and pedestrian safety.

Report Highlights

1. The 33rd Street Corridor Plan from Idylwyld Drive to Confederation Drive was developed in consultation with the community in response to concerns such as traffic congestion and operations, parking restrictions, and pedestrian safety.
2. Recommendations for improvements are provided within the Corridor Plan.

Strategic Goal

This report supports the Strategic Goal of Moving around by providing a plan to guide the implementation of pedestrian infrastructure and intersection improvements that will improve the safety of pedestrians, motorists, and cyclists.

Background

City Council, at its meeting held on May 5, 2014, passed a motion that the proposed changes outlined below be reevaluated after the public consultation process has been completed with all neighbourhoods impacted along the 33rd Street Corridor.

- “Two-hour parking restrictions along 33rd Street from Avenue B to Avenue D, to accommodate sufficient parking turnover for the businesses on 33rd Street West.
- Implementation of left-turn restrictions along 33rd Street West, Monday through Friday, at Avenue B and C eastbound during the morning peak hours of 7:00 am to 9:00 am and westbound during the evening peak hours of 4:00 pm to 6:00 pm. In order to increase pedestrian safety, pedestrian pavement markings and signage will be enhanced along 33rd Street west, from Ave B to E.”

Report

33rd Street Corridor Study

The Administration completed a corridor study of 33rd Street west of Idylwyld to resolve existing traffic issues along the corridor. The completed study is intended to provide a number of recommendations that, once implemented, will improve the level of safety for all road users including pedestrians, cyclists, and motorists.

A long-term review of 33rd Street has been deferred until the completion of the Growth Plan. In consideration of this, significant physical changes such as revising the width of 33rd Street, adding additional lanes or significantly altering intersections was not considered at this time.

The development and implementation of the 33rd Street Corridor Plan includes four stages:

1. Identifying existing problems, concerns and possible solutions through neighbourhood consultation, data collection and assessment;
2. Developing corridor improvement recommendations based on the public's input and engineering assessments;
3. Presenting the draft traffic corridor plan to the 33rd Street businesses and residents at two follow-up meetings; and
4. Implementing the proposed measures in a specific time frame: short-term (1 to 2 years), medium-term (3 to 5 years), or long-term (more than 5 years).

The majority of concerns identified during the 33rd Street corridor consultation included: pedestrian safety, traffic congestion and operations, parking restrictions, and traffic signal issues.

Details of the 33rd Street Corridor Study are included in Attachment 1.

Modifications to Improve Traffic Flow and Safety

The Administration is recommending modifications to improve safety and operations along 33rd Street between Idylwyld Drive to Confederation Drive:

- Replace depressed sidewalks at five locations between Avenue B and Avenue C, and one location between Avenue E and Avenue F;
- Install sidewalk ramps at four locations;
- Install sidewalk between Avenue P and the east edge of Vic Rempel Park (south side);
- Install pathway between the east edge of Vic Rempel Park and Edmonton Avenue (south side);
- Install barrier at Avenue H (north side);
- Replace boulevard with concrete between Avenue B and Avenue C (south side);
- Install an Active Pedestrian Corridor at Avenue C (east side of south leg);
- Install a zebra crosswalk at Avenue E (west side of south leg);
- Replace pedestrian actuated signals with active pedestrian corridor at Avenue F (east side of south leg);

- Upgrade pedestrian actuated signals with traffic signals at Northumberland Avenue;
- Upgrade traffic signals and maintain existing crossings at Avenue D;
- Re-configure southern and northern legs of the Avenue P intersection by constructing protected left turn lanes; and
- Construct a bus lay-by at Avenue W (southeast corner).

The Administration is not recommending the following:

- Changes to the current parking restrictions;
- Implementing left-turn restrictions; or
- Significant major intersection improvements such as revising the cross-section or widening the road.

Installation of each proposed improvement will be implemented in three specific time frames as follows:

Short-term (1 to 2 years)	Sidewalk protection, zebra crosswalk
Medium-term (3 to 5 years)	Depressed sidewalks, sidewalk ramps, concrete boulevard, active pedestrian corridors
Long-term (5 years plus)	Intersection improvements, sidewalk, pathway, bus-lay-by

Public and/or Stakeholder Involvement

- January 2014 mail-out survey requesting feedback on the parking restrictions, left-turn restrictions, and pedestrian improvements at Avenue C and Avenue D.
- Public meetings were held: November 2013 in Mayfair, April 2014 in Caswell Hill and Hudson Bay Park to identify traffic concerns and potential solutions within the 33rd Street corridor. As a result of the meetings, a number of traffic studies were completed to confirm and quantify the concerns raised by the residents.
- Based on the residents input and the completed traffic assessments, the Corridor Plan was developed and presented at follow-up meetings held on April 23, 2015 and April 29, 2015.

Public feedback details are included in the attached study report.

Communication Plan

Once approved, the final 33rd Street Corridor Plan will be shared with the residents and business along 33rd Street and the impacted neighbourhoods using several methods: City website, Community Association communication forums (i.e. website, newsletter). Any construction that impacts businesses, residents or traffic will be communicated via construction letters, Traffic Detour Service Alerts, the Daily Road Report and the Road Restrictions and Construction Projects interactive map.

Financial Implications

The implementation of the 33rd Street Corridor Plan will have financial implications. The costs are summarized in the following table.

Timing for implementation is dependent on available funding in the respective capital programs.

Item	2016 and forward	Capital Program	Funding Source
Pedestrian Improvements (depressed sidewalk, ramp, sidewalk and pathway)	\$115,000	<ul style="list-style-type: none"> CP1963 Corporate Accessibility Implementation CP0948 Sidewalk Retrofit 	<ul style="list-style-type: none"> Transportation Infrastructure Reserve Active Transportation Reserve
Pedestrian crosswalk improvements	\$ 70,000	<ul style="list-style-type: none"> CP2446 Pedestrian Improvements 	<ul style="list-style-type: none"> Traffic Safety Reserve
Traffic Signals	\$235,000	<ul style="list-style-type: none"> CP1036 Traffic Control Upgrades 	<ul style="list-style-type: none"> Transportation Infrastructure Expansion Reserve
Intersection Improvements	\$125,000	<ul style="list-style-type: none"> CP2235 Intersection Improvements 	<ul style="list-style-type: none"> Transportation Infrastructure Expansion Reserve & Traffic Safety Reserve
Safety Improvements	\$ 2,500	<ul style="list-style-type: none"> CP0631 Traffic Safety 	<ul style="list-style-type: none"> Traffic Safety Reserve
TOTAL	\$547,500		

Environmental Implications

The overall impact of the recommendations on traffic characteristics including the impacts on greenhouse gas emissions has not been quantified at this time.

Other Considerations/Implications

There are no options, policy, privacy or CPTED considerations or implications.

Due Date for Follow-up and/or Project Completion

A follow-up report or project completion is not required.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Attachment

- 33rd Street Corridor Study – August 13, 2015

Report Approval

Written by: Jay Magus, Engineering Section Manager, Transportation
 Reviewed by: Angela Gardiner, Director of Transportation
 Approved by: Jeff Jorgenson, General Manager, Transportation & Utilities Department

City of Saskatoon



33rd Street Corridor Study

August 13, 2015

Transportation & Utilities Department

Acknowledgements

The completion of this study would not be possible without the contribution of the following organizations and individuals:

- Mayfair, Caswell Hill, Hudson Bay Park, Massey Place, Westview and Dundonald residents
- Mayfair, Caswell Hill, Hudson Bay Park, Massey Place, Westview and Dundonald Community Associations
- 33rd Street Business Improvement District
- Councillor Darren Hill
- Councillor Pat Lorje
- Councillor Troy Davies
- City of Saskatoon Transit
- City of Saskatoon Transportation
- Great Works Consulting

Prepared By: Jay Magus, P.Eng., Engineering Manager, Transportation
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EXECUTIVE SUMMARY

There has been a significant amount of discussion amongst the public, City Council, and the Administration regarding the 33rd Street corridor west of Idylwyld Drive. The issues discussed typically included number of traffic lanes for through traffic, parking restrictions, the feasibility of restricting left turns, and pedestrian accommodation.

The Administration has completed this corridor assessment of 33rd Street to resolve the outstanding issues, and present recommendations that address today's transportation issues along the corridor. The study is intended to provide a number of initiatives that once implemented will improve the level of safety for all road users, including pedestrians, cyclists, and motorists. A long term review of 33rd Street has been deferred until the completion of the Growing Forward! Shaping Saskatoon project is complete. In consideration of this concurrent long range planning effort currently taking place, significant physical changes such as revising the width of the 33rd Street, adding additional lanes, or significantly altering intersections were not considered at this time.

The 33rd Street Corridor study objectives strongly align with the principles of the City of Saskatoon's Strategic Goal of Moving Around.

There were phased recommendations aimed at improving the flow of traffic that were adopted by City Council at its meeting held on **January 21, 2013**. The first phase recommendations were:

- Restrict parking between Avenue B and Avenue D during AM for eastbound and PM for westbound traffic;
- Restrict left turning movements at Avenue B and Avenue C from 7:00 AM to 9:00 AM for eastbound traffic and from 4:00 PM to 6:00 PM for westbound traffic; and
- Improve pedestrian safety at Avenue K with installation of an Active Pedestrian Corridor (the Active Pedestrian Corridor was installed back in 2012 at Avenue K).

The second phase recommendations were:

- Restrict parking between Avenue F to Confederation from 7:00 AM to 9:00 AM for eastbound traffic and from 4:00 PM to 6:00 PM for westbound traffic;
- Restrict left turning movement for eastbound and westbound traffic at Avenue D with the placement of right in/right out islands;
- Improve pedestrian safety at Avenue C and Avenue D with installation of Active Pedestrian Corridor devices.

Further public consultation with the Mayfair and Caswell Hill communities was to occur with respect to Phase 2. Also, the Administration was asked to co-ordinate with the Mayfair Local Areas Planning process and with the review of the Caswell Residential Parking Permit Program.



City Council, at its meeting held on **May 5, 2014**, adopted a motion to rescind the recommendations regarding parking restrictions and left turning restrictions along 33rd Street pending a re-evaluation after the public consultation process. This report is in response that this motion.

There has been a significant amount of public consultation, including:

- January 2014 mailout survey requesting feedback on the parking restrictions, left turn restrictions, and pedestrian improvements at Avenue C and Avenue D.
- Public meetings were held in November of 2013 in Mayfair and April 2014 in Caswell Hill and Hudson Bay Park to identify traffic concerns and potential solutions within the 33rd Street corridor. As a result of the meetings a number of traffic assessments were completed to quantify the concerns raised by the residents.
- Based on the residents input and the completed traffic assessments, a traffic corridor plan was developed and presented at follow-up meetings held on April 23, 2015 and April 29, 2015.

A summary of the recommended improvements for the 33rd Street corridor are included in **Table ES-1**. The table summarizes the recommendation, the location, the estimated cost, and the time frame for each. The schedule to implement the recommendations can vary on the complexity of the proposed improvement and available funding.



Table ES-1: 33rd Street Corridor Recommended Improvements

Recommendation	Location	Estimated Cost	Time Frame
Replace depressed sidewalks	<ul style="list-style-type: none"> • 5 locations between Avenue B and Avenue C • 1 location between Avenue F and Avenue E 	\$10,000	3-5 yrs
Install sidewalk ramps	<ul style="list-style-type: none"> • SE and NE Corner of Avenue B • SE and SW corner of Avenue D • SE corner of Avenue E (west crosswalk location) • SE corner of Idylwyld Drive 	\$20,000	3-5 yrs
Install Sidewalk	<ul style="list-style-type: none"> • South side of 33rd Street between Avenue P and east edge Vic Rempel Park 	\$35,000	5 yrs
Install Pathway	<ul style="list-style-type: none"> • South side of 33rd Street between east edge Vic Rempel Yards Park and Edmonton Avenue 	\$15,000	5 yrs
Sidewalk protection – replace jersey barrier with bollards	<ul style="list-style-type: none"> • Avenue H (north side) 	\$2,000	1 yr
Replace boulevard with concrete	<ul style="list-style-type: none"> • 33rd Street (south side between Avenue B and Avenue C) 	\$35,000	3-5 yr
Active Pedestrian Corridor	<ul style="list-style-type: none"> • Avenue C east side of south leg 	\$35,000	3-5 yrs
Zebra Crosswalk	<ul style="list-style-type: none"> • Avenue E west side of south leg 	\$500	1 yr
Replace Pedestrian Actuated Signals with Active Pedestrian Corridors	<ul style="list-style-type: none"> • 33rd Street & Avenue F (east side of south leg) 	\$35,000	3-5 yrs
Upgrade Pedestrian Actuated Signals with Traffic Signals (10)	<ul style="list-style-type: none"> • Northumberland Avenue 	\$100,000	5 yrs
Upgrade Traffic Signals and maintain existing crossings	<ul style="list-style-type: none"> • Avenue D 	\$135,000	5 yrs
Re-configure southern and northern legs of intersection by building protected left turn lanes	<ul style="list-style-type: none"> • Avenue P 	\$25,000	5 yrs
Construct bus-lay-by	<ul style="list-style-type: none"> • Avenue W (southeast corner) 	\$100,000	5 yrs



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APPENDIX A – PREVIOUS COUNCIL REPORTS AND MOTIONS

APPENDIX B – PUBLIC MEETINGS INFORMATION



1 INTRODUCTION

1.1 Background

There has been a significant amount of discussion amongst the public, City Council, and the Administration regarding the 33rd Street corridor west of Idylwyld Drive. The issues discussed typically included number of traffic lanes for through traffic, parking restrictions, the feasibility of restricting left turns, and pedestrian accommodation.

The Administration has completed this corridor assessment of 33rd Street to resolve the outstanding issues, and present recommendations that address today's transportation issues along the corridor. The study is intended to provide a number of initiatives that once implemented will improve the level of safety for all road users, including pedestrians, cyclists, and motorists.

In 2013 City Council adopted the City of Saskatoon Strategic Plan 2013-2023 that outlined seven strategic goals, including 'Moving Around'. This strategic goal identified the following as a strategy:

"Develop an integrated transportation network that is practical and useful for vehicles, buses, bikes and pedestrians."

Two priorities within this strategic goal were for the current term of Council are:

"Prepare a transportation plan and table a budget to develop a mix of transportation modes, address downstream effects and promote active transportation."

"Explore options to improve curb, sidewalk and facility accessibility for wheelchair users and citizens with limited physical mobility."

The 33rd Street Corridor study objectives strongly align with the principles of the City of Saskatoon's Strategic Goal of Moving Around.

A long term review of 33rd Street has been deferred until the completion of the Growing Forward! Shaping Saskatoon project is complete. In consideration of this concurrent long range planning effort currently taking place, significant physical changes such as revising the width of the 33rd Street, adding additional lanes, or significantly altering intersections was not considered at this time. It would not be prudent to develop a long term plan that may conflict with the Growing Forward! Shaping Saskatoon project outcomes.

1.2 Study Scope

The study addresses 33rd Street from Idylwyld Drive to Confederation Drive, a distance of approximately 3.8 kilometres. The 33rd Street / Circle Drive interchange ramp intersections were included in the study.



1.3 Study Objectives

The main objective of the study was to develop a transportation plan for 33rd Street from Idylwyld Drive to Confederation Drive. The plan will identify immediate improvements required to address existing transportation demand for all road users.

The following transportation components will be addressed in the plan:

- Number of travel lanes
- On-street parking
- Pedestrian accommodation
- Type of traffic control (signals vs. signs)
- Type of intersection (roundabout vs. conventional)

1.4 Study Methodology

To achieve the study objectives outlined above, the study methodology will employ the following tasks:

- Review of previous City Council reports and documentation
- Review of feedback from the previously completed public consultation
- Collection of traffic and pedestrian data
- Review and analysis of existing traffic conditions
- Identification of preliminary recommendations
- Public meeting to present and discuss preliminary recommendations
- Refine and finalize recommendations for short-term improvements
- Prepare technical report summarizing the study
- Present report to Standing Policy Committee on Transportation
- Present report to City Council
- If adopted, implement recommendations as budget becomes available

1.5 Traffic Analysis Methodology

The daily peak hour volumes were evaluated using Synchro version 9. Synchro is based on the U.S. Highway Capacity Manual (HCM). In the HCM methodology, Level-of-Service (LOS) is the primary evaluation criteria for operating conditions. For signalized and unsignalized intersections, the LOS is based on the computed delays. LOS 'A' represents minimal delay to minor street traffic movements, and LOS 'F' represents a scenario with an insufficient number of gaps on the major street for minor street motorists to complete their movements without significant delays. For signalized intersections the methodology considers the intersection geometry, traffic volumes and composition, the traffic signal/timing plan, and pedestrian



volume. The average delay for each lane group is calculated, as well as the average delay for the overall intersections.

Also for signalized intersections, the ‘volume-to-capacity’ (v/c) ratio is used as an indicator of the extent to which a particular movement’s capacity is being utilized.

The HCM intersections capacity evaluation criteria for both unsignalized and signalized intersections are summarized in the following **Table 1-1**.

Table 1-1: Level of Service Criteria

Level of Service	Average Delay for UNSIGNALIZED Intersection Movements	Average Delay for SIGNALIZED Intersection Movements
A	0-10 sec. per vehicle	0-10 sec per vehicle
B	>10 – 15 sec per vehicle	>10-20 sec per vehicle
C	>15-25 sec per vehicle	>20-35 sec per vehicle
D	>25-35 sec per vehicle	>35-55 sec per vehicle
E	>35-50 sec per vehicle	>55-80 sec per vehicle
F	>50 sec per vehicle	>80 sec per vehicle

Typically an individual intersection movement of LOS E or worse is an indication that improvements may be required.

This report presents the study findings and functional plan recommendations.



2 PREVIOUS DOCUMENTATION

City Council, at its meeting held on **September 17, 2007**, considered correspondence and a presentation from Bruce and Elaine Thomas, Mayfair Hardware, requesting a two-hour parking restriction along 33rd Street West, between Avenue B North and Avenue D North, to facilitate improved parking turnover for local businesses.

The following inquiry was made by former Councillor Heidt at the meeting of City Council held on **September 12, 2011**:

“As we all know, the traffic on the west side is increasing and there is only 22nd Street and 33rd Street that carry all of this traffic from west of Circle Drive to Spadina Crescent. Would the Administration please look at removing the boulevard from street to sidewalk from Confederation Drive to Circle Drive, Circle Drive to Avenue H and Avenue H to Idylwyld Drive and done in phases. Please report on the possibility and cost of this activity, which would provide another lane and much needed safety.”

The following recommendations were adopted by City Council at its meeting held on **January 21, 2013**:

- “1) that left turn restrictions be implemented during peak hours at the intersections of 33rd Street and Avenues B and C as part of the Phase 1 improvements outlined in the December 21, 2012 report of the General Manager, Infrastructure Services Department; and*
- 2) that the Administration proceed with public consultation for Phase 2, and report back to City Council.”*

The first phase recommendations were:

- Restrict parking between Avenue B and Avenue D during AM for eastbound and PM for westbound traffic;
- Restrict left turning movements at Avenue B and Avenue C from 7:00 AM to 9:00 AM for eastbound traffic and from 4:00 PM to 6:00 PM for westbound traffic; and
- Improve pedestrian safety at Avenue K with installation of an Active Pedestrian Corridor (installed back in 2012 at Avenue K).

The second phase recommendations were:

- Restrict parking between Avenue F to Confederation from 7:00 AM to 9:00 AM for eastbound traffic and from 4:00 PM to 6:00 PM for westbound traffic;
- Restrict left turning movement for eastbound and westbound traffic at Avenue D with the placement of right in/right out islands; and



- Improve pedestrian safety at Avenue C and Avenue D with installation of Active Pedestrian Corridor devices.

Further public consultation with the Mayfair and Caswell Hill communities was to occur with respect to Phase 2. Also, the Administration was asked to co-ordinate with the Mayfair Local Areas Planning process and with the review of the Caswell Residential Parking Permit Program.

Copies of the Council Reports discussing the above are included in **Appendix A**.

City Council, at its meeting held on **May 5, 2014**, considered and carried the following motions moved by Councillor Hill, and seconded by Councillor Lorje:

“THAT City Council rescind the first two points of the motion which was passed by City Council at its meeting held on January 21, 2013, approving the adoption of Phase 1 of 33rd Street Changes and that the third point pertaining to the 33rd Street and Avenue K corridor remain...”

“THAT the proposed changes, in the first two points, be reevaluated after the public consultation process has been completed with all neighbourhoods impacted along the 33rd Street corridor.”

A copy of this motion is included in **Appendix A**.

This study is in response to the above motion by reporting on the public consultation and providing new recommendations.



3 PREVIOUS PUBLIC CONSULTATION

3.1 January 2014 Mailout Survey

Public consultation occurred via letters mailed to residents in **January of 2014** requesting feedback on the parking restrictions, left turn restrictions, and pedestrian improvements at Avenue C and Avenue D. Comments received in response to the questions posed are summarized as follows:

Issue 1: Parking Restrictions on 33rd Street from Avenue F to Confederation Drive (Monday to Friday)

- Eastbound parking prohibited between 7 AM to 9 AM
- Westbound parking prohibited between 4 PM to 6 PM

- There are many houses that do not have front driveways and room for only one car in the back. Most people park on the street.
- With the development of the areas, traffic will increase all day long making more noise and travelling too close to our homes. It is impossible to back out of driveways and another lane of traffic will make it more impossible.
- The proposal and improvements make sense, except for those who need to park in front would need to park someplace else. Most places do not have backyard parking and driveways would need to be installed for some of the rental properties
- No parking available for extra vehicles on street.
- The parking restrictions would be dangerous to the pedestrian as vehicle will be speeding down the street. Often garbage/recycle bins are placed on the curb and are frequently hit by vehicles using the curb lane.
- 33rd Street from Avenue H to Edmonton Avenue should not have parking anytime at rush hour.
- 33rd Street is too narrow for 4 lane usage. The 4 lane usage will only lead to more dangerous speeds.

Issue 2: Left turn restrictions at Avenue D

- The left turn restrictions at Avenue D will greatly disrupt access to the back lane for supplies/deliveries and will impact the ability to function as a business in this location.
- Left turn restrictions will make it impossible to deliver goods to businesses.
- The left turn restrictions at Avenue D will impact flow of business activity and service vehicles will have issues getting in and out.



Issue 3: Removal of existing traffic signals at Avenue D and replaced with Active pedestrian Corridor

- At the corner of 33rd Street and Avenue D it is already difficult to turn at this intersection during rush hour. Removing the lights will make it pretty well impossible to make a left turn.
- Do not remove the traffic signals at Avenue D and 33rd Street. 33rd Street is busy as it is and it's impossible at certain times of the day to make a left hand turn heading east.
- Traffic light on Avenue D needs to remain.
- Do not take down light at Avenue D.
- Too much traffic from Safeway to remove traffic light at Avenue D.

Issue 4: Installation of Active Pedestrian Corridor at Avenue C

- Thrilled with the active pedestrian corridor at Avenue C and Avenue D.
- Should be another light at Avenue C where pedestrians continue to be run down.
- Unsure if two pedestrian corridors are needed. Too close together.
- An active pedestrian corridor at Avenue C is must to cross safely. Rarely does anyone stop to let you cross.

Issue 5: General / Other

- 33rd Street during rush hour is very confusing and dangerous. Difficult to see pedestrians. Change is needed!
- Improving right turns at Idylwyld Drive and 33rd Street would be very beneficial. Having two active pedestrian corridors should not be one block apart.

3.2 Feedback from Neighbourhood Traffic Review Meetings

Public meetings were held in 2013 and 2014 as part of the Neighborhood Traffic Reviews in Mayfair, Hudson Bay Park and Caswell Hill neighbourhoods to discuss the phase 2 recommendations and to identify further traffic concerns along 33rd Street. At the meeting, residents were given the opportunity to express their concerns and suggest possible solutions.

The following pages summarize the concerns and suggested solutions identified during the initial consultation with the neighbourhood residents.

Concern 1: Neighbourhood Shortcutting

Shortcutting occurs when non-local traffic passes through the neighbourhood on local streets that are designed and intended for slow volumes of traffic. In the case of 33rd Street, residents were concerned with short cutting from 33rd Street through the adjacent neighbourhoods (Mayfair and Caswell Hill).



Proposed solutions identified by residents:

- Another access from Mayfair Neighbourhood to Circle Drive

Concern 2: Pedestrian Safety

It is important to address pedestrian safety concerns to support active transportation. Walking to nearby amenities, as opposed to driving, reduces traffic volumes on local streets.

Pedestrian crosswalks need to adhere to the City of Saskatoon Council Policy C07-018 *Traffic Control at Pedestrian Crossings*, November 15, 2004 which states the following:

“The installation of appropriate traffic controls at pedestrian crossings shall be based on warrants listed in the document entitled ‘Traffic Control at Pedestrian Crossings – 2004’ approved by City Council in 2004.”

Concerns regarding pedestrian safety were at the following locations:

- Avenue C near the Safeway
- Avenue D
- Avenue F – route used by children going to school

Proposed solutions identified by residents:

- Install pedestrian device at Avenue C
- Install pedestrian bulbing instead of zebra crosswalks

Concern 3: Traffic Congestion

Traffic congestion is a condition on road networks that occurs as use increases, and is characterized by slower speeds, longer trip times, and increased vehicular queueing. The most common example is the physical use of roads by vehicles. When traffic demand is great enough that the interaction between vehicles slows the speed of the traffic stream, this results in some congestion.

Concerns regarding Traffic Congestion:

- Increase of traffic as result of the Kensington Development
- Traffic back ups at Avenue W southbound as result of bus in eastbound lane on 33rd Street

Proposed solutions identified by residents:

- Remove daytime parking in commercial corridor to allow for more traffic flow
- Have 4 lanes between Confederation Drive to Circle Drive



Concern 4: Parking

Parking is allowed on all city streets unless signage is posted.

Concerns regarding parking were at the following locations:

- Business District – between Idylwyld Drive to Avenue D
- Parking for residents and guests on 33rd Street and guests

Proposed solutions identified by residents:

- Install parking meters similar to Broadway Avenue
- Do not remove parking anywhere on 33rd Street

Concern 5: Traffic Signals

Traffic lights alternate the right-of-way according to road users by displaying lights of a standard color (red, yellow, and green) following a universal color code. Traffic Signals can be accompanied by left turn arrow phase. Left-turn arrows allows vehicles to turn without conflicting with other movements.

Concerns regarding traffic signals at the following intersections:

- Difficult making left turn for eastbound and westbound at 33rd Street and Avenue P
- difficult making left turn for northbound and southbound at 33rd Street and Avenue P
- Timing of pedestrian light on 33rd Street and Faulkner Crescent is too long
- Idylwyld Drive and 33rd Street eastbound movement only allows three vehicles through

Proposed solutions identified by residents:

- Left-turn signal for east and west, north and south on Avenue P and 33rd Street
- Dedicated right turn at Idylwyld Drive at all corners



4 SITE VISIT

To capture a first-hand account of the existing conditions, a site visit was completed on April 8, 2015. The site visit included a comprehensive review of the following:

- Intersection traffic control and lane configurations
- Location and type of pedestrian crossing controls
- Existing parking restrictions
- Opportunities for improvements of the pedestrian network (including safety and connectivity)
- Photographs in support of the noted observations



5 EXISTING CONDITIONS ASSESSMENT

5.1 Corridor Description

33rd Street comprises of two to four lanes of traffic depending parking restrictions and carries approximately 12,000 vehicles per day close to Idylwyld Drive, ranging to over 24,000 vehicles per day west of Circle Drive. Land use throughout the corridor is mixed, between Idylwyld Drive and Avenue F consisting of commercial land use, between Avenue F and Avenue P consisting of single family residential, between Avenue P and Avenue W consisting of commercial, and west of Avenue W consisting of residential.

The existing 33rd Street corridor infrastructure and adjacent land use is presented in **Table 5-1**.



Table 5-1: 33rd Street Intersection Infrastructure

Cross Street	Control	Land Use North of 33 rd Street	Land Use South of 33 rd Street
Idylwyld Drive	Traffic Signal	Commercial	Commercial
Avenue B	Non-Signalized	Commercial	Commercial
Avenue C (east leg)	Non-Signalized	Commercial	Commercial
Avenue C (west leg)	Non-Signalized	Commercial	Commercial
Avenue D	Traffic Signal	Commercial	Commercial
Avenue E (east leg)	Non-Signalized	Commercial	Commercial
Avenue E (west leg)	Non-Signalized	Commercial	Commercial
Avenue F (east leg Pedestrian Actuated Signal)	Traffic Signal	Commercial	Commercial
Avenue F (west leg)	Non-Signalized	Commercial, Residential	Commercial, Residential
Avenue H (east leg)	Non-Signalized	Residential	Residential
Avenue H (west leg)	Traffic Signal	Residential	Residential
Avenue P	Traffic Signal	Commercial, Residential	Commercial, Residential
Edmonton Avenue	Traffic Signal	Residential	Residential
Circle Drive East	Traffic Signal	Residential	Residential
Circle Drive West	Traffic Signal	Residential	Residential
Avenue W	Traffic Signal	Commercial, Residential	Commercial, Residential
Junor Avenue	Traffic Signal	Residential	Commercial, Residential
Confederation Drive	Traffic Signal	Residential	Commercial, Residential



5.2 Daily Traffic and Travel Speeds

Daily traffic volumes were measured to assist in determining if the traffic volumes meet the City of Saskatoon guidelines for arterial streets. Traffic volumes (referred to as Average Daily Traffic) on these streets should meet the City of Saskatoon guidelines shown in **Table 5-2**.

Travel speed was conducted to measure the 85th percentile speed which is the speed at which 85 percent of vehicles are travelling at or below. The posted speed limit on 33rd Street is 50kph, except for school zones where the speed limit is 30kph from September to June, 8:00AM to 5:00PM, excluding weekends and holidays.

Table 5-2: City of Saskatoon Street Classifications and Characteristics

Characteristic	Arterials	
	Minor	Major
Traffic service function	Traffic movement major consideration	Traffic movement primary consideration
Land service access	Some access control	Rigid access control
Traffic volume (veh/day)	5,000-20,000	10,000- 30,000
Flow characteristics	Uninterrupted flow except at signals and crosswalks	
Design speed (km/hr)	50-70	60-100
Average running speeds (km/hr) (off peak)	40-60	50-90
Vehicle type	All types	All types up 20% trucks
Desirable connections	Collectors, arterials, expressway, freeways	
Transit service	Express and local buses permitted	
Accommodation of cyclists	Lane widening or separate facilities desirable	
Accommodation of Pedestrians	Sidewalks may be provided, separation for traffic lanes preferred	
Parking (typically)	Peak hour restrictions	Prohibited or peak hour restrictions
Min intersection spacing (m)	200	400
Right-of-way width (m) (typically)	20 – 45 m	



The corridor between Idylwyld Drive to Confederation Drive is classified as a major arterial with a posted speed limit of 50kph. In the following Table 5-3, data was collected at various locations to determine the Average Daily Traffic and the 85th percentile speed.

Table 5-3: Speed Studies and Average Daily Traffic Counts (2014)

Location	Average Daily Traffic (vehicles per day)	85 th Percentile Speed (kph)
Avenue I and Avenue J (School zone)	11,500	49
Valens Avenue to Howell Avenue	11,000	56
Junor Avenue to Catherwood Avenue	13,500	54
Confederation Drive to Junor Avenue	12,000	56

A review of the speed studies and average daily traffic yields the following conclusions:

- The Average Daily Traffic is within the range expected on an arterial street.
- The 85th percentile speed is within the expected range on an arterial street (within 10% over the posted speed limit is typically deemed acceptable).

It is recommended that at this time no specific traffic calming measures be installed on 33rd Street.

5.3 Traffic Operations

Data Collection

The turning movement counts were undertaken over two separate periods at each intersection to determine the peak hours within the peak periods. Turning movement counts were recorded and totalled every 15 minutes. The count periods were as follows:

- Weekday morning peak period: 7:00 AM – 9:00 AM
- Weekday afternoon peak period: 4:00 PM – 6:00 PM

For each study intersection the date of data collection, and the weekday peak hours are presented in Table 5-4.



Table 5-4: Speed Studies and Average Daily Traffic Counts (2014)

Intersection of 33rd Street and	Control	Date data collected	Peak Hours	
			AM	PM
Idylwyld Drive	Traffic Signal	October 23, 2014	7:30 – 8:30	4:15 – 5:15
Avenue B	Non-Signalized	May 29, 2014	7:45 – 8:45	4:00 – 5:00
Avenue C (east leg)	Non-Signalized	June 10, 2014	7:45 – 8:45	4:15 – 5:15
Avenue C (west leg)	Non-Signalized	June 10, 2014	7:45 – 8:45	4:15 – 5:15
Avenue D	Traffic Signal	June 10, 2014	7:45 – 8:45	4:15 – 5:15
Avenue E (east leg)	Non-Signalized	May 29, 2014	7:45 – 8:45	4:15 – 5:15
Avenue E (west leg)	Non-Signalized	May 29, 2014	7:45 – 8:45	4:15 – 5:15
Avenue F (east leg Pedestrian Actuated Signal)	Traffic Signal	June 3, 2014	7:30 – 8:30	4:30 – 5:30
Avenue F (west leg)	Non-Signalized	June 3, 2014	7:45 – 8:45	4:30 – 5:30
Avenue H (west leg)	Traffic Signal	June 5, 2014	7:15 – 8:15	4:30 – 5:30
Avenue H (east leg)	Non-Signalized	June 5, 2014	7:30 – 8:30	4:30 – 5:30
Valens/Faulkner Drive	Traffic Signal	June 4, 2014	7:15 – 8:15	4:30 – 5:30
Avenue P	Traffic Signal	June 5, 2014	7:15 – 8:15	4:30 – 5:30
Edmonton Avenue	Traffic Signal	October 2, 2014	8:00 – 9:00	4:30 – 5:30
Circle Drive East	Traffic Signal	October 7, 2014	7:30 – 8:30	4:30 – 5:30
Circle Drive West	Traffic Signal	October 7, 2014	7:45 – 8:45	4:30 – 5:30
Avenue W	Traffic Signal	October 23, 2014	7:30 – 8:30	4:30 – 5:30
Junor Avenue	Traffic Signal	October 2, 2014	7:45 – 8:45	4:45 – 5:45
Confederation Drive	Traffic Signal	September 30, 2014	7:45 – 8:45	4:45 – 5:45

Intersection Analysis

An analysis of the existing traffic operations of the unsignalized intersections was completed using the methodology described in **Section 1.5** and the existing turning count volumes collected. A summary of the analysis results are presented in **Table 5-5**.



Table 5-5: Existing Operation Conditions for Unsignalized Intersections

Intersection/Movement			Measures of Effectiveness							
			AM Peak Hour				PM Peak Hour			
			LOS	v/c ratio	Delay (s)	Queue (m)	LOS	v/c ratio	Delay (s)	Queue (m)
Avenue B	EB	LT-Thru	A	0.02	0.6	0.4	A	0.03	1.0	0.6
		Thru-RT		0.21	0.0	0.0		0.16	0.0	0.0
	WB	LT-Thru	A	0.01	0.7	0.3	A	0.02	0.7	0.5
		Thru-RT		0.13	0.0	0.0		0.23	0.0	0.0
	NB	LT-Thru-RT	B	0.04	12.6	1.1	B	0.07	14.6	1.7
	SB	LT-Thru-RT	B	0.03	12.5	0.9	C	0.26	17.9	8.0
	Intersection Summary			A	0.21 (max)	0.7	-	A	0.26 (max)	1.8
Avenue C (east leg)	EB	Thru	A	0.04	2.1	1.1	A	0.04	2.2	1.0
		Thru		0.23	0.0	0.0		0.17	0.0	0.0
	WB	Thru		0.00	0.0	0.0		0.25	0.0	0.0
		Thru-RT		0.19	0.0	0.0		0.15	0.0	0.0
	SB	LT-RT	B	0.06	10.4	1.4	B	0.18	13.7	5.2
	Intersection Summary			A	0.23 (max)	0.9	-	A	0.25 (max)	1.3
Avenue C (west leg)	EB	Thru		0.23	0.0	0.0		0.17	0.0	0.0
		Thru-RT		0.12	0.0	0.0		0.10	0.0	0.0
	WB	Thru	A	0.03	2.2	0.8	A	0.05	2.0	1.2
		Thru		0.13	0.0	0.0		0.26	0.0	0.0
	NB	LT-RT	B	0.08	10.6	2.1	B	0.06	10.8	1.4
	Intersection Summary			A	0.23 (max)	0.9	-	A	0.26 (max)	0.8
Avenue E (east leg)	EB	LT-Thru	A	0.02	1.1	0.6	A	0.02	0.8	0.4
		Thru		0.26	0.0	0.0		0.21	0.0	0.0
	WB	Thru		0.15	0.0	0.0		0.35	0.0	0.0
		Thru-RT		0.08	0.0	0.0		0.19	0.0	0.0
	SB	LT-RT	B	0.03	10.7	0.7	B	0.09	12.5	2.3
	Intersection Summary			A	0.26 (max)	0.4	-	A	0.35 (max)	0.5



Table Continued

Intersection/Movement			Measures of Effectiveness							
			AM Peak Hour				PM Peak Hour			
			LOS	v/c ratio	Delay (s)	Queue (m)	LOS	v/c ratio	Delay (s)	Queue (m)
Avenue E (west leg)	EB	Thru		0.27	0.0	0.0		0.22	0.0	0.0
		Thru-RT		0.14	0.0	0.0		0.11	0.0	0.0
	WB	LT-Thru	A	0.01	0.6	0.2	A	0.02	0.9	0.6
		Thru		0.15	0.0	0.0		0.35	0.0	0.0
	NB	LT-RT	B	0.02	10.3	0.4	B	0.03	11.9	0.6
	Intersection Summary			A	0.27 (max)	0.2	-	A	0.35 (max)	0.3
Avenue F (west leg)	EB	Thru		0.25	0.0	0.0		0.22	0.0	0.0
		Thru-RT		0.13	0.0	0.0		0.12	0.0	0.0
	WB	Thru	A	0.02	1.2	0.4	A	0.06	2.1	1.5
		Thru		0.14	0.0	0.0		0.31	0.0	0.0
	NB	LT-RT	B	0.05	12.5	1.1	B	0.06	12.1	1.4
	Intersection Summary			A	0.25 (max)	0.4	-	A	0.31 (max)	0.7
Avenue H (east leg)	EB	LT-Thru	A	0.01	0.2	0.2	A	0.01	0.3	0.2
	WB	Thru		0.19	0.0	0.0		0.51	0.0	0.0
		RT		0.00	0.0	0.0		0.00	0.0	0.0
	SB	LT-RT	C	0.03	15.6	0.8	C	0.06	18.0	1.5
	Intersection Summary			A	0.19 (max)	0.3	-	A	0.51 (max)	0.3
Avenue K (east leg)	EB	LT-Thru	A	0.00	0.1	0.1	A	0.00	0.2	0.1
		Thru		0.30	0.0	0.0		0.21	0.0	0.0
	WB	Thru		0.11	0.0	0.0		0.37	0.0	0.0
		Thru		0.06	0.0	0.0		0.19	0.0	0.0
	SB	LT-RT	B	0.01	11.1	0.4	C	0.02	16.6	0.5
	Intersection Summary			A	0.30 (max)	0.1	-	A	0.37 (max)	0.1



Table Continued

Intersection/Movement			Measures of Effectiveness							
			AM Peak Hour				PM Peak Hour			
			LOS	v/c ratio	Delay (s)	Queue (m)	LOS	v/c ratio	Delay (s)	Queue (m)
Ave K (west leg)	EB	Thru		0.29	0.0	0.0		0.21	0.0	0.0
		Thru-RT		0.15	0.0	0.0		0.11	0.0	0.0
	WB	LT-Thru	A	0.01	0.4	0.1	A	0.02	0.8	0.6
		Thru		0.11	0.0	0.0		0.36	0.0	0.0
	NB	LT-RT	B	0.02	11.5	0.5	C	0.04	15.6	1.0
	Intersection Summary			A	0.29 (max)	0.2	-	A	0.36 (max)	0.3

Note: The cells with no values included in the LOS columns may indicate free flow conditions.

All individual movements in Table 5-5 are operating at LOS C or better, and with v/c ratios of 0.51 or less, indicating uncongested traffic operations with minimal delay. It is concluded no immediate intersection improvements are required for the unsignalized intersections.

An analysis of the existing traffic operations of the signalized intersections was completed using the methodology described in Section 1.5 and the existing traffic volumes collected. A summary of the analysis results are presented in Table 5-6.



Table 5-6: Existing Operation Conditions for Signalized Intersections

Intersection/Movement			Measures of Effectiveness							
			AM Peak Hour				PM Peak Hour			
			LOS	v/c ratio	Delay (s)	Queue (m)	LOS	v/c ratio	Delay (s)	Queue (m)
Idylwyld Drive	EB	LT-Thru	E	1.01	68.7	90.3	C	0.47	21.3	52.8
		Thru-RT								
	WB	LT-Thru	D	1.19	38.5	49.2	C	0.61	24.2	67.1
		RT	A	0.37	6.0	15.7	A	0.42	8.8	18.8
	NB	LT	A	0.19	7.9	7.4	E	0.83	63.3	37.5
		Thru-RT	C	0.70	20.5	92.9	E	1.01	63.1	186.6
	SB	LT	B	0.50	12.8	20.8	C	0.61	29.9	32.9
		Thru-RT	B	0.55	14.5	75.9	D	0.96	47.7	194.4
	Intersection Summary			D	1.19 (max)	28.0	-	D	1.01 (max)	43.2
Avenue D	EB	LT-Thru-RT	A	0.29	5.5	20.3	A	0.23	4.1	11.8
	WB	LT-Thru-RT	A	0.16	5.8	17.1	A	0.32	6.4	35.3
	NB	LT-Thru-RT	B	0.20	12.9	13.0	B	0.25	19.0	16.8
	SB	LT-Thru-RT	B	0.14	11.7	9.6	C	0.59	29.3	45.5
	Intersection Summary			A	0.29 (max)	6.3	-	A	0.59 (max)	9.4
Avenue F (east leg)	EB	LT-Thru	A	0.27	3.9	24.1	A	0.24	3.5	20.5
	WB	Thru-RT	A	0.12	1.8	6.1	A	0.30	2.8	19.1
	SB	LT-RT	C	0.07	21.1	7.5	B	0.19	18.1	13.3
	Intersection Summary			A	0.27 (max)	3.6	-	A	0.30 (max)	3.6
Avenue H (west leg)	EB	Thru	B	0.44	10.2	71.3	A	0.33	8.1	50.1
		RT	A	0.09	4.9	10.0	A	0.09	3.9	9.2
	WB	LT-Thru	A	0.39	9.9	45.2	D	0.97	41.2	#277.3
	NB	LT-RT	C	0.56	25.1	57.6	D	0.79	49.9	#106.6
	Intersection Summary			B	0.56 (max)	13.1	-	C	0.97 (max)	32.9



Table Continued

Intersection/Movement			Measures of Effectiveness							
			AM Peak Hour				PM Peak Hour			
			LOS	v/c ratio	Delay (s)	Queue (m)	LOS	v/c ratio	Delay (s)	Queue (m)
Valens Drive/ Faulkner Crescent	EB	LT-Thru-RT	A	0.32	8.2	39.3	A	0.25	7.1	31.2
	WB	LT-Thru-RT	A	0.14	6.7	16.0	A	0.41	8.3	52.8
	NB	LT-Thru-RT	B	0.06	13.1	7.1	B	0.08	15.5	9.2
	SB	LT-Thru-RT	C	0.08	24.0	11.4	C	0.08	25.8	10.8
	Intersection Summary		A	0.32 (max)	8.4	-	A	0.41 (max)	8.3	-
Avenue P	EB	LT-Thru-RT	B	0.48	13.8	48.3	A	0.43	6.9	m22.2
	WB	LT-Thru-RT	A	0.20	9.3	20.0	B	0.55	15.1	75.6
	NB	LT-Thru-RT	B	0.31	17.7	24.0	D	0.85	36.9	66.9
	SB	LT-Thru-RT	C	0.10	20.5	14.4	C	0.50	29.6	67.1
	Intersection Summary		B	0.48 (max)	13.8	-	B	0.69 (max)	18.9	-
Edmonton Avenue	EB	LT	A	0.08	7.2	m7.7	B	0.28	12.4	14.0
		Thru-RT	A	0.48	7.4	52.8	A	0.49	9.6	47.7
	WB	LT	A	0.15	7.6	8.2	B	0.29	12.7	14.0
		Thru-RT	A	0.19	6.3	25.8	B	0.47	11.1	54.3
	NB	LT-Thru-RT	D	0.67	45.3	#64.8	C	0.53	25.2	41.6
	SB	LT-Thru-RT	B	0.12	13.5	11.3	B	0.33	10.1	22.7
	Intersection Summary		B	0.67 (max)	11.5	-	B	0.53 (max)	11.7	-
Circle Drive East	EB	LT	A	0.46	5.0	20.5	C	0.58	26.0	27.7
		Thru	A	0.36	2.6	15.1	A	0.36	7.3	34.3
	WB	Thru-RT	C	0.70	30.1	54.3	C	0.85	26.2	107.8
	NB	LT	D	0.34	42.2	31.8	C	0.40	26.5	35.6
		RT	B	0.58	14.9	29.7	A	0.36	6.3	14.6
	Intersection Summary		B	0.70 (max)	13.0	-	C	0.64 (max)	19.0	-



Table Continued

Intersection/Movement			Measures of Effectiveness							
			AM Peak Hour				PM Peak Hour			
			LOS	v/c ratio	Delay (s)	Queue (m)	LOS	v/c ratio	Delay (s)	Queue (m)
Circle Drive West	EB	Thru-RT	B	0.66	20.0	135.7	C	0.57	33.4	82.6
	WB	LT	C	0.45	21.5	22.9	C	0.80	28.2	73.6
		Thru	A	0.15	1.3	2.0	B	0.41	13.1	66.3
	SB	LT	D	0.36	42.7	34.2	C	0.33	31.4	51.4
		LT-Thru	D	0.36	42.8	34.4	C	0.32	31.3	50.7
		RT	B	0.42	10.1	17.9	C	0.56	20.5	63.3
	Intersection Summary			B	0.66 (max)	18.0	-	C	0.80 (max)	24.3
Avenue W	EB	LT-Thru-RT	B	0.59	16.3	74.2	A	0.33	8.8	32.3
	WB	LT-Thru-RT	A	0.26	9.2	23.2	C	0.81	21.4	108.0
	NB	LT-Thru-RT	A	0.03	3.6	2.8	B	0.07	12.9	7.5
	SB	LT-Thru-RT	D	0.92	48.8	127.7	D	0.73	37.9	71.2
	Intersection Summary			C	0.92 (max)	22.7	-	C	0.81 (max)	19.9
Junor Avenue	EB	LT-Thru	B	0.60	14.2	75.1	D	1.00	53.5	160.2
	WB	Thru	B	0.28	16.2	44.3	B	0.44	18.0	73.5
		RT	A	0.11	8.5	12.8	B	0.28	11.0	31.4
	SB	LT	D	0.62	36.6	78.5	C	0.43	32.6	53.7
		RT	C	0.53	21.7	51.3	B	0.49	16.1	40.1
Intersection Summary			C	0.62 (max)	20.3	-	C	1.00 (max)	32.1	-
Confederation Drive/Wedge Road	EB	LT	B	0.06	10.1	6.4	B	0.10	10.7	9.0
		Thru-RT	A	0.23	6.9	16.0	A	0.17	6.3	11.9
	WB	LT	C	0.65	21.7	54.2	B	0.59	19.1	51.0
		Thru-RT	A	0.11	6.4	8.4	A	0.20	7.2	14.4
	NB	LT	B	0.14	14.6	9.6	B	0.39	18.3	28.4
		Thru-RT	A	0.28	8.9	18.8	B	0.70	16.5	62.9
	SB	LT	B	0.32	17.1	22.8	D	0.62	44.7	#27.1
		Thru-RT	B	0.38	15.2	35.5	B	0.26	13.5	23.3
Intersection Summary			B	0.65 (max)	12.6	-	B	0.70 (max)	14.9	-



All individual movements in **Table 5-6** are operating at LOS D or better, and with v/c ratios of 1.00 or less, indicating uncongested traffic operations with minimal delay except for two individual movements at the intersection of 33rd Street and Idylwyld Drive, and one individual movement at the intersection of Junor Avenue and 33rd Street. Specifically the westbound shared left turn / through lane in the AM peak hour and the northbound shared through / right turn lane in the PM peak hour are at capacity. There are no immediate improvements available to improve the operations. The Administration monitors the signal timing plans in place at this intersection with the goal of optimizing the traffic flow. At the intersection of Junor Avenue and 33rd Street it is not possible to add an additional eastbound lane to increase capacity as this would result in a loss of residential parking. The overall level of service for both intersections are operating within satisfactory conditions.

5.4 Pedestrian Accommodation

Pedestrian engineering assessments were conducted to determine the need for pedestrian controlled device which in adherence to the City of Saskatoon Council Policy C07-018 Traffic Control at Pedestrian Crossings, November 15, 2004, are typically an Active Pedestrian Corridor (APC) (flashing yellow lights) or a Pedestrian Actuated Signal (PAS). A warrant system assigns points for a variety of conditions that exist at the crossing location, including:

- The number of traffic lanes to be crossed;
- The presence of a physical median;
- The posted speed limit of the street;
- The distance the crossing point is to the nearest protected crosswalk point; and
- The number of pedestrians and vehicles at the location.

Pedestrian and traffic data is collected during the five peak hours of: 8:00AM to 9:00AM; 11:30AM to 1:30PM and 3:00PM to 5:00PM.

Table 5-7: Pedestrian Studies Summary

Location	Number of Pedestrian Crossing During Peak hours	Results
Avenue C	108	Warrants an APC
Avenue E	10	No device warranted
Avenue F	78	Warrants an APC

As a result of the assessment, an Active Pedestrian Corridor is recommended at the Avenue C and Avenue F intersections with 33rd Street.

5.5 Collisions Review

The five year collision history between January 2009 and December 2013 was reviewed for the corridor. The Administration is still waiting for 2014 collision data to be shared by SGI. Even though 2014 data is still forthcoming the five year data is still relevant as 33rd Street is an established street and driver behavior would not have changed over the years with no physical changes to the street. It is not anticipated that 2014 collision history would significantly differ from the previous five years. The collision history summaries provided general data for each collision report. The data included the collision date and time, the general area where the collision occurred, the general cause of the collision, and the type of damage/and or injury (property damage, injury, or fatality.)

During the period for which collision data were provided, a total of 956 incidents were reported along 33rd Street. The chart below illustrates an average of 200 collisions per year over the past five years. On average, this equates to about 16 reported collisions per month along the corridor.

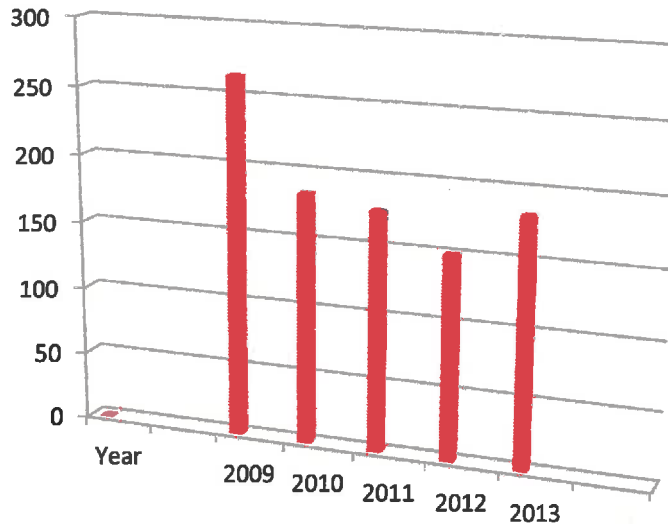


Exhibit 5-1: Total Collisions from 2009 to 2013

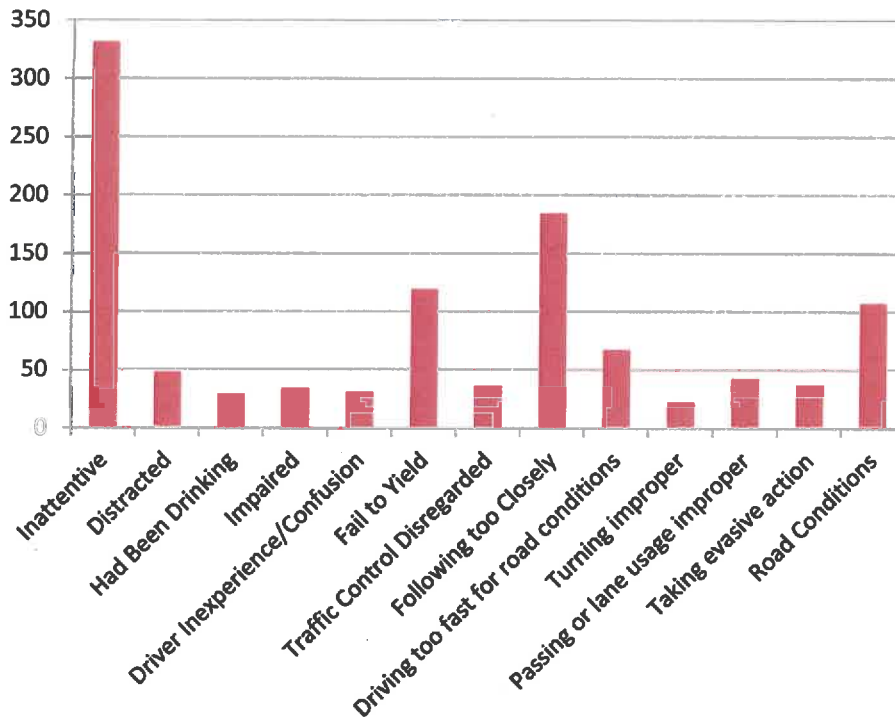


Exhibit 5-2: Major Contributing Factors to Collisions

Based upon a review of the total collisions and the contributing factors to collisions, there is no significant, or specific, safety issue to be addressed via this study. As illustrated in **Exhibit 5-2**, “Inattentive”, “Fail to Yield”, and “Road Conditions” were high as these are typical trends seen on arterial streets. It was interesting to note that “Following too Closely” was also high. This factor is typically not as common, but may be due to the multiple off-set intersections that create multiple turning opportunities from 33rd Street.



6 PUBLIC CONSULTATION

In addition to the significant amount of public consultation completed prior to 2015, public consultation meetings were held in the spring of 2015 to invite feedback from the public, including residents, business owners, and the 33rd Street Business Improvement District. The format of the meetings included a period of time at the start of the meeting for people to view the proposed changes, followed by a presentation by Administration staff that outlined the history of the project, work completed to date, preliminary recommendations, and next steps. The meeting closed with attendees openly discussing the study objectives and draft recommendations with the Administration.

The public consultation meetings were held on Thursday, April 23, and also on Wednesday, April 29. The agenda was the same for both meetings with approximately 50 people attending the first, and approximately 40 people attending the second.

The materials presented at the meetings are attached in **Appendix B**.

The presentation provided at the meetings is attached in **Appendix B**.

7 RECOMMENDATIONS

The recommended plan was achieved by completing the following steps:

- Based on the previous public consultation, data collection, and engineering assessments a draft plan that illustrates the appropriate recommended improvements was prepared
- The draft plan was presented to the residents at two public meetings held in the spring of 2015 for discussion and feedback
- The draft plan was revised based on the feedback received at the public meetings
- A technical document summarizing the recommended plan and project process was prepared

The following sections provide the details of the recommended 33rd Street Corridor plan, including the location, recommended improvement, and the justification of the recommended improvement.

7.1 Pedestrian Accommodation Improvements – Sidewalks, Ramps, and Pathways

The following recommendations are provided.

Recommendation 1 – Replace depressed sidewalks to improve mobility

- ✓ 5 Locations between Avenue B & Avenue C
- ✓ 1 Locations between Avenue F & Avenue E

Existing



Proposed





Recommendation 2 – Install sidewalk ramps to improve mobility

- ✓ SW corner of Idylwyld Drive
- ✓ SE corner of Avenue B
- ✓ NE corner of Avenue B
- ✓ SE corner of Avenue D
- ✓ SW corner of Avenue D
- ✓ SE corner of Avenue E (west crosswalk location)

Existing



Proposed



Recommendation 3 – Install sidewalk to improve mobility and provide connectivity

- ✓ South side of 33rd Street between Avenue P and east edge of Vic Rempel Yards

Existing



Proposed





Recommendation 4 – Install pathway to improve mobility and provide connectivity

- ✓ South side of 33rd Street between east edge of Vic Rempel Yards and Edmonton Avenue

Existing



Proposed



Recommendation 5 – Sidewalk protection to improve safety and mobility

- ✓ Remove temporary jersey barrier with bollard and repair sidewalk

Existing



Example





Recommendation 6 - Replace dirt boulevard with concrete to improve mobility

- ✓ Southside of 33rd Street between Avenue B and Avenue C

Existing



Example



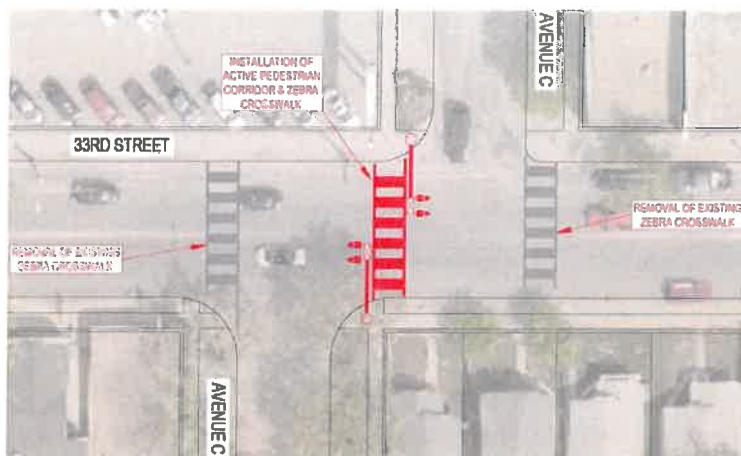
7.2 Pedestrian Accommodation Improvements – Crosswalks

The following recommendations are provided.

Recommendation 7 – Avenue C pedestrian crosswalk improvement to increase the level of safety by clearly defining one crossing, and providing a device to warn drivers

- ✓ Install Active Pedestrian Corridor type crossing on East side of southern leg
- ✓ Remove other existing crossings

Proposed



Example

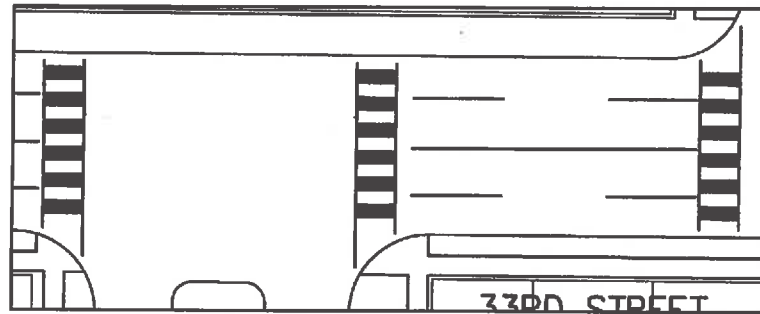




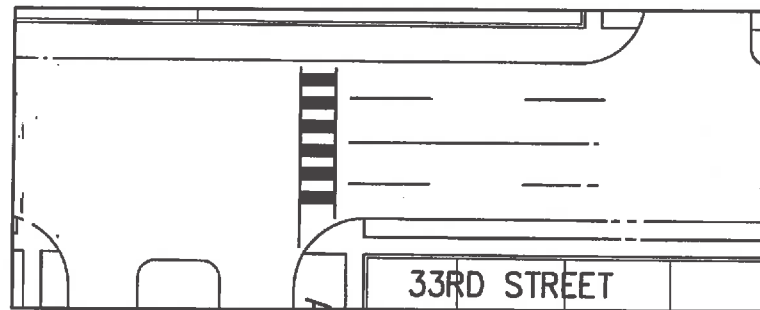
Recommendation 8 – Avenue E pedestrian crosswalk improvement to increase the level of safety by clearly defining one crossing, and providing a device to warn drivers

- ✓ Install zebra crosswalk on east side of Avenue E on southern leg
- ✓ Remove crosswalk on east side

Existing



Proposed

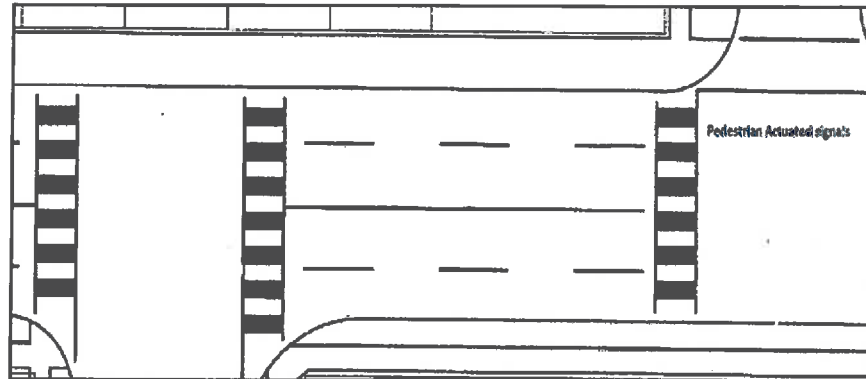




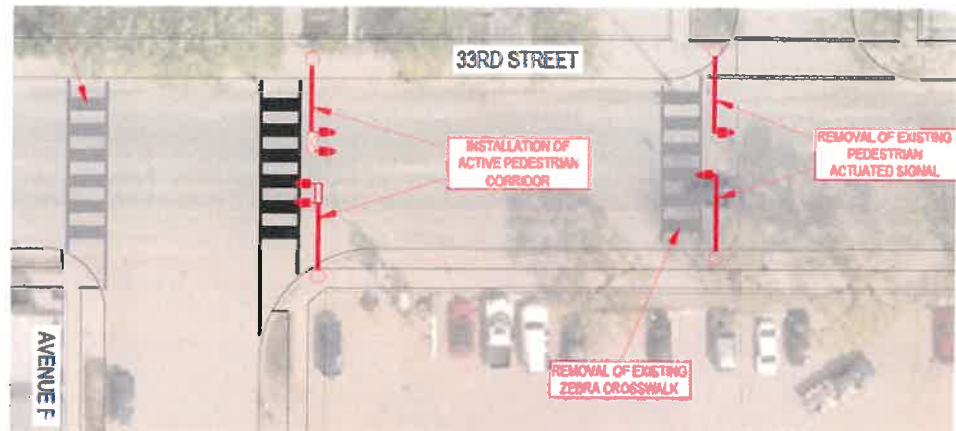
Recommendation 9 – Avenue F pedestrian crosswalk improvement to increase the level of safety by clearly defining one crossing, and providing a device to warn drivers

- ✓ Remove pedestrian Actuated Signals (East leg) and replace with Active Pedestrian Corridor

Existing



Proposed



7.3 Intersection Improvements

The following recommendations are provided.

Recommendation 10 – Northumberland Avenue intersection improvement to increase the level of safety and improve intersection operations

- ✓ Remove pedestrian actuated signals and replace with full-signals

Existing



Example



Recommendation 11 – Avenue D intersection improvement to increase the level of safety and improve intersection operations

- ✓ Upgrade traffic signal poles and traffic signals

Existing



August 13, 2015

Example



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City of Saskatoon

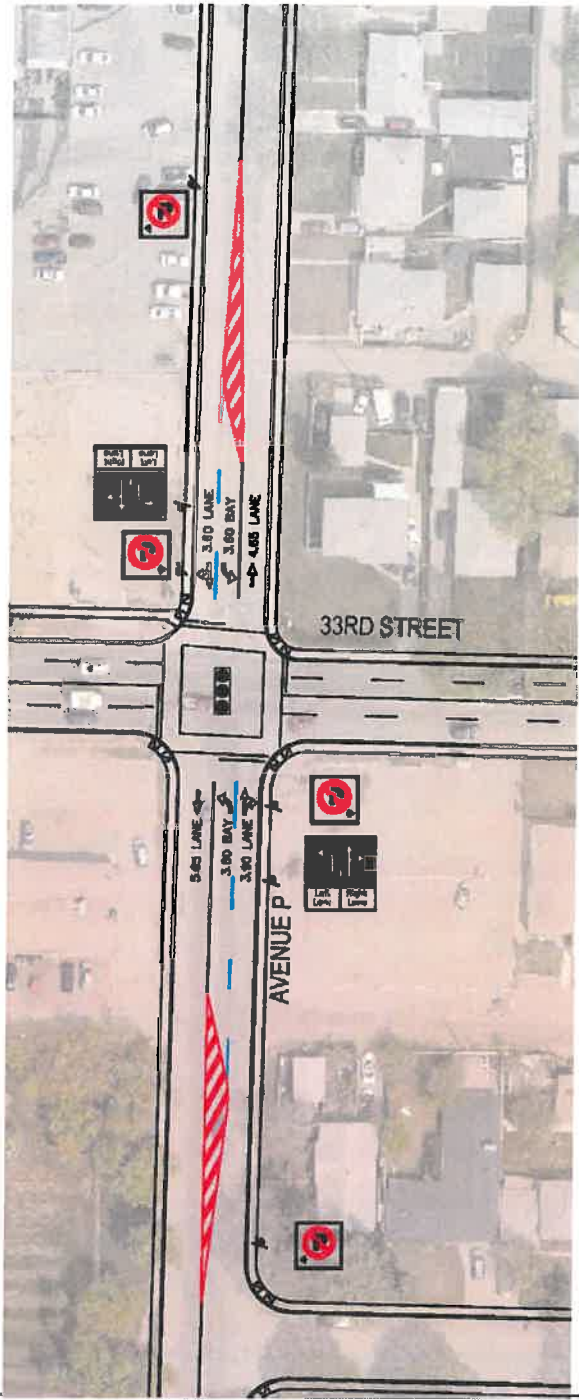
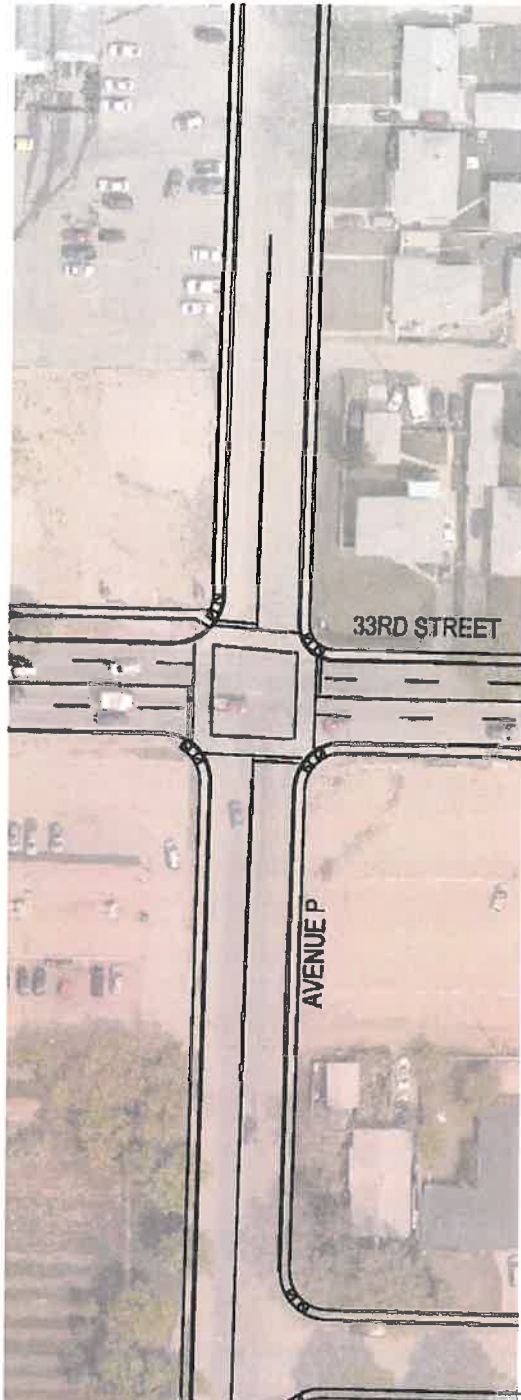


Recommendation 12 – Avenue P intersection improvement to increase the level of safety by providing northbound and southbound drivers improved sight distance, and also improve the intersection operation by separating the northbound and left turn movements

- ✓ Re-configure southern and northern legs

Existing

Proposed



August 13, 2015

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City of Saskatoon



Recommendation 13 – Avenue W intersection revision will improve intersections operations by allowing free flow movement for the southbound to eastbound dual left turns

- ✓ Construct a bus lay-by on 33rd Street (southeast corner), also waiting upon Transit comments

Existing



Proposed





7.4 Not Recommended

Council previously directed the Administration to report back after additional public consultation, regarding specific potential changes to 33rd Street, that would potentially reduce driver delay along the corridor. Based on the feedback received from the public and the assessment completed by the Administration, additional parking restrictions and left turn restrictions are not recommended at this time.

Also, as referenced earlier, no significant alterations to the cross-section of 33rd Street or significant major intersection improvements are recommended at this time in consideration of the Growing Forward! Shaping Saskatoon project. It may be appropriate to re-visit 33rd Street in light of the conclusions and recommendations provided at the end of this City-wide planning project, and the decision to do so can be made at a later date.



8 COST ESTIMATES

The estimate cost to complete all of the work along the corridor is approximately \$550,000. The work is not intended to be installed as one capital project, but rather will be implemented from specific capital projects and constructed according to the time frame indicated in **Table 8-1** below. It is noted that the timing of the various capital projects is dependent on available funding from each year's budget.

Table 8-1: Cost Estimate

#	Recommendation	Location	Estimated Cost	Capital Project	Time Frame
1	Replace depressed sidewalks	<ul style="list-style-type: none"> • 5 locations between Avenue B and Avenue C • 1 location between Avenue F and Avenue E 	\$10,000	0948- Sidewalk Retrofit	3-5 yrs
2	Install sidewalk ramps	<ul style="list-style-type: none"> • SE and NE Corner of Avenue B • SE and SW corner of Avenue D • SE corner of Avenue E (west crosswalk location) • SE corner of Idylwyld Drive 	\$20,000	P1963 Corp Curb Ramps	3-5 yrs
3	Install Sidewalk	South side of 33 rd Street between Avenue P and east edge Vic Rempel Park	\$35,000	0948- Sidewalk Retrofit	5 yrs
4	Install Pathway	South side of 33 rd Street between east edge Vic Rempel Park and Edmonton Avenue	\$15,000	0948 Sidewalk Retrofit	5 yrs
5	Sidewalk protection – replace jersey barrier with bollards	Avenue H (north side)	\$2,000	0631 Traffic Safety	1 yr
6	Replace boulevard with concrete	33 rd Street (south side between Avenue B and Avenue C)	\$35,000	0948 Sidewalk Retrofit	3-5 yrs
7	Active Pedestrian Corridor	Avenue C east side of south leg	\$35,000	2446 Pedestrian Improvements	3-5 yrs
8	Zebra Crosswalk	Avenue E west side of south leg	\$500	0631 Traffic Safety	1 yr
9	Replace Pedestrian Actuated Signals with Active Pedestrian Corridors	33 rd Street & Avenue F (east side of south leg)	\$35,000	2446 Pedestrian Improvements	3-5 yrs



#	Recommendation	Location	Estimated Cost	Capital Project	Time Frame
10	Upgrade Pedestrian Actuated Signals with Traffic Signals	Northumberland Avenue	\$100,000	CP1036 Traffic Control Upgrades (TIER)	5 yrs
11	Upgrade Traffic Signals and maintain existing crossings	Avenue D	\$135,000	CP1036 Traffic Control Upgrades (TIER)	5 yrs
12	Re-configure southern and northern legs of intersection by building protected left turn lanes	Avenue P	\$25,000	CP2235 Intersection Improvements (TIER, TSR)	5 yrs
13	Construct bus-lay-by	Avenue W (southeast corner)	\$100,000	CP0631 Traffic Safety (TSR)	5 yrs

APPENDIX A: PREVIOUS COUNCIL REPORTS AND MOTIONS

The following is a copy of **Clause 2, Report No. 2-2013** of the **Administration and Finance Committee**, which was **ADOPTED** by City Council at its meeting held on **January 21, 2013**:

- 2. Enquiry – Former Councillor M. Heidt (September 12, 2011)
Possible Removal of Side Boulevards – 33rd Street
AND
Communications to Council**
From: Bruce and Elaine Thomas, Mayfair Hardware
Date: September 1, 2007
Subject: Parking on 33rd Street West
(Files CK. 6320-1 x 6120-2 and IS. 6320-1)

- RECOMMENDATION:**
- 1) that left turn restrictions be implemented during peak hours at the intersections of 33rd Street and Avenues B and C as part of the Phase 1 improvements outlined in the December 21, 2012 report of the General Manager, Infrastructure Services Department; and
 - 2) that the Administration proceed with public consultation for Phase 2, and report back to City Council.

Attached is a report of the General Manager, Infrastructure Services Department dated December 21, 2012, providing information regarding proposed changes to parking and traffic operations along 33rd Street West to improve traffic flow along the corridor.

Your Committee has reviewed the report with the Administration. As outlined in the submitted report, two phases are proposed. Phase 1 includes left turn restrictions during peak hours at the intersections of 33rd Street and Avenues B and C, two-hour parking restrictions from Avenue B to D, and an active pedestrian corridor at 33rd Street West and Avenue K North. The active pedestrian corridor has already been installed.

Your Committee has reviewed the matter of enforcement of current and proposed parking restrictions along 33rd Street. While there is the ability within the existing Bylaw provisions for ticketing and towing if a vehicle is parked in an area where parking is restricted, the Administration has advised that they would want to ensure that appropriate warning and awareness periods are provided.

Your Committee has been advised that Phase 2 will involve further parking restrictions and turning movements, as well as the installation of pedestrian-actuated crossing facilities. Further public consultation with the Mayfair and Caswell Hill communities will occur with respect to Phase 2. The Administration will look at co-ordination with the review of the traffic portion of the Mayfair Local Area Planning process and with a review of the Caswell Residential Parking Permit Program.

Following consideration of this matter, your Committee is supporting the above recommendations of the Infrastructure Services Department.

TO: Secretary, Administration and Finance Committee
FROM: General Manager, Infrastructure Services Department
DATE: December 21, 2012
SUBJECT: Enquiry – Former Councillor M. Heidt (September 12, 2011)
Possible Removal of Side Boulevards – 33rd Street
AND
Communications to Council
From: Bruce and Elaine Thomas, Mayfair Hardware
Date: September 1, 2007
Subject: Parking on 33rd Street West
FILES: CK. 6120-2 and IS. 6320-1

RECOMMENDATION: that the following report be submitted to City Council recommending:

- 1) that the information regarding implementation of Phase 1 of improvements to 33rd Street, as outlined in this report, be received as information; and
- 2) that the Administration proceed with public consultation for Phase 2, and report back to City Council.

TOPIC AND PURPOSE

This report is to provide the Committee and City Council with information regarding changes to parking and traffic operations along 33rd Street West, in order to improve traffic flows along the corridor.

REPORT HIGHLIGHTS

1. It is estimated that the costs to provide additional driving lanes on 33rd Street West, between Confederation Drive and Idylwyld Drive would be in excess of \$7 million, plus additional costs to purchase and demolish buildings that currently abut the property line.
2. It is anticipated that population growth and the development of the Kensington and Blairmore 2 neighbourhoods will result in an increase in traffic volumes on 33rd Street of approximately 200 to 300 vehicles during the afternoon peak hours.
3. Improvements to 33rd Street will be undertaken in two phases, with Phase 1 to be completed in 2013.
4. The Administration will proceed with public consultation for the changes outlined in Phase 2, and will report back to City Council.

STRATEGIC GOALS

The implementation of traffic and parking restrictions outlined in this report supports the City of Saskatoon Strategic Goal, Moving Around, as they will help to ensure that 33rd Street will be a practical transportation route into the future, useful for vehicles, buses, bikes and pedestrians.

BACKGROUND

City Council, at its meeting held on September 17, 2007, considered correspondence and a presentation from Bruce and Elaine Thomas, Mayfair Hardware, requesting a two-hour parking restriction along 33rd Street West, between Avenue B North and Avenue D North, to facilitate improved parking turnover for local businesses. Council passed a motion that the matter be referred to the Administration to report to the Planning and Operations Committee.

The following enquiry was made by former Councillor Heidt at the meeting of City Council held on September 12, 2011:

"As we all know, the traffic on the west side is increasing and there is only 22nd Street and 33rd Street that carry all of this traffic from west of Circle Drive to Spadina Crescent. Would the Administration please look at removing the boulevard from street to sidewalk from Confederation Drive to Idylwyld Drive. This could be looked at from Confederation Drive to Circle Drive, Circle Drive to Avenue H and Avenue H to Idylwyld Drive and done in phases. Please report on the possibility and cost of this activity, which would provide another lane and much needed safety."

REPORT

For most of its length, 33rd Street, west of Idylwyld Drive, consists of one driving and one parking lane in each direction, with residential development and front street garbage pickup. However, between Avenue F and Idylwyld Drive, parking restrictions are in place during the peak hours (7:00 a.m. to 9:00 a.m. eastbound; and 4:00 p.m. to 6:00 p.m. westbound), making the road two driving lanes per direction. Right-of-way widths and cross-sectional elements vary, as shown in Attachment 1.

It is estimated that the direct construction costs (including concrete curbing and pavement) for additional driving lanes on 33rd Street West, between Confederation Drive and Idylwyld Drive, would be approximately \$3 to \$4 million. A cost breakdown of the two sections (Avenue W to Confederation Drive and Idylwyld Drive to Avenue D) is provided in attached Table 1 (Attachment 2). The cost of property acquisition, relocation of City utilities, amenities and infrastructure would require detailed analysis, however, it is estimated that the total cost of creating four lanes of traffic with parking on both sides would exceed \$7 million, plus additional costs to purchase and demolish buildings which currently abut the property line.

It is anticipated that population growth and the development of the Kensington and Blairmore 2 neighbourhoods will result in an increase in traffic volumes on 33rd Street of approximately 200 to 300 vehicles during the afternoon peak hour (at approximately 5:00 p.m.). This would mean a 17% increase in traffic near Idylwyld Drive and a 36% increase near Confederation Drive. Both the existing and future traffic volumes are directional, and only prevalent during the peak traffic hours.

Table 2 (Attachment 2) outlines existing and future levels of service along 33rd Street for a two lane and a four lane configuration (two travel lanes plus directional peak hour parking restrictions). The table indicates a small improvement in the level of service by having four travel lanes along the corridor during the peak hours. These improvements can be accomplished either by implementing parking restrictions during the peak hours, or making the capital investment to construct new parking lanes as outlined above. For reference, Attachment 3 shows a visual display of the levels of services among varying roadway classifications.

Given the anticipated demand for increased capacity during the peak hours from the expanding west side neighbourhoods, the Administration will be taking steps to provide for additional capacity during these peak hours, while still maintaining a safe environment for pedestrians. The improvements will be undertaken in two phases, with details of each outlined in Attachment 4:

- Phase 1: Two-Hour Parking Restrictions (Avenue B to D); Peak-Hour Left Turning Movement Restrictions (Avenues B and C); and Active Pedestrian Corridor (Avenue K); and
- Phase 2: Peak Hour Parking Restrictions (Avenue F to Confederation Drive); Avenue D turning movement restriction.

Phase 1 will be completed in 2013. The Administration will proceed with public consultation for Phase 2, and will report back to City Council.

OPTIONS TO THE RECOMMENDATION

Complete conversion of 33rd Street to a four lane cross section is an option should additional capacity be required beyond the peak hours in the future. The Administration does not recommend this option at this time as the increased traffic demands do not require an increase in capacity outside of the peak traffic hours. The Administration will continue to monitor the traffic conditions to determine if additional capacity is required, keeping in mind a balance between roadway construction, improvements in transit and other opportunities to move people to and from the west side of the City.

POLICY IMPLICATIONS

There are no policy implications.

FINANCIAL IMPLICATIONS

The costs associated with Phase 1 of this plan are \$30,000. Adequate funding is allocated within Capital Project 631 - Traffic Safety Program.

Capital Project 631 will also fund Phase 2 of the plan, which is estimated to be \$100,000. Given the priorities within the Traffic Safety Reserve, it is estimated that this work could proceed in 2015, if approved.

	Budgeted	Unbudgeted	Capital	Operating	Non-Mill Rate	External Funding
Phases 1, 2	\$30,000	\$100,000	\$130,000			

PUBLIC AND/OR STAKEHOLDER CONSULTATION

The modifications outlined in Phase 1 were presented at an Open House which was held in relation to the Mayfair Traffic Management Plan, on June 16, 2011, and were generally supported by those in attendance. In addition, the properties along 33rd Street between Avenues B and D were surveyed regarding the planned peak hour parking restrictions, to gauge the level of support for such measures. Over 20 surveys were distributed, with only 1 response opposed to the proposal.

The turning movement restrictions at Avenue D, outlined in Phase 2, were also presented at the Open House on June 16, 2011, with minimal opposition. The Administration will initiate consultation with adjacent residents regarding the specifics of the proposed peak hour parking restrictions, as outlined in Phase 2.

Saskatoon Transit has been consulted regarding the proposed operational changes and is in favour of the proposed peak hour left-turn restrictions; however, they have several routes that will require signed exemptions to the left-turn restrictions. Transit is also supportive of the proposed parking restrictions identified for Phase 2.

Preliminary discussions have been held with the Environmental Services Branch to determine the impact of the peak hour parking restrictions on blocks that have front street garbage pickup. Further discussions will be required and public consultation will be undertaken.

COMMUNICATIONS PLAN

An information sheet will be distributed to residents and business owners with information on the modifications. Details will be shared with the general public through Public Service Announcements and on the City's website. Signage will also be in place to advise motorists and pedestrians of the changes.

ENVIRONMENTAL IMPLICATIONS

The plan outlined in this report balances vehicular and alternative modes of transportation, which is consistent with the Strategic Goal of Environmental Leadership.

PRIVACY IMPACT

There are no privacy implications.

SAFETY/CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN (CPTED)

A CPTED Review will not be conducted as part of the design process.

PUBLIC NOTICE

Public Notice, pursuant to Section 3 of Policy C01-021, Public Notice Policy, is not required.

ATTACHMENTS

1. Existing and proposed cross sections;
2. Tables 1 and 2;
3. Traffic Level of Service; and
4. Phasing of Planned Modifications – 33rd Street West.

Written by: David LeBoutillier, Planning and Design Engineer
Transportation Branch

Lana Dodds, Traffic Program Coordinator
Transportation Branch

Reviewed by: Don Cook, Planning and Design Engineer
Transportation Branch

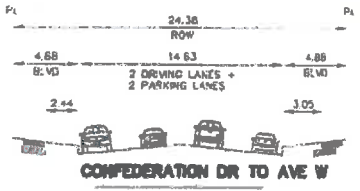
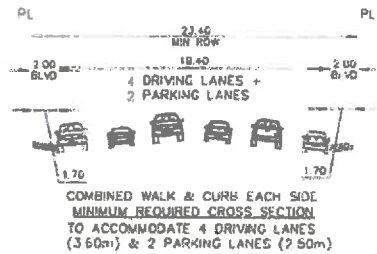
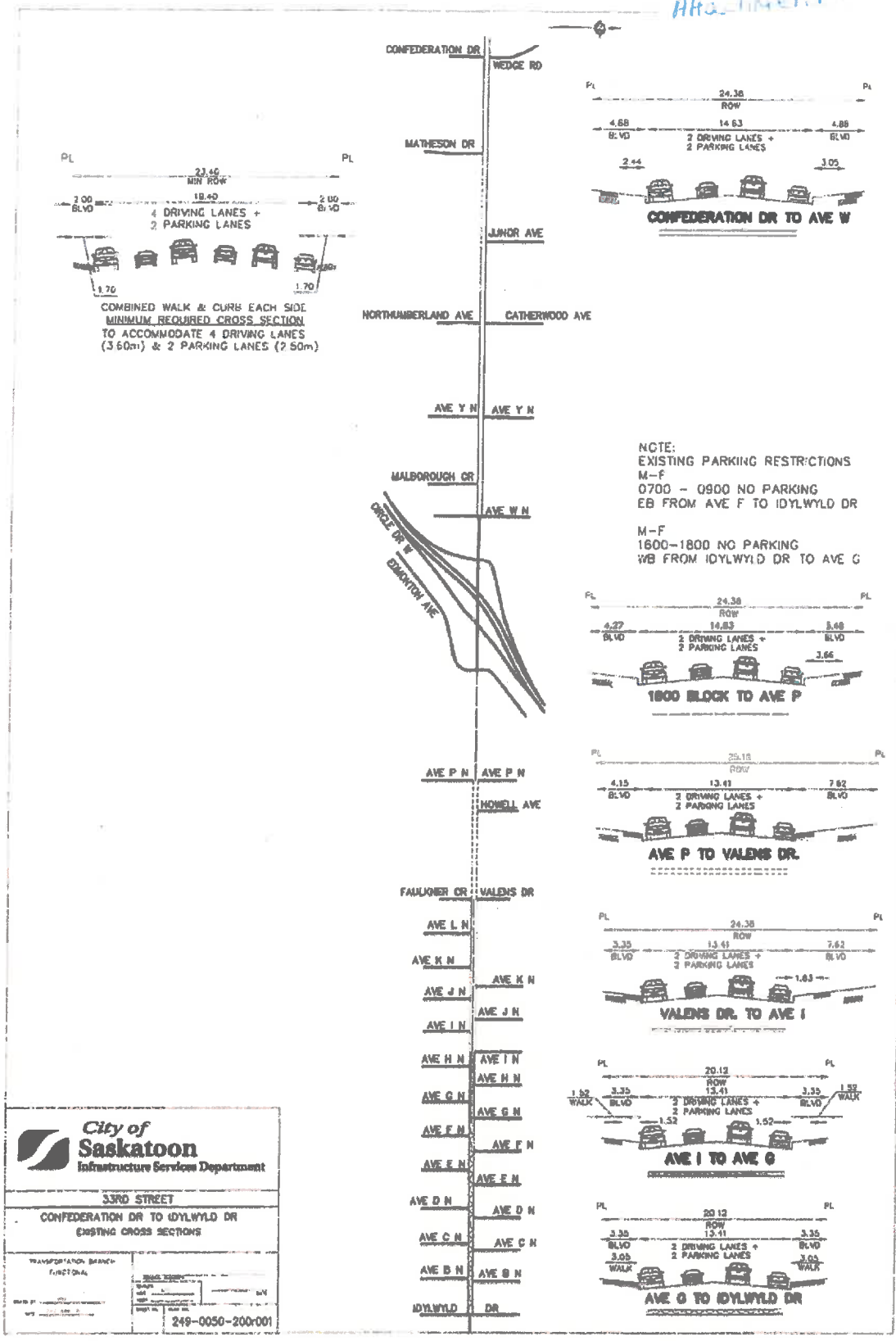
Approved by: Angela Gardiner, Manager
Transportation Branch

Approved by: _____
Mike Gutek, General Manager
Infrastructure Services
Dated: _____

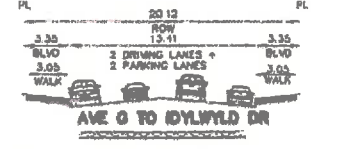
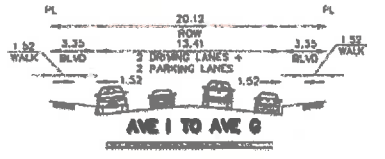
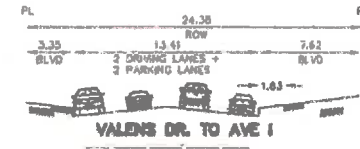
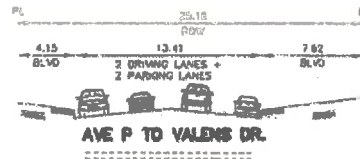
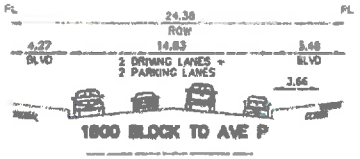
Copy to: Murray Totland
City Manager

PO DL 33rd St boulevards

Attachment 1



NCTE:
 EXISTING PARKING RESTRICTIONS
 M-F
 0700 - 0900 NO PARKING
 EB FROM AVE F TO IDYLWYLD DR
 M-F
 1600-1800 NO PARKING
 WB FROM IDYLWYLD DR TO AVE G



City of Saskatoon
 Infrastructure Services Department

33RD STREET
 CONFEDERATION DR TO IDYLWYLD DR
 EXISTING CROSS SECTIONS

TRANSPORTATION BRANCH
 FUNCTIONAL

249-0050-200-001

Tables 1 and 2

Table 1: Cost Breakdown

Costs	Idylwyld Drive to Avenue W	Avenue W to Confederation Drive
Construction	\$2.7 Million	\$1.3 Million
Property Acquisition/Utility Relocation	\$3.0 Million	\$0.5 Million
Purchase/Demolish Buildings	TBD	N/A

Table 2: Level of Service for Various Scenarios

Scenario	AM		PM	
	EB	WB	EB	WB
33rd Street west of Circle Drive (Avenue W to Confederation Drive)				
<u>Existing</u> conditions: 2011 Traffic Volumes, 1 lane each direction + parking	C	A	B	C
<u>Proposed</u> conditions: 2011 Traffic Volumes, 1 lane each direction + peak hour parking restrictions (2 lanes each direction)	B	A	A	A
<u>Existing</u> conditions: Future Traffic Volumes, 1 lane each direction + parking	D	B	B	D
<u>Proposed</u> conditions: Future Traffic Volumes, 1 lane each direction + peak hour parking restrictions (2 lanes each direction)	B	A	A	A
33rd Street east of Circle Drive (Idylwyld Drive to Avenue D North)				
<u>Existing</u> conditions: 2011 Traffic Volumes, 1 lane each direction + peak hour parking restrictions (2 lanes each direction)	C	B	B	A
<u>Proposed</u> conditions: Future Traffic Volumes, 1 lane each direction + peak hour parking restrictions (2 lanes each direction)	C	B	B	A

	Level-of-Service (LOS)					
	A	B	C	D	E	F
42 nd STREET & FAITHFULL AVENUE Eastbound/Westbound (looking west)	 Wednesday morning (6:30 am) Low control delay, up to 10 s/veh. Progression is extremely favourable and most vehicles arrive during the green phase. Many vehicles do not stop at all.	 Wednesday morning (6:30 am) Control delay is in the range 10 - 20 s/veh. Good progression and/or short cycle lengths. Most vehicles stop less than 10 s. A, causing higher levels of delay.	 Wednesday morning (6:30 am) Control delay is in the range 20 - 35 s/veh. There is higher delay than in the previous levels. Progression, longer cycle lengths, or both. Individual cycle lengths may begin to appear at this level (a green phase does not serve required vehicle, and overflows occur). The number of vehicle stopping is noticeable, though many still pass through the intersection without stopping.	 Wednesday morning (6:30 am) Control delay is in the range 35 - 55 s/veh. The influence of congestion becomes more noticeable. Higher delay may result from some combination of unfavorable progression, long cycle lengths, high v/c ratios. Many vehicles stop, and the proportion of vehicles not stopping doubles. Individual cycle failures are noticeable.	 Wednesday morning (6:30 am) Control delay is in the range 55 - 80 s/veh. Poor progression, long cycle lengths, and high v/c ratios. Individual cycle failures are frequent.	 Wednesday morning (6:30 am) Control delay is in excess of 80 s/veh. Unacceptable for most drivers, often occurs with oversaturation (when arrival flow rates exceed the capacity of lane groups). It may also occur at high v/c ratios, with many individual cycle failures. Poor progression and long cycle lengths may also contribute significantly to high delay levels.
42 nd STREET & FAITHFULL AVENUE Southbound (looking north)	 Wednesday morning (6:30 am) Travel speed is higher than 90% of free-flow speed. Primarily free-flow operations at average travel speeds. Vehicles are completely unimpeded in their ability to manoeuvre within the traffic stream. Control delay at signalized intersections is minimal.	 Wednesday morning (6:30 am) Travel speed is higher than 70% of free-flow speed. Reasonably unimpeded operations at average travel speeds. The ability to manoeuvre within the traffic stream is only slightly restricted, and control delays at signalized intersections are not significant.	 Wednesday morning (6:30 am) Travel speeds are about 50% of free-flow speed. Stable operations; ability to manoeuvre and change lanes is restricted, but not severely. Control delay is more than at LOS B, and longer queues, adverse signal coordination, or both may contribute to lower average travel speeds.	 Wednesday morning (6:30 am) Travel speeds are about 40% of free-flow speed. Small increases in flow may cause substantial increases in delay and decrease in travel speed. LOS D may be due to adverse signal progression, inappropriate signal timing, high volumes, or a combination of these factors.	 Wednesday morning (6:30 am) Travel speeds are about 35% of free-flow speed. Characterized by significant delays and low average travel speeds. Such operations are caused by a combination of adverse progression, high signal density, high volumes, excessive delays at critical intersections, and inappropriate signal timing.	 Wednesday morning (6:30 am) Travel speeds are 25% - 30% of free-flow speed. Not selected by when travel flow is extremely low speed. Intersection congestion is likely at critical signalized locations, with high delays, high volumes, and excessive queuing.
Urban Streets (Arterials)	 Wednesday morning (6:30 am) Travel speed is higher than 90% of free-flow speed. Primarily free-flow operations at average travel speeds. Vehicles are completely unimpeded in their ability to manoeuvre within the traffic stream. Control delay at signalized intersections is minimal.	 Wednesday morning (6:30 am) Travel speed is higher than 70% of free-flow speed. Reasonably unimpeded operations at average travel speeds. The ability to manoeuvre within the traffic stream is only slightly restricted, and control delays at signalized intersections are not significant.	 Wednesday morning (6:30 am) Travel speeds are about 50% of free-flow speed. Stable operations; ability to manoeuvre and change lanes is restricted, but not severely. Control delay is more than at LOS B, and longer queues, adverse signal coordination, or both may contribute to lower average travel speeds.	 Wednesday morning (6:30 am) Travel speeds are about 40% of free-flow speed. Small increases in flow may cause substantial increases in delay and decrease in travel speed. LOS D may be due to adverse signal progression, inappropriate signal timing, high volumes, or a combination of these factors.	 Wednesday morning (6:30 am) Travel speeds are about 35% of free-flow speed. Characterized by significant delays and low average travel speeds. Such operations are caused by a combination of adverse progression, high signal density, high volumes, excessive delays at critical intersections, and inappropriate signal timing.	 Wednesday morning (6:30 am) Travel speeds are 25% - 30% of free-flow speed. Not selected by when travel flow is extremely low speed. Intersection congestion is likely at critical signalized locations, with high delays, high volumes, and excessive queuing.
CIRCLE DRIVE AT WARMAK ROAD Westbound (looking east)	 Wednesday morning (6:30 am) Completely free-flow conditions. The operation of other vehicles and operations are constrained only by the presence, location of the highway and the driver's preferences. Manoeuvrability within the traffic stream is good. Minor disruptions in flow are easily absorbed with a change in travel speed.	 Wednesday morning (6:30 am) Free-flow, although the presence of other vehicles becomes noticeable. Average travel speeds are the same as in LOS A, but drivers may slightly less easily manoeuvre. Minor disruptions in flow will be more obvious.	 Wednesday morning (6:30 am) The influence of traffic density on operations becomes marked. The ability to manoeuvre within the traffic stream is clearly affected by other vehicles, (minor disruptions can cause serious local deterioration in service, and queues will form behind any significant traffic disruption).	 Wednesday morning (6:30 am) The ability to manoeuvre is severely restricted due to traffic congestion. Travel speed is reduced by the increasing volume. Only minor disruptions can be absorbed without extensive queues forming and the service deteriorating. Drivers experience reduced physical and psychological comfort levels.	 Wednesday morning (6:30 am) Operations at or near capacity, an unstable level. Vehicles are operating at with the minimum spacing for maintaining uniform flow, operations are volatile. Disruptions cannot be dissipated readily, often causing queues to form and serve to deteriorate to LOS F. Passenger car speeds are highly variable and unpredictable. Disruption waves can propagate throughout the upstream flow.	 Wednesday morning (6:30 am) Forward or breakdown flow. It occurs either when vehicles arrive at a rate greater than the rate at which they are discharged or when the forecast demand exceeds the computed capacity of a segment. Although operations at these points - and on sections immediately downstream - appear to be at capacity, queues form behind these breakdowns. Operations within queues are highly variable, with vehicle experiencing brief periods of movement followed by stoppages.
Multi-lane Expressways	 Wednesday morning (6:30 am) Completely free-flow conditions. The operation of other vehicles and operations are constrained only by the presence, location of the highway and the driver's preferences. Manoeuvrability within the traffic stream is good. Minor disruptions in flow are easily absorbed with a change in travel speed.	 Wednesday morning (6:30 am) Free-flow, although the presence of other vehicles becomes noticeable. Average travel speeds are the same as in LOS A, but drivers may slightly less easily manoeuvre. Minor disruptions in flow will be more obvious.	 Wednesday morning (6:30 am) The influence of traffic density on operations becomes marked. The ability to manoeuvre within the traffic stream is clearly affected by other vehicles, (minor disruptions can cause serious local deterioration in service, and queues will form behind any significant traffic disruption).	 Wednesday morning (6:30 am) The ability to manoeuvre is severely restricted due to traffic congestion. Travel speed is reduced by the increasing volume. Only minor disruptions can be absorbed without extensive queues forming and the service deteriorating. Drivers experience reduced physical and psychological comfort levels.	 Wednesday morning (6:30 am) Operations at or near capacity, an unstable level. Vehicles are operating at with the minimum spacing for maintaining uniform flow, operations are volatile. Disruptions cannot be dissipated readily, often causing queues to form and serve to deteriorate to LOS F. Passenger car speeds are highly variable and unpredictable. Disruption waves can propagate throughout the upstream flow.	 Wednesday morning (6:30 am) Forward or breakdown flow. It occurs either when vehicles arrive at a rate greater than the rate at which they are discharged or when the forecast demand exceeds the computed capacity of a segment. Although operations at these points - and on sections immediately downstream - appear to be at capacity, queues form behind these breakdowns. Operations within queues are highly variable, with vehicle experiencing brief periods of movement followed by stoppages.

Phasing of Planned Modifications – 33rd Street West

Phase 1: Two-Hour Parking Restrictions, Peak-Hour Turning Movement Restrictions and Active Pedestrian Corridor

The Administration will be installing two-hour parking restrictions along 33rd Street, from Avenue B to Avenue D, to accommodate sufficient parking turnover for the businesses on 33rd Street West. Currently, in order to facilitate improved traffic flow during peak hours, parking is prohibited along 33rd Street West, from Avenue B to Avenue F, Monday through Friday, on the south side from 7:00 a.m. to 9:00 a.m.; on the north side from 4:00 p.m. to 6:00 p.m.; and on both sides, from Avenue B to Idylwyld Drive, at all times.

Additionally, to improve traffic flow near Idylwyld Drive during the peak hours, the Administration will be implementing left-turn restrictions along 33rd Street West, Monday through Friday, at Avenues B and C eastbound during the morning peak hours of 7:00 a.m. to 9:00 a.m.; and westbound during the evening peak hours of 4:00 p.m. to 6:00 p.m. In order to increase pedestrian safety, pedestrian pavement markings and signage will be enhanced along 33rd Street West, from Avenues B to E.

An active pedestrian corridor was installed at the intersection of 33rd Street West and Avenue K North, to improve pedestrian safety across 33rd Street leading to Henry Kelsey Park. Active pedestrian corridors are enhanced pedestrian crossings that utilize amber flashing beacons to notify motorists that a pedestrian is at the crosswalk and intending to cross.

The remainder of Phase 1 improvements, as shown in Attachment 5, will be completed in 2013.

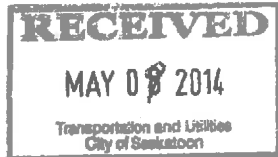
Phase 2: Further Restriction of Parking and Turning Movements, and Installation of Pedestrian-Actuated Crossing Facilities

Phase 2 will involve the implementation of parking restrictions west of Avenue F to Confederation Drive during peak hours, Monday through Friday, as shown in Attachment 6. Westbound parking will be prohibited between 4:00 p.m. and 6:00 p.m. on weekdays; and eastbound parking will be prohibited between 7:00 a.m. and 9:00 a.m. on weekdays. The removal of on-street parking in front of residential properties has been used along Clarence Avenue to effectively create a four-lane arterial street to improve capacity, while maintaining reasonable crossing opportunities for pedestrians.

In addition, left turn movements will be further restricted at Avenue D, with the use of small islands (right turns in and out only). To accommodate the turning restrictions and to improve pedestrian safety, the existing traffic signals at Avenue D will be removed and active pedestrian corridors will be installed at Avenues C and D.

A Public Hearing is required to physically restrict turning movements at Avenue D, which will be undertaken upon funding approval.

Phase 2 will be implemented in 2015, upon approval of funding.



CITY OF SASKATOON

Office of the City Clerk

To: General Manager
Utilities and Transportation Department

Date: May 8, 2014

Phone: (306) 975-3240

Our File: CK. 6320-1

From: Joanne Sproule
City Clerk

Your File:

Re: **Motion - Proposed Changes to Parking
and Traffic Operations along 33rd Street**

Attached is an excerpt from the minutes of meeting of City Council held on May 5, 2014, regarding the above matter.

The following motions were considered:

Moved by Councillor Hill, Seconded by Councillor Lorje,

THAT City Council rescind the first two points of the motion which was passed by City Council at its meeting held on January 21, 2013, approving the adoption of Phase 1 of 33rd Street Changes and that the third point pertaining to the 33rd Street and Avenue K corridor remain.

Phase 1 - Two-hour Parking Restrictions, Peak-hour Turning Movement Restrictions and Active pedestrian Corridor.

- *Two-hour parking restrictions along 33rd Street from Avenue B to Avenue D, to accommodate sufficient parking turnover for the businesses on 33rd Street West*
- *Implementation of left-turn restrictions along 33rd Street West, Monday through Friday, at Avenue B and C eastbound during the morning peak hours of 7:00 am to 9:00 am and westbound during the evening peak hours of 4:00 pm to 6:00 pm. In order to increase pedestrian safety, pedestrian pavement markings and signage will be enhanced along 33rd Street west, from Ave B to E.*
- *An Active Pedestrian Corridor installed at 33rd Street and Ave K to improve pedestrian safety.*

CARRIED.

Memorandum

THAT the proposed changes, in the first two points, be reevaluated after the public consultation process has been completed with all neighbourhoods impacted along the 33rd Street corridor.

CARRIED.


JS:rmr

Attachment

Memorandum

The following is an excerpt from the minutes of meeting of City Council held on Monday, May 5, 2014:

MOTIONS

REPORT OF THE CITY CLERK:

"Councillor Hill provided the following Notice of Motion to members of City Council via email dated May 2, 2014:

'TAKE NOTICE that in accordance with Section 35(1)(b) of *The Council and Committee Procedure Bylaw, 2003*, this is to advise that at the meeting of City Council scheduled for Monday, May, 5, 2014, I will move the following motion:

'THAT City Council rescind the first two points of the motion which was passed by City Council at its meeting held on January 21, 2013, approving the adoption of Phase 1 of 33rd Street Changes and that the third point pertaining to the 33rd Street and Avenue K corridor remain.

Phase 1 - Two-hour Parking Restrictions, Peak-hour Turning Movement Restrictions and Active pedestrian Corridor.

- Two-hour parking restrictions along 33rd Street from Avenue B to Avenue D, to accommodate sufficient parking turnover for the businesses on 33rd Street West
- Implementation of left-turn restrictions along 33rd Street West, Monday through Friday, at Avenue B and C eastbound during the morning peak hours of 7:00 am to 9:00 am and westbound during the evening peak hours of 4:00 pm to 6:00 pm. In order to increase pedestrian safety, pedestrian pavement markings and signage will be enhanced along 33rd Street west, from Ave B to E.
- An Active Pedestrian Corridor installed at 33rd Street and Ave K to improve pedestrian safety.'

Should the above motion be passed by City Council, I will then move the following motion:

'THAT the proposed changes, in the first two points, be reevaluated after the public consultation process has been completed with all neighbourhoods impacted along the 33rd Street corridor.'

Excerpt
Notice of Motion
Monday, May 5, 2014
Page Two

Moved by Councillor Hill, Seconded by Councillor Lorje,

THAT City Council rescind the first two points of the motion which was passed by City Council at its meeting held on January 21, 2013, approving the adoption of Phase 1 of 33rd Street Changes and that the third point pertaining to the 33rd Street and Avenue K corridor remain.

Phase 1 - Two-hour Parking Restrictions, Peak-hour Turning Movement Restrictions and Active pedestrian Corridor.

- Two-hour parking restrictions along 33rd Street from Avenue B to Avenue D, to accommodate sufficient parking turnover for the businesses on 33rd Street West*
- Implementation of left-turn restrictions along 33rd Street West, Monday through Friday, at Avenue B and C eastbound during the morning peak hours of 7:00 am to 9:00 am and westbound during the evening peak hours of 4:00 pm to 6:00 pm. In order to increase pedestrian safety, pedestrian pavement markings and signage will be enhanced along 33rd Street west, from Ave B to E.*
- An Active Pedestrian Corridor installed at 33rd Street and Ave K to improve pedestrian safety.*

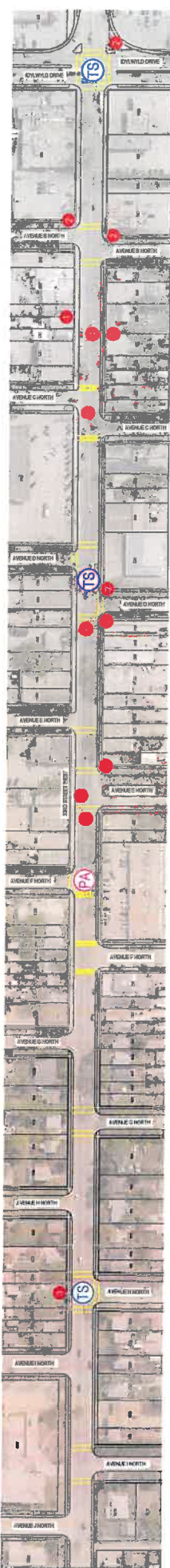
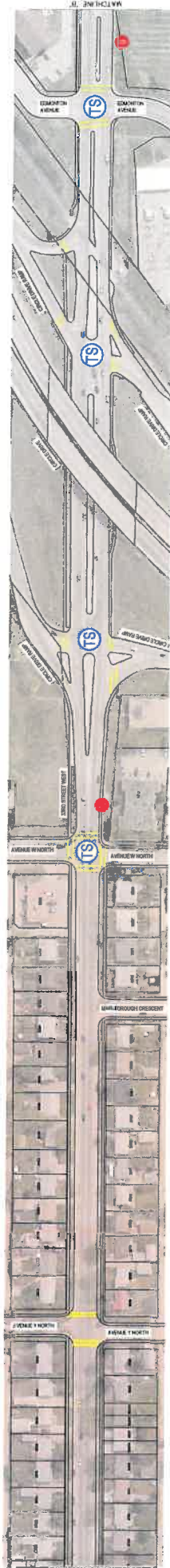
CARRIED.

Moved by Councillor Hill, Seconded by Councillor Lorje,

THAT the proposed changes, in the first two points, be reevaluated after the public consultation process has been completed with all neighbourhoods impacted along the 33rd Street corridor.

CARRIED.

APPENDIX B: PUBLIC MEETINGS INFORMATION



LEGEND

-  EXISTING TRAFFIC SIGNAL
-  EXISTING PEDESTRIAN ACTUATED SIGNAL
-  EXISTING ACTIVE PEDESTRIAN CORRIDOR

Item	Location	Proposed	Existing
●	Prohibit crosswalk improvement	Prohibit crosswalk improvement	Prohibit crosswalk improvement
●	Prohibit crosswalk improvement	Prohibit crosswalk improvement	Prohibit crosswalk improvement
●	Install Sidewalk	Install Sidewalk	Install Sidewalk
●	Install Pathway	Install Pathway	Install Pathway
●	Intersection Improvement	Intersection Improvement	Intersection Improvement
●	Coordinate Intersection Improvements	Coordinate Intersection Improvements	Coordinate Intersection Improvements
●	Coordinate Intersection Improvements	Coordinate Intersection Improvements	Coordinate Intersection Improvements

RECOMMENDED PEDESTRIAN IMPROVEMENTS

1 – REPLACE DEPRESSED SIDEWALKS

- ✓ 5 LOCATIONS BETWEEN AVENUE B & AVENUE C
- ✓ 1 LOCATION BETWEEN AVENUE F & AVENUE E

Existing



Proposed



3 – INSTALL SIDEWALK

- ✓ SOUTH SIDE OF 33RD STREET BETWEEN AVENUE P AND EAST EDGE OF VIC REMPEL YARDS

Existing



Proposed



2 – INSTALL SIDEWALK RAMPS

- ✓ SW CORNER OF IDYLWYLD DRIVE
- ✓ SE CORNER OF AVENUE B
- ✓ NE CORNER OF AVENUE B
- ✓ SE CORNER OF AVENUE D
- ✓ SW CORNER OF AVENUE D
- ✓ SE CORNER OF AVENUE E (WEST CROSSWALK LOCATION)

Existing



Proposed



4 – INSTALL PATHWAY

- ✓ SOUTH SIDE OF 33RD STREET BETWEEN EAST EDGE OF VIC REMPEL YARDS AND EDMONTON AVENUE

Existing



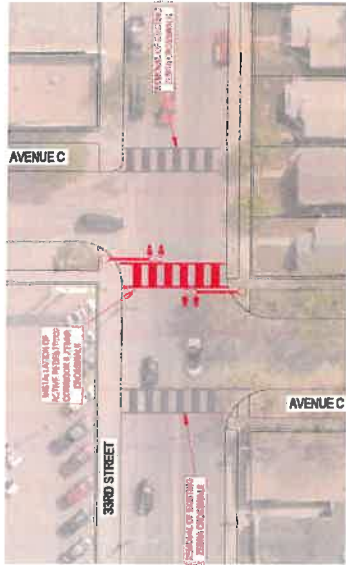
Proposed



5 – AVENUE C PEDESTRIAN CROSSWALK IMPROVEMENT

- ✓ INSTALL ACTIVE PEDESTRIAN CORRIDOR TYPE CROSSING ON EAST SIDE OF SOUTHERN LEG
- ✓ REMOVE OTHER EXISTING CROSSINGS

Proposed



Example



12 –SIDEWALK PROTECTION

- ✓ REMOVE TEMPORARY JERSEY BARRIER WITH BOLLARD AND REPAIR SIDEWALK

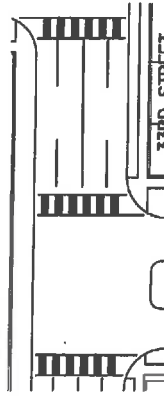
Example



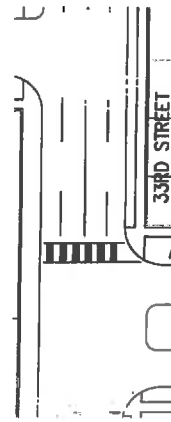
6 – AVENUE E PEDESTRIAN CROSSWALK IMPROVMENT

- ✓ INSTALL ZEBRA CROSSWALK ON EAST SIDE OF AVENUE E ON SOUTHERN LEG
- ✓ REMOVE CROSSWALK ON EAST SIDE

Existing



Proposed



13 - REPLACE DIRT BOULEVARD WITH CONCRETE

- ✓ SOUTHSIDE OF 33RD STREET BETWEEN AVENUE B AND AVENUE C

Existing

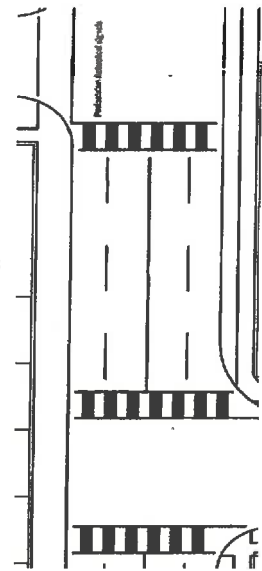


Example

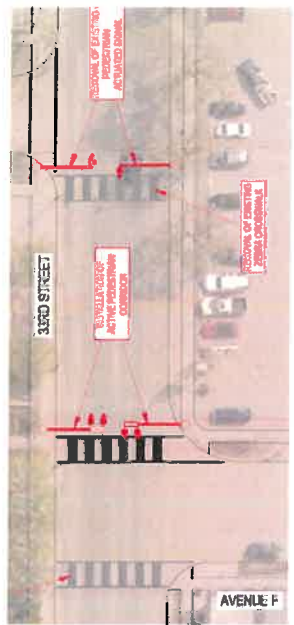


7 - AVENUE F

- ✓ REMOVE PEDESTRIAN ACTUATED SIGNALS (EAST LEG) AND REPLACE WITH ACTIVE PEDESTRIAN CORRIDOR



Proposed



Proposed

RECOMMENDED INTERSECTION IMPROVEMENTS

8 - NORTHUMBERLAND AVENUE

- ✓ REMOVE PEDESTRIAN ACTUATED SIGNALS AND REPLACE WITH FULL-SIGNALS

Existing



Example



9 - AVENUE D

- ✓ UPGRADE TRAFFIC SIGNAL POLES AND TRAFFIC SIGNALS

Existing



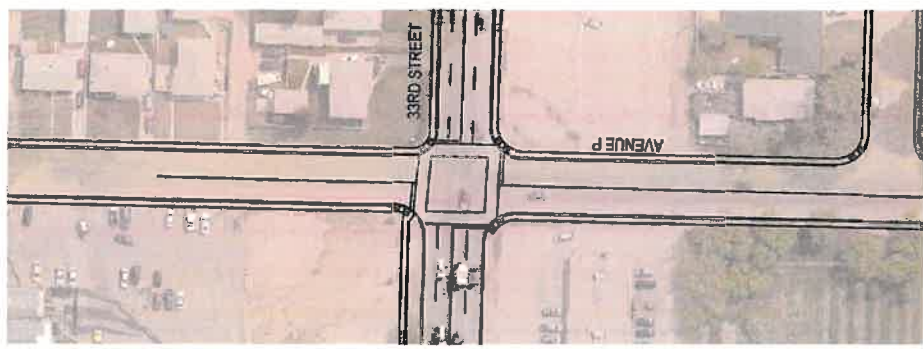
Example



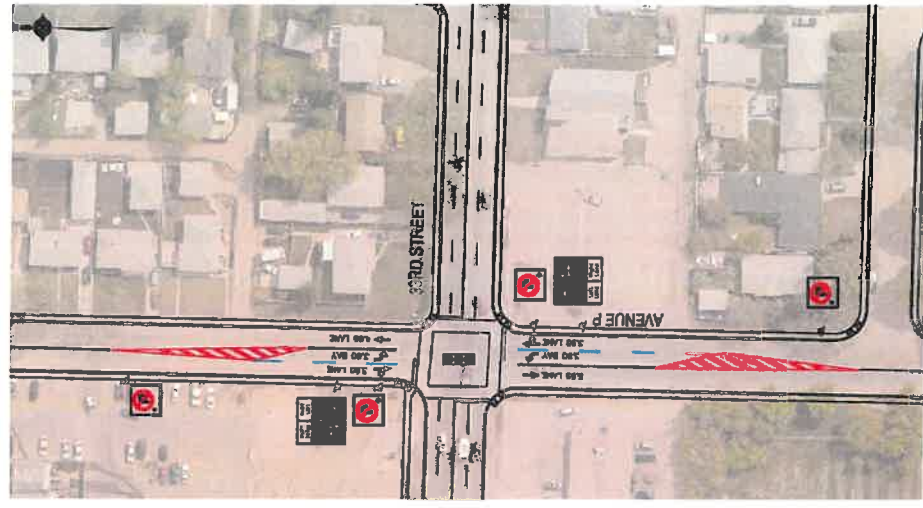
10 – AVENUE P

- ✓ RE-CONFIGURE SOUTHERN AND NORTHERN LEGS

Existing



Proposed



11 – AVENUE W

- ✓ CONSTRUCT A BUS LAY-BY ON 33RD STREET (SOUTHEAST CORNER), ALSO WAITING UPON TRANSIT COMMENTS

Existing



Proposed





STANDING POLICY COMMITTEE ON ENVIRONMENT, UTILITIES & CORPORATE SERVICES

Continuous Improvement Strategy – Internal Process Review Update

Recommendation of the Committee

That the report of the General Manager, Corporate Performance Department dated October 13, 2015, be received as information.

History

At the October 13, 2015 Standing Policy Committee on Environment, Utilities & Corporate Services meeting, a report of the General Manager, Corporate Performance Department dated October 13, 2015 was considered.

Attachment

October 13, 2015 Report of the General Manager, Corporate Performance.

Continuous Improvement Strategy – Internal Process Review Update

Recommendation

That the report of the General Manager, Corporate Performance Department dated October 13, 2015, be forwarded to City Council for information.

Topic and Purpose

The purpose of this report is to provide an update on the Internal Process Reviews (IPR) undertaken across the organization to date.

Report Highlights

1. Internal Process Reviews focus on identifying and removing redundancies and waste within existing processes to increase efficiencies in civic operations.
2. They also build capacity in the corporation through innovation coaches and empowering employees.
3. IPRs are bringing teams together and increase efficiencies.
4. IPRs focus on:
 - streamlining the process for citizens;
 - ensuring consistency in the service provided to citizens;
 - reduction of manual processes and increase in digital first approach; and
 - review of tax-supported and user-pay models to cover the cost to deliver the service.
5. Employees find solutions for better service based on their experience and knowledge of the operational realities and finding savings as a result.

Strategic Goal

This report supports the long-term strategy to focus on continuous improvement and use innovative and creative means to provide the best possible services under the Strategic Goal of A Culture of Continuous Improvement.

Background

The Continuous Improvement (CI) Strategy was launched in 2013 as a corporate-wide approach to ensuring effectiveness and improving efficiencies in municipal services and operations. An ongoing approach including regular service level reviews encourages the Administration to go beyond conventional needs to provide high-quality service to the citizens of Saskatoon using innovative and creative means.

Internal Process Reviews (IPRs) are a component of the CI Strategy and are designed to improve effectiveness and efficiency of the operations through identification of waste and redundancy in internal processes. The approach to an IPR will depend on the size and scope of the project.

- Innovation Teams will be encouraged for projects with a larger scope or multi-departmental impact.

- Smaller review teams will be encouraged to establish quick wins in a section, division or department.

In the 2015 Corporate Business Plan and Budget, each business line identified continuous improvement initiatives for 2015, many of which will be addressed through an IPR. Additionally, on an ongoing basis each division is identifying opportunities to increase efficiency and improve on effectiveness. These initiatives are tracked and reported out annually in the *Report on Service, Savings and Sustainability: How the City of Saskatoon is Improving Productivity*.

Report

Focus on Identifying and Removing Redundancies and Waste within Existing Processes

In an organization the size of the City of Saskatoon, it is not uncommon that there is duplication of some processes and functions. For example, one division may be responsible for graffiti removal on a park bench and yet another is responsible for removal if the graffiti is found on a power pole; creating opportunity for inconsistency in service provided to citizens as well as potential for duplication of time spent investigating the issue.

The IPRs formalize the opportunity for teams from multiple divisions to come together and understand services delivered by each divisions and/or section, how each division addresses issues and concerns as they arise and identify areas of duplication. The reviews examine all aspects of a process, identifying steps that are redundant or no longer necessary and seeks opportunities to automate processes, reducing manual entry, paper use and staff time required to locate missing information and/or correct errors.

Building Capacity for Innovation in the Organization

An Innovation Practitioner Training (boot camp) in 2014 brought 30 employees from a variety of divisions together with a purpose of fostering innovation and creativity within all levels of the organization. Through the use of tools the process is designed to take an opportunity from idea to implementation, teams are building capacity within each department to assist in driving change.

The Innovation League, as the group is called, meets once per month to share information on how they have used the tools with their team in meetings and discussions. The tools that the Innovation League members have been trained on through the Innovation Boot Camp are an excellent resource that teams will utilize to complete many of the CI initiatives outlined in the 2015 Corporate Business Plan and Budget.

IPRs are Bringing Divisions Together and Increasing Efficiencies

Collaboration across divisions is a key component to ensuring we are providing quality service to citizens and good value for tax payer's dollars. IPRs bring a team together, formally or informally, to address a challenge or opportunity. Attachment 1 provides highlights for some of the IPRs underway in 2015: Customer Service at Public Works, Streamlining of Graffiti Reduction, and a review of the Special Events process.

Employees have found solutions for better service and savings of approximately \$200,000 to date.

The focus of the IPRs is on:

- streamlining the process for citizens;
- ensuring consistency in the service provided to citizens;
- reduction of manual processes and increase in digital first approach;
- review of tax –supported and user pay models to cover the cost to deliver the service; and
- creating a repeatable process allowing teams the opportunity to jump into the next issue or challenge.

Employees Finding Solutions for Better Service and Savings

Feedback from the teams participating in the IPRs has been excellent. Meetings have been described as productive, informative, and fun. The interactive process helps a team set a common goal and achieve clarity on objectives and outcomes, creating an environment where everyone can have a voice and is able to contribute to the conversation.

Front-line employees have commented that they are happy to be involved in the process as a member of the team and to be part of the change within their workgroup. They are often the closest to our citizens and bring great insight regarding feedback from citizens as well as the challenges and opportunities they face daily with work they are doing. They also bring some of the most innovative ideas to the table and are instrumental in helping achieve change within their work groups.

Improved collaboration fosters team building and employee engagement; employees can more easily understand the downstream impact of their work, which enables them to provide a better experience for citizens.

Communication Plan

Saskatoon's Performance Improvement Plan, "Making Strides", a communication strategy focusing on continuous improvement and performance targets was launched at the end of April. The goal of this strategy is to increase internal and external awareness for the City's success in achieving savings through efficiency, improving service and progress towards strategic goals.

Results from the Internal Process Reviews will be communicated on the City's website in the 'Latest Strides' and/or 'City Spotlight' sections of the *Our Performance* page at www.saskatoon.ca/strides.

Financial Implications

The financial implications related to efficiency gains continue to be quantified with the focus of reallocating to other strategic and operational priorities and/or may contribute to a reduction in the base budget. The annual budget will be prepared to reflect the approved service levels.

Other Considerations/Implications

There are no policy, environmental, Privacy, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

Reports related to key findings of the Internal Process Reviews will be brought forward to the Standing Policy Committee on Environment, Utilities and Corporate Services as they reach completion. Action plans and recommendations will be incorporated into the annual business planning and budgeting process for the respective divisions.

A report on the Continuous Improvement initiatives identified within each business line in the 2015 Corporate Business Plan and Budget will be brought forward as part of the 2016 Corporate Business Plan and Budget deliberations.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Attachments

1. Highlights from the Internal Process Reviews

Report Approval

Written by: Kim Matheson, Director, Strategic and Business Planning

Reviewed and

Approved by: Catherine Gryba, General Manager, Corporate Performance Department

Highlights from the Internal Process Reviews

Customer Service at Public Works

Description of the Process Under Review

- The review group brings together front line staff and management from Public Works (PW), Information Technology (IT), and Strategic and Business Planning (SBP) to begin the process of transitioning Public Works Customer Service to Service Saskatoon's Customer Relationship Management System/311. Public Works has over 200 services that result in inquiries and complaints from citizens, councillors and other civic divisions. As a starting point, the team began the review by documenting six current state water and sewer trouble processes.
- The team also discussed the opportunity to improve service to citizens by creating one point of contact for citizens which ensures they receive consistent, accurate, and relevant information, tracking and follow up. This was further defined as:
 - Informed customer service representatives able to answer a variety of questions quickly and completely in one call
 - Fast and effective service responses to emergency situations
 - Understand the levels of service provided by the City so they know what to expect
 - Options for inquiries (email, phone, web, social media, etc.)
- To date all six processes have been mapped and are undergoing revisions where 'quick wins' are possible to improve customer service.
- Moving forward, recommendation and action plan investigations focus on:
 - Developing a 311-like experience for the six processes with the support of IT, enabling gains in the digital first approach and consistent service for citizens.
 - Developing scripts for customer service representatives that outline consistent customer levels of service for these six processes. Information here highlights standards already in place at PW.
 - Review of tax costing – outlining customer levels of service supported by PW (emergency service) and user pay models (non-emergent service, for example – a curb box location request from a resident looking to pave a driveway).
 - Digital consolidation options for database/email information often searched by customer service representatives – enabling more efficient service (quicker calls and calls that answer questions without a call back to the citizen).

Anticipated Efficiencies and Service Enhancements Resulting From the Review

While the team is still working through the current processes and identifying opportunities for improvement it is expected that the revised process will result in the following opportunities that will be quantified in future reports to Committee:

- Streamlining of IT systems for the Public Works Call Centre will reduce the amount of time staff spend searching for information, improving the response time to citizens and allowing staff to address a greater number of inquiries or complaints within the same timeframe.
 - The length of time Call Centre staff currently spends on a call varies with the nature and complexity of the call. Calls can take anywhere from 1.5 minutes

- for a simple inquiry to several minutes (or longer if the information isn't available) on a complex inquiry or complaint.
 - On average staff spend 2.5 minutes per call searching for information. Each staff member takes approximately 83 calls per day which translates into 3.47 hours searching for information.
 - The staff time has a value of approximately \$164,000 per year.
- Moving from manual to online management of work orders in the field will improve the accuracy in reporting delays and/or work completion and eliminate the need for staff to manually enter work order information in the system. The future state will also allow citizens to be able to track their inquiry or complaint in real-time with an online tracking system, reducing the number of calls the customer service staff receive.
- Improved communication between the Public Works Call Centre and field crews will improve response time and accuracy of issue and location.

Streamlining Graffiti Reduction

Description of the Process Under Review:

- Traditionally, the City of Saskatoon department responsible for removal was determined by the location of the graffiti. For example, if the graffiti was on a park bench, the Parks division would coordinate its removal. As a result, citizens were always left to guess who to report the graffiti to, and would have to call multiple people before getting to the appropriate division.
- The longer graffiti remains, the more likely other people will add to it (also known as "tagging"). Therefore, it was identified that a much more efficient process for dealing with graffiti was needed.
- The review brought staff together from, among others, Community Development, Parks, Fire Department, Police Services, and Recreation and Sport. The team has been integrating innovation tools into their meetings, providing a more structured focus and encouraging them to look at the problem in a more comprehensive way; working together toward a citizen-centric solution.
- The team examined what success would look like from the citizen's perspective. This was defined as a seamless, coordinated approach which provides a single point of contact for reporting graffiti, as well as prompt, consistent response and time to remedy.
- The team is now also beginning to think not just about removal, but how to proactively prevent graffiti from happening. For example, group members have begun to look at the issue from a construction perspective, considering building materials that could be cleaned instead of having to be repainted; and identifying building locations less likely to be spray-painted throughout the city.

Anticipated Efficiencies and Service Enhancements Resulting From the Review:

While the team is still working through the current processes and identifying opportunities for improvement, it is expected that the revised process will result in the following opportunities that will be quantified in future reports:

- Streamlining of the graffiti reporting process through an app will improve the quality of information received, and reduce staff time currently spent figuring out the issue and location of the graffiti.
 - The length of time spent investigating a graffiti inquiry or complaint varies with the nature and complexity of the issue. On average staff spend 2-4 hours per inquiry searching for information and determining responsibility for remediation based on location, type of property, etc.
 - There are approximately 250 external inquiries/complaints per year with approximately 20% (50 inquiries/complaints per year) requiring detailed investigation and follow up.
 - At an average of 3 hours per investigation by a manager, the value of staff time is potentially between \$6,000 - \$10,000 per year.
 - Staff addressing these inquiries/complaints would be able to reallocate this time to other duties.
- The focus on preventing graffiti from happening will result in savings associated with equipment, materials and time required to remove the graffiti as well as time spent addressing reports of graffiti.
- Currently, several divisions incur costs related to removal of graffiti depending on where it is located and costs are tracked individually within divisions and/or sections. In order to provide potential efficiency gains in this area, information is being gathered from each of these divisions as part of the review to gain an understanding of the current cost of graffiti removal. The estimated annual cost for removal of graffiti across all divisions is approximately \$250,000 per year. Reducing the amount of graffiti through prevention efforts by 10% would result in an annual savings of equipment, materials and time of approximately \$25,000.

Special Events

Description of the Process Under Review:

- The review group brings together 9 of the 14 divisions currently involved in the special events process.
- Meetings to date have focused on understanding the current state of the special events process through process mapping, brainstorming activities including defining success from the stakeholders perspective and the collection of costing and stakeholder feedback data.
- There are many stakeholders involved in the special events process, each with their own definition of success. That being said, some of the common opportunities include:
 - An application process that is streamlined and reflective of the proposed event (simple application for small, low risk events versus detailed application for larger, higher risk events and/or those requiring civic services)
 - A single point of contact at the City regardless of the number of divisions or departments their event needs may touch
 - Responsive service in processing of applications and answering questions
 - Special events to occur in Saskatoon with minimal disruption to the daily lives of citizens

- Moving forward, recommendation and action plan investigations focus on:
 - Streamlining the process for citizens in two ways
 1. Customizing the application process, ensuring small low risk events have a concise application free from questions that are not applicable. This action stems from the recent customer feedback survey and supports a recommendation in the 2014 Special Events Report by Recreation & Sport that events be categorized by attendance and risk.
 2. The single point of contact model, ensuring citizens get efficient and consistent service regardless of how many divisions/departments are involved.
 - Streamlining the process for internal stakeholders involved with the creation of a SharePoint site to host all information relevant to the process. (complete)
 - Ensuring consistency in decisions made about event approvals and support through research and discussion around special events policy and procedure creation and documentation.
 - Creating online application processes to support the digital first strategy and the reduction of manual processes.
 - Reviewing the current payment model and discussing options that move toward a combination of supported events and user-pay events.

Anticipated Efficiencies and Service Enhancements Resulting From the Review:

The team is still working through the current processes and identifying opportunities for improvement. They are also currently quantifying costs associated with special events as there are many divisions that incur costs that are not currently recovered. A future report will outline these costs, options for application and event fees, cost recovery, and will provide a recommendation to the Committee.

It is expected that the revised process will result in the following opportunities that will be quantified in future reports:

- Streamlining the application process will eliminate the requirement of each division to receive an application from the event organizer. A significant amount of time is currently spent following up on incorrect or missing information by each division. Reduction in time spent following up on information has a value of approximately \$5,000 which would help reduce budgetary over-expenditures.
- Providing a single point of contact before, during and following an event, is expected to reduce complaints as presumably they will be addressed in a more timely fashion and reduce the number of phone calls and emails to City Councillors and the Administration regarding an event. Reduced complaints and inquiries would result in an increase in staff time of approximately 25 hours per event season that would be reallocated to other priorities.
- The team is also investigating options related to application and event fees, staff time and cost recovery for labour and equipment required for events as well as damage resulting from an event; particularly in civic parks.

Next Steps

It is expected that each of the reviews highlighted in this attachment will result in efficiencies such as staff time through automation of processes and reduction in duplication of effort across multiple divisions. Service enhancements are also expected that will reduce the time required to respond to citizen and Councillor inquiries and complaints as well as improving the ability for the information to be located online and/or through an app on a smart phone. Recommendations and action plans will be brought forward to Committee as the opportunities are investigated and savings quantified.



STANDING POLICY COMMITTEE ON ENVIRONMENT, UTILITIES & CORPORATE SERVICES

Environmental Policy (C02-036) Update

Recommendation of the Committee

That the outlined revisions to the Environmental Policy (C02-036) be approved.

History

At the October 13, 2015 Standing Policy Committee on Environment, Utilities & Corporate Services meeting, a report of the General Manager, Corporate Performance Department dated October 13, 2015 was considered.

Attachment

October 13, 2015 Report of the General Manager, Corporate Performance.

Environmental Policy (C02-036) Update

Recommendation

That the Standing Policy Committee on Environment, Utilities and Corporate Services recommend to City Council:

That the outlined revisions to the Environmental Policy (C02-036) be approved.

Topic and Purpose

This report recommends revisions to the City of Saskatoon's Environmental Policy (Policy) (C02-036).

Report Highlights

1. A meaningful Environmental Policy is expected to strengthen the City of Saskatoon's (City) ability to achieve its goal of Environmental Leadership by providing a framework for the work of employees and contractors across the corporation.
2. The City's Environmental Policy was compared to policies from other cities to identify deficiencies in themes and language. This comparison guided the changes suggested in the amended policy.
3. The revised policy better reflects the City as a member of the community in Saskatoon that all share responsibility for environmental sustainability.

Strategic Goals

Improvement of our Environmental Policy strengthens the City's commitment to many of the priorities and strategies set out in the Strategic Goals of Continuous Improvement, Environmental Leadership, Sustainable Growth, Moving Around, and Asset and Financial Management.

Background

Environmental Policy (C02-036) has existed since December 18, 2006 without edit or review. The policy states that it should be reviewed and approved by City Council annually.

Report

Importance of an Up-to-Date Policy

A meaningful Environmental Policy aims to provide a framework for developing and providing not only environmental programs and services, but also to facilitate responsible investments in services and infrastructure. The proposed amendments to the Policy will strengthen the City's commitments to the citizens of today and tomorrow, employees, and the natural environment.

The recommended updates consider environmental trends, regulatory requirements, the needs of the City's programs and services, and a comparison with other cities. A summary of the revisions is available in Attachment 1.

Updating Themes to Align with Other Cities

Eight themes were identified in many of the environmental policies or similar documents for cities across the country (Attachment 2). The City's overarching goal of becoming environmentally sustainable aligns with best practises. However, deficiencies exist in the following areas and amendments to the Policy have been suggested to improve this:

- shared responsibility
- protection and/or enhancement of natural areas and resources
- biodiversity

Clarifications and Language Improvements

Comparisons also looked at the language used in environmental objectives, the level of detail in documents, and the overall intention. In general, the City's Policy performed well in the comparison but identified the following suggested improvements:

- use of specific language reflecting current global trends in environmental leadership and management, and
- broadening of our policy objectives to satisfy thematic deficiencies.

Language in the Policy has been updated to reflect this. The amended Policy is shown as Attachment 3.

Options to the Recommendation

City Council may choose to reject the proposed amendments to the Environmental Policy.

Communication Plan

The Environmental Policy is available to the general public on the City's website. Amendments, if approved, will be communicated to civic employees through emails, meetings, and information sessions.

Policy Implications

This report proposes revisions to the Environmental Policy (C02-036).

Other Considerations/Implications

There are no immediate financial, environmental, stakeholder, privacy or CPTED implications to report.

Due Date for Follow-up and/or Project Completion

The Environmental Policy will be reviewed again in 2016. If amendments are warranted, a report will be submitted to City Council by September.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Attachments

1. Summary of Revisions
2. Comparison with other cities
3. Revised Environmental Policy

Report Approval

Written by: Kristin Bruce, Policy Intern, Environmental and Corporate Initiatives

Reviewed by: Amber Jones, Manager, Education and Environmental Performance

Approved by: Brenda Wallace, Director, Environmental and Corporate Initiatives
Catherine Gryba, General Manager, Corporate Performance Department

Administrative Report – Environmental Policy (C02-036) Update.docx

Summary of Revisions

Purpose

The purpose statement was revised to clarify that it “*serves as a statement of intentions and objectives in relation to the desired overall performance of the City of Saskatoon towards community sustainability*” and not just for individual staff members contributions as stated in 2006.

Definitions

Policy definitions were updated, added and deleted to better reflect current understanding of sustainability, incorporate themes from other Municipal policies, to strengthen the ability of the policy to be used as a framework for decision making, and to expand the intention of the policy to include civic government within the context of the community of Saskatoon.

Important Additions:

Sustainability - Systems thinking is ingrained such that each employee recognizes the connections between actions and the resulting impacts, considering a four pillars framework. This framework focuses on ecologic (the natural environment), social (society and justice), cultural (values and perspectives), and economic factors, today and into the long-term future.

Sustainable Community – Thriving in harmony with nature, in that citizen quality of life today does not negatively affect the ability of future generations to live by those same or improved standards.

Biodiversity - The variety of life in the Saskatoon region, particularly species, habitats and ecosystems typical to the Canadian prairie.

Policy and Objectives

Policy and objectives were updated to establish shared responsibility, meaningful engagement, accountability, and environmentally responsible behaviour for both the community and the City including civic staff, City Council Members, stakeholders, business owners, partners, and citizens of Saskatoon. The policy was updated to read:

3.1 GENERAL

The City of Saskatoon is committed to becoming an environmentally sustainable community. As a result, the organization encourages and expects environmentally responsible behaviour from all employees and contractors working at civic facilities and grounds. Achieving the ultimate goal involves shared responsibility between the community at large and our organization. As such the City of Saskatoon has a responsibility to facilitate and provide programs and services that move toward sustainability.

The Objectives in the policy were updated to read:

3.1 OBJECTIVES

- 3.2.1 *To promote and engage in the protection, enhancement, and responsible use of natural areas and resources; through attention to: biodiversity, protection of soils as a valued resource, climate change, water and air quality, land use, transportation patterns, and civic development.*
- 3.2.2 *To ensure asset and financial sustainability through life cycle accountability, risk management, and responsible environmental management.*
- 3.2.3 *To continually improve overall environmental performance through ongoing research and appropriate revision to objectives and targets, as well as transparent plain language reporting.*
- 3.2.4 *To seek the commitment of all internal staff, management, council members, businesses, and citizens to environmental stewardship and safety by building relationships and understanding through communication, education, training, and support.*
- 3.2.5 *To design, construct, retrofit, and operate new and existing City facilities so that reduced environmental impacts are considered through all phases of life by incorporating: energy efficiency, water conservation, waste minimization, energy generation that reduces greenhouse gas emissions, pollution prevention, and investments in indoor environmental quality.*
- 3.2.6 *To meet or exceed environmental requirements and other performance targets the City has committed to meet.*
- 3.2.7 *To create meaningful stakeholder partnerships and civic collaboration through consultation and integration with programs and initiatives.*

Reporting and Responsibilities

Responsibilities were updated to align with the City's current structure. A section on reporting was added to strengthen accountability for environmental objectives by requiring annual reporting on performance, and to formalize the requirement for reporting on all topics to consider environmental implications.

Comparison with other Canadian Cities

Saskatoon's Environmental Policy was compared against 26 other Canadian Cities' with environmental plans and policies to identify common themes and language used in these key documents, the results are shown in the table below. A '1' indicates the theme or language was used in the reviewed document and '0' implies the opposite.

Municipality	Document for Analysis	Waste Reduction/Prevention	Environmental Stewardship	Overall Sustainability Goal	Continuous Policy or Environmental Performance Improvement	Shared Responsibility in Environmental Improvement and Protection	Lower and/or Prevent Pollution	Protect and/or Enhance Natural Areas and Resources	Bio-diversity
Vancouver	Planning Document	1	0	1	1	1	1	1	1
Brampton	Planning Document	1	1	1	1	1	1	1	0
Hamilton	Environmental Policy	0	0	1	1	1	1	1	0
Surrey	Planning Document	1	1	1	1	1	1	1	1
Burnaby	Environmental Policy	0	1	1	1	0	0	1	0
Saskatoon	Environmental Policy	1	1	1	1	0	1	0	0
Windsor	Planning Document	0	0	1	1	1	1	1	0
Winnipeg	Environmental Policy	0	0	1	0	1	0	0	0
Edmonton	Environmental Policy	0	1	1	1	1	0	1	1
Calgary	Environmental Policy	1	0	1	1	1	1	1	0
Toronto	Planning Document	1	0	1	0	1	1	1	1
Ottawa	Planning Document	1	0	1	0	1	1	1	0
Mississauga	Planning Document	1	0	1	0	1	1	1	0
Laval	Planning Document	1	0	1	0	1	0	1	1
Halifax	Planning Document	1	0	1	1	1	1	1	0
London	Planning Document	1	1	1	0	0	1	1	1
Markham	Environmental Policy	0	1	1	0	1	1	1	1
Longueuil	Planning Document	1	0	1	0	1	1	1	0
Kitchener	Planning Document	1	1	1	1	1	1	1	0
Richmond	Planning Document	1	1	1	0	0	1	1	1
Richmond Hill	Environmental Policy	0	0	1	1	1	1	1	0
Greater Sudbury	Planning Document	1	1	1	0	1	1	1	1
Oakville	Environmental Policy	1	1	1	0	1	1	1	0
Burlington	Planning Document	1	0	1	0	1	1	1	0
Gatineau	Environmental Policy	1	0	1	0	1	1	1	1
Vaughan	Planning Document	1	0	1	0	1	1	1	0
TOTAL		19	11	26	12	22	22	24	10

Results suggest the City of Saskatoon's overarching goal of becoming environmentally sustainable is similar to other Canadian cities. However, deficiencies exist in the following areas:

- shared responsibility,
- protection and/or enhancement of natural areas and resources, and
- biodiversity.

Edits to the current policy have been suggested based on the results above. Although biodiversity is included in less than half of other municipal policies/plans, the updated version of the environmental management standard (ISO 14001) suggests an increased focus on biodiversity justifying its inclusion in the Saskatoon Environmental Policy.

No formal environmental policy or planning documents could be found for Regina, Montreal, Quebec City, or Sherbrooke. As such these municipalities were not used in the comparison.

CITY OF SASKATOON COUNCIL POLICY

NUMBER
C02-036

POLICY TITLE <i>Environmental Policy (REVISED)</i>	ADOPTED BY: <i>City Council</i>	EFFECTIVE DATE <i>Month, Day, Year</i>
ORIGIN/AUTHORITY <i>Clause 8, Report No. 17-2006 of the Administration and Finance Committee</i>	CITY FILE NO. <i>CK. 375-1</i>	PAGE NUMBER <i>1 of 4</i>

1. PURPOSE

This document serves as a statement of intentions and objectives in relation to the desired overall performance of the City of Saskatoon towards community sustainability.

2. DEFINITIONS

2.1 Biodiversity

The variety of life in the Saskatoon region, particularly species, habitats and ecosystems typical to the Canadian prairie.

2.2 Environment

The surroundings in which a community is located and an organization operates, including air, water, land, natural resources, flora, fauna, humans, and their interrelations. The environment in this context extends from within the community of Saskatoon to the prairie region and the broader global system.

2.3 Environmental Management

Those elements of the overall planning and management function of the City of Saskatoon that develop, implement, and maintain its environmental policy and objectives.

2.4 Environmental Requirements

2.4.1 Regulatory Requirements - The collection of laws such as: Acts, Regulations, Guidelines, Standards, Permits to Operate, and Bylaws, as enacted by any federal, provincial, municipal, territorial or Aboriginal governments that specifically apply to the plans,

CITY OF SASKATOON COUNCIL POLICY

NUMBER
C02-036

POLICY TITLE	EFFECTIVE DATE:	PAGE NUMBER
<i>Environmental Policy (REVISED)</i>	<i>Month, Day, 2015</i>	<i>2 of 4</i>

constructed works, operations, maintenance, and services of the City of Saskatoon that pertain to the environment;

2.4.2 Non-regulatory Requirements - The collection of non-regulatory items such as: industry codes of practice, agreements with public authorities, internal Council or Administrative policies or requirements or any other non-regulatory guideline pertaining to the environment to which the organization subscribes to.

2.5 Environmental Sustainability

Maintaining qualities that are valued in the natural environment by living within the Earth's limits through:

- Energy efficiency and reliance on renewable energy sources;
- Preventing waste;
- Transportation and land-use patterns that protect the environment;
- Maintaining the capacity of the environment to sustain living conditions for people and other species.

2.6 Sustainability

Employee recognition of the connections between actions and the resulting impacts, considering a four pillars framework. This framework focuses on ecologic (the natural environment), social (society and justice), cultural (values and perspectives), and economic factors, today and into the long-term future.

2.7 Sustainable Community

Thriving in harmony with nature to ensure that citizen quality of life today does not negatively affect the ability of future generations to live by the same or improved standards.

3. POLICY

3.1 General

The City of Saskatoon is committed to becoming an environmentally sustainable community. As a result, the organization encourages and

CITY OF SASKATOON COUNCIL POLICY

NUMBER
C02-036

POLICY TITLE	EFFECTIVE DATE:	PAGE NUMBER
<i>Environmental Policy (REVISED)</i>	<i>Month, Day, 2015</i>	<i>3 of 4</i>

expects environmentally responsible behaviour from all employees and contractors working at civic facilities and grounds. Achieving the ultimate goal involves shared responsibility between the community at large and our organization. As such the City of Saskatoon has a responsibility to facilitate and provide programs and services that move toward sustainability.

3.2. Objectives

The following environmental performance objectives guide the development and implementation of programs, initiatives, and services.

- 3.2.1 To promote and engage in the protection, enhancement, and responsible use of natural areas and resources; through attention to: biodiversity, protection of soils as a valued resource, climate change, water and air quality, land use, transportation patterns, and civic development.
- 3.2.2 To ensure asset and financial sustainability through life cycle accountability, risk management, and responsible environmental management.
- 3.2.3 To continually improve overall environmental performance through ongoing research and appropriate revision to objectives and targets, as well as transparent plain language reporting.
- 3.2.4 To seek the commitment of all internal staff, management, council members, businesses, and citizens to environmental stewardship and safety by building relationships and understanding through communication, education, training, and support.
- 3.2.5 To design, construct, retrofit, and operate new and existing City facilities so that reduced environmental impacts are considered through all phases of life by incorporating: energy efficiency, water conservation, waste minimization, energy generation that reduces greenhouse gas emissions, pollution prevention, and investments in indoor environmental quality.

CITY OF SASKATOON COUNCIL POLICY

NUMBER
C02-036

POLICY TITLE	EFFECTIVE DATE:	PAGE NUMBER
<i>Environmental Policy (REVISED)</i>	<i>Month, Day, 2015</i>	<i>4 of 4</i>

3.2.6 To comply with environmental requirements and meet or exceed other performance targets the City has committed to meet.

3.2.7 To create meaningful stakeholder partnerships and civic collaboration through consultation and integration with programs and initiatives.

4. RESPONSIBILITIES

4.1 General Manager, Corporate Performance - is responsible for administering this policy and recommending updates to this policy.

4.2 City Council - is responsible for reviewing this policy and any recommended changes on an annual basis, as well as final approval.

4.3 Reporting

4.3.1 Environmental and Corporate Initiatives – is responsible for annual reporting on environmental performance objectives. Reports are to be made available internally for City Council and Administration as well as externally to the general public.

4.3.2 All City of Saskatoon Employees – are responsible for including Environmental Implications in all reports being put forward to Committees and City Council.



STANDING POLICY COMMITTEE ON ENVIRONMENT, UTILITIES & CORPORATE SERVICES

Integrated Waste Management Annual Report 2014

Recommendation of the Committee

1. That the report of the General Manager, Corporate Performance Department dated October 13, 2015, be received as information; and
2. That a copy of this report be forwarded to the Saskatoon Environmental Advisory Committee for information.

History

At the October 13, 2015 Standing Policy Committee on Environment, Utilities & Corporate Services meeting, a report of the General Manager, Corporate Performance Department dated October 13, 2015 was considered.

Attachment

October 13, 2015 Report of the General Manager, Corporate Performance.

Integrated Waste Management Annual Report 2014

Recommendation

That the report of the General Manager, Corporate Performance Department dated October 13, 2015, be forwarded to City Council for information.

Topic and Purpose

The performance of civic waste handling and reduction programs are reported in the Integrated Waste Management Annual Report for 2014.

Report Highlights

1. Saskatonians continue to dispose less waste than the national average, but at a rate higher than most Canadian cities at 249 kilograms per person.
2. A Waste Diversion Rate of 70% by 2023 has been established as a Performance Target. The 2014 rate is 22.5%, below the national average (2012) of 33.7%.
3. The compost program contributes 13% toward the waste diversion rate of 22.5%. The next biggest contributor to waste diversion is the curbside recycling program at 7% followed by recycling depots at 2%.
4. A new education blitz at the landfill reminded 600 customers about covering or tarping loads; residents were provided information about alternative disposal options for compostable materials, paint, tires and household hazardous waste.
5. More than 86,000 vehicles delivered loads to the landfill. Total material deposited in 2014 was 125,238 tonnes.
6. 2.8 million garbage carts are emptied each year with a reliability rate of 99.9%. In 2014, City staff responded to 2,400 calls about missed garbage collections.
7. 1.1 million blue recycling carts were tipped in 2014 with 1,110 occurrences of incorrectly placed or overfilled carts (for a non-compliance rate of 0.1%).

Strategic Goal

The information in this report supports the four-year priorities to promote and facilitate city-wide composting and recycling and implement energy-efficient practices in City operations, along with the long-term strategy to eliminate the need for a new landfill under the Strategic Goal of Environmental Leadership.

Background

City Council received an Integrated Waste Management Annual Report for 2013, prepared by Environmental and Corporate Initiatives Division in 2014.

Report

Attachment 1 is the *Integrated Waste Management Annual Report* for 2014. The report provides a description of the waste handling, waste reduction, and waste diversion programs and services provided by the City of Saskatoon (City). These include curbside garbage and recycling collections for all single-family households; a regional

landfill; optional leaves and grass curbside collections; 2 compost depots; 4 recycling depots; household hazardous waste drop-off events; garbage collection for many multi-unit and commercial customers (other buildings and businesses contract to private haulers), and multi-unit recycling collection. Other waste diversion programs include home composting education, curbside swap, and integrated waste education.

The report highlights changes measurable outcomes achieved, and describes the responsibilities of Environmental & Corporate Initiatives and Public Works. Highlights from the report are outlined above. Now that the City has adopted a Waste Diversion target of 70% by 2023, this report and future Integrated Waste Management Annual Reports will serve as a progress report toward this ambitious target.

Communication Plan

The 2014 *Integrated Waste Management Annual Report* will be available for viewing on the City's website. A Public Service Announcement and social media posts will be distributed.

Environmental Implications

Greenhouse gas (GHG) emissions implications and other environmental protection measures are included in the annual report.

Other Considerations/Implications

There are no public and/or stakeholder involvement, policy, financial, privacy or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

An Integrated Waste Management Annual Report will continue to be produced each year. The next report will be prepared for the 2015 year, submitted to the Standing Policy Committee on Environment, Utilities and Corporate Services in April 2016.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Attachment

1. Integrated Waste Management Annual Report 2014

Report Approval

Written by: Amber Jones, Education and Environmental Performance Manager
Reviewed by: Brenda Wallace, Director of Environmental and Corporate Initiatives
Michelle Jelinski, Environmental Operations Manager
Pat Hyde, Director of Public Works
Approved by: Catherine Gryba, General Manager, Corporate Performance Department

2014 Integrated Waste Management Annual Report

Introduction – What is Integrated Waste Management

Integrated Waste Management is a systems approach to waste handling that focuses on reducing waste where possible, re-using whatever can be given a new purpose, recycling and recovering resources to minimize demand for raw materials, and conscientiously managing what remains to ensure the safety of people and the environment.

The City's core services include:

- curbside garbage and recycling collections for all single-family households;
- a regional landfill;
- optional leaves and grass curbside collections;
- 2 compost depots;
- 4 recycling depots;
- household hazardous waste drop-off events;
- garbage collection for many multi-unit and commercial customers (other buildings and businesses contract to private haulers); and
- multi-unit recycling collection

Other waste diversion programs include home composting education, curbside swap, and integrated waste education.

Reducing Waste – Targets

The City of Saskatoon has adopted a 10-year target to divert 70% of waste from the Saskatoon landfill with a longer term vision of achieving Zero Waste. This ambitious target shows a commitment by the City of Saskatoon (City) to establish new options for waste reduction in recycling, reusing, and composting as well as continuously improving our existing programs. Joining the National Zero Waste Council (NZWC) in 2014 sets a clear visionary direction for reducing waste to the highest degree possible. This global movement towards eliminating waste includes not only recycling, reusing, and composting of produced waste but also requires a philosophical shift towards reducing the waste in the first place through design and planning decisions.

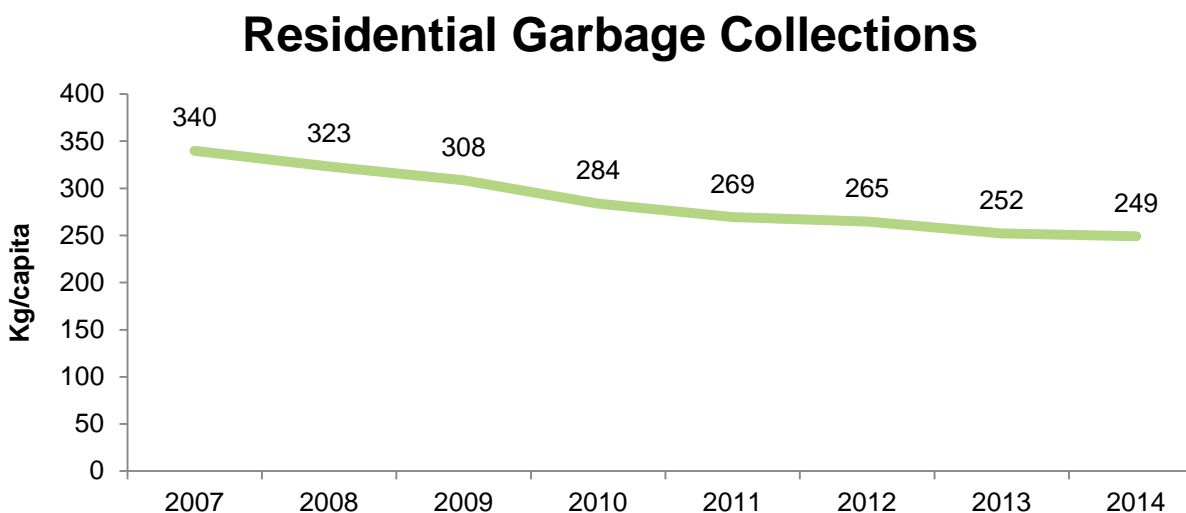
The City's Waste Diversion Rate, at 22.5%, has been relatively stable over the last two years. For this reason the progress toward the target of 70% has been identified as needing improvement.

Saskatoon's Waste Disposal

The amount of garbage collected is reducing every year. In 2014:

- 125,238 tonnes of garbage was disposed of at the City landfill from all sources
- Residential collections by the City accounted for 64,091 tonnes (53,000 tonnes from black carts, the rest from multi-units)
- 249 kg of garbage was disposed per person.

As shown in the figure below, the rate of garbage disposal per person has been declining over time.



Saskatoon's disposal was compared to the national and provincial averages as shown in the table below and compares favorably to both the national and provincial averages.

Region	Year	Total Waste Disposed (tonnes)	Total Waste Rate (kg/capita)	Residential Component (tonnes)	Residential Rate (kg/capita)
Saskatoon	2012	117,660	486	64,363	265
Saskatoon	2014	125,238	484	64,091	249
Saskatchewan	2012	957,669	881	315,987	290
Canada	2012	25,013,204	720	9,586,511	276

Source: Stats Canada

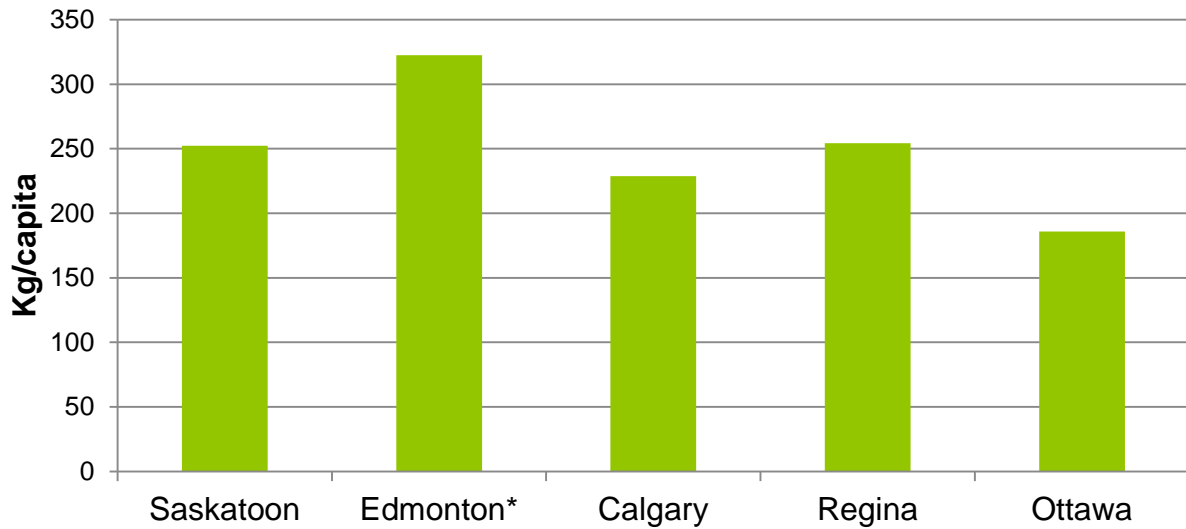
Saskatoon's Residential Waste Disposal Rate was also compared to other cities in Canada using data from the National Solid Waste Benchmarking Initiative¹ (NSWBI).

As shown in the table below, residents in Saskatoon dispose of a similar amount of waste to residents in Regina, but more waste than three other comparable cities in

¹ National Solid Waste Benchmarking Initiative collects data on waste management in select cities, in 2013 twelve (12) cities participated

2013. NSWBI also reported that on average 0.60 tonnes of waste is collected per single family household, while Saskatoon's average is approximately 0.79 tonnes per household.

2013 Residential Garbage Collection



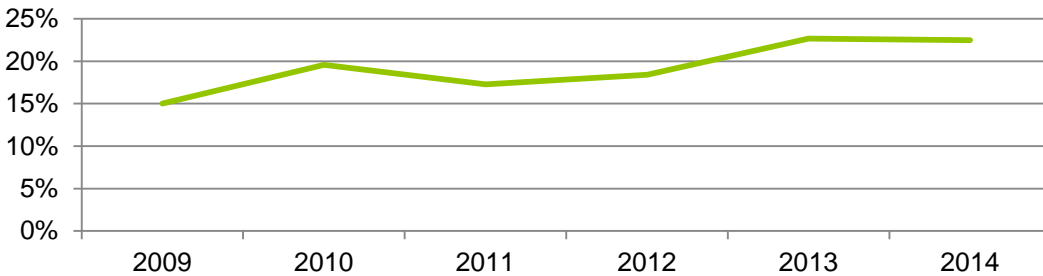
Source: National Solid Waste Benchmarking Initiative 2014

* Edmonton collects organics and garbage in a single-stream. An estimated 37% of the organics is diverted from the collected waste.

Saskatoon's Waste Diversion

Saskatoon's Waste Diversion rate remained stable since last year and is below the national average. On average in 2012 (Stats Canada), Canadians diverted 243 tonnes of waste per capita resulting in an overall diversion rate of 33.7% (almost 8.5 million tonnes) while in Saskatchewan, approximately 143 tonnes per capita was diverted resulting in a diversion rate of 16.3%. Saskatoon's diversion rate in 2012 was well below the national average at 18.4%, it increased to 22.7% in 2013, and remained relatively stable at 22.5% in 2014.

Saskatoon's Waste Diversion Rate



Saskatoon's 2014 diversion rate was also benchmarked against other Canadian Cities, as shown in the table below. Saskatoon's diversion rate is second last amongst compared Cities.

City	2014 Diversion Rate
Halifax	61%
Toronto	53%
Ottawa	52%
Edmonton	51%
Calgary	34%
Saskatoon	23%
Regina	19%

City of Saskatoon Waste Service Delivery

Integrated Waste Management falls within the **Environmental Health** Business Line which is jointly managed by Public Works (Waste Handling Service Line) and Environmental & Corporate Initiatives (Waste Reduction & Resource Recovery Service Line). The goal of the Waste Handling Service Line is to provide efficient, effective, and customer-oriented waste management services. Waste Handling includes:

- solid waste collections;
- the management of City-owned garbage carts; and
- operating the landfill to optimize available space and comply with environmental requirements.

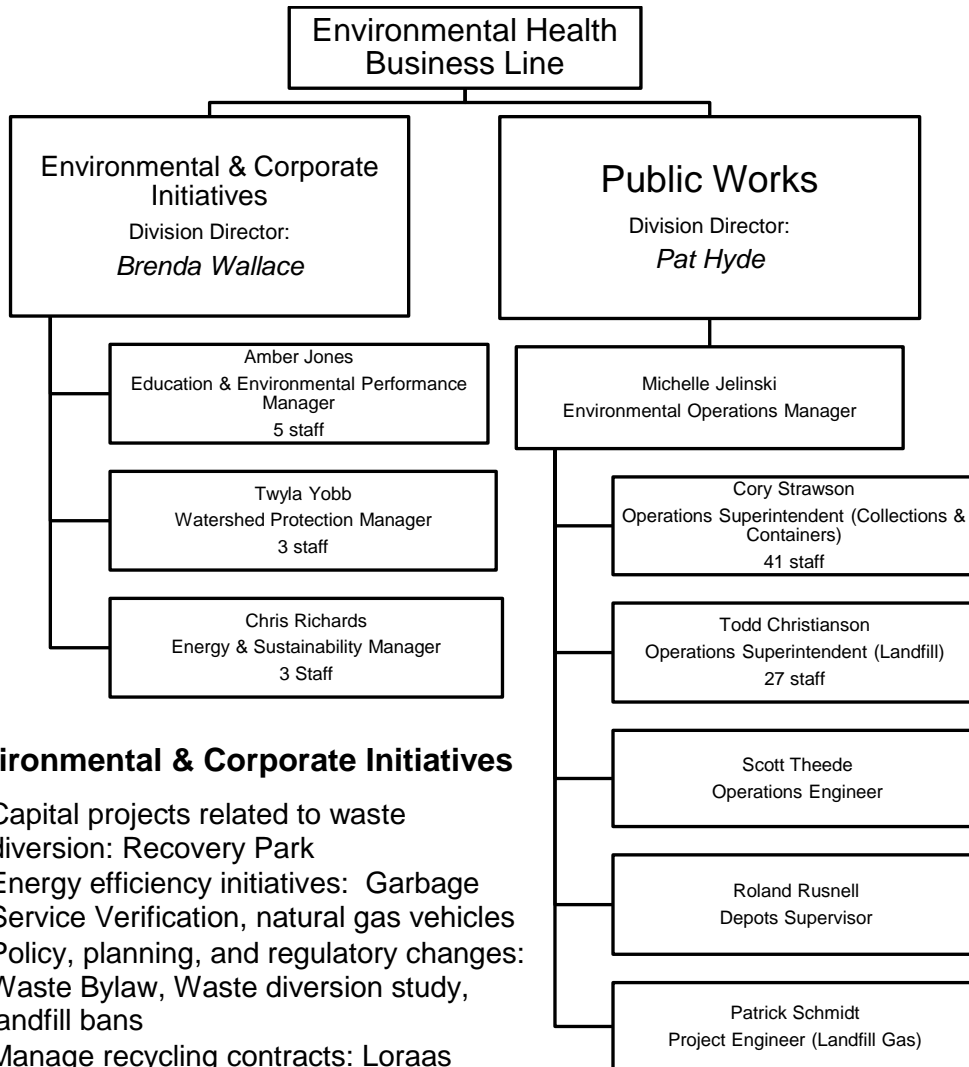


The goal of Waste Reduction is to provide a focus on reducing, reusing, recycling, and recapturing resources while seeking operational efficiencies. Initiatives under this program include:

- recycling contracts for curbside and multi-unit dwellings;
- support for recycling depots located across the community;
- composting of yard waste at drop-off depots; and
- residential subscription yard waste collection initiative.

Role of Public Works and Environmental & Corporate Initiatives

Integrated Waste Management is delivered by Public Works and Environmental & Corporate Initiatives under the **Environmental Health Business Line**.



Environmental & Corporate Initiatives

- Capital projects related to waste diversion: Recovery Park
- Energy efficiency initiatives: Garbage Service Verification, natural gas vehicles
- Policy, planning, and regulatory changes: Waste Bylaw, Waste diversion study, landfill bans
- Manage recycling contracts: Loraas Recycle, Cosmopolitan Industries Ltd.
- Coordinate Household Hazardous Waste Days
- Waste diversion programs and initiatives: Saskatoon Curbside Swap, public space recycling, festival and event sustainability, civic recycling
- Education, outreach and communications pertaining to waste reduction and diversion programs and future diversion initiatives

Public Works

- Garbage containers and garbage collection services
- Manage the Saskatoon Regional Waste Management Facility (Landfill)
- Operate compost and recycling depots
- Leaves & Grass collections including providing green carts
- Deliver recyclable materials from depots to Cosmo
- Communications pertaining specifically to waste management operations

Garbage Handling Service – Keeping Saskatoon Safe and Clean

Containers

Providing waste carts to new homes and repairing carts when they are damaged.

Containers Provided to Residential (Curbside) Properties

The City of Saskatoon owns and maintains the black roll-out carts provided to street-oriented residential properties. In 2014:

- 1,300 new carts were delivered to new homes
- 6,100 carts were repaired or replaced (430 of these were stolen and could not be recovered)
- Current cart failure rate has been reduced to 0.55% due to improvements in purchasing specifications made in late 2012. In neighbourhoods with carts purchased under old specifications, failure rates can be as high as 15% (i.e. Caswell Hill, College Park, Confederation Park)

Carts are replaced because they reached the end of their useful life or because they were not performing in the field.

Multi-Unit Residential Containers

The City of Saskatoon does not provide garbage containers for multi-unit dwellings and instead offers a Multi-Unit Dwelling Waste Bin Grant to offset the cost borne by condominium associations and property managers for the purchase and maintenance of metal waste bins.

The grant provides \$8 per year per residential unit. \$265,764 was paid to 740 buildings as part of this program in 2014.

Resources

The containers crew is comprised of 3 permanent staff plus 1 additional seasonal staff in the summer. These employees deliver new garbage containers to new homes, as well as repair or replace damaged containers in the field. They also deliver and retrieve the larger 300 gallon garbage containers for Taste of Saskatchewan, the Fringe Festival, Rib Fest, and other special events.

The containers crew, rear loader crew and a customer service representative are overseen by the Utility Customer Service Supervisor (Debbie Donahue).

Garbage Collections

Collecting garbage from black carts and steel bins across the city.



Garbage Collection from Residential (Curbside) Properties

Garbage collection services are provided to 67,000 single-family homes by a fleet of 21 garbage trucks.

In Saskatoon, the average weight of the contents of each cart on collection day ('tip') is 18 kilograms (40 lbs). The national average was 14 kilograms (31 lbs) according to national benchmarking.

Scheduled garbage collections occur from Monday to Friday, including Stat holidays, except for Christmas Day and New Year's Day. In 2014, weekly collections were conducted from April to October inclusive, with the remainder of the year on a bi-weekly collection schedule. Additional garbage collections were also provided during the Christmas holiday season.

Garbage Collection from Multi-Unit Residential Properties

The City of Saskatoon provides one collection per week for each multi-unit residential property as part of the regular service supported by property taxes. Additional levels of

service may be contracted with the City or through a private waste management company.

The City operates 2 fork-lift trucks in the provision of metal bin garbage collection services to multi-unit residential properties and commercial contracts. Approximately 64% of multi-unit residential properties use the services provided by the City of Saskatoon. The remaining 36% of property managers and condominium associations choose to contract services through private options.

Resources

The Collections group in Public Works is comprised of 17 permanent Utility Collections Operators plus an additional 6 seasonal Utility Collections Operators during weekly collections in the summer.

Operators drive specialty equipment called sideloaders for residential collections and commercial collections of 300 gallon poly containers. Fork truck operators drive large waste trucks outfitted with fork-lift arms to collect waste from metal bins located at multi-unit dwellings and from the commercial sector. They also operate the fork trucks to collect recycling materials from the City-owned recycling depots and deliver this material to Cosmopolitan Industries.



A rear loader crew, consisting of two operators and two labourers, conduct special garbage collection services for elderly or disabled residents. These 4 employees also service the recycling depots by collecting illegally dumped and overflowing materials from in and around the bins. The rear loader crews provide some back lane collection of

illegally dumped materials in the public right of way if the identity of the individual or property from which the material originated cannot be identified.

The Collections crew is overseen by a Supervisor VI (Jose Juarez) and a Supervisor II (Debbie Kautzman).

Commercial Collections

618 commercial customers used the services of the City of Saskatoon in 2014. Rates were last set in 2011 and have remained unchanged. The number of commercial customers has remained relatively stable.

Efficient Waste System

Improving the waste collection system to save fuel, operator time, and improve overall service for residents.

Installation of the Efficient Waste System began in 2014 with the application of barcode tags to carts throughout the city. This initiative was part of a larger project that utilized an internally-provided \$1.2 million productivity improvement loan.

The project components include outfitting trucks with GPS, wireless communications, on-board computers, cameras, and barcode readers and a new software tool to achieve the following benefits:



- Efficient routing for Garbage and Green Cart collections services
- Real-time information about the status of collections activities for improved customer service
- More accurate billing for contracted collections services
- Improved drive logs that include photos of collection issues encountered in the field
- Improved tracking of garbage, recycling, and green carts
- An effective and reliable database that merges cart, collection and customer service requests
- Support tools for Environmental Protection Officers
- Enhanced analysis and reporting capabilities to ensure efficient and effective management of waste services

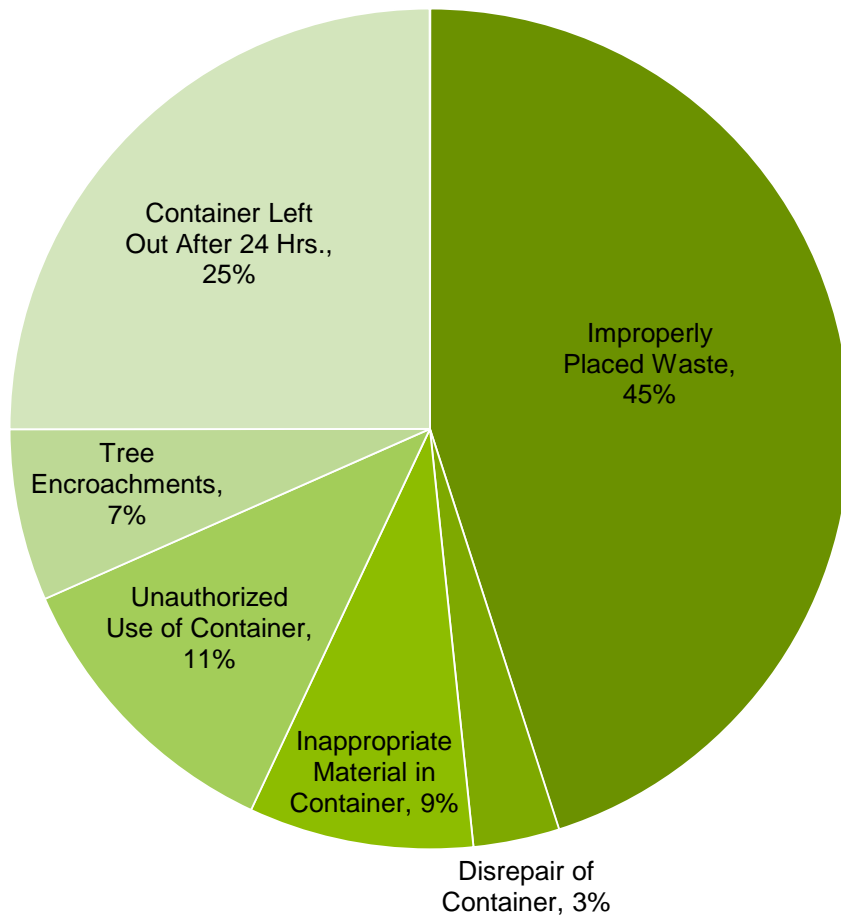
The project also includes improvements to the '306-975-2486' customer service line people call for waste-related matters. System commissioning will be complete in 2015 and new, efficient collection routes will be launched in January 2016.

Waste Bylaw Enforcement

Providing education and enforcement to ensure garbage and recycling are managed by the community in a way that is safe for people and the environment.

Two Environmental Protection Officers (EPO's) focus on issues in the community related to the Waste Bylaw 2004 (Bylaw No. 9071). In addition to enforcing the Waste Bylaw, EPOs are primarily responsible for responding to hydraulic spills, illegal dumping and waste & recycling cart complaints as well as conducting bylaw related education initiatives for the general public.

In 2014, the EPO's addressed more than 486 individual complaints. A breakdown of the types of complaints received is identified below.



New in 2014, the EPOs and landfill staff conducted load inspection blitzes at the landfill during three different summer weekends. These blitzes were intended to educate residential customers on the Waste Bylaw requirements for covering/tarping loads as well as to clarify what materials are accepted or not accepted at the landfill. Nearly 600 individual loads were inspected and customers were provided information on:

- appropriate disposal options for compostable materials;
- paint;
- tires; and
- household hazardous waste.

No tickets were issued during these blitzes as they were conducted as an educational public outreach initiative.

Also new in 2014, was the implementation of neighbourhood blitzes for cart placement requirements under the Waste Bylaw. This education campaign was developed in response to numerous complaints about:

- unsightliness;
- congestion;
- scavenging; and
- theft and damage to waste and recycling carts left in back lanes after collection day.

Back lane inspections were carried out by the EPOs in the Buena Vista, North Park and Caswell Hill neighbourhoods since these neighbourhoods generated the most complaints and operational challenges for back lane garbage and recycling collection.

The EPOs delivered education letters to residents whose black or blue carts were left out for more than 24 hours (contravention of the Waste Bylaw) followed by a warning letter and ultimately a notice of violation (\$100 ticket) if subsequent inspections showed that compliance was not achieved. Initial inspections showed that up to 35% of homes in certain areas were in non-compliance resulting in more than 1,400 education and warning letters being issued to residents. Following the distribution of education and warning letters to these homes, only 7% of homes were still found to be in non-compliance and a total of 68 tickets were issued in 2014. Cart placement education blitzes have continued in different neighbourhoods in 2015.

Saskatoon Regional Waste Management Centre (Landfill)

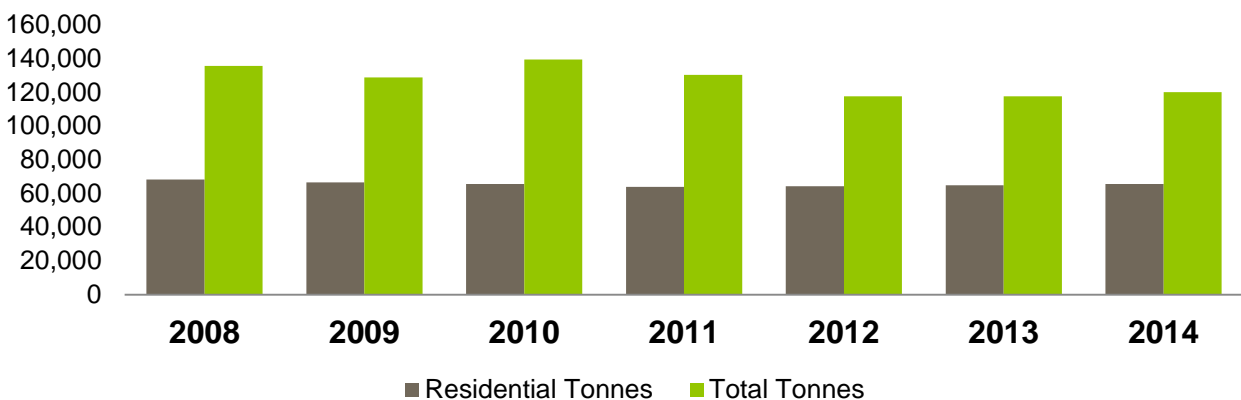
A sophisticated facility that is designed and operated to maximize available space and ensure solid waste is managed in a safe and environmentally sound manner.



The Saskatoon Regional Waste Management Centre (landfill) has been in operation since 1955. The Saskatchewan Ministry of Environment currently regulates the operations of the Facility under a Permit to Operate a Waste Disposal Ground.

In 2014, approximately 168,300 tonnes of material was accepted at the landfill, 125,238 tonnes of garbage requiring burial and 43,400 tonnes of clean earth fill used for construction purposes or landfill cover. Over 800 tonnes of metals, white goods, propane tanks, batteries and 49,000 litres of used oil were removed from the site for recycling or disposal as hazardous waste.

Tonnes of Garbage Received at Landfill



The landfill is open to the public every day of the year except for Christmas Day and New Year's Day. The site is a secure compound with fencing and security checks after-hours. Six transfer bins are located on site for public waste disposal. In addition, several public drop-off areas are provided for metals, white goods, batteries, propane tanks, used oil, oil containers, filters and used antifreeze.

Resources

Costs incurred at the Landfill for each tonne of material disposed has increased significantly over the past 10 years, due in large part to significant investment in capital improvements during this period, and increased operating costs to meet regulatory requirements and service levels. Also of significance is the fact that annual tonnages are trending downward, resulting in fixed costs being spread over fewer tonnes. A Continuous Improvement initiative is now underway and cost mitigation strategies are being implemented, including reducing overtime, reducing fleet downtime impacts, and general operating improvements.

The landfill experiences a lot of equipment downtime due to the age of equipment (older than industry averages) and the number of pieces of equipment (fewer than industry averages). A significant proportion of the cost escalation is also attributable to the capital improvement program required to ensure the landfill achieves its full lifespan and complies with current environmental regulations. 41% of 2014 expenditures at the landfill were put toward capital projects.

Landfill attendants carry out various duties including:

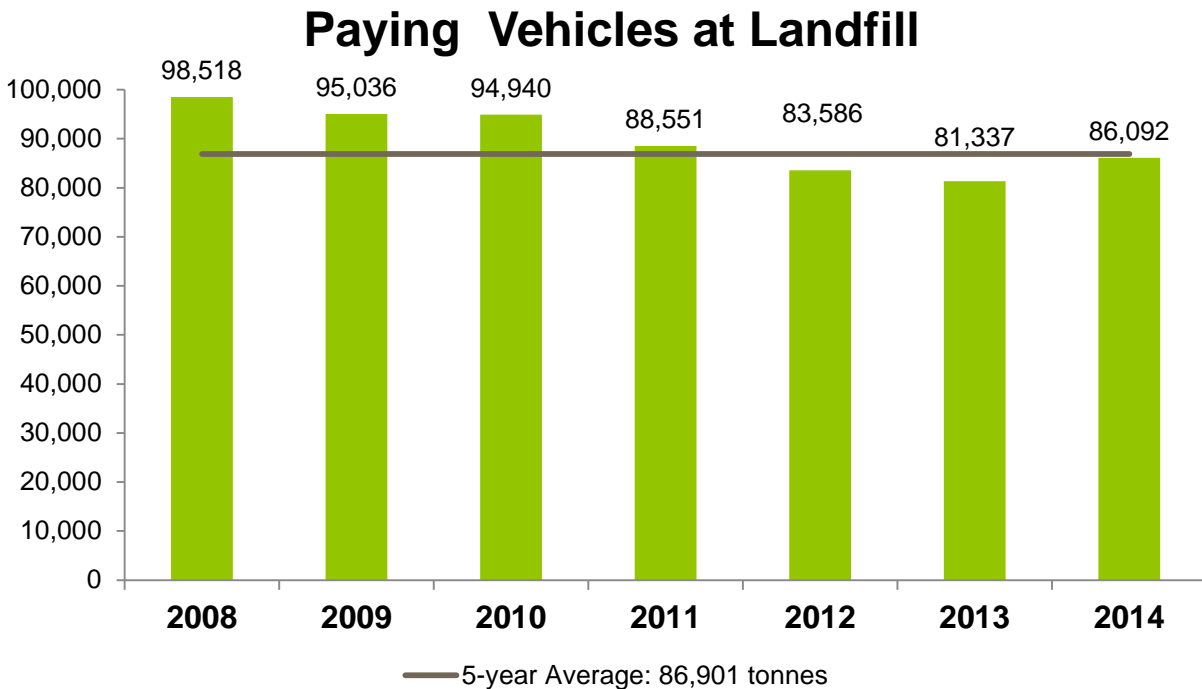
- operation of the landfill scales
- traffic control and load inspection duties at the blue transfer bins and at the active face
- operation of roll-off trucks to haul waste from the public transfer station to the active face
- operation of water trucks for dust control and fire suppression
- operation of equipment for snow removal, sanding
- grading and road maintenance or repairs
- safe operation of the used oil facility and other recycling areas on site

Labourers are primarily responsible for litter management and other site maintenance requirements. Equipment operators ensure that all waste delivered to the active face is compacted and buried in a safe and environmentally acceptable manner.

The landfill employs 5 heavy equipment operators, 12 permanent landfill attendants, 4 seasonal landfill attendants, and labourers. Landfill operations are overseen by two Supervisor VI's and two Supervisor II's who report to the Landfill Superintendent, Todd Christianson.

Landfill Customers

Paying customers at the landfill increased slightly in 2014 over 2013 with a total of 86,092 visitors. While more customers used the landfill in 2014 than 2013, the number of commercial haulers has decreased while residential and other subsidized haulers have increased. Some of the decline in commercial visitors may be a result of competition from the Northern Landfill operated by Loraas Disposal and more recently, competition from Green Prairie Environmental, the new owner and operator of the former South Corman Park Landfill.



Saskatoon has one of the highest rates of residential garbage self-hauled to the Landfill in Canada. The average size of chargeable loads delivered to the Facility has been shrinking and was approximately 0.6 tonnes in 2014. A review of rates and subsidies in 2012 led to changes to the schedule of fees. As of January 1, 2013, all subsidies previously available to customers at the Landfill were removed with the exception of a 75% subsidy which is applied to very small loads weighing less than 250 kilograms. The rationale for this subsidy is to offer options to residents in the absence of a curbside service for the handling of bulky items that may not fit in City-provided roll-out garbage carts. This subsidy, however, is contributing to the shrinking sizes of average loads at the landfill. This, in turn, increases demands for traffic management and can increase wait-times for customers.

According to nation-wide benchmarking for 2014, the range of fees for tipping waste among 31 municipal landfills across Canada is \$36.15 to \$140 per tonne. Saskatoon's fees, at \$95 in 2014, fall in the middle of this range.

Environmental Protection

A number of environmental protection measures are part of the regular operations of the landfill.

Surface water management ensures that any storm water that may have come in contact with garbage does not flow off-site. Storm water management infrastructure also helps minimize the creation of leachate ('garbage juice') and protects roadways and other customer-serving assets of the site. Improvements to storm water infrastructure are integrated into the capital improvement plan for the site.

Ground water monitoring results are reviewed by an independent party each year to ensure the landfill is not impacting the environment beyond its site. In 2014, there were 43 monitoring locations analyzed. Additional leachate collection system development is planned on the basis of monitoring results. 27 hectares of the waste footprint at the Landfill Facility is unlined and leachate interception is therefore an important environmental protection measure.

Fire prevention and suppression is critical. Materials within the mound have the potential to burn for prolonged periods and to release toxins into the atmosphere. Landfill staff are trained in fire prevention and suppression and operate a water truck on-site to respond to incidents in a timely manner. A strong relationship with Saskatoon Fire and Protective Services has also been developed as landfill fires pose a unique challenge.

Landfill gas management began in 2012 with the completion of the clay cap on the north mound of the site. Since then, 29 vertical gas collection wells have been drilled into the waste, the Landfill Gas Collection and Power Generation Facilities were completed and the system was fully commissioned in late 2013. In 2014, the landfill gas system collected and combusted over 190,000,000 cubic feet of landfill gas, reducing the greenhouse gas emissions from the landfill by more than 49,000 tonnes of CO₂e or the equivalent of removing 10,200 vehicles off Saskatoon roads.

Greenhouse gas emissions reduction efforts occur at two levels at the landfill. The destruction of landfill gases (which have greenhouse gas concentrations up to 21 times stronger than carbon dioxide) is addressed through landfill gas management. As equipment is replaced, Tier IV emissions-compliant equipment is being selected to significantly reduce this impact.

Customer Service for Garbage Handling Services

Over 10,000 phone calls and voicemail messages are received on an annual basis regarding containers, missed collections, and other inquiries. In addition, the garbage@saskatoon.ca webmail address received more than 2,600 emails which were all responded to in a timely manner.

In 2014, collections staff responded to over 2,400 calls about missed garbage collections. With over 2.8 million cart collections conducted on an annual basis, this equates to a 99.9% garbage collection reliability. Approximately 75% of the recorded missed collection calls originated from collections operators themselves when they were unable to collect carts for various reasons such as carts placed too close to fences/power poles/parked vehicles, carts on wrong side of the lane, overfilled carts, inappropriate materials identified in carts (i.e. propane tanks), or other issues.

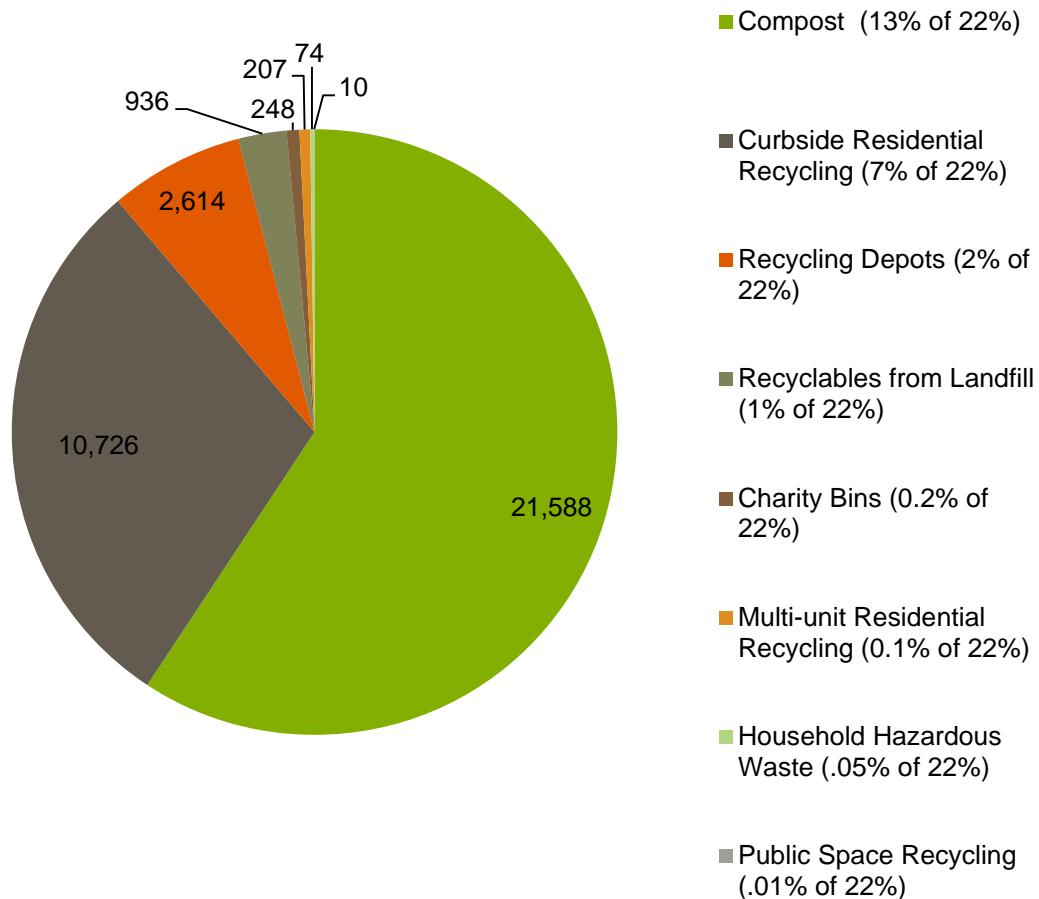
One customer service representative responds to phone calls received at the 306-975-2486 Waste Stream Management Customer Service Line.

Towards a Waste Diversion Target of 70%

Waste Diversion

Waste is diverted from going to the landfill through a variety of programs and initiatives including recycling, composting, and hazardous waste collection. Additional outreach and education programs include the Saskatoon Curbside Swap, charity bin program, and home composting education that contribute to waste reduction and raise awareness. The figure below shows the tonnes of recyclables diverted from each program (total of 36,338 tonnes), and how each contributed to the diversion rate of 22.5% in 2014.

Tonnes of Diverted Materials



Recycling

Providing convenient programs for all residents to recycle their paper and packaging.

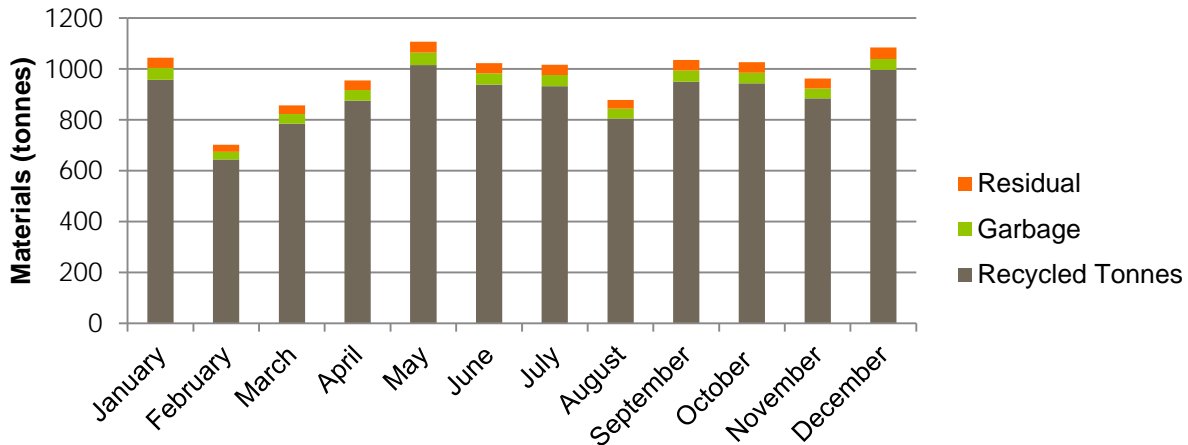
Collections Provided to Residential (Curbside) Properties

Loraas Recycle has been providing curbside recycling service to single family households in Saskatoon since 2013. Each of the 66,793 households participating in the City's curbside program is provided with a blue cart that is collected from their curb every second week based on a published schedule.

- 10,724 tonnes of recyclable materials were recycled, contributing 7% toward the City's waste diversion rate
- 11,104 tonnes of material were collected
- 4% contamination (materials placed in the cart that are not recyclable) and 4% residual (materials that are technically recyclable but could not be recovered usually because of their small size).
- A net reduction in greenhouse gas of 29,027 tonnes can be attributed to the recycling program (includes the 387 tonnes from diesel fuel used in recycling trucks)



Materials collected through the Curbside Residential Recycling Program



The set out rate (or proportion of carts placed at the curb on collection day) averaged 67% for the program in 2014. This does not indicate how many people use their blue carts since many do not necessarily put them out each collection. The participation rate (or proportion of residents using their recycling carts) will be calculated annually starting in 2016 as part of a waste characterization study.

Carts not being collected due to incorrectly placed or overfilled carts continue to occur; there were 1,110 occurrences in 2014. To put this in context, there were over 1.1 million blue carts tipped in 2014, a non-compliance rate of 0.1%. Other ongoing concerns include carts left out in back lanes for more than 24 hours and scavenging of high value materials. These issues are addressed through communications and through warnings and tickets issued by the Environmental Protection Officers (EPO's).

Multi-Unit Residential Properties

The Multi-Unit Residential Recycling (MURR) program was launched on October 6, 2014; approximately 207 tonnes of material was collected through the program in 2014, a greenhouse gas reduction of 448 tonnes over landfilling the same amount of material.

Program deployment went well and residents seem generally pleased to have the service in place. A challenge with the program is the high volume of garbage being found in the recycling containers. Cosmo's initial characterization established that approximately 18% of the materials being collected are non-recyclable. A multi-unit recycling program could be expected to have higher rates of garbage than curbside programs due to the use of large bins placed in publicly-accessible areas.

Customer Service for Recycling Programs

Resident inquiries on curbside and multi-unit recycling programs are responded to by the service providers, Loraas and Cosmo. Inquiries regarding the curbside residential recycling program continue to be low at an average of 0.6% or 368 inquiries per month.

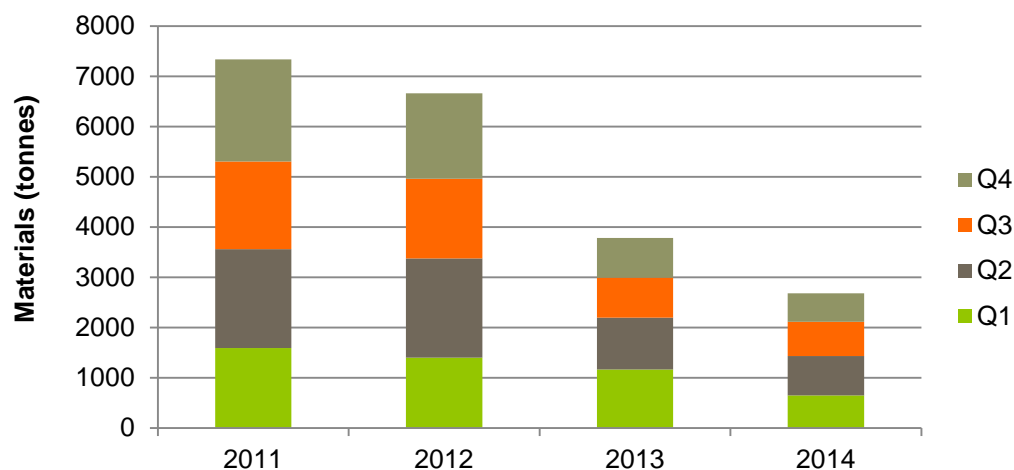
Inquiries were fairly high for the MURR program in the first month at 276, mostly regarding container information. These inquiries decreased to 138 in December.

Recycling Depots

In 2014, 2,679 tonnes of material were collected at the depots contributing 2% to the City’s diversion rate, a monthly average of 223 tonnes. Changes were made to the depots program including the closure of the Edmonton Avenue depot and depots on private land. These closures were in response to the decrease in material being received at the depots as shown in the figure below.

In 2014, the Edmonton Avenue depot was closed and the City ceased collections from any depots on private land. These closures were in response to the decrease in material being received at the depots as shown in the figure below. The four remaining depots now accept the same materials accepted in the MURR and curbside programs in a single stream with no sorting, as does all recycling at Civic facilities (started at the end of 2014). The bins at the depots remain the property of Cosmopolitan Industries Ltd. and all materials are collected by the City and processed at Cosmo’s Material Recovery Facility. New signage and bin decals have been updated to communicate the change.

Materials Collected at City Depots



Materials from the recycling depots are collected and delivered to Cosmopolitan Industries by Civic staff Monday, Wednesday, Friday, and Saturday; and clean-up crews visit the depots Monday, Wednesday, and Friday.

Education and Engagement

Making sure Saskatoon residents know how to Put Waste in the Right Place.

Information about the City's waste and recycling programs are available online, through Facebook, Twitter, and at festivals and events.

Visitors to the City's website (Saskatoon.ca) and the SaskatoonRecycles.ca microsite access up-to-date information on anything they need to know to manage their waste including:

- collection schedules;
- how to use their blue, black, and green carts;
- landfill rates and hours; and
- other tips on reducing or diverting waste.

There were 164,809 page views on SaskatoonRecycles.ca.



Each household received a personalized print calendar with the garbage, recycling, leaves and grass, and household hazardous waste schedules. Residents can also:

- view their calendar online;
- download a PDF calendar from the website;
- sign up for collection reminders by e-mail, phone, or twitter; or
- add their schedule to their own online calendar.

Personalized online collection calendars were viewed by 24,589 unique households, 5,455 calendar PDF downloads, and 4,418 residents were receiving collection reminders.

The public can also find out about other local recycling opportunities through the Saskatchewan Waste Reduction Council's province-wide online database of information on where to recycle a variety of materials. The City of Saskatoon partners with the Council in promoting this online tool: <http://www.saskwastereduction.ca/>



Citizens were engaged at events using the “Spin and Sort” and “Yep, Nope” sorting game. Summer staff attended 12 events in 2014, where they encouraged children and adults to learn about waste management through interactive activities. Other communications occurred through the “Put Waste in the Right Place” campaign on radio, newspaper, social media, and online advertising.

Saskatoon Curbside Swap

Building community while keeping reusable items from ending up in the landfill.

Curbside Swaps are fun and environmentally friendly ways for residents to get rid of unwanted items that are still in good condition, as well as “hunt” for new treasures. In 2014, four curbside swaps were held in College Park, City Park, Buena Vista, and Meadowgreen. The City supported the community association to organize and promote the swap in their neighbourhood. The swaps were well attended as long as the weather held and feedback through a survey of participants showed that survey respondents wanted an event to occur the next year.

Want free, useable stuff? Come to a Curbside Swap!

What is a Curbside Swap?

When a neighbourhood has a Curbside Swap date, participating households mark items as “FREE” and set them on their curb for others to collect. This keeps reusable items out of our landfill and reduces our community’s environmental footprint!

What will I find at a Curbside Swap?

- appliances
- books
- electronics
- bicycles
- yard equipment
- furniture

Where can I find more info?

Simply visit Saskatoon.ca and click on “C” for Curbside Swap.

SASKATOON
**CURBSIDE
SWAP**

Check out Curbside Swaps in these neighbourhoods:

Sept 6 in College Park

Sept 13 in City Park

Sept 14 in Buena Vista

Sept 20 in Meadowgreen

9 a.m. to 6 p.m.

Watch for the signs!



Composting Programs

Services such as voluntary curbside collection, drop-off depots, and education programs help residents divert organic waste from the landfill.

The Leaves and Grass Subscription Program served 3,900 households in 2014. Leaves, grass, and non-woody yard waste is collected at the curb from May to November on a bi-weekly basis. Materials are composted at the City's Highway 7 compost depot along with materials from public and commercial customers.

Two compost depots, one on McOrmond Drive and the other on Highway 7, provided residents the opportunity to drop off yard waste at no charge. These temporary locations accept leaves, grass, non-elm tree and shrub branches, as well as garden waste that would otherwise end up in the landfill.



More than 51,000 vehicle visits were made to the depots in 2014, including 40,000 visits made by residents, 9,400 visits from commercial haulers and 2,000 visits from City departments. An estimated 22,000 tonnes of compostable material was managed at the depots in 2014, resulting in the avoidance of approximately 5,000 tonnes CO₂e.



Approximately 150 commercial companies purchased \$200 permits which provided unlimited access to the depots for the season. Commercial haulers brought in approximately 28% of the materials delivered to the depots.

The depot on McOrmond Drive closed at the end of the 2014 season. A new compost depot transfer station opened on Highway 5 in 2015.

Compost depot operations are overseen by the Depots Supervisor (Roland Rusnell).

Compost Education

The City provides composting education through a partnership with the Saskatchewan Waste Reduction Council (SWRC). The SWRC has been running composting education programs since 1993; and in partnership with the City since 1995. New Master Composters are trained each year (there is now over 200) and in turn engage the community through volunteer work providing education at events, presentations, workshops, and home visits. Home visits were a new initiative in 2014 that was expanded in 2015. Master Composters visit a new composter at their home to help them set up a composting system or troubleshoot an existing one.

In 2014, 16 new Master Composters were trained and they along with existing Master Composter volunteers attended 13 events and made 10 home visits.

Compost bin rebates of \$20 were also offered to residents in 2014 (an increase from \$10 in previous years). 60 compost bin rebates were administered.

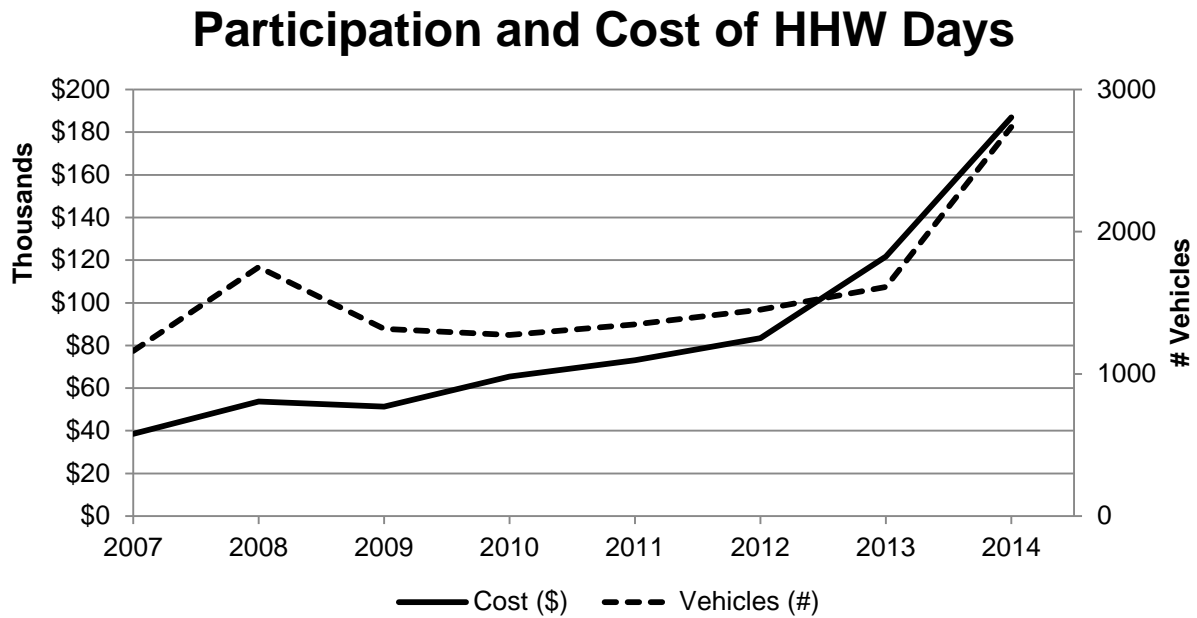
Composting, and the use of compost, will also be encouraged through the Healthy Yards program started in 2015; a partnership with the University of Saskatchewan Master Gardeners and the Food Bank and Learning Centre.



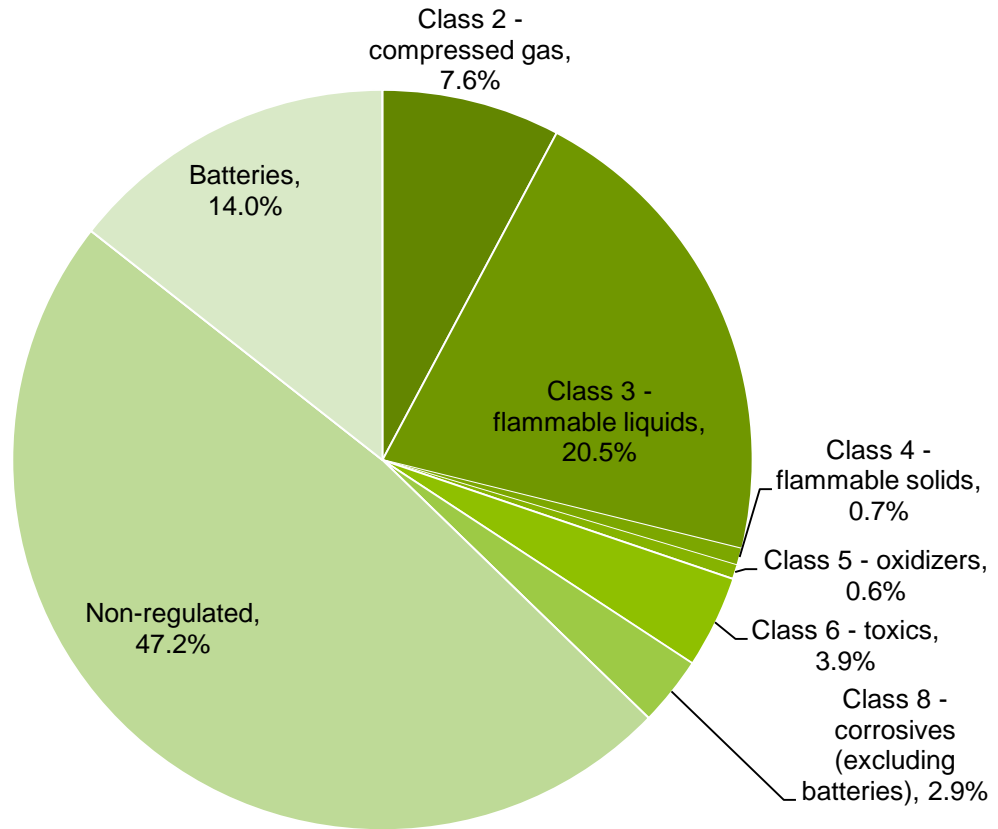
Hazardous Waste

Keeping hazardous waste out of the landfill, waterways, and environment.

In 2014, 12 Household Hazardous Waste Collection Days collected 72,126 kilograms (an increase over 2013 of 40%) of hazardous materials from 2,737 customers. The number of vehicles, the amount of hazardous materials collected, and the cost of the program have all risen since 2013.



Hazardous Materials Collected through HHW Days in 2014



- Class 2 (compressed gas): aerosols, propane tanks, inhalers, fire extinguishers
- Class 3 (flammable liquids): flammable liquids, paint and paint-related materials, adhesives, printing ink
- Class 4 (flammable solids): flammable solids
- Class 5 (oxidizers): oxidizing solids and liquids, organic peroxide
- Class 6 (toxics): toxic liquids, pesticides, pharmaceuticals
- Class 8 (corrosives): corrosive liquids and solids, mercury, ammonia solution
- Batteries: alkaline, lead-acid, lithium
- Non-regulated: oil, oil filters, oil containers, antifreeze, fertilizers, liquid cleaners, non-PCB light ballasts, fluorescent tubes, CFL bulbs, ink cartridges, lighters, treated railway ties
- Other/miscellaneous: PCB light ballasts, sharps, smoke detectors, e-waste, test samples (unknowns)

Household hazardous waste materials are also responsibly managed at the landfill. An Eco-Centre for the recovery of oil, oil containers, and oil filters has been established in partnership with Saskatchewan Association for Resource Recovery Corporation (SARRC). The Eco-Centre (one of 36 across Saskatchewan) captures in excess of 20,000 litres of used oil, over 1,000 used oil filters, and more than 1,200 kilograms of oil containers each year. These materials are reused and recycled, saving approximately 53 tonnes of CO_{2e} each year.



STANDING POLICY COMMITTEE ON ENVIRONMENT, UTILITIES & CORPORATE SERVICES

2015 Update to Our Environment: The City of Saskatoon's Environmental Leadership Report

Recommendation of the Committee

1. That the report of the General Manager, Corporate Performance Department dated October 13, 2015, be received as information; and
2. That a copy of this report be forwarded to the Saskatoon Environmental Advisory Committee for information.

History

At the October 13, 2015 Standing Policy Committee on Environment, Utilities & Corporate Services meeting, a report of the General Manager, Corporate Performance Department dated October 13, 2015 was considered.

Attachment

October 13, 2015 Report of the General Manager, Corporate Performance.

2015 Update to Our Environment: The City of Saskatoon's Environmental Leadership Report

Recommendation

That the report of the General Manager, Corporate Performance Department dated October 13, 2015, be forwarded to City Council for information.

Topic and Purpose

The purpose of this report is to provide the 2015 update to the City of Saskatoon's Our Environment Report.

Report Highlights

1. The 2015 update to the City of Saskatoon's 'Our Environment' Report is provided in the form of an online reporting tool highlighting the status of 12 selected indicators representing the key components of the state of Saskatoon's environmental health – air, land, water and waste.
2. Key findings include:
 - The waste diversion rate for 2014 of 22.5% was relatively unchanged from the 2013 rate of 22.7% and needs improvement to meet the target of 70%.
 - The amount of landfilled residential waste per capita is down to 249 kilograms per capita in 2014 from 340 kilograms per capita in 2007.
 - The percentage of residents choosing to walk, cycle or take transit to work has been relatively stable between 11.5% and 14.4 % for the past two decades and needs improvement to meet the target of 20%.
 - Saskatoon's air quality has been showing a slow downward trend over the past decade, with the average Air Quality Index ranking as 'Good' over the past 5 years, down from the ranking of 'Excellent' that it has for the previous 5 years.
 - The water quality both upstream and downstream of Saskatoon has consistently ranked as Good in the Water Quality Index since 2005.
 - The amount and distribution of new development investment has been added as a baseline indicator and illustrates that investment in development activities are balanced across the city.

Strategic Goals

The 2015 update addresses the Strategic Goal of Environmental Leadership. It includes the following success indicators from the Strategic Plan: waste diversion, water consumption, corporate greenhouse gas (GHG) emissions, the ecological footprint of Saskatoon, and community gardens. It further addresses the vision, priorities and strategies of the Strategic Plan in the areas of sustainable growth and transportation, protected lands, water quality, and reducing landfilled waste.

Background

On August 19, 2014 the Standing Policy Committee on Environment, Utilities & Corporate Services received Our Environment: The City of Saskatoon's 2014 Environmental Leadership Report.

Report

The first Our Environment report was released in 2014 based on the City of Saskatoon's (City) Strategic Plan Goals and success indicators. The report established baseline data for 44 environmental indicators in Saskatoon.

In 2015, the first online update has been created to update 12 selected indicators, represent key components of environmental health where data is available and highlight recently set Corporate Targets. The update builds on the baseline data in the 2014 Our Environment report. Additional indicators will be added in future online updates and new data posted when it is made available. The update can be found at saskatoon.ca/ourenvironment.

The "Where are we now?" section includes maps and charts to visually display the data in an easy to read and web-friendly format. The update compares the most recent data with the baselines published in the 2014 report and, when applicable, the targets adopted by the City. Graphics are used to indicate the trend shown by the data:

- An upward trending arrow for "improving"
- A horizontal arrow for "stable"
- A downward trending arrow for "needs improvement"

The "What are we doing?" section shows how the City is taking action to maintain or improve the environment. The section highlights three initiatives, provides a brief summary and provides a link to where more information can be found.

The "What can you do?" section is newly added as part of the update. It provides residents with opportunities to take action and get involved, highlighting simple things people can do in their homes, schools and workplaces, and ways to participate in city led initiatives.

The 12 selected indicators and their trends (Attachment 1) are:

Ecological Footprint	Needs Improvement
New Development Investment	Baseline
Protected Lands	Stable
Community Gardens	Improving
Corporate Greenhouse Gas Emissions	Needs Improvement
Community Greenhouse Gas Emissions	Needs Improvement
Transportation Choices	Needs Improvement
Air Quality	Needs Improvement
Water Quality	Stable
Water Consumption	Stable
Landfilled Waste	Improving
Waste Diversion	Stable

Communication Plan

Communications include the news media and the City's social media accounts along with the City's website, which has been updated to reflect the performance of key indicators for the Our Environment report.

Environmental Implications

As a report focused on environmental health, Our Environment provides an overview of key performance indicators relating directly to the City's Strategic Goal of Environmental Leadership. Where applicable, the implications associated with the indicators have been identified in the report.

Other Considerations/Implications

There are no policy, financial, privacy, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

The full Our Environment report is anticipated to be updated and published every 3 years, with the next full report in 2017. Updates on specific indicators are anticipated to be communicated through the City's website annually.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Attachment

1. Summary of Updated Indicators

Report Approval

Written by: Katie Burns, Environmental Coordinator
Reviewed by: Amber Jones, Manager of Education and Environmental Performance
Brenda Wallace, Director of Environmental & Corporate Initiatives
Approved by: Catherine Gryba, General Manager, Corporate Performance Department

Administrative Report – 2015 Update to Our Environment.docx

SUMMARY OF UPDATED INDICATORS

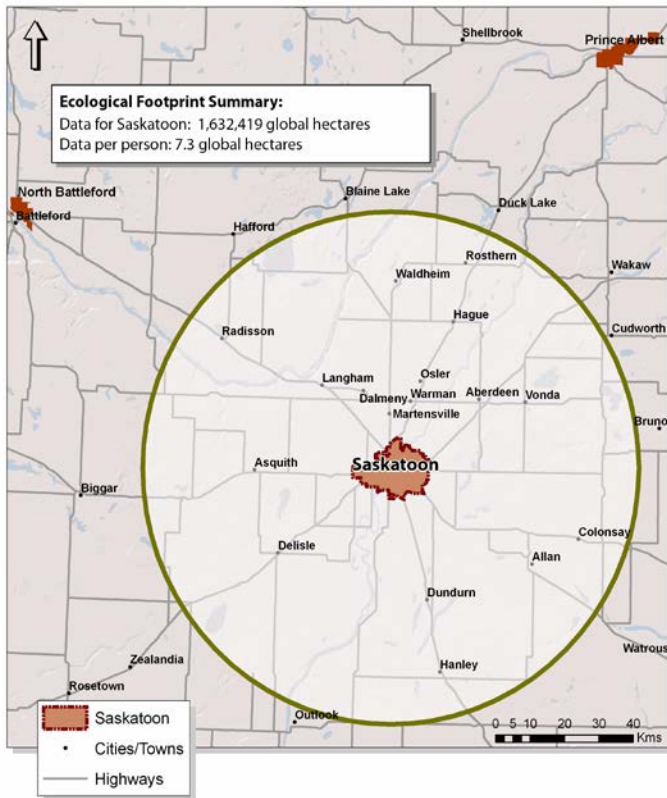
Ecological Footprint (needs improvement)

The Ecological Footprint is a tool that compares the total resources consumed by a community, to the amount of land it would take to support that consumption. The ecological footprint is a success indicator in the City's Strategic Plan.

Where are we now?

In 2010, Saskatoon's Ecological Footprint was 75 times larger than the city's total land area. The average ecological footprint per resident was 7.3 global hectares which was 4% higher than the national average in Canada. A new Ecological Footprint report for Saskatoon will be released at the end of 2015.

City of Saskatoon's Ecological Footprint



Source: City of Saskatoon Ecological Footprint Analysis, 2011

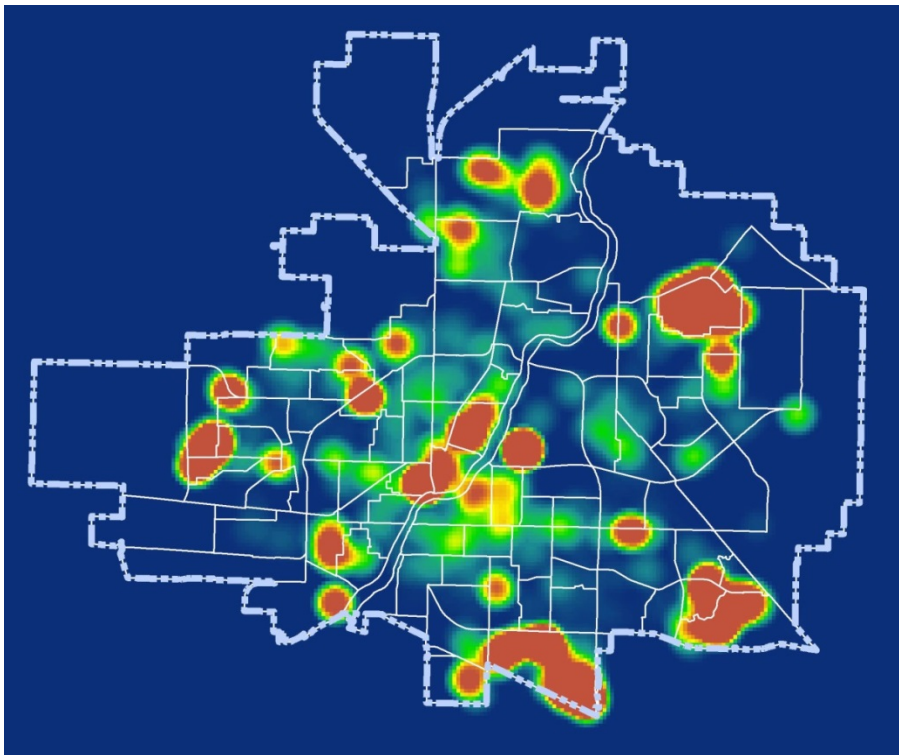
New Development Investment (baseline)

Saskatoon is growing. The location of new development and the amount that is being invested shows how the City's policies on the built environment are being put into action.

The City's Strategic Goal of Environmental Leadership and the success drivers of energy efficiency and responsible land use impact how we grow. A balance between investing in the downtown core, established neighbourhoods and new neighbourhoods with more sustainable design, will help conserve natural and agricultural lands, conserve water and energy, and reduce the cost of building new infrastructure like roads, water and sewer lines.

Where are we now?

Investment in new developments is spread throughout the city, with the greatest concentrations in the downtown and new Greenfield communities. In 2014, there were 4,968 building permits with a total value of \$871,254,000. \$2,112,000 of this value was for demolition permits. The total value of permits is higher in the red areas and the dark blue area has no values.



Source: City of Saskatoon – Planning & Development

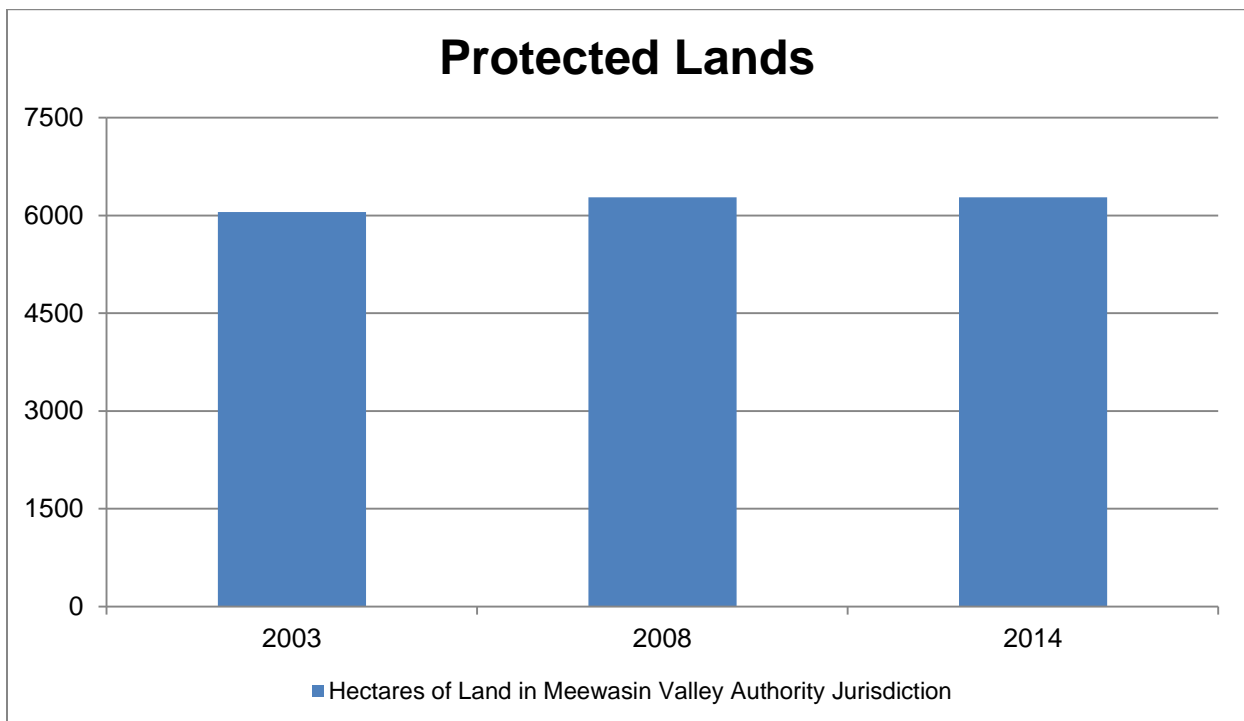
Protected Lands (stable)

The natural, undeveloped areas within our city support a diversity of plants and wildlife, perform “ecological functions” such as reducing and filtering storm water, storing greenhouse gases, removing harmful pollutants from the air, and contribute to our overall well-being and health.

Protected lands are an important part of our natural areas network and help meet the long-term Environmental Leadership strategy in the Strategic Plan to improve access to ecological systems.

Where are we now?

The amount of protected lands in the Meewasin Valley Authority jurisdiction has remained unchanged between 2008 and 2014.



Source: Meewasin Valley Authority

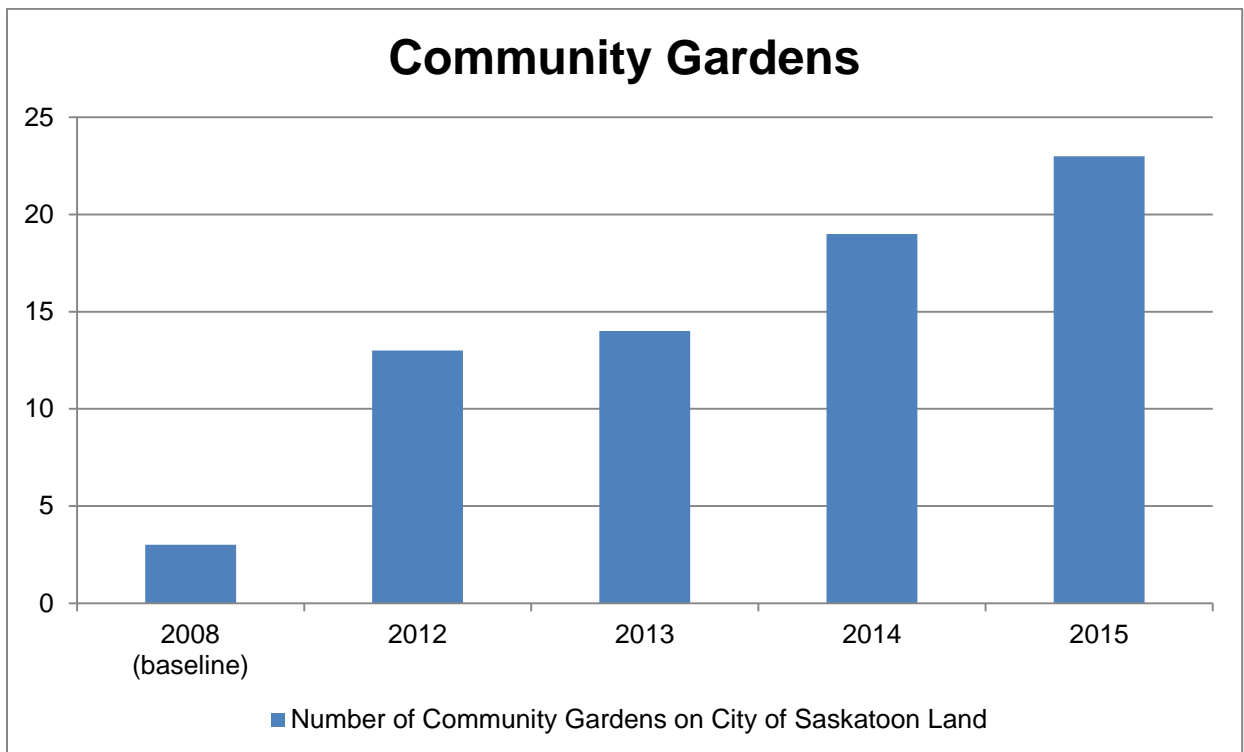
Community Gardens (improving)

Growing food in our neighbourhoods provides fresh, healthy food that has a lower environmental impact. Community gardens on public land provide opportunities to grow food for residents that do not otherwise have access to land suitable for gardening.

The number of community gardens is one of the success indicators in the Strategic Plan.

Where are we now?

The number of community gardens on city-owned land is increasing.



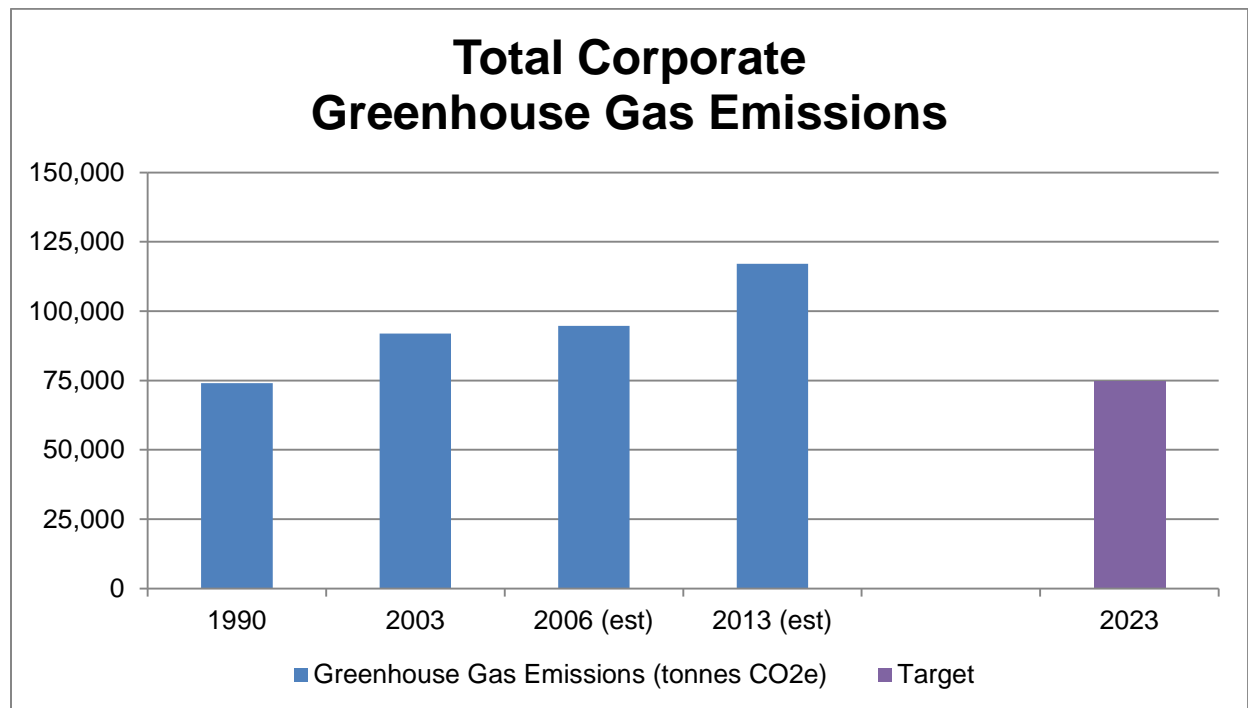
Source: City of Saskatoon – Community Development

Corporate Greenhouse Gas Emissions (needs improvement)

Greenhouse Gases (GHGs) are linked to climate change which is predicted to increase the frequency, and intensity of extreme weather events such as droughts, floods and storms. The City of Saskatoon has a role to play in reducing GHGs from its own operations and showing leadership in our community. The City of Saskatoon has adopted the target of 75,000 tonnes of CO₂e by 2023.

Where are we now?

The City of Saskatoon's corporate GHG emissions have increased since 2003. A new inventory is being compiled for 2014 emissions.



Source: ICLEI Energy Services, 2005; City of Saskatoon Environmental & Corporate Initiatives

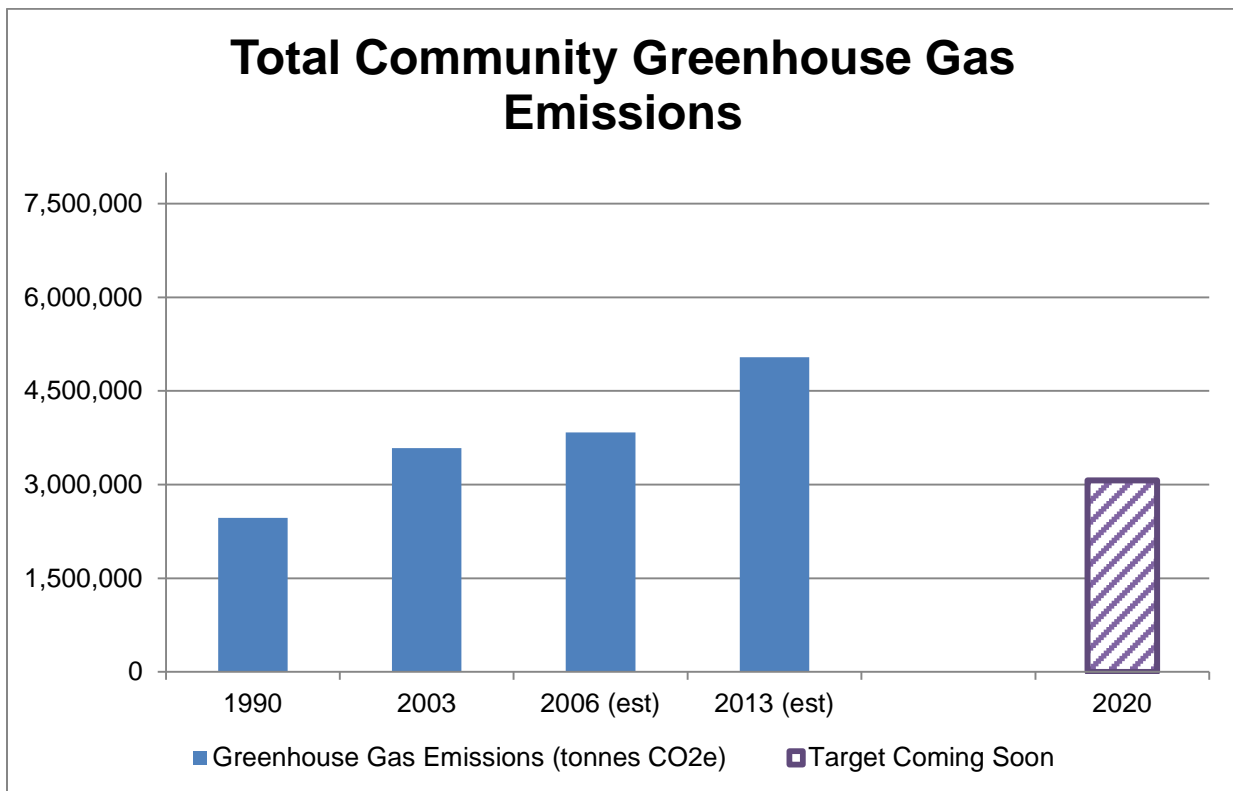
Community Greenhouse Gas Emissions (needs improvement)

The emissions from the community as a whole show the combined impact of businesses, institutions and residents.

The City of Saskatoon is in the process of adopting a target for community GHG emissions.

Where are we now?

The community's GHGs have increased since 2003. A new inventory is being compiled and the Saskatoon Environmental Advisory Committee is selecting a reduction target.



Source: ICLEI Energy Services, 2005; City of Saskatoon Environmental & Corporate Initiatives

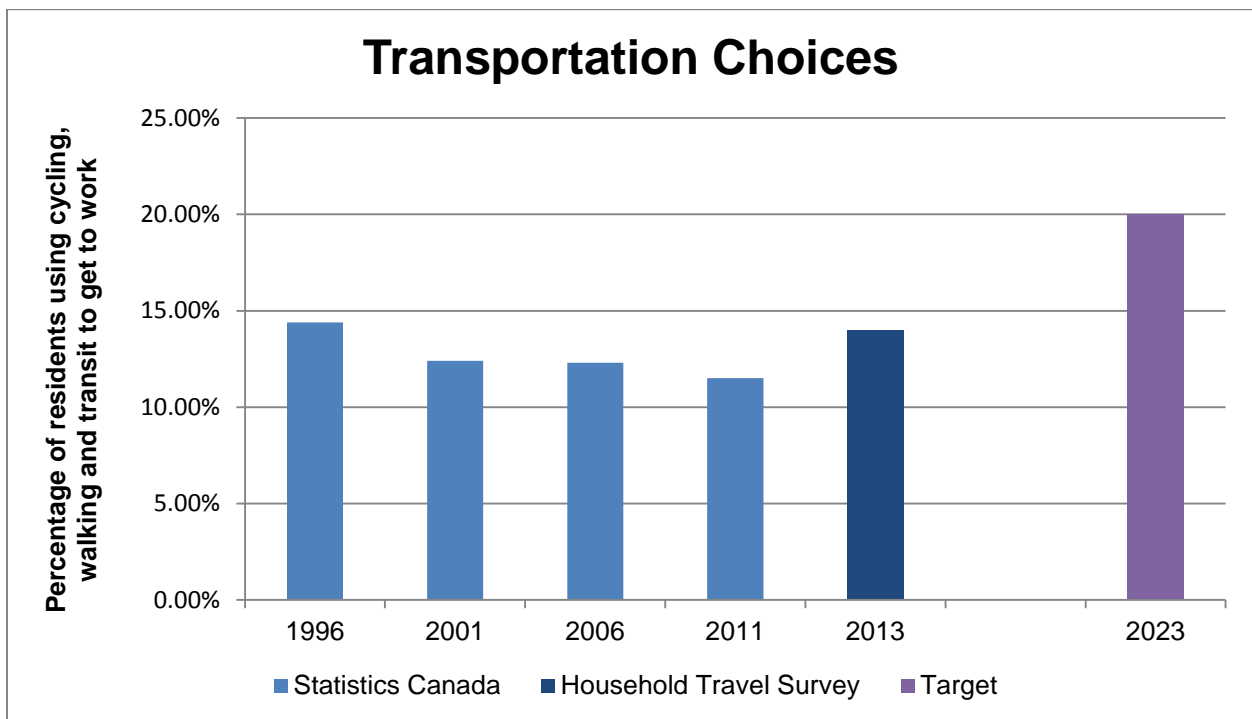
Transportation Modes (needs improvement)

Transportation impacts the environment, with vehicles generating more GHG emissions and requiring more land for road infrastructure than public transportation, cycling and walking.

The City of Saskatoon has adopted the target of 20% of residents using cycling, walking or transit to get to work by 2023, and is in the process of developing new targets for 2045 as part of the Growing Forward Growth Plan.

Where are we now?

The percentage of residents using cycling, walking or transit to get to work has been relatively stable since 2001, and needs improvement to meet the 2023 target adopted by City Council.



Source: Statistics Canada

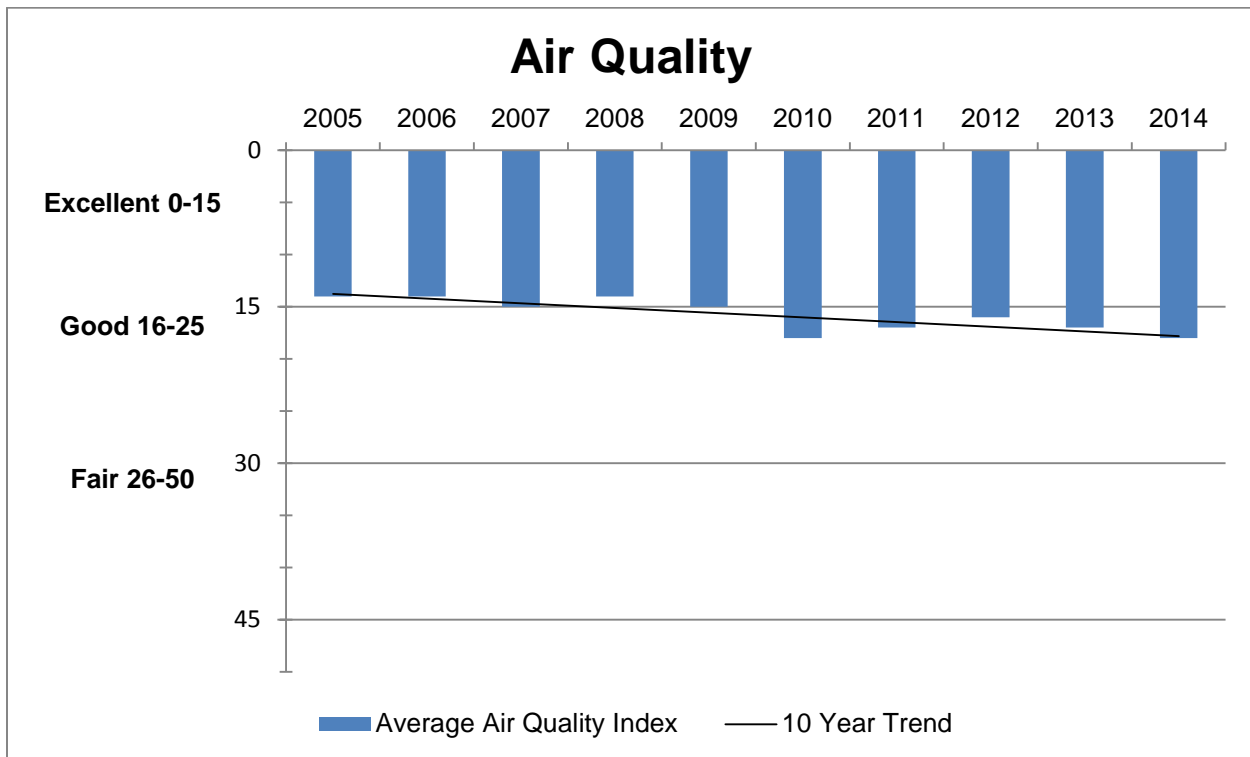
Air Quality (needs improvement)

Good air quality is important to our health and the environment. While Saskatoon has many favourable features for good air quality, sources of pollution make ongoing monitoring important.

The Strategic Plan vision statement for Environmental Leadership includes clean air.

Where are we now?

Saskatoon's average air quality was ranked as 'Good' by the Air Quality Index in 2014, and has been showing a slow downward trend over the past decade. This means that on average residents with severe respiratory ailments now may notice minor effects, when prior to 2009 the air quality was ranked 'Excellent' and there were no known health impacts.



Excellent	0-15	No known harmful effects to soil, water, vegetation, animals, materials, visibility or human health.
Good	16-25	No known harmful effects to soil, water, vegetation, animals, materials, visibility or human health. Persons with severe respiratory ailments sensitive to air pollution may notice some minor effects.
Fair	26-50	Adequate protection against harmful effects to soil, water, vegetation, animals, materials, visibility and human health. Persons with severe respiratory ailments sensitive to air pollution may need to modify their usual outdoor activities if experiencing effects.

Poor	51-100	Not all aspects of the environment and human health are adequately protected from possible adverse effects. The general population should consider reducing or rescheduling strenuous outdoor activities and higher-risk populations should reduce or reschedule outdoor activities.
Very Poor	>100	Continued air quality in this range could pose high risk to environment and public health.

Source: Government of Saskatchewan: 2015 State of the Environment Report

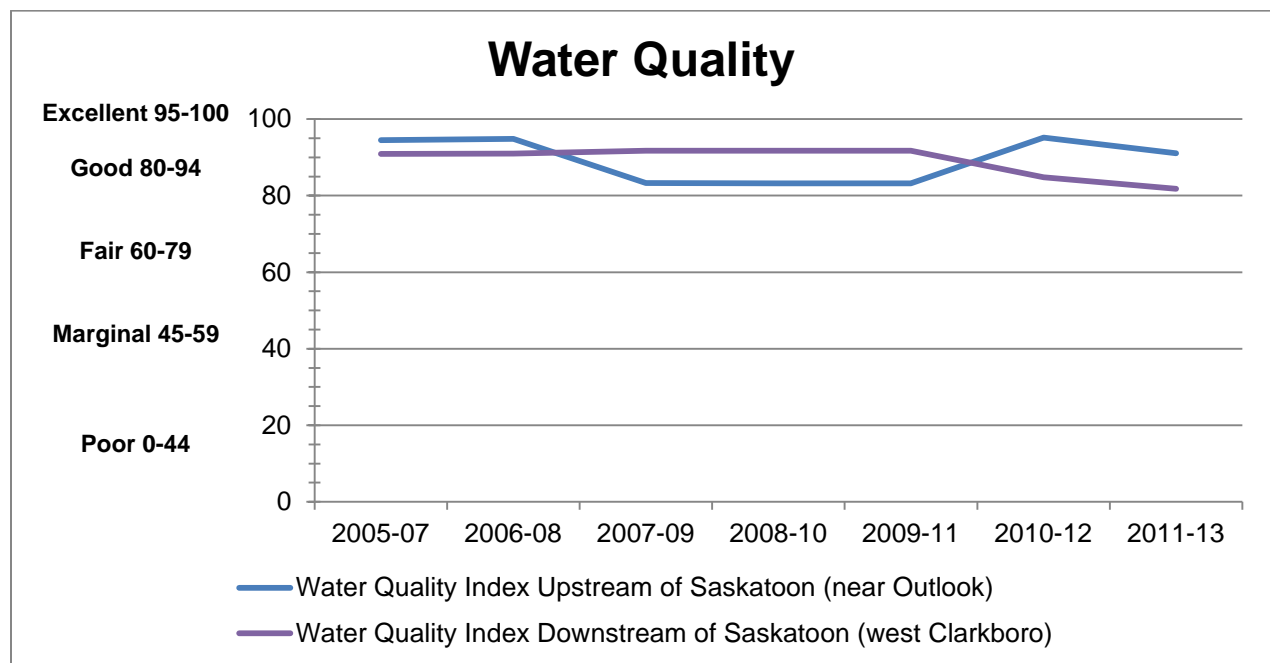
Water Quality Index (stable)

Water is essential for our health and for our community to prosper. As water passes through our community, the impact can be determined through comparing upstream and downstream water quality.

The Strategic Plan has the long term strategy to reduce the impact of storm water runoff that is going into the river.

Where are we now?

The South Saskatchewan River, upstream and downstream of Saskatoon, has consistently averaged 'Good' water quality.



Rating	Value	Description
Excellent	95-100	Water quality is protected with a virtual absence of threat or impairment; conditions very close to desirable levels. These index values can only be obtained if all measurements are within objectives virtually all of the time.
Good	80-94	Water quality is protected with only a minor degree of threat or impairment; conditions rarely depart from desirable levels.
Fair	60-79	Water quality is usually protected, but occasionally threatened or impaired; conditions sometimes depart from desirable levels.
Marginal	45-59	Water quality is frequently threatened or impaired; conditions often depart from desirable levels.
Poor	0-44	Water quality is almost always threatened or impaired; conditions usually depart from desirable levels.

Source: Government of Saskatchewan, Water Security Agency, Annual Report for 2014-15 State of Drinking Water Quality in Saskatchewan

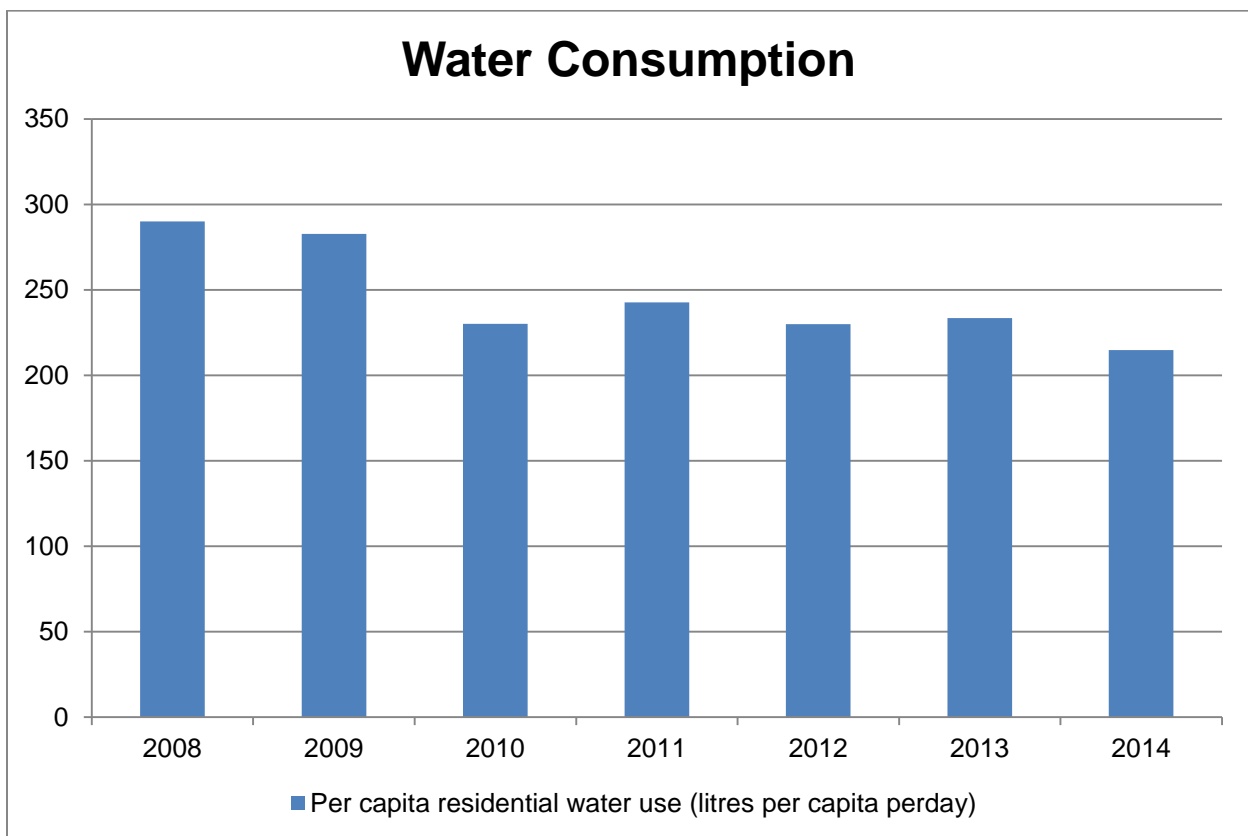
Water Consumption (stable)

Saskatoon benefits from an abundance of water from the South Saskatchewan River. The treatment of water and waste water; however, use significant energy and chemical inputs, generating environmental and financial concerns as the city grows.

Measuring the per capita water consumption is a success indicator in the Strategic Plan.

Where are we now?

Per capita residential use improved when new conservation-based water rates were introduced in 2010, and have remained fairly stable over the past 5 years.



Source: City of Saskatoon – Saskatoon Water

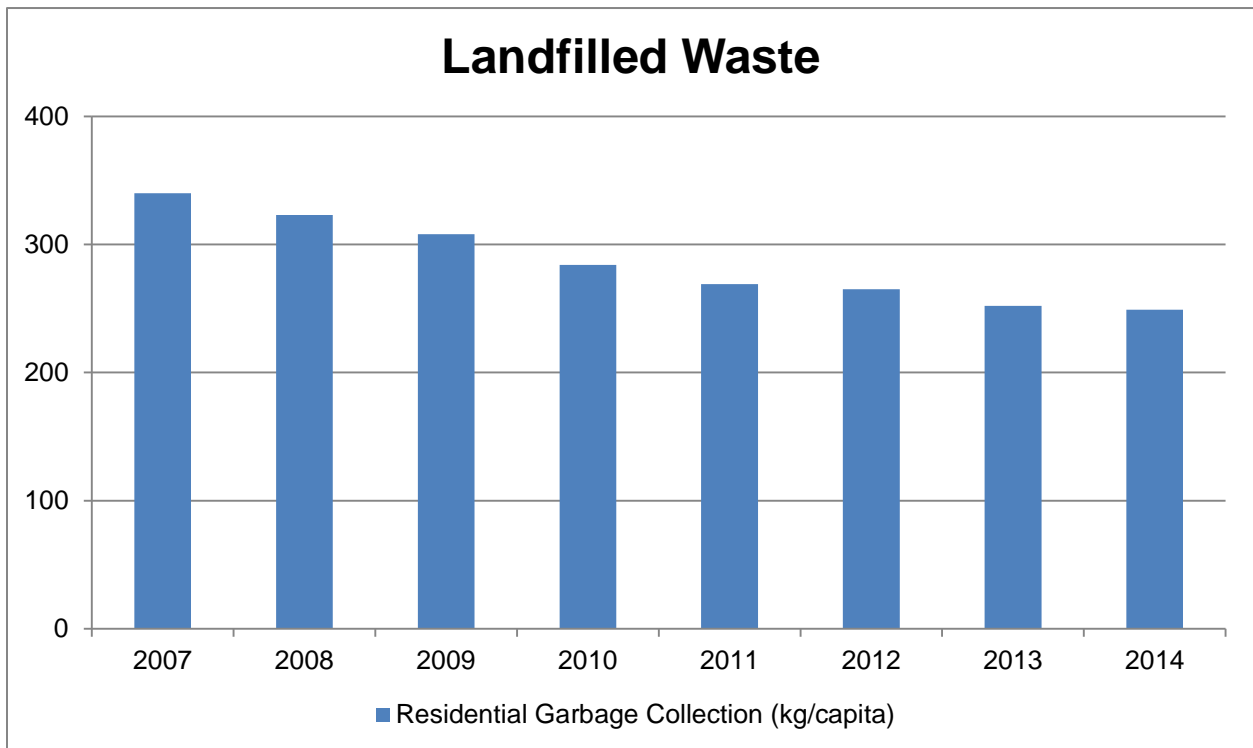
Landfilled Waste (improving)

As our population grows the amount of waste we are generating increases and changes. The current landfill has a lifespan of 40 years and the construction of a new landfill would be costly.

Reducing the waste that is landfilled is a priority in the Strategic Plan.

Where are we now?

The amount of waste each resident sends to the landfill is decreasing.



Source: City of Saskatoon – Environmental & Corporate Initiatives and Public Works

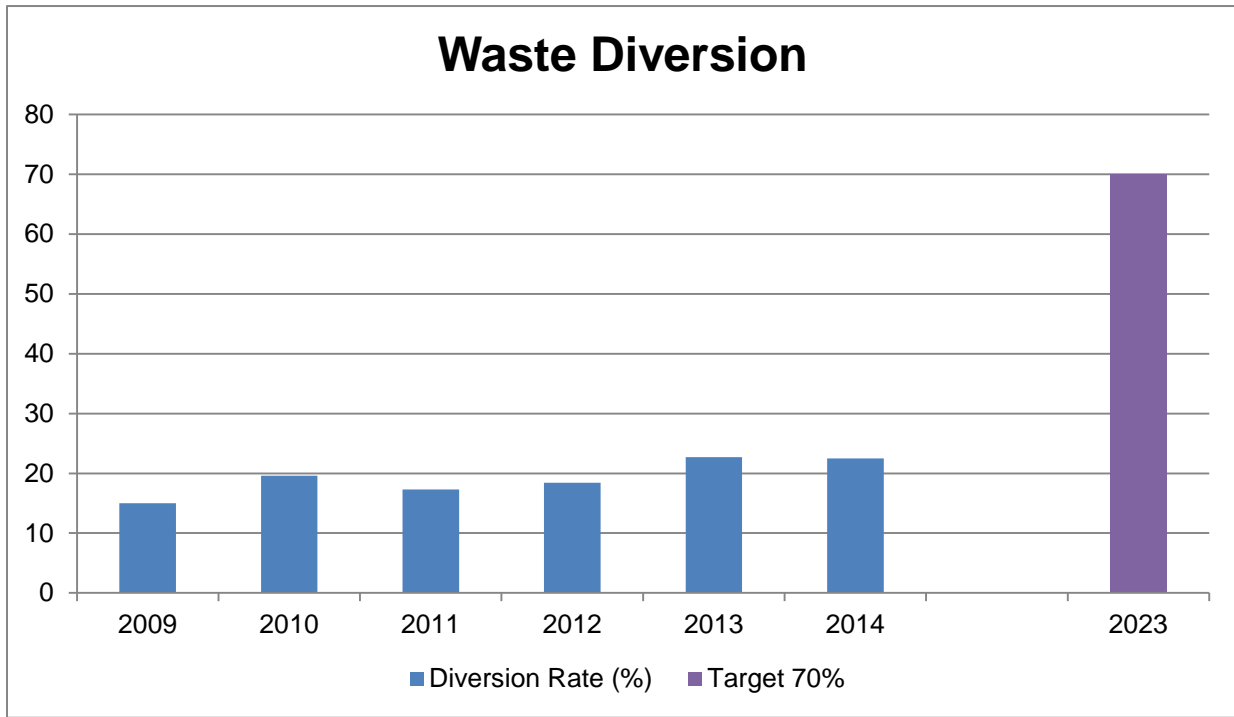
Waste Diversion (needs improvement)

The amount of material diverted from the landfill can reduce environmental impacts and extend the operating life of the current landfill.

The diversion rate is a success indicator in the Strategic Plan.

Where are we now?

The percentage of waste being diverted from the landfill has remained stable over the past year; however, improvement is needed to meet City Council's 2023 target of 70%.



Source: City of Saskatoon – Environmental & Corporate Initiatives and Public Works



STANDING POLICY COMMITTEE ON ENVIRONMENT, UTILITIES & CORPORATE SERVICES

Community Solar Power Co-operative – Memorandum of Agreement

Recommendation of the Committee

That the City Solicitor be requested to prepare a Memorandum of Agreement, in accordance with the terms set out in this report with the SES Solar Co-operative Ltd. and that His Worship the Mayor and the City Clerk be authorized to execute the agreement under the Corporate Seal.

History

At the October 13, 2015 Standing Policy Committee on Environment, Utilities & Corporate Services meeting, a report of the General Manager, Transportation & Utilities Department dated October 13, 2015 was considered.

Attachment

October 13, 2015 Report of the General Manager, Transportation & Utilities.

Community Solar Power Co-operative – Memorandum of Agreement

Recommendation

That the Standing Policy Committee on Environment, Utilities and Corporate Services recommend to City Council:

That the City Solicitor be requested to prepare a Memorandum of Agreement, in accordance with the terms set out in this report with the SES Solar Co-operative Ltd. and that His Worship the Mayor and the City Clerk be authorized to execute the agreement under the Corporate Seal.

Topic and Purpose

Saskatoon Light & Power (SL&P) and SES Solar Co-operative Ltd. (the Solar Co-operative) are prepared to jointly own and operate a solar photovoltaic generation system. The purpose of this report is to recommend that a Memorandum of Agreement (MOA) prepared collaboratively by both parties be executed.

Report Highlights

1. The solar photovoltaic (PV) generation system (PV System) will create a long-term demonstration site of PV technologies and the evaluation of their associated cost and performance that will direct future deployment.
2. This solar PV demonstration project will create a source of PV generation and revenue for the Solar Co-operative, the first solar co-operative in the province.
3. SL&P and the Solar Co-operative have prepared an MOA that defines how the PV System will be designed, constructed, operated and maintained, and decommissioned, including financial terms.

Strategic Goals

This report supports the four-year priority to continue implementation of the Energy and Greenhouse Gas Management Plan, under the Strategic Goal of Environmental Leadership. The Energy and Greenhouse Gas Management Plan lays out a number of actions related to the development of renewable energy, and promotes Community Wide Demonstration Projects.

Background

At its meeting held on May 25, 2015, City Council resolved that the Administration be directed to develop a MOA with the Solar Co-operative for the purpose of partnering on the capital costs for construction of a solar power facility, and report on the final terms of the MOA and issue and report on an Request for Proposals (RFP) for the PV System. As the project developed, and as the Solar Co-operative wants to play a role in the development of the RFP, both parties are completing the MOA and will then report further to City Council on the RFP.

Report

Solar PV Demonstration Project

SL&P, in partnership with the Solar Co-operative, are prepared to co-own a PV System. The objective of this project is to design, supply, install, and commission a PV System for the long-term evaluation of the technologies, and to generate electricity from a renewable energy source for the facility and the Solar Co-operative. In the future, it is anticipated that a much larger scale solar project (1 megawatt range) will be built near this smaller demonstration project.

The Solar Co-operative is an initiative of the Saskatchewan Environmental Society. It is organized as a consumer co-operative whose members are investing in solar power generation assets for the purpose of producing renewable energy, reducing their greenhouse gas footprint, and advancing research in solar energy. They currently have 74 members.

SL&P and the Solar Co-operative will develop a fixed-angle PV array and a single-axis seasonal tilt PV array. Each party will contribute up to \$90,000 toward capital infrastructure, which is estimated to build a 36 kilowatt (kW) system. The contribution from the Solar Co-operative will come from both its own member share capital base and also by way of a contribution from the Saskatchewan Environmental Society.

The partnership provides a unique opportunity for local residents, who otherwise may not have the means to install solar panels on their house or apartment, to generate some of their own electricity from a renewable source. The co-operative is best equipped to organize and sell collective PV ownership.

Memorandum of Agreement

SL&P and the Solar Co-operative created a committee made up of members from both parties who have prepared a MOA. The central purpose of this MOA is to record the basis upon which the PV System will be designed, constructed and commissioned, operated and maintained, decommissioned, and upon which the associated electrical energy generated by the Solar Co-operative will be recorded and paid for by the City of Saskatoon (City) to the Solar Co-operative.

Electricity generated from the Solar Co-operative's portion of the PV System will be purchased by SL&P under a long-term power purchase agreement. The electricity purchased from the Solar Co-operative replaces electricity that would be purchased from SaskPower. Starting in year one, this PV System is estimated to provide the City with \$3,000 in reduced electricity costs and the Solar Co-operative with \$3,000 in revenue.

The key terms of the MOA will be as follows:

- a) the Agreement defines how the PV System will be designed, constructed, operated and maintained, and decommissioned, including financial terms;
- b) the PV System will be located adjacent to the Landfill Gas Power Generation Station;

- c) the term of the Agreement will be 25 years with an option to renew for a further five (5) years subject to the mutual agreement of the parties;
- d) the participating interest of each Party shall be based on the percentage shares owned by each Party;
- e) the parties will develop and the City will issue an RFP for the PV System;
- f) both parties will play a role in the evaluation of Proposals;
- g) acceptance of a preferred Proposal will be subject to City Council's agreement prior to award;
- h) the City will manage the construction of the PV System;
- i) the parties will establish an operations committee to address post-construction matters;
- j) the City will charge the Solar Co-operative an annual fee to look after day-to-day operations, and the parties will share the costs of any required replacement equipment;
- k) the City shall pay the Solar Co-operative for the electricity produced by their share of ownership;
- l) the data collected from the PV System will be shared by both parties and with the Saskatchewan Environmental Society;
- m) there will be standard mutual requirements for insurance and indemnity;
- n) the agreement will contain an option to terminate at any time, and there will be a process to address this possibility; and
- o) the agreement does not create a legal partnership, each party remains separately responsible and cannot bind the other unless all parties agree.

Public and/or Stakeholder Involvement

If the solar power demonstration project proceeds, and on successful installation of the PV System, the public will be invited to an open house.

Communication Plan

If the solar power demonstration project proceeds, appropriate communication materials will be jointly prepared by the City and the Solar Co-operative to share the project details with city residents. This may include website content, social media posts, news release, video, and/or brochures.

Financial Implications

The capital cost will be funded from Capital Project No. 1281 - Sustainable Power Generation Options. By reducing the electricity purchased from SaskPower, the project payback of the capital cost is estimated at 20 years. Payment to the Solar Co-operative will be funded from Operating Budget 03-200 – Landfill Gas.

Environmental Implications

The Saskatchewan grid has one of the highest carbon emissions per kWh generation in Canada. The electricity bought from SaskPower for use in the facility has 107 tonnes of CO₂e per year associated with it. By generating electricity using solar panels rather than buying it from SaskPower, a 36 kW project is estimated to reduce annual Greenhouse Gas emissions by 32 tonnes CO₂e.

Other Considerations/Implications

There are no options to the recommendation, and no policy, privacy, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

An RFP for supply and installation of the solar power system is planned for the winter 2015/2016. A recommendation for award of the successful proposal will be presented to City Council following the closing of the RFP.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Report Approval

Written by: Nathan Ziegler, Sustainable Electricity Engineer
Reviewed by: Trevor Bell, Director of Saskatoon Light & Power
Approved by: Jeff Jorgensen, General Manager, Transportation & Utilities
Department

EUCS NZ - Community Solar Power Co-operative – Memorandum of Agreement



STANDING POLICY COMMITTEE ON ENVIRONMENT, UTILITIES & CORPORATE SERVICES

Network Connectivity Services – Request for Proposal Award

Recommendation of the Committee

1. That a contract with Shaw Business for the provision of network connectivity services for the civic facilities with the two Active Data Centers DC1 (City Hall) and DC2 (SaskTel) for up to thirty six months at an estimated fee of \$412,065, plus applicable taxes be approved; and
2. That Purchasing Services be authorized to issue the necessary Purchase Order.

History

At the October 13, 2015 Standing Policy Committee on Environment, Utilities & Corporate Services meeting, a report of the General Manager, Corporate Performance Department dated October 13, 2015 was considered.

Attachment

October 13, 2015 Report of the General Manager, Corporate Performance.

Network Connectivity Services – Request for Proposal Award

Recommendation

That the Standing Policy Committee on Environment, Utilities and Corporate Services recommend to City Council:

1. That a contract with Shaw Business for the provision of network connectivity services for the civic facilities with the two Active Data Centers DC1 (City Hall) and DC2 (SaskTel) for up to thirty six months at an estimated fee of \$412,065, plus applicable taxes be approved; and
2. That Purchasing Services be authorized to issue the necessary Purchase Order.

Topic and Purpose

The purpose of this report is to request approval to proceed with a contract with Shaw Business for the supply of network connectivity services for up to a thirty six (36) month period.

Report Highlights

1. Network connectivity infrastructure connects the civic facilities around the City of Saskatoon (City) to the two active Data Centers.
2. On August 6, 2015, the City issued a Request for Proposal (RFP) for a Metropolitan Area Network (RFP #15-0719) to provide this service.
3. The recommendation is that Shaw Business, the Preferred Proponent, be awarded the contract for the supply of these network services.

Strategic Goal

This report supports the long term strategies of Continuous Improvement by improving the City's network connectivity services and reducing the operational costs from a Network Service Provider.

Background

The City's Civic Facilities are interconnected using both private and internet based networking services with the two Active Data Centers. This network (Metropolitan Area Network) design allows our staff to connect with the applications and systems anywhere in the corporation. This also gives staff access to websites and services outside our corporate network. At the time of the previous contract, there was only one vendor with the capabilities to provide the City of Saskatoon with these services.

Report

At present, the network consists of both private and internet based services from a single vendor. The Saskatchewan technology market is changing and there is more than one vendor with the infrastructure to provide these services. The goals of this RFP were to secure networking services that would at a minimum meet or exceed the current

Network Connectivity Services – Request for Proposal Award

service levels, provide a clear roadmap to increase capacities as the corporate requirements increase, and realize operational savings.

Work to be completed under the scope of this contract will include:

- provide network services to the civic facilities identified in this proposal,
- connect these services at each of the facilities and at both the Active Data Centres,
- provide the appropriate network interfaces to the City's internal network equipment, and;
- provide test data indicating the appropriate network capacity is fully operational and available at each of the civic facilities.

RFP for Metropolitan Area Network

An RFP for a Metropolitan Area Network was issued on August 6, 2015, with a closing date of September 8, 2015.

Responses were received from:

- Shaw Business
- SaskTel Business Solutions
- WiBand Communications Corp.

The RFP Review Team is composed of the Manager, IT Operations, IT Consultants from the Systems and Networks teams, and Procurement Specialist. The team evaluated the proposals based on the following criteria detailed in the RFP.

Category	Points
Private Service vs. Internet Based	15
Performance and Service Description	25
Service Termination Costs	5
Availability (Up-Time)	10
Cost of Additional Bandwidth	5
Three Year (36 month) Cost	40
TOTAL	100

Preferred Proponent

Upon completion of the evaluation, the RFP Review Team determined that the proposal submitted by Shaw Business achieved the highest score and met the requirements of the RFP; therefore the recommendation is to award the RFP to Shaw Business as detailed below:

For up to 36 month supply of network connectivity infrastructure for the civic facilities as indicated in the RFP proposal review for an estimated fee of \$412,065, plus applicable taxes.

Financial Implications

The total cost of \$412,065 (plus taxes) will be allocated over 36 months starting January 1, 2016. The first year costs will be managed within the proposed 2016 Operating Budget for the IT Division. The second and third year costs will be part of the 2017 and 2018 proposed operating budgets. This proposal will realize \$117,585 in operational cost savings over the 36 month contract based on estimates from this proposal against our current contract in service.

Other Considerations/Implications

There are no policy, environmental, Privacy, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

This project will be completed in three major phases. The first phase will be to complete installation work for eight of the sites starting in December 2015 for a January 2016 implementation, Phase 2 installation work of twelve sites will start as soon as the 2016 spring construction season begins with a May 2016 implementation, and finally, Phase 3 will be the remaining 9 sites that are under contract with the incumbent service provider until November, 2016.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Report Approval

Written by: Robert Moncur, Manager, IT Operations
Reviewed by: Paul Ottmann, Director, Information Technology
Catherine Gryba, General Manager, Corporate Performance Department
Approved by: Murray Totland, City Manager



STANDING POLICY COMMITTEE ON ENVIRONMENT, UTILITIES & CORPORATE SERVICES

Parks, Design, Construction, and Maintenance – Civic Service Review

Recommendation of the Committee

1. That the improved efficiencies and effectiveness in delivering the service as outlined in this report be received as information;
2. That the Service Level for Parks Pathway Snow Clearing be increased to include snow clearing services to 14 kilometres of Naturalized Park pathways;
3. That the Service Level for Shelterbelt/Buffer Strip Maintenance be increased so that a proactive service level that includes mowing, trimming and garbage pick-ups within the shelterbelt areas can be established; and
4. That the Administration report on a Park Infrastructure Asset Management Plan including a funding strategy to achieve desired service levels.

History

At the October 13, 2015 Standing Policy Committee on Environment, Utilities & Corporate Services meeting, a report of the General Manager, Corporate Performance Department dated October 13, 2015 was considered.

Attachment

October 13, 2015 Report of the General Manager, Corporate Performance.

Parks Design, Construction, and Maintenance - Civic Service Review

Recommendation

That the Standing Policy Committee on Environment, Utilities and Corporate Services recommend to City Council:

1. That the improved efficiencies and effectiveness in delivering the service as outlined in this report be received as information;
2. That the Service Level for Parks Pathway Snow Clearing be increased to include snow clearing services to 14 kilometres of Naturalized Park pathways;
3. That the Service Level for Shelterbelt/Buffer Strip Maintenance be increased so that a proactive service level that includes mowing, trimming and garbage pick-ups within the shelterbelt areas can be established; and
4. That the Administration report on a Park Infrastructure Asset Management Plan including a funding strategy to achieve desired service levels.

Topic and Purpose

The purpose of this report is to provide an overview of the Parks Division Civic Service Review.

This review explored opportunities for improvement within the design, construction, and maintenance of civic parks and open spaces. Implementation of these findings will improve service to citizens, increase the efficiency of park design, construction and maintenance, and improve accuracy in budgeting for the Parks Division.

Additionally, this report recommends Service Levels for Parks Pathway Snow Clearing and Shelterbelt/Buffer Strip Maintenance.

Report Highlights

1. There are two key components of the Civic Service Review (CSR) Process; efficiencies identified by employees delivering the service, and where warranted, recommending new service levels for activities where public expectations are not being met.
2. The efficiencies include improved coordination of park design/development and coordinated maintenance to reduce costs, improved customer service to avoid duplication of efforts, and the implementation of new satellite maintenance facilities. Cost savings are estimated in the range of 10% or \$1,500/ha in new park development areas.
3. The recommended Service Level for Parks Pathway Snow Clearing would include snow clearing services to 14 kilometres of Naturalized Park pathways.
4. The recommended Service Level for Shelterbelt/Buffer Strip Maintenance would be increased to increase mowing, trimming and garbage pick-ups within the shelterbelt areas.

5. The CSR identified the need for an asset management approach for key park “hard” assets, like pathways and irrigation systems.

Strategic Goal

This report supports the Strategic Goal of a Culture of Continuous Improvement. Process improvements focus on identification of root cause issues and innovative and creative solutions that will provide optimal service improvements.

Background

City Council, at its meeting held on December 3 and 4, 2013, approved the Continuous Improvement Strategy which includes the following three components:

- Annual Civic Service Reviews - an operational review process to find ways to control expenditures and to seek efficiencies in the delivery of municipal programs and services.
- Internal Process Reviews - focus on identifying and removing redundancies and waste within existing processes to increase efficiencies in civic operations.
- Building capacity in the corporation through innovation coaches and empowering employees.

See Attachment 1 for the Continuous Improvement Strategy Overview.

Report

The impact on citizens is a key consideration during the planning, design, construction, and maintenance of civic parks and open spaces. Citizens expect parks to be clean, green and safe; effective planning, communication and budgeting in future parks design and construction will have a positive impact on citizen satisfaction and ensure they are receiving good value for their tax dollars.

There are two aspects to this report. The first highlights key findings from the CSR on improving efficiencies and effectiveness in aligning our services to citizen expectations. The second aspect is a review of Services Levels for Parks Pathway Snow Clearing and Shelterbelt/Buffer Strip Maintenance; two areas where current service levels appear to not meet citizen expectations.

Review for Efficient and Effective Service

Employees involved in the design of parks, and the construction and the maintenance of parks were all involved to bring forward perspectives on how to provide safe, green and clean parks to our citizens.

Processes were reviewed so there was clarity of how we are currently delivering our services. This was followed by identifying what our citizens expect, and how can we change our work so that we are as efficient as possible, and we are effective in delivering what citizens want.

The following issues were addressed in the CSR:

- Park Design, Development and Maintenance Standards
- Drainage Issues

- Customer Service
- Reducing Downtime and Improving Productivity
- Staffing and Scheduling of Work
- Special Events

Attachment 2 is an overview of the CSR. The following provides some highlights of changes being implemented:

- Optimization Teams established to ensure the impact of design on long term maintenance is considered at the planning stage for new developments and reflected in the operating budget.
- Improved customer service with one point of contact to reduce duplication of effort.
- In 2015, two crews piloted a model where horticultural and turf maintenance staff formed one crew that works together to maintain an entire park as opposed to each arriving in the park on varying days or at varying times as per previous maintenance schedules. This initiative is estimated to save 5% (\$750/ha) in labour costs due to reduced travel time.
- Beginning in 2016, new satellite maintenance facilities will be established in appropriate locations in new development areas to reduce travel times and improve safety. This initiative is also estimated to save up to 5% in labour costs (\$750/ha), due to reduced travel.

Service Level Review

By approving the Service Level, citizens will know what they can expect for this service, and the Administration will allocate resources accordingly – this may require a phased approach.

The Service Levels identify the following for Park Pathway Snow Clearing and Shelterbelt and Buffer Strip Maintenance:

- Total inventory on Municipal Property
- Total inventory being maintained
- Current Service Level
- Total Annual Cost
- Annual Budget Allocation

Each service has an accompanying set of options that describes increased service levels, the related costs, and the funding gap.

- The Administration is recommending that the Service Level for Parks Pathway Snow Clearing be increased to include snow clearing services to 14 kilometres of Naturalized Park pathways (Attachment 3).
- The Administration is recommending that the Service Level for Shelterbelt/Buffer Strip Maintenance be increased so that a proactive service level that includes mowing, trimming and garbage pick-ups within the shelterbelt areas can be established (Attachment 4).

Communication Plan

The CSRs provide an opportunity for the public to learn more about the City's operations, the costs to deliver the services, and to provide feedback and input into how the City can deliver any of its services more efficiently. The approved Levels will be communicated through 311/Service Saskatoon so citizens know what services they can expect. Citizens will have the opportunity to provide input into levels of service as well as the budget using the Shaping our Financial Future budget tools.

Results from the Civic Service Reviews will be communicated on the City's website in the 'Latest Strides' and/or 'City Spotlight' sections of the *Our Performance* page at www.saskatoon.ca/strides.

Financial Implications

As part of the Parks CSR, a review of the operating budget estimates was completed and estimates were adjusted to more accurately reflect the operating unit cost per hectare of park space, as well as the number of staff required per hectare. Unit cost will be utilized in the definition of maintenance service levels to ensure the Parks Division budget is an accurate reflection of the total costs required to maintain the current inventory at the approved service level. Additionally, this information will provide accurate information for future operating budget requests to maintain additional parks coming online as Saskatoon continues to grow.

The Administration will provide a future report on Park Infrastructure Asset Management Plan including a funding strategy to achieve desired service levels.

Operating and capital cost impacts are currently under review and will be brought forward in future reports to Committee and/or Council. The financial implications related to efficiency gains will be quantified and will be reallocated to fund other strategic and operational priorities and/or may contribute to a reduction in the base budget.

The 2016 Business Plan and Budget proposed funding of \$30,000 to partially implement the new Service Levels recommended in this report. This would include snow clearing on 14 km of additional park pathways and improved shelter belt maintenance.

Other Considerations/Implications

There are no policy, environmental, privacy, or CPTED implications or considerations.

Due Date for Follow-up

Further reports will review additional Parks-related service levels and continue to refine the savings outlined in this report.

Reports related to key findings of this report (i.e. asset and maintenance service levels) will be brought forward to the SPC on Planning, Development and Community Services Committee for approval. Action plans and recommendations will be incorporated into the annual business planning and budgeting process for the Parks Division.

The CSR identified the need for an asset management approach for the key “hard” assets within civic parks and open spaces, including: pathways, irrigation systems, lighting, maintenance buildings, play structures, park furniture, and other similar assets. A further report on an overall asset management approach for Parks facilities will be forthcoming later in 2015.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Attachments

1. Continuous Improvement Strategy Overview
2. Executive Summary – Parks Efficiencies and Effective Service Delivery
3. Service Levels – Park Pathway Snow Clearing
4. Service Levels – Shelterbelt and Buffer Strip Maintenance

Report Approval

Written by: Kim Matheson, Director, Strategic and Business Planning
Reviewed by: Darren Crilly, Director, Parks
Randy Grauer, General Manager, Community Services Department
Approved by: Catherine Gryba, General Manager, Corporate Performance Department

Administrative Report - Parks Civic Service Review.docx

Continuous Improvement Strategy Overview

In 2014, the Administration began our Civic Service Reviews. We have made good progress, and have learned much. One of our learnings has been that there are several components to a complete Service Review process. To try and bring some clarity, we are recommending a framework to the overall program.

Framework for Civic Service Reviews:

1. Service Level

a. Asset Service Level

How the assets and services are preserved, renewed, and funded to ensure the quality of life for citizens is sustained or improved, and include:

- Inventory of Asset(s)
- Condition of Asset(s)
- Costs to Preserve Asset(s)
- Gap in Funding
- Funding Plan/Investment Strategy

b. Maintenance Service Level

The maximum interval between tasks or activities required to maintain the defined level of service are referred to as Maintenance Service Levels, and include:

- Description of Service
- Definition of Service Level
- Cost to Maintain Asset
- Timelines to achieve Service Level
- Service Level Approval

2. Efficiency

a. Operational Efficiency

A review of current processes identifies opportunities to improve efficiency and increase the effectiveness of the service and /or program. Savings resulting from the improvements will be quantified and reported as part of the overall Civic Service Review.

Knowledge Base for Service Saskatoon

All of this information can be used to prepare our knowledge base for Service Saskatoon and the 311 Call Centre.

Communication Plan

The CSRs provide an opportunity for the public to learn more about the City's operations, the costs to deliver the services, and to provide feedback and input into how the City can deliver any of its services more efficiently. Citizens will have the opportunity to provide input into levels of service as well as the budget using the Shaping our Financial Future budget tools.

Results from the Civic Service Reviews will be communicated on the City's website in the 'Latest Strides' and/or 'City Spotlight' sections of the *Our Performance* page at

www.saskatoon.ca/strides.

Executive Summary - Parks Efficiencies and Effective Service Delivery

The Parks Division is responsible for the maintenance and preservation of more than 1,500 hectares of City of Saskatoon parks and civic open spaces. In total, the City controls and maintains over 2,600 hectares of green space (includes parks, cemeteries, green spaces inside and outside city limits, as well as open spaces around civic facilities). As an example, Parks maintains about 930 ha of neighbourhood parks, at an average annual cost of \$15,000/ha for irrigated areas and about half that for non-irrigated areas. In some new neighbourhood parks, the annual maintenance cost is approaching \$20,000/ha.

Introduction

The Executive Committee approved the recommendation that the Parks Division undergo an Civic Service Review (CSR) in 2014. Two main issues were explored during the review: opportunities for operational efficiencies, and the definition of service levels including cost to deliver the service and options for alternate service levels.

During the CSR, the team reviewed the current state of parks design, construction and maintenance. They examined what success would look like from the citizen's perspective and finally examined what the process for design, construction and maintenance of civic parks should be. The team analyzed the data available and determined what key information should be tracked in the future.

This report identifies the key findings and recommendations from the Parks Division CSR and also outlines current and optional service levels, including the cost to deliver the service at each level, for Pathway Snow Clearing, and Shelterbelt/Buffer Strip Maintenance. Future reports will address other Parks-related service levels.

Report

As Saskatoon continues to grow, so do expectations and demand related to the use of parks and open spaces. As an example, the number of special events held in parks and open spaces has increased 84% over the past five years. Parks and open spaces are of interest to a wide variety of stakeholders, including: homeowners, land developers, sports organizations, other civic divisions, utility companies, business owners, tourists, etc. Regardless of the increased usage and variety of stakeholders, citizens expect that parks are:

- Clean, green and safe,
- Accessible and provide a wide variety of activities, and
- Contain some naturalized elements, with appropriate preservation and renewal plans.

During the Civic Service Review, the following challenges were identified with Parks Design, Construction and Maintenance:

1. Impact of growth and development requires increased consideration during the planning, design, construction and maintenance of civic parks and open spaces.

- Horticulture and turf maintenance staff should be included in the early stages of park design, in order to effectively communicate the potential impacts of a design on the maintenance operations and also to increase the effectiveness of the budget process as it would allow them to plan effectively for maintenance of future hectares of parks coming into inventory.
2. A review of the operating budget estimates confirmed that adjustments were needed to more accurately reflect the operating cost per unit to deliver Parks services and that the approved service level needs to be clearly defined.
 3. As Saskatoon continues to grow and the footprint of the city expands, crews are travelling greater distances to complete their daily maintenance schedules. Increased travel has resulted in an increase of downtime and safety concerns as staff are traveling on major roadways with equipment and increasing wear and tear on equipment originally meant for use in parks and open spaces, not roadways. Existing Parks maintenance yards are also filled to capacity.
 4. Current parks scheduling potentially means that several City crews could be in the same park at different times on any given day, leaving a perception of inefficiency with residents in the area and a lack of ownership amongst Parks employees.
 5. There has been an 84% increase in the number of special events held in parks and/or open spaces over the past five years. This increase in events has resulted in increased damage to the parks and at times does not allow time for revitalization of the park/open space before the next event is scheduled. Parks is incurring increased costs due to the maintenance required following a major event.
 6. Parks and Facilities Divisions are under consistent pressure to maintain “hard” assets such as asphalt pathways, irrigation systems, and playground equipment. Sustainable funding through an appropriate asset management approach is recommended.

Opportunities to improve efficiency/effectiveness and Address Challenges:

Park Design, Development and Maintenance Standards

Current Process and Procedure

- Park designs have become more elaborate and complex. A complete review of the standards for park design, landscape design and maintenance is needed to ensure we are not over-designing our parks, leading to extensive and excessive maintenance. All relevant stakeholders must be involved in the review.
- Our timing of new park development needs to be coordinated with residential construction to reduce or eliminate irrigation, turf, etc. being damaged once installed. Coordinating park development with residential construction will reduce these costs and allow for staff time to be reallocated to other maintenance activities.
- New staff and developers should be provided all relevant standards and guidelines to increase understanding of costs associated with complex and/or detailed parks designs and allow for value-based decision making if a design exceeds the standards.

Changes for More Efficient and Effective Service

- Detailed and elaborate park designs result in increased maintenance costs.
- Optimization Teams established on new developments will ensure the impact of design on long term maintenance is considered at the planning stage and reflected in the operating budget. Subject to stakeholder review, the implementation of a parks design process that approximately recognizes maintenance cost, could reasonably reduce future costs by 5% or \$750/ha, on a current base cost of \$15,000/ha.
- Since the service review, the process for allowing residents, contractors, and utility agencies to enter developed parks has been enhanced. This has resulted in reduced park damage and improved cost recovery, estimated at \$50,000 per year.

Drainage Issues

Current Process and Procedure

- Residents with houses backing parks and open spaces often drain sump pumps directly into the park or open space increasing drainage issues and negatively impacting ability of Parks to maintain the area.
- Increased drainage results in areas that must be maintained manually by hand mowers and line trimmers as opposed to the more efficient mowing equipment.

Changes for More Efficient and Effective Service

- The Community Standards Division will be engaged to discuss enforcement options for issues related to bylaws; specifically Bylaws 8175 and 8379.
- An improved contingency process needs to be included in park development projects in order to re-establish proper drainage patterns in select cases.
- Linear parks are popular with residents and developers. However, due to configuration and on-going drainage issues, maintenance costs are at least 10% higher in these areas. Stakeholder engagement is required to review the future use of linear parks in new neighbourhoods.

Customer Service

Note: Parks is one of the priority areas for a review for Service Saskatoon – 311 following Public Works.

Current Process and Procedure

- There are a number of citizen calls and inquiries about the design and maintenance of parks and open spaces. These calls and requests for service are being addressed by multiple staff, resulting in duplication of effort.

Changes for More Efficient and Effective Service

- Streamline the inquiry/concern process through a single point of contact, such as a Customer Service Coordinator, in order to reduce duplication of effort and improve consistency in response to citizen and Councillor inquiries and concerns.

- Streamlining this process would reduce the amount of time spent by the Parks Director and Superintendents, allowing their time to be redirected to other management priorities such as schedule and program optimization. Cost savings are estimated at \$20,000 per year by deploying a dedicated customer service representative.
- Ultimately, the process will transition to the 311 system.

Reducing Downtime and Improving Productivity

Current Process and Procedure

- Aging equipment results in breakdowns and operational down time.
- Parks Satellite Maintenance Facilities and Locations - as Saskatoon continues to grow and the footprint of the city expands, crews are traveling greater distances from the main Parks yards on Avenue P, Nutana-Kiwanis, Umea Park and the Forestry Farm, to the parks they maintain on a daily basis. This increased travel has resulted in an increase in safety concerns as staff are travelling on major roadways with equipment and increasing wear and tear on equipment that was meant to be used in parks and open spaces, not on roadways. Travel time to remote locations was also identified as a barrier to productivity, resulting in significant downtime. Existing storage yards are also full to capacity.
- The use of one-ton trucks in parks also results in damage to pathways, irrigation lines, turf, etc.

Changes for More Efficient and Effective Service

- Satellite maintenance yards are being established in remote park locations to ensure that an appropriate-sized facility is within reasonable access. This change will have an immediate positive impact on the safety of staff as they are no longer travelling with slow moving equipment on busy roadways. Downtime due to travel will also be reduced, saving an estimated 5% in labour costs, or about \$750/ha per year.
- Conduct a CSR with Fleet Services and Parks as a stakeholder to address issues with aging equipment and preventative maintenance. This CSR is underway and the results will be reported to Committee near the end of 2015.

Staffing and Scheduling of Work

Current Process and Procedure

- Specialized crews work on a specific component of the park, such as mowing, pruning trees, weeding plant beds, etc. Crews are scheduled to complete their area of maintenance and then move on to the next park. This requires a significant amount of travel time for each specialized crew.
- When the growing conditions are excellent during the middle of summer, there is overgrowth of grass and weeds that are not being maintained to the level that citizens expect.
- New seasonal employees are hired at the end of May and require training, reducing possible productivity levels until staff are comfortable with the equipment and the maintenance requirements.

Changes for More Efficient and Effective Service

- Horticulture and Turf crews formed multi-disciplinary crews which increased efficiency. For the 2015 season, two crews piloted a model where horticultural and turf maintenance staff formed one crew that works together to maintain an entire park as opposed to each arriving in the park on varying days or at varying times as per previous maintenance schedules. Savings in travel time is estimated to reduce labour cost by 5% or \$750/ha per year.
 - Furthermore, the advantage of the combined crew model is the crews becoming experts in the specific maintenance requirements of a particular park and an increase in ownership within the crews. Residents become familiar with the day in which the crew is expected to arrive in their park and potentially get to know the staff. This model also reduces confusion as to why there may be 3 or 4 different Parks Division vehicles in a park on any given day.
- Double shift or contract out certain maintenance activities in busy months when weather patterns may result in a need for increased frequency of maintenance activities such as mowing or trimming in order to reduce complaints related to overgrowth of grass or weeds.
- Bring new Parks staff on two weeks earlier to ensure they are trained and ready to go when the busy season starts.

Special Events

Note: There is a detailed Service Review currently underway for Special Events and the results of this review will be reported to Committee.

Current Process and Procedure

- There has been a significant increase in the number of special events held in parks and open spaces.
- If an event causes damage to the park, there is little accountability for the event organizer and no incentive to minimize the potential for damage.

Changes for More Efficient and Effective Service

- Review the impact of special events on parks and open spaces. An increase in the number of special events increases the potential for damage to turf, irrigation lines and trees, results in increased garbage collection and requires Parks resources pre and post event.
- Consideration should be given to introducing a damage deposit for events as well as restricting the use of certain parks for some events to minimize potential for damage.
- Proactively design and construct event sites with appropriate infrastructure.

The opportunities outlined above continue to ensure a wide variety of park types and designs, are maintained and preserved by the Parks Division so residents, as well as visitors to Saskatoon can enjoy the parks today and for years to come. A new event site is planned to be established in Kinsmen Park in 2016. Other event site opportunities are being explored. Future event sites, like Kinsmen Park, will be

designed with appropriate infrastructure to significantly reduce park repair costs which are typically \$3,000 to \$5,000 per incident.

Next Steps

Parks Division staff are identifying actions required to implement the suggested improvements. Consideration is given to benefit citizens, cost of implementation and potential efficiencies to be gained following implementation. Future reports will be brought to the SPC on Planning, Development and Community Services regarding service levels and options for consideration for the remainder of Parks services.

Summary of Recommendations:

Recommendation	Estimated Savings
1. Reduce future maintenance costs through appropriate park design review (subject to stakeholder consultation).	\$750/ha per year in new park development areas
2. Enhanced process for allowing contractor/utility vehicles to enter developed parks, to improve service, reduce damage, and improve cost recovery.	\$50,000 per year
3. Streamline the inquiry/concern process to a single point of contact, such as a customer service coordinator, to reduce redundancy and improve service to citizens.	\$20,000 per year.
4. Establish satellite maintenance yards in new development areas, reducing travel time, improving safety, and providing needed storage space.	\$750/ha per year in new park development areas
5. Utilize a “combined crew model” where one integrated team takes ownership of designated park areas, resulting in improved quality and reduced labour cost due to reduced travel.	\$750/ha per year
6. Provide improved site infrastructure in new festival sites to reduce park damage.	\$3,000 to \$5,000 per incident
7. Introduce a systematic asset management approach for ongoing maintenance of “hard” park assets.	Overall benefit is consistent funding for approved service levels.

Parks Civic Service Review: Park Pathway Snow Clearing

Description: Pathway Snow Clearing

Includes all district and neighbourhood park pathways, and sidewalks adjacent to parks, that have been designated for snow clearing services.

Target Service Level: Cleared within 48 hours of the completion of a snowfall or drifting event of 2.5 cm or greater

Description	Area (Km)	Cost per Kilometer		Total Cost		Total cost
		Clearing	Pathway Maintenance	Clearing	Pathway Maintenance	
Facilities that are cleared:						
Park Pathways:						
Lighted Asphalt	56	\$ 1,200	\$ -	\$ 67,000	\$ -	\$ 67,000
Non Lighted Asphalt	20	1,200	-	24,000	-	24,000
Sidewalks Adjacent to Parks	26	1,200	-	31,000	-	31,000
Total Kilometers Cleared	102			\$ 122,000	\$ -	\$ 122,000
Facilities not cleared:						
Pathways: Crusher Dust	32	\$ -	\$ -	\$ -	\$ -	
Non Lighted Asphalt	33					
Total Inventory	167					
Existing Budget						\$ 122,000
Funding Gap*						\$ -

*Budget allocation is not sufficient to absolutely (100%) meet a 48 hour service level but is reasonable for current inventory.

Issues/Challenges

- There are approximately 141 km of park pathways within City parks. Parks Division currently clears snow from the following pathway and sidewalk areas:
 - 100% or 56 km of lighted asphalt pathways;
 - 38% or 20 km of non-lighted asphalt pathways; and
 - 26 km of sidewalks adjacent to parks.
 - Pathway clearing priorities are based on the asphalt pathways with the highest pedestrian activity established over time.
- Current service level (48 hour clearing) is met about 75% of the time. Work management tools continue to be implemented so that performance and cost can be accurately captured.
- 33 kilometers of non-lighted asphalt pathways and 32 kilometers of crusher dust park pathways are not currently cleared.
- 14 kilometers of crusher dust pathway inventory is located within Naturalized Parks including: Donna Birkmaier Park, Hyde Park, Lakewood Park, Heritage Park, and Mark Thompson Park.
- Remaining 18 kilometers of crusher dust pathway inventory includes secondary pathways spread through many parks where primary lighted and some non-lighted asphalt pathways are cleared and allow good access through the parks during the winter season, in the absence of cleared crusher dust pathways.

- Crusher dust pathways are typically constructed within naturalized parks to minimize runoff potential and account for an average 93% of the pathway system within naturalized parks.
- Crusher dust pathways located within non-naturalized parks are typically smaller sections of secondary pathways that do not serve as the main access through the park.
- Due to the lack of pathway snow clearing, accessibility can be limited within naturalized parks during winter seasons.
- Accessibility within non-naturalized parks is good with all lighted main asphalt and some non-lighted asphalt pathways being cleared.
- Conditions that can sometimes cause a delay in meeting clearing service level include:
 - Equipment breakdown;
 - Heavy snowfall or drifting event resulting in 10 cm or greater accumulation;
 - Snow accumulation along edges of pathway greater than 45 cm; and
 - Successive snowfall or drifting events that interrupt clearing progress, forcing operators to start clearing high priority routes prior to completing entire pathway inventory. For example, during periods of heavy snowfall, resources are reallocated in an effort to prioritize the clearing of sidewalks adjacent to parks (26 km) so that the 48 hour Snow Clearing Bylaw is met.
- 76 km of park pathway is cleared as quickly as possible, with a target of completion in 48 hours in accordance with the Sidewalk Snow Clearing Bylaw.
- Park pathway inventory in new neighbourhoods are prone to drifting and often require additional clearing efforts, like special equipment and related labour.
- Crusher dust surfaces, where snow has been cleared, often require substantial repair to the surface at the end of winter operations. This represents a significant repair cost above and beyond the costs of snow removal and regular maintenance.
- Annual clearing costs are seasonably variable, and depend on the total amount of snowfall that occurs during a winter season.
- Additional inventory due to City growth is not accounted for in this review.

Optional Service Levels: Pathway Snow Clearing

Total Inventory Cleared	Optional Service level	Estimated Total Cost	Budget Allocation	Gap
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Recommendation - Current Service Level plus **14 km** of crusher dust pathways in Naturalized Parks

Description	Area (Km)	Cost per Kilometer		Total Cost		Total cost
		Clearing	Pathway Maintenance	Clearing	Pathway Maintenance	
Park Pathways:						
Lighted Asphalt	56	\$ 1,200	\$ -	\$ 67,000	0	\$ 67,000
Non Lighted Asphalt	20	1,200	-	24,000	0	\$ 24,000
Crusher Dust	14	1,200	660	17,000	9,000	\$ 26,000
Sidewalks Adjacent to Parks	26	1,200	-	31,000	0	\$ 31,000
	116			\$ 139,000	\$ 9,000	\$ 148,000
Existing Budget						\$ 122,000
Funding Gap						\$ 26,000

Option 1: Maintain Current Service Level

Description	Area (Km)	Cost per Kilometer		Total Cost		Total cost
		Clearing	Pathway Maintenance	Clearing	Pathway Maintenance	
Park Pathways:						
Lighted Asphalt	56	\$ 1,200	\$ -	\$ 67,000	\$ -	\$ 67,000
Non Lighted Asphalt	20	1,200	-	24,000	-	\$ 24,000
Sidewalks Adjacent to Parks	26	1,200	-	31,000	-	\$ 31,000
	102			\$ 122,000	\$ -	\$ 122,000
Existing Budget						\$ 122,000
Funding Gap*						\$ -

*Budget allocation is not sufficient to consistently (100% of the time) meet a 48 hour service level but is reasonable for current inventory

Option 2: Current Service Level plus full inventory (32 km) of crusher dust pathway, and full inventory (33) of non lighted asphalt pathway

Description	Area (Km)	Cost per Kilometer		Total Cost		Total cost
		Clearing	Pathway Maintenance	Clearing	Pathway Maintenance	
Park Pathways:						
Lighted Asphalt	56	\$ 1,200	\$ -	\$ 67,000	\$ -	\$ 67,000
Non Lighted Asphalt	53	1,200	-	64,000	-	\$ 64,000
Crusher Dust	32	1,200	660	38,000	21,000	\$ 59,000
Sidewalks Adjacent to Parks	26	1,200	-	31,000	-	\$ 31,000
	167			\$ 200,000	\$ 21,000	\$ 221,000
Existing Budget						\$ 122,000
Funding Gap						\$ 99,000

Parks Civic Service Review: Shelterbelt and Buffer Strip Maintenance

Description: Shelterbelts and Buffer Strips

- **Shelterbelt** – a row of trees or larger shrubs planted to separate incompatible uses or to protect an area from noise or strong winds.
- **Buffer strips** – a parcel of land used to separate incompatible land uses, through the use of landscaping, open space, or other features.
- **A drip line** is the area defined by the outmost circumference of a tree canopy.
- Buffer strips containing shelterbelts are typically found along major arterials and freeways.

Current Service Level: buffer strips adjacent to shelterbelts mowed up to the shelterbelt drip line twice per season; garbage pick up to the drip line twice per year; 1:13 year pruning cycle; refuse pick up from roadway shelterbelt on complaint basis. No turf maintenance is done within the 12 meter width of shelterbelt.

Description	Area (Ha)	Mowing, Trimming, Garbage		Pruning	Reactive Clean up Response	Total Costs
		Per Ha Cost	Total			
Roadway Shelterbelts	37	\$ -	\$ -			
Buffer adjacent to Roadway Shelterbelts	96	770	74,000			
	133		\$ 74,000	\$ 50,000	\$ 15,000	\$ 139,000
Existing Budget						124,000
Funding Gap*						\$ 15,000

*\$15,000 required for reactive refuse pick up does not have budget allocated.

Issues/Challenges

- Roadway shelterbelt maintenance is a service level that was discontinued in 2012 due to resource limitations.
- All reactive responses are currently being absorbed by operating resources and are considered to be a significant pressure point on operating budgets.
- Many shelterbelt areas have had sound walls constructed in close proximity, making access to the areas difficult and decreasing sight lines, creating safety and CPTED issues.
- Some sound walls have been installed at the top or bottom of steep slopes reducing safe turn around areas for equipment including the risk of equipment roll over. These areas are now classified “no mow” areas.
- Lack of maintenance in shelterbelts results in citizen dissatisfaction as dumping, refuse build up and occurrences of rough camping continue in these unkept areas. These issues can have a negative impact on property perceptions as well as reducing the quality of life for those who utilize these areas for passive recreation opportunities.
- Labour for roadway shelterbelt maintenance has been reallocated over the last 5 years in an effort to maintain mowing and horticultural service levels throughout the growing park and open space inventory.
- Previous service level including mowing under shelterbelts, basic tree pruning, and picking up garbage twice per year on a dedicated rotation.
- The current service level is very reactive. When complaints are received about a service, such as mowing and garbage pick-up in shelterbelts, we are forced to reduce service elsewhere.

Optional Service Levels: Shelterbelt and Buffer Strip Maintenance

Total Inventory Cleared	Optional Service level	Estimated Total Cost	Budget Allocation	Gap
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Recommendation – Increase service level to include roadway shelterbelt mowing and trimming twice per season and two garbage pick-ups per season; 1:13 year pruning cycle.

Description	Area (Ha)	Mowing, Trimming, Garbage		Pruning	Total Costs
		Per Ha Cost	Total		
Roadway Shelterbelts	37	\$ 1,200	\$ 44,000		
Buffer adjacent to Roadway Shelterbelts	96	770	74,000		
	133		\$ 118,000	\$ 50,000	\$ 168,000
Existing Budget					124,000
Funding Gap					\$ 44,000

Option 1: Maintain Current Service Level

Buffer Strips adjacent to shelterbelts mowed up to the shelterbelt drip line twice per season; garbage pick up to the drip line twice per year; 1:13 year pruning cycle; refuse pick up from roadway shelterbelt on complaint basis. No turf maintenance is done within the 12 meter width of shelterbelt.

Description	Area (Ha)	Mowing, Trimming, Garbage		Pruning	Reactive Clean up Response	Total Costs
		Per Ha Cost	Total			
Roadway Shelterbelts	37	\$ -	\$ -			
Buffer adjacent to Roadway Shelterbelts	96	770	74,000			
	133		\$ 74,000	\$ 50,000	\$ 15,000	\$ 139,000
Existing Budget						124,000
Funding Gap						\$ 15,000

*\$15,000 required for reactive refuse pick up does not have budget allocated.



STANDING POLICY COMMITTEE ON ENVIRONMENT, UTILITIES & CORPORATE SERVICES

Xylem Water Solutions – Supply of Proprietary Parts – Blanket Purchase Order

Recommendation of the Committee

1. That the Administration prepare a blanket purchase order with Xylem Water Solutions for the supply of Flygt equipment for up to five years, for a maximum total cost of \$750,000 (including taxes); and
2. That Purchasing Services issue the appropriate blanket purchase order.

History

At the October 13, 2015 Standing Policy Committee on Environment, Utilities & Corporate Services meeting, a report of the General Manager, Transportation & Utilities Department dated October 13, 2015 was considered.

Attachment

October 13, 2015 Report of the General Manager, Transportation & Utilities.

Xylem Water Solutions – Supply of Proprietary Parts – Blanket Purchase Order

Recommendation

That the Standing Policy Committee on Environment, Utilities and Corporate Services recommend to City Council:

1. That the Administration prepare a blanket purchase order with Xylem Water Solutions for the supply of Flygt equipment for up to five years, for a maximum total cost of \$750,000 (including taxes); and
2. That Purchasing Services issue the appropriate blanket purchase order.

Topic and Purpose

The purpose of this report is to request that City Council approve a blanket purchase order from Xylem Water Solutions (Xylem), a division of Xylem Canada Company, for the supply of proprietary parts, exclusive to Flygt equipment, for the Wastewater Treatment Plant (WWTP).

Report Highlights

1. A blanket purchase order is recommended to improve maintenance efficiency for the WWTP.
2. Xylem is the exclusive distributor that can supply Flygt equipment and maintenance parts for the WWTP.
3. A multi-year blanket purchase order with Xylem is recommended to streamline the procurement process.

Strategic Goals

This report supports the Strategic Goals of Asset and Financial Sustainability, and Continuous Improvement. These goals are met by improving maintenance efficiency through the standardization of equipment. It also simplifies the procurement process by establishing multi-year blanket orders for standard equipment.

Background

The WWTP provides sewage treatment to the citizens of Saskatoon. This is accomplished by operating the plant and 28 lift stations. The plant and lift stations utilize Flygt pumps, mixers, and proprietary electrical and instrumentation equipment.

Report

Improve Maintenance Efficiency

Regular maintenance and repairs of plant equipment is performed by WWTP personnel. This is done by rebuilding equipment using rebuild kits and by purchasing the pertinent parts for repair. An inventory of critical parts is maintained for emergency repairs to mitigate risks associated with equipment failure and to improve maintenance efficiency. Attachment 1 lists the pumps, mixers, electrical and instrumentation equipment

(Mactechs), and repair kits that are used at the WWTP and lift stations. Required maintenance parts, rebuild kits, and new equipment is forecasted to cost \$150,000 annually.

Xylem Water Solutions

The Flygt brand is currently owned by Xylem, making them the only authorized distributor of Flygt pumps, mixers, rebuild kits, and associated electrical and instrumentation equipment. Attachment 2 is a letter from Xylem confirming this.

Multi-year Blanket Purchase Order

Blanket purchase orders allow for more effective purchasing by the WWTP personnel by reducing administration time in obtaining sole source approvals and by allowing the City to take advantage of bulk pricing discounts. With a commitment for orders up to \$150,000 per year, a blanket purchase order for Flygt equipment for the WWTP will have higher purchasing power and will result in better unit prices for the City.

Options to the Recommendation

The supply of pumps, mixers, and pump rebuild kits could be individually sole sourced to Xylem.

Financial Implications

Funds are available in the Maintenance Program of the WWTP 2015 (and future) Operating Budgets.

Other Considerations/Implications

There are no public and/or stakeholder involvement, communication, policy, environmental, privacy, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

This blanket purchase order should be in place as soon as possible, provided the supplier provides acceptable pricing and maintains the status as the sole supplier of Flygt equipment and maintenance parts.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Attachments

1. List of Pumps, Mixers, Repair Kits, and Mactechs
2. Letter – Xylem Water Solutions - Dated August 24, 2015

Report Approval

Written by: Leslie Squire, Engineering Intern, Wastewater Treatment Plant
Reviewed by: Amanda Conway, Maintenance Engineer, Wastewater Treatment Plant
Reviewed by: Tim Bushman, Plant Manager, Wastewater Treatment Plant

Xylem Water Solutions – Supply of Proprietary Parts – Blanket Purchase Order

Reviewed by: Reid Corbett, Director of Saskatoon Water
Approved by: Jeff Jorgenson, General Manager, Transportation & Utilities
Department

EUCS LS – BPO - Xylem Water Solutions – Supply of Proprietary Parts.docx

WWTP Pumps, Mixers, and Repair Kits

WWTP Pumps		
Pump Model	Quantity	Repair Kit
C-3085-171	2	6018947
3085-181	13	6018947
3085-183	3	6018948
C-3102-180	7	6018906
3102-185	2	6018906
3127-180	2	6018909
3152-181	7	6018921
3153-181	1	6571708
3201-180	2	6018932
3300-180	1	6018924
3501-805	5	N/A
7061-705	4	N/A
4670-410	10	6340106
4680-410	8	6340106
N-3102-181	1	
3067-180	4	
2201-320		
A-C150	2	
Type NSW Model 100 and 150	1	

WWTP Mixers		
Mixer Model	Quantity	Repair Kit
4410-011	20	7263800
4650-410	8	6340103
4660-412	2	6656804
4660-410	4	

Lift Station Pumps and Mactechs

Lift Station Pumps		
Lift Station	Pump Model	Quantity
14th Street	CT 3201.180 MT	2
58th Street	CT 3300.181 MT	3
Agriplace	Submersible CT 3152.120 HT	2
Avenue P	3085.091	2
Bandstand	CT 3300	2
Blairmore	NT 3171.181 MT	2
Borden	NT 3171.181 MT	2
	CT 3231.665	1
	CT 3127.181 HT	1
Briarwood	CT 3152.181 MT	2
Diefenbaker	NT 3102	1
Dundonald Green	CT 3102	2
Fletcher Road (New)	NT 3153.185 MT	2
Hampton Village	NT 3202.180 HT	2
Jasper Avenue (Old)	CT 3201	2
Jasper Avenue (New)	NT 3202.180 MT	2
	NT 3301.180 MT	1
Kingsmere and Boychuk	CT 3127.180 MT	2
Lakeridge	CT 3127.180 MT	2
Lakewood	NT 3202.180 MT	2
Landfill	NP 3153.180 HT	2
Landfill South	CP 3102.180 HT	2
Landfill Heavy Grit	CP 3102.181 MT	2
McKinnon Avenue	CT 3085	2
Powerhouse	CT 3300 MT	2
Rotary Park	NT 3301.180 MT	4
Spadina Crescent	CT 3530	4
Stonebridge Sanitary	NT 3202.180 HT	3
Stonebridge Storm	NT 3127.181 MT	1
	NT 3400.735 MT	2
Storm Lift	PL 7050.680	3
	CS 3102	1

Mactechs	
Station	Control Type
Stonebridge	APP 721
Borden Place	APP 721
Rotary Park	APP 741
Jasper	APP 741
Lakewood	FMC 200
Landfill	FMC 200
Landfill	APP 521
Melville	APP 521
Hampton Village	FMC 300
Fletcher	APP 721
Blairmore	FMC 200
58th Street	APP 741

Xylem
Bay 10, 3111 Millar Avenue, Saskatoon, SK S7K 6N3
Tel (306) 933-4849 Fax (306) 931-0051



August 24, 2015

City of Saskatoon
470 Whiteswan Drive
Saskatoon, Sask. S7K 6Z7

Attention Leslie Squire,

Xylem Water Solutions, a division of Xylem Canada Company is the manufacturer and sole supplier of Flygt products.

If you should have any questions concerning this matter, please feel free to contact us at 306-933-4849.

Sincerely,

Don Ball
Sales Representative
Xylem Water Solutions

SN:cg



EXECUTIVE COMMITTEE

The Marr Residence Management Board – Request to Increase Membership on Board

Recommendation of the Committee

That the terms of Reference of the Marr Residence Management Board be amended to provide an increase in the composition of the number of members appointed from the general public from three to four.

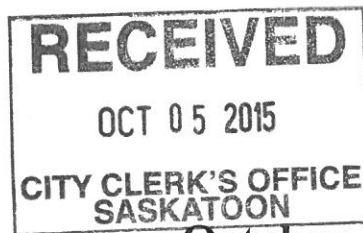
History

At its meeting held on October 19, 2015, Executive Committee considered a request of the Marr Residence Management Board to increase the number of members-at-large on the Marr Management Board from three to four.

Executive Committee supports the requested increase in members appointed from the general public from three to four, and it has also instructed the Administration to report further on whether one additional appointee is sufficient or whether it should be two.

Attachment

1. Letter dated October 5, 2015



225-52

October 5, 2015

**Your Worship Mayor Atkinson
and Saskatoon City Council,**

**The Marr Residence Management Board is requesting
Saskatoon City Council approve the increase in the
number of Members-at-Large on the Marr Management
Board from three to four.**

**This request is based on the staffing needs for when the
Marr Residence is open. During the summer a board
member is always needed to open and be responsible for
the house. Having one more board member available
would lessen the load that the other board members
currently take on.**

Thank you for considering this request.

**Della Greer
Board Chair
Marr Management Board**

Boychuk Drive and Highway 16 Interchange Funding Application

Recommendation

That it be resolved that City Council supports the application for a Provincial-Territorial Infrastructure Component grant for Highways and Major Road infrastructure, for the Boychuk and Highway 16 Interchange, from the New Building Canada Fund, and City Council agrees to meet legislated standards, to meet the terms and conditions of the Provincial-Territorial Infrastructure Component (PTIC) program, to conduct an open tendering process, to manage the construction of the project, to fund the municipal share of the construction cost, to fund ongoing operation and maintenance costs, and to follow any mitigation measures as required by the *Canadian Environmental Assessment Act* and *The Environmental Assessment Act (Saskatchewan)*.

Topic and Purpose

The purpose of this report is to provide a resolution that satisfies the requirements of the application for funding under the New Building Canada Fund – PTIC program for the Boychuk Drive and Highway 16 Interchange.

Report Highlights

1. A specific resolution from City Council is required for the PTIC application.

Strategic Goal

This report supports the long-term strategy of optimizing the flow of people and goods in and around the city under the Strategic Goal of Moving Around.

Background

In May 2015 the Province of Saskatchewan indicated that the City of Saskatoon (City) would be allocated approximately \$70M (\$35M each from the Government of Canada and the Province of Saskatchewan) from the PTIC program.

At its meeting on June 22, 2015, City Council resolved that the funding strategy for the interchange at Boychuk Drive and Highway 16 be approved and that the Administration proceed with making application under the New Building Canada Fund for the Boychuk Drive and Highway 16 Interchange project.

Report

On June 24, 2015, the application for PTIC funding for the Boychuk Drive and Highway 16 Interchange project was submitted for review and approval. As part of the review and approval process, the senior levels of government have requested that City Council provide a specifically worded resolution for the project. The resolution, as written in the recommendation, will satisfy the requirements of the application and will allow the approval process to continue.

Boychuk Drive and Highway 16 Interchange Funding Application

Once approved by the senior levels of government, the City will receive a letter confirming formal approval or approval-in-principle. Upon receipt of this letter, the City can proceed to award the project through the regular tendering process. The formal approval or the approval-in-principle date will be delayed until after the federal election.

Financial Implications

The interchange at Boychuk Drive and Highway 16 is estimated to cost \$45.15M of which \$29.73M has been applied for under the PTIC program. The remaining funding will come from the Interchange Levy and the reallocation of the special levy collected for the Rosewood flyover project that has been cancelled.

Other Considerations/Implications

There are no options to this recommendation, and there are no environmental, privacy, or CPTED implications or considerations. Neither public and/or stakeholder involvement nor a communication plan is required.

Due Date for Follow-up and/or Project Completion

A further report will be presented to award the contract for the Boychuk Drive and Highway 16 Interchange.

The allocation of the remaining \$40M in PTIC funding will be brought forward in the near future for City Council's consideration and approval for priority projects.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Report Approval

Written by: Kari Smith, Manager of Financial Planning
Reviewed by: Clae Hack, Director of Finance
Approved by: Kerry Tarasoff, CFO/General Manager, Asset & Financial Management Department

Boychuk_Hwy 16_Res Requirement.docx

Snowmobile Bylaw Amendment

Recommendation

That City Council consider Bylaw No. 9323, *The Snowmobile Amendment Bylaw, 2015*.

Topic and Purpose

This report provides City Council with Bylaw No. 9323, *The Snowmobile Amendment Bylaw, 2015*. If passed, this Bylaw would implement City Council's decision to amend Bylaw No. 7983, *The Snowmobile Bylaw, 2000*, to replace the existing snowmobile route with three new snowmobile routes.

Report

At its meeting held on February 23, 2015, City Council considered a recommendation contained in the report of the Standing Policy Committee on Transportation that designated snowmobile routes within city limits be updated.

City Council resolved that the designated snowmobile routes within city limits be updated and that the City Solicitor prepare necessary amendments to Bylaw No. 7983.

In accordance with City Council's instructions, we are pleased to submit proposed Bylaw No. 9323, *The Snowmobile Amendment Bylaw, 2015*, for City Council's consideration.

Attachment

1. Proposed Bylaw No. 9323, *The Snowmobile Amendment Bylaw, 2015*.

Report Approval

Written by: Jon Danyliw, Solicitor

Approved by: Patricia Warwick, City Solicitor

Admin Report – Snowmobile Bylaw Amendment.docx
224-0154-jwd-1.docx

BYLAW NO. 9323

The Snowmobile Amendment Bylaw 2015

The Council of The City of Saskatoon enacts:

Short Title

1. This Bylaw may be cited as *The Snowmobile Amendment Bylaw 2015*.

Purpose

2. The purpose of this Bylaw is to amend *The Snowmobile Bylaw, 2000*, to remove the existing snowmobile route and designate new routes within the City of Saskatoon on which the operation of snowmobiles is authorized.

Bylaw No. 7983 Amended

3. Bylaw No. 7983 is amended in the manner set forth in this Bylaw.

Schedule "A" Amended

4. Schedule "A" is repealed and the schedule marked as Schedule "A" to this Bylaw is substituted.

Coming into Force

5. This Bylaw comes into force on the day of its final passing.

Read a first time this _____ day of _____, 2015.

Read a second time this _____ day of _____, 2015.

Read a third time and passed this _____ day of _____, 2015.

Mayor

City Clerk

Schedule "A" to Bylaw No. 9323

Schedule "A"

The following highways and portions of highways, as shown on the attached diagrams, are designated for operation of snowmobiles:

North Route:

1. Along the east ditch of Highway #11 from Wanuskewin Road to Marquis Drive.
2. Along the east ditch of Idylwyld Service Road from 71st Street to Marquis Drive.

East Route:

1. Along the north and south ditch of Fleury Road from Blackley Road to Highway #41.
2. Along the east and west ditch of Blackley Road from Fleury Road to Highway #41.
3. Along north and south ditch of Highway #41 from Fleury Road to Highway #5.
4. Along the east and west ditch of Llewellyn Road from Fleury Road to Highway #41.
5. Along the north and south ditch of Highway #5 from Highway #41 to Llewellyn Road.

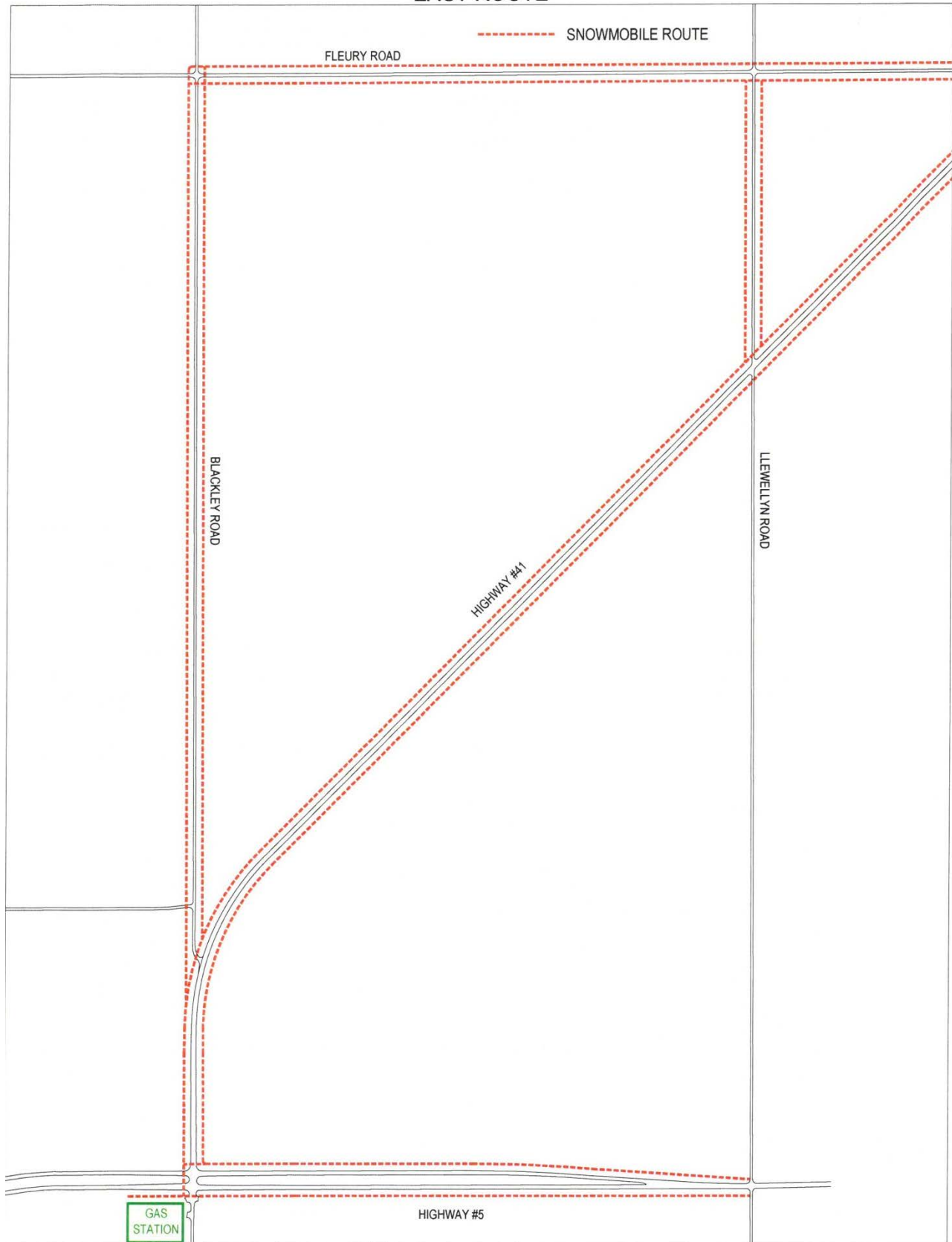
West Route:

1. Along the north and south ditch of Township Road 364 from Range Road 3065 to Highway #7.
2. Along the north and south ditch of 11th Street from Highway #7 to 4402 - 11th Street West.

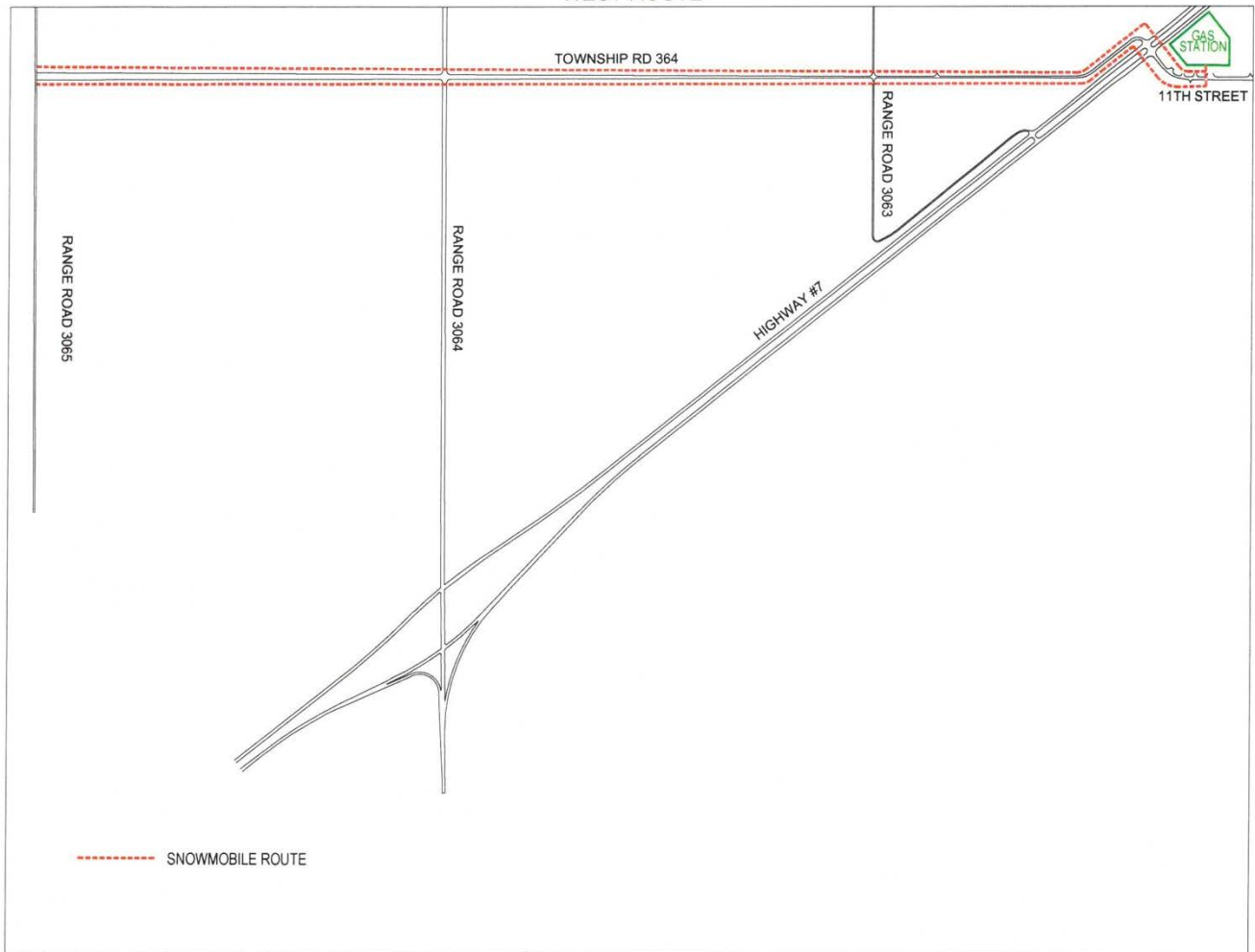
NORTH ROUTE



EAST ROUTE



WEST ROUTE



Farm Land Fixed Taxation Agreement Extension

Recommendation

That City Council consider Bylaw No. 9322, *The Brian Robert Sutherland Farm Land Fixed Rate of Taxation Renewal Bylaw, 2015*.

Topic and Purpose

The purpose of this Report is to receive City Council's approval to extend the Farm Land Fixed Taxation Rate Agreement with Mr. Brian Sutherland.

Report

City Council at its meeting on September 28, 2015, resolved to renew its agreement with Mr. Sutherland providing for a fixed rate of taxation on certain farm land owned by Mr. Sutherland in the City of Saskatoon. The original agreement covered the period 2005 to 2009 inclusive. Renewal of the original agreement covered the period 2010 to 2014 inclusive. Council resolved to renew the agreement for a further period of five years commencing in the 2015 taxation year.

Farm land taxation agreements must be authorized by bylaw. Similarly, a renewal of any such agreement must also be authorized by bylaw.

We are pleased to enclose for Council's consideration Bylaw No. 9322, *The Brian Robert Sutherland Farm Land Fixed Rate of Taxation Renewal Bylaw, 2015*. The Bylaw authorizes His Worship the Mayor and the City Clerk to sign the renewal agreement appended as Schedule "A" to the Bylaw. The Agreement renews the original agreement with Mr. Sutherland for a further period of five years commencing in 2015. As a result, Mr. Sutherland's property will be taxed at a fixed rate, so that the tax levy after applying the fixed rate of taxation will be equivalent to the taxes that would be payable on the property if the dwelling and other improvements used exclusively in connection with the agricultural operation were exempt from taxation on the terms and conditions similar to section 293 of *The Municipalities Act*.

Attachment

1. Proposed Bylaw No. 9322, *The Brian Robert Sutherland Farm Land Fixed Rate of Taxation Renewal Bylaw, 2015*.

Report Approval

Written by: Christine G. Bogad, Director of Administrative Law
Approved by: Patricia Warwick, City Solicitor

Admin Report – Farm Land Fixed Taxation Agreement Extension.docx
227-2008-cgb-3.docx

ROUTING: City Solicitor – City Council
October 26, 2015 – File No. CK 1920-1
Page 1 of 1

DELEGATION: P. Warwick

cc: City Manager, CFO/GM Asset & Financial Management,
Manager of Commercial Valuation

BYLAW NO. 9322

The Brian Robert Sutherland Farm Land Fixed Rate of Taxation Renewal Bylaw, 2015

Whereas under the provisions of subsection 168(1) of *The Cities Act*, S.S. 2002, c. C-11.1, if, within a city, there is land used exclusively for farming purposes, and a person whose principal occupation is farming is assessed with respect to the land, the council may enter into an agreement with the owner of that land providing for a fixed rate of taxation on the assessed value of the property;

Whereas The City of Saskatoon entered into such an agreement with Brian Robert Sutherland on or about June 12, 2006;

Whereas the said agreement was authorized by Bylaw No. 8498 of The City of Saskatoon passed on May 23, 2006;

Whereas under the provisions of subsection 168(3) of *The Cities Act*, the said agreement may be renewed from time to time for periods not exceeding five years each;

Whereas the parties entered into a Renewal Agreement on October 28, 2010 for a period of five years expiring on December 31, 2014;

Whereas the Renewal Agreement was authorized by Bylaw No. 8883 of The City of Saskatoon passed on September 13, 2010;

Whereas Brian Robert Sutherland has requested that the said agreement be renewed for a further period of five years upon the same terms and conditions as set out in the said agreement;

Whereas the Council of The City of Saskatoon at its meeting on September 28, 2015 resolved to renew the said agreement for a further period of five years;

Whereas no renewal agreement is to be entered into unless it is authorized by bylaw;

Now Therefore, the Council of The City of Saskatoon enacts:

Short Title

1. This Bylaw may be cited as *The Brian Robert Sutherland Farm Land Fixed Rate of Taxation Renewal Bylaw, 2015*.

Purpose

- 2. The purpose of this Bylaw is to authorize the City to renew the agreement with Brian Robert Sutherland providing for a fixed rate of taxation on the assessed value of certain property owned by the said Brian Robert Sutherland in the City of Saskatoon.

Fixed Rate of Taxation

- 3. The property hereinafter described, being used exclusively for farming purposes and owned by a person whose principal occupation is farming, shall be subject to the fixed rate of taxation described in the Renewal Agreement appended as Schedule "A" to this Bylaw, for a period of five years beginning in the 2015 taxation year. The property to which this Bylaw applies is described as:

Parcel Number: 117153132
 Title Number: 110416878
 Legal Land Description: SE Sec 05 Twp 37 Rge 06 W3 Extension 0
 As described on Certificate of Title 81S03100.

Agreement Forms Part of Bylaw

- 4. The Renewal Agreement between Brian Robert Sutherland and The City of Saskatoon appended as Schedule "A" to this Bylaw shall form part of this Bylaw.

Execution of Agreement

- 5. The Mayor and City Clerk are hereby authorized to sign the Renewal Agreement appended as Schedule "A" to this Bylaw on behalf of The City of Saskatoon.

Coming Into Force

- 6. This Bylaw comes into force on the day of its final passing.

Read a first time this _____ day of _____, 2015.

Read a second time this _____ day of _____, 2015.

Read a third time and passed this _____ day of _____, 2015.

Mayor

City Clerk

Schedule "A"

Renewal Agreement

This Agreement made the _____ day of October, 2015.

Between:

Brian Robert Sutherland, of the City of Saskatoon, in the Province of Saskatchewan (hereinafter referred to as the "Owner")

-and-

The City of Saskatoon, a municipal corporation pursuant to the provisions of *The Cities Act*, S.S. 2002, c. C-11.1 (hereinafter referred to as the "City")

Whereas:

1. The parties entered into a Farm Land Fixed Taxation Rate Agreement dated June 12, 2006 (the "Fixed Taxation Rate Agreement");
2. The Fixed Taxation Rate Agreement expired on December 31, 2009;
3. The parties entered into a Renewal Agreement on October 28, 2010 for a period of five years, which expired on December 31, 2014;
4. The Owner wishes to renew the Fixed Taxation Rate Agreement for a further period of five years upon the terms and conditions set forth in this Agreement; and
5. The Council of The City of Saskatoon at its meeting on September 28, 2015 resolved to renew the Fixed Taxation Rate Agreement for a further period of five years upon the terms and conditions set forth in this Agreement.

Now Therefore this Agreement witnesses that in consideration of the mutual covenants and agreements herein and subject to the terms and conditions in this Agreement the parties agree as follows:

1. To renew the Fixed Taxation Rate Agreement upon the same terms and conditions as set out in it, except as specifically varied in this Agreement, and that the Fixed Taxation Rate Agreement shall remain in full force, except as varied in this Agreement.

2. The Fixed Taxation Rate Agreement shall only apply to the following property:

Parcel Number: 117153132
 Title Number: 110416878
 Legal Land Description: SE Sec 05 Twp 37 Rge 06 W3 Extension 0
 As described on Certificate of Title 81S03100.

3. The term of this Agreement shall be for five years commencing on January 1, 2015 and ending on December 31, 2019.

4. This Renewal Agreement and the Fixed Taxation Rate Agreement, attached hereto, together constitute the entire agreement between the parties.

Signed, Sealed and Delivered in the presence of _____)
 _____)
 _____) **Brian Robert Sutherland**
 _____)
 Witness _____)

The City of Saskatoon

Mayor

c/s

City Clerk

Affidavit of Execution

Canada)
Province of Saskatchewan)
To Wit:)

I, _____, of the City
of Saskatoon, in the Province of Saskatchewan, _____,
make oath and say:

1. That I was personally present and did see **Brian Robert Sutherland**, named in the within instrument, who is personally known to me to be the person named therein, duly sign, seal and execute the same for the purposes named therein.
2. That the same was executed at the City of Saskatoon, in the Province of Saskatchewan, and that I am the subscribing witness thereto.
3. That I know the said **Brian Robert Sutherland**, and he is, in my belief, of the full age of eighteen years.

Sworn before me at the City of Saskatoon,)
 in the Province of Saskatchewan, this _____)
 day of October, 2015.)
 _____)
 A Commissioner for Oaths for Saskatchewan.)
 My commission expires _____)
 (or) Being a solicitor.)

COPY

Farm Land Fixed Taxation Rate Agreement

Between:

Brian Robert Sutherland, of the City of Saskatoon, in the Province of Saskatchewan (hereinafter referred to as the "Owner")

-and-

The City of Saskatoon, a municipal corporation pursuant to the provisions of *The Cities Act*, S.S. 2002, c. C-11.1 (hereinafter referred to as the "City")

Whereas:

A. The owner is the registered and assessed owner of the property described as:

Parcel Number: 135829008
Title Number: 110251921
Legal Land Description: NE 32 - 36 -06 - 3 Ext 13
As described on Certificate of Title 89S51468A, description 13.

Parcel Number: 135829031
Title Number: 110251954
Legal Land Description: Blk/Par A, Plan 101621629 Ext 15
As described on Certificate of Title 89S51468, description 15.

Parcel Number: 117153132
Title Number: 110416878
Legal Land Description: SE 05 - 37 - 06 - 3 Ext 0
As described on Certificate of Title 81S03100.

(hereinafter referred to as the "Property");

- B. The City annexed the Property into its corporate limits effective January 1, 2005;
- C. The Property is used exclusively for farming purposes;
- D. The Owner's principal occupation is farming;

Page 2

- E. The Owner occupies the dwelling that is situated on the Property;
- F. Prior to the annexation, the dwelling and other improvements used exclusively in connection with the agricultural operation situated on the Property were exempt from taxation pursuant to section 293 of *The Municipalities Act*, S.S. 2005, c. M-36.1;
- G. There is no equivalent exemption for the dwelling in *The Cities Act*;
- H. The taxes levied by the City in 2005 on the dwelling were approximately \$2,416.00;
- I. The City believes that the Owner should not be unduly prejudiced by the annexation of the Property and desires to place the Owner in a similar property tax situation as he was in prior to the annexation;

Now therefore in consideration of the premises and mutual covenants contained in this Agreement, the parties hereto covenant and agree as follows:

Fixed Rate of Taxation

- 1. Subject to the terms hereof, the City agrees that during the term of this Agreement, a fixed rate of taxation shall be placed on the assessed value of the Property so that the tax levy after applying the fixed rate of taxation is equivalent to the taxes that would be payable on the Property if the dwelling and other improvements used exclusively in connection with the agricultural operation were exempt from taxation on terms and conditions similar to section 293 of *The Municipalities Act*.

Conditions

- 2. (1) Notwithstanding anything contained herein, this Agreement is deemed to have been terminated and is void on:
 - (a) the placing, erecting or constructing of any additional improvement on the land to which this Agreement applies after the date on which this Agreement became effective, unless the improvement is used exclusively in connection with the agricultural operation that is owned or operated by the Owner;
 - (b) the use of any part of the Property for any purpose other than farming;
 - (c) the Owner ceasing to own a part of the Property that results in reducing the Owner's ownership to less than eight hectares;

- (d) the subdivision of the Property or any portion of the Property into lots; or
 - (e) the Owner ceasing to be assessed with respect to the Property.
- (2) In the event of termination pursuant to subsection (1), the tax rate and the amount of taxes levied on the Property for the year in which termination occurs shall be adjusted to correspond with the portion of the year following the date on which this Agreement was terminated.

Term

3. The term of this Agreement shall be for five years commencing on January 1, 2005 and terminating on December 31, 2009.

Payment of 2005 Taxes

4. (1) The Owner acknowledges that the 2005 taxes levied on the Property are in arrears. The Owner agrees that, immediately upon the execution of this Agreement, he shall pay to the City the 2005 taxes in full as adjusted using the fixed rate of taxation herein described.
- (2) The City agrees that if it receives payment of the 2005 taxes as provided for in subsection (1), it will cancel any additional percentage charges imposed upon the unpaid taxes on the Property as a penalty for default in payment.

Renewal

5. Upon the expiration of the term hereof, this Agreement may be renewed from time to time for periods not exceeding five years each; provided however, that the decision to renew this Agreement shall be in the absolute discretion of the City.

Waiver

6. No party is to be deemed to have waived the exercise of any right that the party holds under this Agreement unless such waiver is made in writing. No waiver made with respect to any instance involving the exercise of any such right is to be deemed to be a waiver with respect to any other instance involving the exercise of the right or with respect to any other such right.

Assignment

7. This Agreement is not assignable without the prior written consent of the City. Any attempt to assign any of the rights, duties, or obligations of this Agreement without written consent is void.

Time

8. Time is of the essence of this Agreement and no extension or variation of this Agreement operates as a waiver of this provision.

Notices

9. (1) Any notice or consent required or permitted to be given by either party to this Agreement to the other party shall be in writing and shall be delivered or sent by registered mail (except during a postal disruption or threatened postal disruption) or facsimile transmission or other electronic communication to the applicable address set forth below:
 - (a) in the case of the Owner:

Mr. Brian Sutherland
R. R. #2
Saskatoon, SK
S7K 3J5
 - (b) in the case of the City:

The City of Saskatoon
City Hall
222 - 3rd Avenue North
Saskatoon, SK S7K 0J5
Att: Accounting Control/Support Services Manager
Facsimile: (306) 975-7975
- (2) Any notice delivered personally shall be deemed to have been validly and effectively given and received on the date of such delivery provided same is on a business day (Monday to Friday, other than a statutory holiday).

- (3) Any notice sent by registered mail shall be deemed to have been validly and effectively given and received on the fifth business day following the date of mailing.
- (4) Any notice sent by facsimile transmission or other electronic communication shall be deemed to have been validly and effectively given and received on the business day next following the date on which it was sent (with confirmation of transmittal received).
- (5) Any party to this Agreement may, from time to time by notice given to the other party, change the party's address for service under this Agreement.

Entire Agreement

- 10. This Agreement supersedes and invalidates all other commitments, representations and warranties relating to the subject matter hereof which may have been made by the parties either orally or in writing prior to the date hereof, and all of which become null and void from the date this Agreement is signed.

Severability

- 11. If any provision of this Agreement is determined to be invalid or unenforceable in whole or in part, such invalidity or unenforceability attaches only to such provision and the remaining terms and provisions of this Agreement remain in full force and effect.

Amendment

- 12. No change or modification of this Agreement is valid unless it is in writing and signed by each party

Headings

- 13. The headings in this Agreement are for ease of reference only and are not to be taken into account in the construction or interpretation of any provision to which they refer.

Governing Law

14. This Agreement is governed by and is to be construed in accordance with the laws of the Province of Saskatchewan and the laws of Canada applicable therein and treated in all respects as a Saskatchewan contract. The parties to this Agreement hereby irrevocably and unconditionally attorn to the exclusive jurisdiction of the courts of the Province of Saskatchewan and all courts competent to hear appeals therefrom.

Further Assurances

15. Each party shall at any time and from time to time, upon each request by the other party, execute and deliver such further documents and do such further acts and things as the other party may reasonably request to evidence, carry out and give full effect to the terms, conditions, intent and meaning of this Agreement.

Binding Effect and Enurement

16. This Agreement enures to the benefit of and is binding upon the parties hereto and their respective successors and permitted assigns.

Signed by the Owner, Brian Robert Sutherland, this 9TH day of JUNE, 2006.

Signed, Sealed and Delivered in the presence of)

W. J. Darrow
Witness)

Brian Robert Sutherland
Brian Robert Sutherland)

Signed by The City of Saskatoon this 12 day of June, 2006.

The City of Saskatoon

Donald F. Astle
Mayor

Janice Mann
City Clerk



Affidavit of Execution

Canada)
Province of Saskatchewan)
To Wit:)

I, WILLIAM JOHN DAVERN, of the City
of Saskatoon, in the Province of Saskatchewan, SOLICITOR,
make oath and say:

1. That I was personally present and did see **Brian Robert Sutherland**, named in the within instrument, who is personally known to me to be the person named therein, duly sign, seal and execute the same for the purposes named therein.
2. That the same was executed at the City of Saskatoon, in the Province of Saskatchewan, and that I am the subscribing witness thereto.
3. That I know the said **Brian Robert Sutherland**, and he is, in my belief, of the full age of eighteen years.

Sworn before me at the City of)
Saskatoon, in the Province of)
Saskatchewan, this 9th day of)
June, 2006.)
Denette Fruistadt)
A Commissioner for Oaths in and for)
the Province of Saskatchewan.)
My Commission expires)
October 31, 2006.)
(or) Being a Solicitor. ✓ 107)

W. J. Davern



STANDING POLICY COMMITTEE ON PLANNING, DEVELOPMENT AND COMMUNITY SERVICES

Growth Plan to Half a Million – Long-Term Directions and Fall 2015 Public Engagement

Recommendation of the Committee

That the report of the General Manager, Community Services Department, dated October 5, 2015, be received as information.

History

At the October 5, 2015 meeting of the Standing Policy Committee on Planning, Development and Community Services, a report of the General Manager, Community Services Department, dated October 5, 2015, was considered.

Attachment

October 5, 2015 report of the General Manager, Community Services Department.

Growth Plan to Half a Million – Long-Term Directions and Fall 2015 Public Engagement

Recommendation

That the report of the General Manager, Community Services Department, dated October 5, 2015, be forwarded to City Council for information.

Topic and Purpose

This report and accompanying presentation provides an update on the draft Growth Plan to Half a Million (Growth Plan) and recommended implementation priorities for Corridor Growth, Transit, and Core Area Bridges that will be presented during the final round of public engagement in October.

Report Highlights

1. Major investments in the transportation network will benefit all road users. Investment in transit, both capital and operating, will also be required in order to increase transit mode share and rides per capita.
2. The Corridor Growth initiative has already identified high priority locations for redevelopment along major corridors. Greater detail will be available in terms of future planning, design guidelines, and integration with rapid transit investments.
3. Enhancing the transit system by adding new types of service, increased service on select routes, and dedicated transit facilities are some of the key recommendations of the Growth Plan's Long-Term Transit Plan. This includes more public discussion about "coverage" and transitioning towards a more direct, frequent transit service.
4. Long-term implementation of a new river crossing at 33rd Street and dedicated rapid transit lanes on the University Bridge will help alleviate future bridge congestion, as well as enable development of the University of Saskatchewan's (University) land holdings.
5. The Growth Plan project is now in the fifth phase of development, and the final round of public engagement will take place from October 14 to November 8, 2015. The intent of the final round of Growth Plan public engagement is to communicate the final plan to the community, as well as the recommended implementation and phasing strategy.

Strategic Goals

The development of the Growth Plan supports a number of strategies and priorities for the City of Saskatoon's (City) Strategic Goals of Asset and Financial Sustainability, Sustainable Growth, Moving Around, and Environmental Leadership by enhancing the range of choices for Moving Around and providing a new model for growth that more effectively utilizes infrastructure.

Growth Plan to Half a Million – Long-Term Directions and Fall 2015 Public Engagement

Background

Development of the Growth Plan began in 2013 with the award of the Transit Plan; Rapid Transit Business Case; Core Area Bridge Strategy; and the Nodes, Corridors, and Infill Plan to Urban Systems Ltd. Attachment 1 contains a complete list of all Growth Plan reports to date.

Major Public Engagement Events

Three rounds of major public engagement have occurred to date for the Growth Plan. The first round described Saskatoon's aspirations for growth, introduced strategies, and gathered input to guide the development of options for the Growth Plan's core initiatives of Corridor Growth, Transit, and Core Area Bridges. The second round sought feedback on specific options for each initiative, and the third round presented the recommended long-term options for Corridor Growth, Transit, and Core Area Bridges, and sought input on the implementation possibilities and priorities for these core initiatives.

Report

Transportation Network Improvements

A robust transportation network supports the movement of people, goods, and services and must balance investments to serve all modes of travel. The City has been working with residents, as well as the Province of Saskatchewan, on the provision of new and improved roadways that will serve all residents as the city grows to half a million. Major investments in roads, interchanges, and new bridges will improve travel throughout the city for all residents and all modes of transportation. A separate report outlining the roadway network for a population of 500,000 is forthcoming in Fall 2015.

Long-Term Direction for Corridor Growth

High priority locations for redevelopment along major corridors have been identified in previous rounds of engagement. These locations are consistent with the identified rapid transit routes to bring increased ridership and build on the investment potential that rapid transit supports.

This round of engagement will focus on more detailed information related to potential redevelopment including:

- Areas of focus and suggested phasing for future Secondary Plans;
- Transit Oriented Development (TOD) design guidelines; and
- Complete Streets Policy and Design Guide.

Long-Term Transit Plan and Rapid Transit Business Case

Saskatoon Transit is one component of the overall transportation system that will play an increasingly important role as growth in the city continues, and also plays a critical role in the overall Growth Plan. Investment will be required in order to provide a system that meets the needs and expectations of all residents. This includes more public discussion about "coverage" and transitioning towards a more direct, frequent transit service.

Growth Plan to Half a Million – Long-Term Directions and Fall 2015 Public Engagement

The Long-Term Transit Plan lays out a series of changes that will allow residents to choose from a broader range of services to meet their various needs. Residents will be able to choose various ways to use transit: by walking to their nearest bus stop; walking or cycling further to access a more frequent service; driving or cycling to a park-and-ride station close to the rapid transit corridor to access a more frequent, direct service; or in areas of low demand, potentially using a dial-a-ride or on-demand service.

Rapid transit will serve as the spine to the transit network, providing a facility that will be used by many transit routes to improve travel time and reliability. Rapid transit corridors and stations will be planned to support and connect higher-density, mixed-use areas of the city in order to enhance mobility for residents and visitors. The facility along the corridor will include transit priority measures such as dedicated lanes and transit priority signals, which will facilitate reliable service, while various routes accessing the corridor will provide frequency along the spine.

The general direction for the Transit Plan has been shared in previous rounds of public engagement. This round will have a greater focus on the implementation priorities, along with the incremental service increases required, and suggested locations for enhanced transit frequency.

Long-Term Direction for Core Area Bridges

The recommended direction for core area bridges is a new river crossing at 33rd Street combined with dedicated rapid transit lanes on the University Bridge. Implementation would be in the long-term, and timing will be affected by the degree of implementation of the Transit Plan. The proposed 33rd Street bridge will also be a factor in the ability of the University to develop its endowment lands as planned. Factors that will affect implementation of this direction will be shared during this round of engagement.

Final Round of Public Engagement

The Growth Plan project is now in the fifth phase of development, and the final round of public engagement will run from October 14 to November 8, 2015. The purpose of the final round of Growth Plan public engagement is to communicate the draft plan to the community, as well as the recommended implementation and phasing strategy. Attachment 2 provides an overview of the engagement plan, as well as the promotions for this phase.

In the previous round of engagement, a representative survey of 800 Saskatoon residents showed high levels of support for the preliminary directions of the Growth Plan. These results were consistent with the levels of support found during the project's engagement activities. Based on this, the long-term directions in the draft plan will be consistent with what has been shared previously.

Public and/or Stakeholder Involvement

Public and stakeholder engagement has been continuous throughout the project, primarily at key project milestones. In addition to general public engagement, numerous

Growth Plan to Half a Million – Long-Term Directions and Fall 2015 Public Engagement

stakeholder groups have been specifically engaged throughout the project, including, but not limited to:

- i) stakeholders internal to the City of Saskatoon;
- ii) business groups and representatives;
- iii) Aboriginal leaders and organizations;
- iv) community service organizations representing neighbourhoods, newcomers, low income, seniors, and youth; and
- v) other interested groups/organizations.

Communication Plan

A variety of promotions have been organized to promote public engagement opportunities, including utility bill inserts, newspaper/online/radio ads, online video ads, social media posts/ads, posters, and handbills. News Releases and Public Service Announcements are distributed to announce key project events and information.

The project website, www.growingfwd.ca, is the primary location for Growth Plan-related updates and information. To date, nearly 1,200 people have subscribed to the project website to receive Growth Plan updates. Growth Plan Summary Reports and Engagement Summary Reports are also provided on the website at key milestones throughout the development of the Growth Plan. In addition, a new video has been produced to help explain the recommendations that have been developed for the Growth Plan. It will be released to support this final round of engagement. The first Growth Plan video, which provides a big picture overview of how the Growth Plan will guide the way we build, service, and move around a city of half a million people, has received over 6,500 views to date.

Other Considerations/Implications

There are no options, policy, financial, environmental, privacy, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

The final Growth Plan will be brought to City Council for consideration in March 2016.

Public Notice

Public notice, pursuant to Section 3 of Public Notice Policy No. C01-021, is not required.

Attachments

1. Chronological List of Growth Plan Reports
2. Fall 2015 Public Engagement and Promotion Plan

Report Approval

Written by: Lesley Anderson, Project Manager, Growing Forward! Shaping Saskatoon
Jennifer Pesenti, Marketing Coordinator, Special Projects, Community Services

Reviewed by: Alan Wallace, Director of Planning and Development
Carla Blumers, Director of Communications

Approved by: Randy Grauer, General Manager, Community Services Department

S/Reports/CP/2015/PDCS – Growth Plan to Half a Million – Long-Term Directions and Fall 2015 Public Engagement/ks

Chronological List of Growth Plan Reports

2012

City Council
March 26, 2012

INTEGRATED GROWTH PLAN

The purpose of this report was to seek approval for the new Integrated Growth Plan (IGP), in principle.

City Council
June 18, 2012

INTEGRATED GROWTH PLAN

The purpose of this report was to provide an update on the work completed by the Future Growth Delivery Team of the civic Administration which had been tasked with developing a work plan, schedule, and resource strategy to implement the new IGP. As a first step, the Delivery Team was asked to prepare a preliminary High Level Growth Plan to accommodate a population of 500,000, and a “bridging” document to provide an overview of the IGP to the development community and interested members of the public. This report presented both of these documents to City Council.

2013

City Council
February 11,
2013

INTEGRATED GROWTH PLAN

The purpose of this report was to present the next level of detail in the IGP project, including subject area updates, a Community Engagement and Communications Strategy, and a preliminary timeline and process for 2013 to 2015.

City Council
July 17, 2013

AWARD OF CONTRACT – URBAN SYSTEMS FOR THE DEVELOPMENT OF THE DETAILED IGP AND IMPLEMENTATION PROCESS

The purpose of this report was to award the contract and prepare the agreement with Urban Systems for the consultant work required to carry out the Development of the Detailed Integrated Growth Plan (IGP) and Implementation Process.

Planning and Operations Committee July 16, 2013	INTEGRATED GROWTH PLAN – FINANCING GROWTH – REQUEST FOR PROPOSALS The purpose of this report was to present the information to be contained in the Request for Proposals (RFP) for a Financing Growth Study.
Executive Committee October 15, 2013	INTEGRATED GROWTH PLAN BRANDING STRATEGY This report presented the proposed branding strategy for the IGP Project.
City Council October 21, 2013	AWARD OF CONTRACT – INTEGRATED GROWTH PLAN – FINANCING GROWTH STUDY This report was to award the contract and necessary agreement for the consulting work required to undertake the Financing Growth Study as part of the City of Saskatoon’s (City) IGP.

2014

Planning and Operations Committee January 14, 2014	PROPOSED SCOPE ADDITION TO GROWING FORWARD! SHAPING SASKATOON CONTRACT This report proposed a scope addition to the <i>Growing Forward! Shaping Saskatoon</i> project. The proposed scope addition was for the completion of a Long-Term Transit Plan.
Planning and Operations Committee January 28, 2014	COMMUNICATION AND ENGAGEMENT UPDATE FOR GROWING FORWARD! SHAPING SASKATOON This report provided an overview of the goals guiding project-related communications and engagement activities, and an update on the launch of the <i>Growing Forward! Shaping Saskatoon</i> website with highlights of the upcoming milestone public event.

**GROWING FORWARD! SHAPING SASKATOON PROJECT
UPDATE**

**Planning and
Operations
Committee
June 17, 2014**

This report provided an update on two reports prepared for public release that:

- (a) provided a technical overview of the Growing Forward! Shaping Saskatoon initiative to date and outlined early possibilities being considered for the Growth Plan to Half a Million; and
 - (b) summarized what was heard in the public engagement to date.
-

**SPC on
Planning,
Development
and Community
Services
November 3,
2014**

**GROWTH PLAN TO HALF A MILLION NOVEMBER 2014
PUBLIC ENGAGEMENT**

This report provided an overview of “Growth Plan to Half a Million” public engagement activities from November to December 2014.

2015

**SPC on
Planning,
Development
and Community
Services
March 2, 2015**

**GROWTH PLAN TO HALF A MILLION - FEBRUARY/MARCH
2015 PUBLIC ENGAGEMENT**

This report provided an overview of “Growth Plan to Half a Million” public engagement activities from February 25 to March 18, 2015.

**Executive
Committee
April 20, 2015**

FINANCING GROWTH STUDY

The purpose of this report was to receive the final Financing Growth report from Hemson Consulting Ltd., and summarize the key findings of this study.

**SPC on
Planning,
Development
and Community
Services
July 20, 2015**

GROWTH PLAN TO HALF A MILLION UPDATE

This report provided an update on the “Growth Plan to Half a Million”, including two new reports that:

- a) provided a summary of the proposed long-term possibilities being considered for the Growth Plan to Half a Million; and
- b) summarized what was heard during the latest round of public engagement from February to June 2015.

**SPC on
Planning,
Development
and Community
Services
October 5, 2015**

GROWTH PLAN TO HALF A MILLION – LONG TERM DIRECTIONS AND FALL 2015 PUBLIC ENGAGEMENT

This report and accompanying presentation are intended to inform, prepare, and engage the community and City Council for the final round of public engagement to discuss the draft plan and recommended implementation priorities for Corridor Growth, Transit, and Core Area Bridges.

Fall 2015 Public Engagement and Promotion Plan

City-Wide Engagement

Two city-wide public engagement events will be held, one on each side of the river, as below:

Wednesday, October 21, 2015
Mayfair United Church
902 33rd Street West
4:30 p.m. to 8:30 p.m.

Thursday, October 22, 2015
Saskatoon Field House, Lobby
2020 College Drive
4:30 p.m. to 8:30 p.m.

These events will be open-house style, with staff available for discussion and questions. Participants will be able to view boards, project videos, and provide feedback via surveys and comment sheets.

Stakeholder Engagement

Additional stakeholder engagement activities will occur from October 14 to November 8, with various stakeholder groups, including:

- i) major stakeholders, such as business groups, community groups, and major organizations; and
- ii) land and business owners along the priority corridors.

Neighbourhoods most likely to be impacted by the proposed 33rd Street Bridge will be sent flyers inviting them to the city-wide public engagement events. These flyers will be sent to the neighbourhoods of City Park, North Park, Richmond Heights, Sutherland, Forest Grove, and Silverspring.

Online Engagement

All engagement materials, including an online survey, will be available on the Growing Forward! Shaping Saskatoon project website, www.growingfwd.ca, from October 14 to November 8, 2015, to provide additional opportunities for the community to view the information and provide feedback via an online survey or a comment form.

Public input received during this round of engagement will be utilized to evaluate general levels of support for the main growth plan initiatives and will also help inform later stages of implementation.

Promotions and Awareness

As in past phases of the Growth Plan project, promotions and advertisements will be placed in print, social, and radio media throughout the community. In addition, pop-up event promotions will be undertaken at community locations prior to the city-wide engagement events. These pop-ups will aim to raise project awareness and promote event attendance, as well as online participation.



STANDING POLICY COMMITTEE ON PLANNING, DEVELOPMENT AND COMMUNITY SERVICES

2014 Annual Report - Saskatoon Housing Initiatives Partnership

Recommendation of the Committee

That the report of the General Manager, Community Services Department, dated October 5, 2015, be received as information.

History

At the October 5, 2015 meeting of the Standing Policy Committee on Planning, Development and Community Services, a report of the General Manager, Community Services Department, dated October 5, 2015, was considered. Mr. Shaun Dyck, Executive Director, Saskatoon Housing Initiatives Partnership, also provided a PowerPoint presentation.

Mr. Shaun Dyck, Executive Director, Saskatoon Housing Initiatives Partnership, will provide a PowerPoint presentation to City Council.

Attachment

October 5, 2015 report of the General Manager, Community Services Department.

2014 Annual Report - Saskatoon Housing Initiatives Partnership

Recommendation

That the report of the General Manager, Community Services Department, dated October 5, 2015, be forwarded to City Council for information.

Topic and Purpose

The purpose of this report is to provide an update on the activities of the Saskatoon Housing Initiatives Partnership (SHIP) in 2014.

Report Highlights

1. SHIP serves to support the implementation of the City of Saskatoon's (City) Housing Business Plan and strengthen the community capacity to deliver affordable housing projects in Saskatoon.
2. SHIP's 2014 Annual Report to the City provides details of their activities in 2014, which includes a number of projects that will bring affordable units to several areas with a low concentration of affordable housing.

Strategic Goal

This report addresses the City's Strategic Goal of Quality of Life by supporting the provision of affordable housing throughout Saskatoon. SHIP provides a range of services to assist in the development of affordable housing projects in the community and also serves as an advocate for the importance of affordable housing.

Background

SHIP was created in 1999. It strives to bring the necessary expertise, experience, and assistance to housing providers in order to help build the capacity to develop new affordable housing projects in Saskatoon.

When SHIP was established, non-profit housing providers and the City agreed there was a gap in services and capacity that limited the development of affordable housing. SHIP's role was to fill that gap by providing a community-based service to help affordable housing providers to develop business plans for affordable housing projects.

At its March 21, 2011 meeting, City Council approved a service agreement with SHIP for a five-year term, set to expire at the end of 2016. Annual funding of \$100,000 from the Affordable Housing Reserve would be provided to SHIP. In return, SHIP would help support the goal of 100 units of affordable housing every year in Saskatoon. In addition, the agreement requires SHIP to provide the City with an annual report and a five-year strategic plan.

Report

SHIP is a community-based organization that has a mission to help develop creative solutions for the provision of affordable housing by bringing together expertise and experience in the community. The work of SHIP has been critically important in this period of sustained economic and population growth in Saskatoon, which has been accompanied by a decreasing supply of rental units and an increase in housing prices.

In SHIP's 2014 Annual Report (see Attachment 1), the details of its contributions to achieving over 100 units of affordable housing are outlined. A number of the identified projects are in areas with a low concentration of affordable housing units. The report also includes information related to the funding that SHIP has disbursed in its role as the Community Entity for the Federal Government's Homelessness Partnering Strategy funding.

SHIP's role in Saskatoon continues to be important to the provision of affordable housing and continues to contribute to the success of the City's Housing Business Plan.

Financial Implications

The service agreement between SHIP and the City includes an annual payment to SHIP of \$100,000 from the Affordable Housing Reserve, with the following scaled performance bonus when SHIP facilitates the creation of more than 100 units in a year:

- 101 to 120 units - \$5,000 bonus
- 121 to 140 units - \$10,000 bonus
- 141 or more units - \$15,000 bonus

As SHIP has facilitated the creation of 102 units in 2014, they qualify for a bonus payment of \$5,000.

Other Considerations/Implications

There are no options, policy, environmental, CPTED, or privacy implications or considerations. A communication plan is not required.

Due Date for Follow-up and/or Project Completion

Under the terms of the service agreement between the City and SHIP, an annual report is required each year of the five-year agreement. The contract between the City and SHIP expires at the end of 2016. A report will be presented to City Council in 2016, evaluating the service and recommending whether the City should continue to provide funding to SHIP based on a review of community housing priorities.

Public Notice

Public notice, pursuant to Section 3 of Public Notice Policy No. C01-021, is not required.

Attachment

1. Creative Solutions to Affordable Housing and Homelessness in Saskatoon - 2014 Annual Report

Report Approval

Written by: Paul Whitenect, Acting Manager, Neighbourhood Planning
Reviewed by: Alan Wallace, Director of Planning and Development
Approved by: Randy Grauer, General Manager, Community Services Department

S/Reports/CP/2015/PDCS – 2014 Annual Report – Saskatoon Housing Initiatives Partnership/lc



SASKATOON
HOUSING
INITIATIVES
PARTNERSHIP

2014

ANNUAL REPORT

CREATIVE SOLUTIONS TO AFFORDABLE HOUSING
AND HOMELESSNESS IN SASKATOON



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Types of Affordable Housing

Supportive housing
is permanent housing for people who need assistance (personal support services, medical assistance, counselling) to live independently. Supportive housing may house people who are elderly or who have disabilities, addictions, or mental illness.

Emergency housing
provides shelter and accommodation for short periods of time and serves people who are homeless, displaced, or who are fleeing violence or abuse.

Homeownership
Owning a home is an attractive option for many households. Options include single and multi-family homes and prices vary by location and quality. Affordable homeownership options may be available.

Non-profit housing
Rental housing for low and moderate income individuals and families, built by the province, a municipality, or by a community group. Most tenants pay rent that's geared to their income, others pay rent that's at the low end of private market rent.

Private market purpose-built rental housing
Housing that's been built for the purpose of renting to tenants is called 'purpose-built rental.' Apartment buildings are a common type of purpose-built rental. This housing is owned by an individual or a private company and may be managed by the owner or by a hired property manager. Units vary widely based on location, age and condition of building. The owner of the property sets the amount to be paid for rent based on the local housing market.

Secondary suites
are emerging as a private-market option for affordable housing. These units are created on the same land as a private home, perhaps in a basement, on an upper floor, above a garage, or detached from the home. The homeowner acts as the landlord and charges rent to the tenant based on the local housing market.

Cooperative housing
provides housing for people with low and moderate incomes. Residents contribute their time towards the upkeep of the building(s) and the governance of the co-op. Some tenants pay rent that's geared to income; the rest pay market rent. Some units may be modified for seniors or for people with disabilities.

Transitional housing
offers a bridge between emergency and permanent housing, often for a specified time period. Transitional housing may serve people leaving homelessness, people leaving the corrections system, or families receiving specialized supports.

Private market condo rentals
In major cities, there has been a decline in the building of new, purpose-built rental housing. Rental condos, owned by investors and rented to tenants, are the fastest growing segment of many cities rental housing sector. These units tend to be priced above purpose-built rental housing and serve middle- and high-income households.

Rooming houses or congregate living
Residents rent out single rooms and share common kitchens, bathrooms, and living space. Depending on a resident's needs, this kind of housing may be permanent or transitional.

WHAT WE DO

Working across the entire affordable housing continuum, SHIP is helping create solutions to housing and homelessness in Saskatoon through project development assistance, community engagement, and making investments to address homelessness.

SHIP is a unique affordable housing development organization that brings together a wealth of expertise and experience from a diverse group of individuals involved with affordable housing in Saskatoon. SHIP's Board, committees and staff are teeming with experienced advisors that bring a wealth of knowledge to the organization. By identifying the needs of the community, SHIP develops creative solutions to the housing challenges faced by many Saskatoon residents. This is achieved by conducting market research, reviewing best practices, engaging organizations and individuals through meetings and outreach, and building strategic partnerships with groups that are working toward goals and outcomes common to our organization.

SHIP's housing project development team works with clients to create a customized business plan to help bring their project to fruition. Plans review zoning and building requirement and explores development options. By performing financial feasibility assessments and establishing timelines and goals, SHIP helps mitigate risks often encountered during the project development phase.

Our project development expertise also provides a strong knowledge base for the investments we manage (\$1 Million per year) for initiatives that address homelessness in our city. By working together with our community partners, SHIP is helping build communities, revitalize neighborhoods and address homelessness.

With expertise in project development and research, and proven strength as prudent fund managers, SHIP has all the elements needed to empower organizations to develop solutions to affordable housing and homelessness.



SHIP'S VISION

SHIP will help create a safe, healthy and prosperous community by ensuring that there is an ample supply of affordable housing choices that contributes to the economic, social and personal well-being of the residents of Saskatoon.

SHIP'S MISSION

SHIP provides leadership to foster creative housing solutions by engaging developers, builders, lenders, and housing providers.

SHIP WORKS ACROSS THE ENTIRE AFFORDABLE HOUSING CONTINUUM TO IMPROVE THE LIVES OF SASKATONIANS

- Shelters
- Supported Housing
- Non-market Rental
- Purpose-built Rental
- Secondary Suites
- Rented Condos
- Condos
- Affordable Home Ownership

OUR STRENGTH IS IN OUR PARTNERSHIPS

Over the past three years, SHIP has expanded its funding partnerships, membership base, and our housing research and business planning social enterprise. This has helped SHIP grow from a staff of one to four, and helped us to do more to build creative solutions for affordable housing and homelessness in Saskatoon.

The support of the City of Saskatoon is integral to SHIP's success. Providing five years of stable funding for SHIP with an investment of \$100,000 to \$115,000 per year (based on performance) that covers the cost of the Executive Director and the Contractor Services Fund (a grant for predevelopment activities). This strategic investment creates a cornerstone for the entire organization with a strong network and knowledge base that facilitates the development of affordable housing and helps reduce and prevent homelessness in Saskatoon.



Since 2012, SHIP has administered the Federal Government's Homelessness Partnering Strategy funding in Saskatoon. In March 2014, SHIP renewed its contract to 2019. Over the next five years, we will invest close to \$5 Million to prevent, reduce and end homelessness in Saskatoon. Focus areas for investments include:

- Housing First for chronic and episodically homeless individuals;
- Services for individuals experiencing or at-risk of homelessness;
- Capital Investments (residential and non-residential facilities);
- Regional Initiatives (partnership building, knowledge transfer); and
- Data Collection and Analysis



SHIP receives administrative fees that to administer the HPS funding. This has increased our staffing complement, and expanded our impact across the housing continuum in a significant way.

Over the past year, SHIP has partnered with the United Way of Saskatoon & Area in order to address homelessness in Saskatoon. Initiatives include the highly successful **YXE Connects** fair that brought over seventy service providers together under one roof to provide assistance to people experiencing or at-risk of homelessness. Currently, SHIP and the United Way of Saskatoon & Area are collaborating to develop a **Community Plan to Prevent and End Homelessness** that combines the wisdom of past community plans, consultations and experience gained through implementation of various projects that address homelessness in Saskatoon.



Along with the funding from the City of Saskatoon and the Government of Canada, our membership sales help us make a positive impact on our great city. We are truly grateful to all the organizations that support us.



AFFORDABLE HOUSING PROJECT DEVELOPMENT ASSISTANCE

Over the past year, the Saskatoon Housing Initiatives Partnership (SHIP) has been involved with projects that are targeting the needs of various groups in Saskatoon.

Paradigm Island Estates Limited (96 Units in Development)

With the increased migration to Saskatchewan over the last few years, demand for rental and ownership housing in Saskatoon have increased dramatically. The result is that an increasing number of immigrants and refugees with low and moderate income who can not easily access appropriate accommodations. The objective of Paradigm Island Estates is to provide affordable housing and provide on-site support services to help newcomers to establish themselves in the community. This will be achieved by constructing and managing affordable rental housing for new immigrants, refugees, international students with families, and other low income groups of newcomers, and creating effective partnerships with organizations that help individuals and families settle and adapt to living in a new country and culture.

SHIP provided research services to produce a Need and Demand Study that demonstrated the lack of affordable housing for immigrants and refugees in Saskatoon. We are now working together on site selection and financing options.

Stewart Properties Holdings (6 Units in Development)

Building upon seven years of successful affordable rental housing development in Saskatoon, Stewart Properties is looking to reintroduce the positive benefits of a new, purpose built, seven unit co-housing facility. The proposed complex will be a beautiful, safe, haven for single males, aged 18-25. Although considered “hard to house” because of their demographics, these youth will flourish if given the right environment and opportunity. With experienced service delivery partners, these youth will work administer and manage the house. Stewart Properties will be empowering the residents throughout the build process by offering an apprenticeship program called *Pathways to Independence*, an ongoing program that will continue after new residents move in and until they obtain employment.



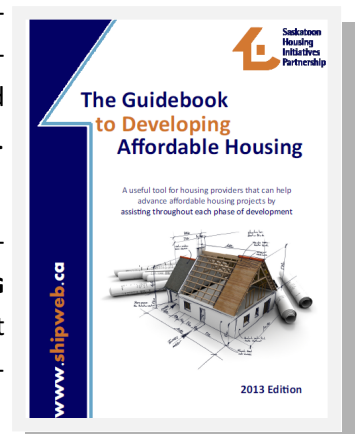
SHIP provided proposal coordination services in order to submit applications for funding to the Saskatchewan Housing Corporation. Our development team worked in conjunction with Stewart Properties to ensure that the proposal was complete and met the terms and conditions of the Province’s Rental Development Program on two separate occasions.

Another service SHIP provided to Stewart Properties Holdings over the past year was research management. With multiple projects on the go, SHIP has developed working relationships with other research and development organizations in Saskatoon to help with the workload and ensure timely delivery of research and development projects.

Project Development Partners and Resources

As a unique organization in Saskatchewan, SHIP fields calls from all over the province regarding affordable housing. While our focus is affordable housing development in Saskatoon, our resources are relevant to property development anywhere. We have fielded questions and provided information to cities, towns and First Nations across the province. This demand shows the need and value of an organization like SHIP.

We often refer organizations to our partners at CMHC and the Saskatchewan Housing Corporation for assistance, as well as providing our newly updated **GUIDEBOOK TO DEVELOPING AFFORDABLE HOUSING**. This guidebook helps organizations throughout the development process, and contains a wealth of information on the development processes, funding programs and a multitude of checklists to mitigate risk and expedite the housing project.



INVESTING TO PREVENT, REDUCE AND END HOMELESSNESS

In October 2014, the Saskatoon Housing Initiatives Partnership (SHIP) was renewed as the Community Entity for the Homelessness Partnering Strategy (HPS) in Saskatoon. The Government of Canada is investing over \$5 Million in funding from 2014-2019, which will be administered by SHIP in partnership with the Community Advisory Board on Saskatoon Homelessness (CAB-SH). The focus of the renewed funding is to move towards a “Housing First” approach to addressing homelessness. The Government of Canada aims to target people who are chronically or episodically homeless, moving them from the street or shelter into permanent homes or their own.



With ongoing advice and guidance from the CAB-SH, SHIP approved and allocated funding in Saskatoon through a Call for Proposal in November 2014. Approved projects included timely building upgrades for homeless services, food security investments and innovative pilot programs to increase front-line support to homeless individuals and families.

The focus for SHIP staff working on HPS in 2014/15 was community development, identifying gaps, preparation for Housing First, connecting with mentors and coordination. These efforts were central to developing projects that we believe will create meaningful results and truly reduce homelessness in the coming years. The front-line housing supports SHIP funded were developed to complement the existing Housing First program at Saskatoon Crisis Intervention Services, which is funded by the United Way of Saskatoon and Area. SHIP worked with community agencies to identify the critical gaps in housing services and develop some made-in-Saskatoon solutions. Early outcome measures show that there was a huge need for front-line support in housing placement and case management. Those who have partnered with us to implement Housing First and support services for people who are homeless are doing a remarkable job. In 2015/16 more expansions are expected to support families who are chronically or episodically homeless.

The community truly came together this year, coordinating efforts, reducing duplication and increasing understanding about what is happening and what needs to happen to reduce and prevent, and hopefully even end homelessness in Saskatoon. SHIP is grateful for the hard work and dedication of Saskatoon’s outstanding Community Based Organizations, government employees and numerous volunteers that are making a difference in so many lives each day.

INVESTMENT SUMMARY

Over \$1.8 Million has been invested in both service and capital projects, including:

- ◆ **Saskatoon Indian and Métis Friendship Centre:**
 - ◆ Two Rapid Rehousing Case Managers and one Centralized Intake staff person that will assess needs and support individuals and families to find housing, access income supports and work towards housing stability (\$217,734 in multi-year funding)
 - ◆ Renovate the client meeting space and lobby (\$41,098)
- ◆ **The Lighthouse Supported Living Inc.**
 - ◆ Housing Locator position to help locate and secure housing for Housing First and non-Housing First clients in Saskatoon and to pay for set-up costs for clients (\$150,261 in multi-year funding)
 - ◆ Develop a bed bug heating chamber and renovate the laundry facilities for their supported living clients (\$55,941);
- ◆ **Cress Housing Corporation:** Purchase of a 12-unit apartment complex for transitional housing (\$781,401);
- ◆ **Central Urban Métis Federation Inc.:** renovate and update a 16 suite apartment block, known as Infinity House (\$511,999);
- ◆ **Crocus Co-operative Inc.:** Replace their roof, repair interior ceiling; insulate and side the outside of the building (\$116,131);
- ◆ **The Saskatoon Food Bank and Learning Centre:** Purchase of a new refrigerated truck. (\$95,000);
- ◆ **YWCA:** Replace heating units in the YWCA shelter building (\$94,859)
- ◆ **CHEP Good Food Inc.:** A one-day community engagement workshop on gaps in food security and potential solutions (\$2,500).

CAB-SH Membership	
Jo-Ann Coleman-Pidskalny (Chair)	Saskatoon Housing Coalition
Karyn Kawula	Saskatoon Health Region
Peter Wong	Ministry of Child & Family Services
Barry Downs	Cress Housing
Deb Horseman	Saskatoon Housing Corp
Ray Neale	Saskatchewan Housing Authority
Shirley Isbister	CUMFI
Drew Bilboe	Salvation Army
Daryl Sexsmith	City of Saskatoon
Linda Gaudet	Ministry of Social Services
Don Meikle	EGADZ
Heidi Gravelle	White Buffalo Youth Lodge

HOMELESS SERVICE AND CAPITAL PROJECT SUMMARY

Transitional Housing Apartments for Aboriginal People who are homeless or at-risk

Project Partners: Cress Housing Corporation, Saskatoon Tribal Council, SHIP

SHIP Funding Allocation: \$781,401

Overview: Cress Housing Corporation purchased a 12 unit apartment complex to provide transitional housing to prevent homelessness and help those who are homeless build landlord references, through appropriate housing and culturally appropriate mentorship.

Infinity House Repairs

Project Partners: Central Urban Métis Federation Inc.(CUMFI), SHIP

SHIP Funding Allocation: \$511,999

Overview: CUMFI was granted funding to renovate the interior of a 16 suite apartment block known as Infinity House. In the home, CUMFI provides wrap-around, 24-7 transitional support to woman and their children who have experienced issues such as homelessness and domestic violence. The services offered at Infinity House target root causes of homelessness and support women to maintain custody of their children and safely transition to independence. The CAB was aware of the incredible value of CUMFI's housing programs in reducing homelessness, both in the present and in the future as children grow up in families that have received intentional support to heal and move forward.

The Power of Friendship – Rapid Rehousing and Centralized Intake

Project Partners: Saskatoon Indian and Métis Friendship Centre (SIMFC), SHIP

SHIP Funding Allocation: \$217,734 (Multi-year funding contract)

Overview: SIMFC was granted HPS funding for three new staff positions to address homelessness. The Centralized Intake staff person works with all programs, providing a single entry point for those looking for housing services or those referred by another agency. This position supports the work of the existing Housing First program at Saskatoon Crisis Intervention Services and the Rapid Rehousing program. Two Rapid Rehousing Case Managers support individuals and families to find housing, access income supports and work towards housing stability.

Housing Locator

Project Partners: The Lighthouse Supported Living Inc., SHIP

SHIP Funding Allocation: \$150,261 (Multi-year funding contract)

Overview: The Housing Locator position was a response to the need expressed by Saskatoon Crisis Intervention Services to support their Housing First case management team. Ideally, Housing First programs separate housing search and landlord relations from case management. This ensures that the person the landlords interact with is consistent and able to intervene in partnership with the case manager if there is a problem. Creating a database of housing options and building relationship with landlords supports the success of both the Housing First and Rapid Rehousing programs.

Mobile Bed Bug Chamber and Laundry Facilities

Project Partners: The Lighthouse Supported Living Inc., SHIP

SHIP Funding Allocation: \$55,941

Overview: Bed bugs are a big problem in Saskatoon and are one reason why people end up homeless. Landlords sometimes require new tenants to have their personal items treated for bed bugs before they move in to a new place. The Lighthouse was the need and created a solution that will support people who are low-income or at-risk of homelessness due to pest control issues. This project has the potential to prevent homelessness in Saskatoon. The laundry facility upgrade in the supported living tower was a needed renovation for a program that provides housing to people who are unable to live independently, most of whom would be homeless without the Lighthouse support.

Upgrade of Heating Units and Kitchen Equipment

Project Partners: YWCA of Saskatoon, SHIP

SHIP Funding Allocation: \$94,859

Overview: The YWCA received funding from HPS to install new heating units in their shelter and purchase new equipment and furniture for the kitchen in their transitional housing program. Both of these areas of the building are used by women and children experiencing homelessness.

HOMELESS SERVICE AND CAPITAL PROJECT SUMMARY

Refrigerated Food Truck

Project Partners: Saskatoon Food Bank and Learning Centre, Food Banks Canada , SHIP

SHIP Funding Allocation: \$95,000

Overview: The Saskatoon Food Bank was granted funding to purchase a the purchase of the truck it would greatly increase their capacity to provide, accept, and distribute perishable food as well as increase the nutritional value of foods provided such as fruits, vegetables, milk, eggs, meat, and frozen items.

Roof Repair, and Exterior and Interior Renovation

Project Partners: Crocus Co-operative Inc., SHIP

SHIP Funding Allocation: \$116,131

Overview: The Crocus Co-operative Inc. building was in need of renovations to continue to offer their essential programs. Crocus offers drop-in day programs for people with a mental illness. Many members of Crocus struggle with homelessness and are often at-risk of losing housing. The CAB saw value in the services and supports offered by Crocus, supports that many people who struggle with homelessness would not get elsewhere.

Rebuilding our Home Fire—Upgrades to facility

Project Partners: Saskatoon Indian and Métis Friendship Centre (SIMFC), SHIP

SHIP Funding Allocation: \$41,098

Overview: SIMFC received funding to renovate their client meeting space and lobby, which is used as a drop-in centre. SIMFC's clients are diverse, but they serve many people who are homeless or at-risk through their supports. The renovation shows clients they are valued and welcome.

Community Engagement Workshop on Food Security

Project Partners: CHEP Good Food Inc. , SHIP

SHIP Funding Allocation: \$2,500

Overview: CHEP was contracted to offer a solution-focused, facilitated workshop to identify solutions to food security issues in Saskatoon. High rents and low incomes result in increased food insecurity for many people. This is an issue that affects all people who are homeless or at-risk of homelessness.

2014 SUMMARY

SHIP continues to provide valuable services to the community. Our project development services for affordable housing and the proactive community development we utilize to ensure that our investments complement current programming for homelessness are making significant strides in our community. We are very proud of our achievements and are excited about the projects currently in development.

FINANCIAL SUMMARY

The summary financial information presented here is derived from SHIP's audited consolidated statements for the year ended July 31, 2014. With an increased interest in SHIP's services, income from the research and planning division helped strengthen the financial position last year, allowing us to hire additional support. Consequently, the net loss in 2014 offset gains from 2013.

SASKATOON HOUSING INITIATIVES PROGRAMS, INC.		
BALANCE SHEET AS AT JULY 31, 2014 (with comparative figures for 2013)		
	2014	2013
ASSETS		
Current Assets	\$	\$
Cash	420,745	1,098,204
Short-term investments	109,995	108,758
Accounts receivable	89,132	44,771
Prepaid expenses	1,692	1,692
	621,566	1,253,425
Tangible capital assets	1,202	1,758
	<u>\$622,768</u>	<u>\$1,255,183</u>
LIABILITIES		
Current liabilities		
Accounts payable & accrued liabilities	31,484	11,364
Deferred revenue	430,728	1,072,672
Total Liabilities	462,212	1,084,036
Retained Earnings	160,556	171,147
EQUITY	<u>\$622,768</u>	<u>\$1,255,183</u>

Summary Statement of Receipts and Expenditures	2014	2013
	\$	\$
Receipts	1,990,132	1,070,044
Operating Expenditures	2,000,723	1,059,075
Net Operating Income	<u>(10,591)</u>	<u>10,969</u>

SHIP's Executive Director and the Executive Committee, a sub-committee of the Board of Directors, adhere to responsible policies and procedures to ensure fiscal responsibility.

SHIP holds itself fiscally accountable to its core financial contributors and members.

102 UNITS

AFFORDABLE HOUSING PROJECT DEVELOPMENT ASSISTANCE

Paradigm Island Estates Inc.
(96 Units for Immigrants)

Stewart Properties Holdings
(6 Units for Youth)

\$1.8 MILLION

INVESTED TO PREVENT AND END HOMELESSNESS

Saskatoon Indian & Metis
Friendship Centre
(Rapid Rehousing Case Mgmt.)

Saskatoon Indian & Metis
Friendship Ctr. / Friendship Inn
(Centralized Intake Program)

The Lighthouse Supported Living
(Housing Locator Staff)
(Bedbug Chamber/Laundry)

Cress Housing
(12 Unit Transitional Apt. Bldg.)

Central Urban Metis Fed. Inc.
(CUMFI's Infinity House Retrofit)

Saskatoon Food Bank
(New Refrigerated Food Truck)

Crocus Co-op
(Facility Upgrades)

CHEP Good Food Inc.
(Food Security Community Mtg.)

YWCA of Saskatoon
(Heating & Kitchen Upgrades)

\$5 Million

SECURED (2014-19)

SHIP renewed as Saskatoon's
Community Entity, managing
funding for Gov't of Canada's
Homeless Partnering Strategy

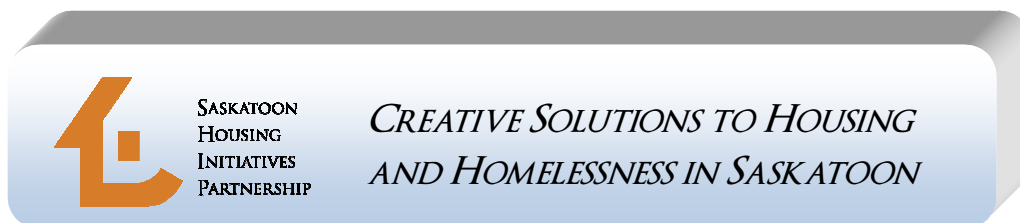
SHIP'S BOARD OF DIRECTORS

NAME	OFFICE HELD	ORGANIZATION
Tyler Stewart	President	Stewart Properties Holdings
Leigh Spencer	Vice-President	Affinity Credit Union*
Ray Neale	Secretary / Treasurer	Saskatoon Housing Authority*
Barb Cox-Lloyd	Past-President	Habitat for Humanity – Saskatoon
David Hnatyshyn	Director	Hnatyshyn Gough
Greg Jensen	Director	Royal Bank of Canada*
Barry Downs	Director	Cress Housing / Saskatoon Tribal Council*
Kent Mohn	Director	Affordable New Home Development Foundation
Bob Jeanneau	Director	KC Charities Inc.*
Tennille Thompson	Director	Provincial Metis Housing Corporation*
Jo-Ann Coleman-Pidskalny	Director	Saskatoon Housing Coalition*
Nicholas Fraser	Director	Saskatchewan Association for Community Living
Karen Kobussen	Director	Saskatoon & Region Home Builders' Assoc.*
Terry Gibson	Director	Saskatoon Health Region*
Tyler Mathies	Director	Innovative Residential
Alan Wallace	Ex Officio	City of Saskatoon*
Doug Schweitzer	Ex Officio	Saskatchewan Housing Corporation*
Liane Dagenais	Ex Officio	Canada Mortgage and Housing Corporation*
Mairin Loewen / Charlie Clark	Ex Officio	Councilor, City of Saskatoon*

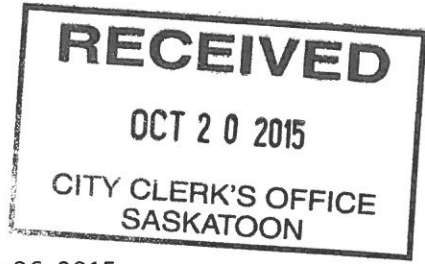
* organization has a permanent Board seat

SHIP'S STAFF

NAME	POSITION
Shaun Dyck	Executive Director
Dionne Miazdyck-Shield	Manager of Community Development
Lilani Samarakoon	Project Development Assistant
Carolyn Topping	Administrative Assistant



750-1



From: Shaun Dyck <sdyc@shipweb.ca>
Sent: October 20, 2015 10:46 AM
To: Web E-mail - City Clerks
Subject: Request to Speak

Good Morning Katelyn,

As per your email, I would like to make a request to speak to City Council on October 26, 2015.

My mailing address is below. Thank you for your assistance on this matter.

Best Regards,

Shaun Dyck
Executive Director
Saskatoon Housing Initiatives Partnership (SHIP)
203 – 220 20th Street West
Saskatoon, Saskatchewan S7M 0W9
Phone: 306.979.6707
Fax: 306.955.5852
Email: sdyc@shipweb.ca
Website: www.shipweb.ca

Confidentiality Notice

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Dear Shaun,

Thank you for submitting your PowerPoint that you intend to provide to City Council at its meeting to be held on October 26, 2015. In order for the City Clerk's Office to include you on its agenda, please provide a formal request to speak either by using the online form as follows or by email city.clerks@saskatoon.ca, making sure to include your mailing address.

The link to the online submission form is: <https://www.saskatoon.ca/write-letter-councilcommittees>.

We will ensure that your PowerPoint is set up and request that you be in the Council Chamber about 15 minutes before the meeting so that Deputy City Clerk Bryant can provide you with presentation information. You will be provided 5 minutes total, including your PowerPoint.

Thank you,
Katelyn Kuspira | tel 306.986.1693
Committee Assistant - City Clerk's Office
City of Saskatoon | 222 3rd Avenue North | Saskatoon, SK S7K 0J5
katelyn.kuspira@saskatoon.ca
www.saskatoon.ca

If you receive this email in error, please do not review, distribute or copy the information. Please contact the sender and delete the message and any attachments.

Sproule, Joanne (Clerks)

From: Olauson, Eric (City Councillor)
Sent: Monday, September 28, 2015 3:53 PM
To: Sproule, Joanne (Clerks)
Subject: Fwd: Notice of Motion

“Take Notice that at the next regular meeting of City Council, I will move the following motion:

That the following May 25, 2015 resolution of City Council be rescinded:

“that a directional closure be installed at 9th Street and Lorne Avenue (Idylwyld Drive) to restrict westbound vehicular usage.”

6320-1

From: Robert Clipperton <robert.clipperton@gmail.com>
Sent: October 11, 2015 10:35 AM
To: Web E-mail - City Clerks
Cc: president@nutana.ca; Clark, Charlie (City Councillor)
Subject: Request to Present to Council - October 26, 2015



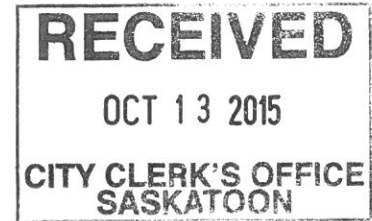
Greetings:

Please accept this communication as my request to present to Council in regards to Councillor Olauson's motion to rescind the 9th Street directional closure.

Thank you,

Robert Clipperton
427 - 9th Street
Saskatoon
306-651-0475

From: Tony Scissons <tscissons@sasktel.net>
Sent: Tuesday, October 13, 2015 10:40 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Tuesday, October 13, 2015 - 10:40
Submitted by anonymous user: 71.17.145.71
Submitted values are:

Date: Tuesday, October 13, 2015
To: His Worship the Mayor and Members of City Council
First Name: Tony
Last Name: Scissons
Address: 617 McPherson Ave.
City: Saskatoon
Province: Saskatchewan
Postal Code: S7x0n7
Email: tscissons@sasktel.net

Comments:

I am writing in regards to the closer of 9th Street E. between McPherson and the freeway. I was out of town for the general area meeting this summer so did not comment at the time. I am opposed to the closer of the street and removal of the traffic circle. The narrowest part of McPerson is between 9th and 8th so why increase traffic and cause bottlenecks there? I live at the bottom of McPerson and there is a lot of traffic but I would never ask for it to be closed. We live in a city and the streets belong to all of us not just the people that live on that block.

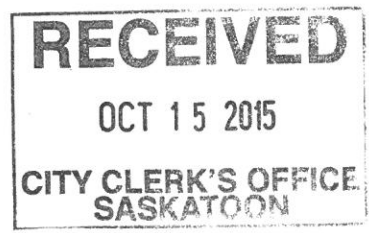
I would like to speak to council on this matter at your meeting on the 26th of this month.

Regards

Tony Scissons

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/44932>

From: Bruce Stone <bruce.stone@sasktel.net>
Sent: Wednesday, October 14, 2015 8:17 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Wednesday, October 14, 2015 - 20:17
Submitted by anonymous user: 207.47.244.226
Submitted values are:

Date: Wednesday, October 14, 2015
To: His Worship the Mayor and Members of City Council
First Name: Bruce
Last Name: Stone
Address: 321 9th St. E.
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N-0A5
Email: bruce.stone@sasktel.net

Comments:
In the September 28, 2015 council meeting the minutes indicates that Councillor E. Olauson provided the following notice of the motion:

'That the following May 25, 2015 resolution of City Council be rescinded:
"That a directional closure be installed at 9th Street and Lorne Avenue/Idylwyld Drive to restrict westbound vehicular usage."

I would appreciate the opportunity to comment on this motion by making a presentation to City Council at the next scheduled council meeting.

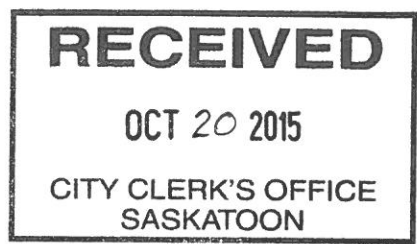
Thank you,

Bruce Stone
321 9th St. E.
Saskatoon
306-652-9321
bruce.stone@sasktel.net

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/45354>

6350-1

From: Mike McKague <mikemckague@sasktel.net>
Sent: October 20, 2015 9:58 AM
To: Web E-mail - City Clerks
Subject: Request to speak to council - Oct 26, 2015



Greetings:

Please accept this communication as my request to present to Council in regards to Councillor Olauson's motion to rescind the 9th Streetdirectional closure.

Thank you,

Mike McKague
President - Nutana Community Association
712 11th St East
Saskatoon
3062201625

From: Peter Brown <peterbrown@sasktel.net>
Sent: October 01, 2015 3:58 PM
To: City Council
Subject: Form submission from: Write a Letter to Council

Submitted on Thursday, October 1, 2015 - 15:57
Submitted by anonymous user: 64.110.197.187
Submitted values are:

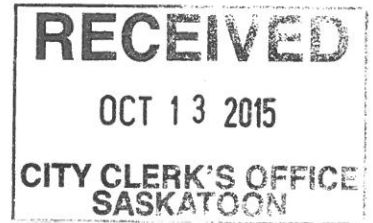
Date: Thursday, October 01, 2015
To: His Worship the Mayor and Members of City Council
First Name: Peter
Last Name: Brown
Address: 114 9th St. E.
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0A2
Email: peterbrown@sasktel.net

Comments:

Dear City Council,
This missive is to encourage council members to vote against rescinding the approval of the directional closure on 9th Street East.
First, the process - three Neighbourhood Traffic Review meetings and a Public Hearing of City Council - which arrived at the decision of the directional street closure was a sterling example of participatory democracy. All meetings were advertised throughout the Nutana neighbourhood and once convened provided accurate information to the participating public. The meetings provided lengthy opportunities for speaking to all the issues around the directional street closure. It was a stellar example of how City Council should deal with local, neighbourhood issues. Thank you.
Secondly, the directional street closure, in my estimation, speaks to the often competing issues of safety and convenience. The public meetings referenced above provided specific information that traffic on the blocks in question of 9th Street was both too heavy and often too speedy for the safety of residents. Anyone attending the meetings would have clearly heard that the inconvenience of having to travel one block off 9th Street to the major throughfare of 8th Street was completely and thoroughly secondary to the safety provided by the directional street closure.
Finally, I ask Council members to vote against rescinding the motion and in doing so support the city neighbourhood process described above, support citizens' proactive rather than reactive response to city issues, and embrace the safety for citizens that the directional street closure on 9th Street will provide.
Regards,
Peter Brown

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/43404>

From: Robert Sykes <sykesrob@hotmail.com>
Sent: Monday, October 12, 2015 11:48 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Monday, October 12, 2015 - 23:47
Submitted by anonymous user:
Submitted values are:

Date: Monday, October 12, 2015
To: His Worship the Mayor and Members of City Council
First Name: Robert
Last Name: Sykes
Address: 131 Main St
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0B3
Email: sykesrob@hotmail.com

Comments:

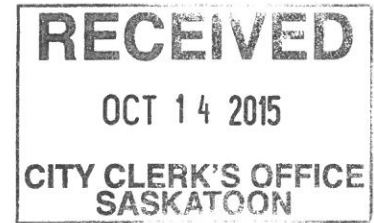
Re: Closure of 9th St freeway access

There is no need to close this road. It has a low volume of use (as your traffic counts will confirm), it mainly helps only the local neighborhood and IT IS NOT COSTING ANYTHING !! Why spend \$30,000 to close it. This is our tax dollars being used against our neighborhood FOR NO GOOD REASON.

I repeat \$30,000 dollars. \$30,000 dollars! There are many better uses for this money.

The results of this submission may be viewed at:
<http://www.city.saskatoon.sk.ca/node/398/submission/44890>

From: Gordon Punter <flashgordon40@hotmail.com>
Sent: Tuesday, October 13, 2015 11:01 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Tuesday, October 13, 2015 - 23:01
Submitted by anonymous user: 216.174.134.2
Submitted values are:

Date: Tuesday, October 13, 2015
To: His Worship the Mayor and Members of City Council
First Name: Gordon
Last Name: Punter
Address: 233 9th St. East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N0A3
Email: flashgordon40@hotmail.com

Comments:

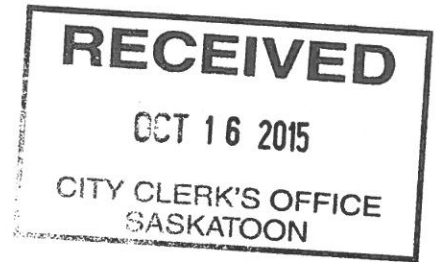
To Whom It May Concern,

This letter is in regards to the road access directional closure on 9th Street at the Idylwyld ramp. I personally used this access numerous times a day and am against its closure. To have one person complain about traffic and not have the voice of the community about such a closure is unfair and so I am voicing my displeasure of said actions by the city. This would be a waste of city funds and a frustration to many in the area who use the access. Thank you for time,
Gordon Punter

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/45061>

6320-1

From: Carolyn Jones <carrots1@sasktel.net>
Sent: October 16, 2015 7:22 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



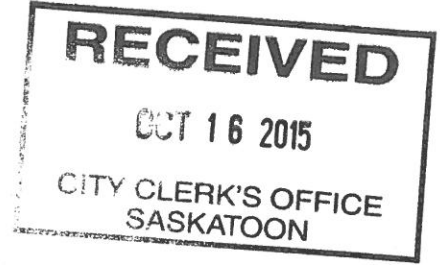
Submitted on Friday, October 16, 2015 - 07:22
Submitted by anonymous user: 207.47.247.123
Submitted values are:

Date: Friday, October 16, 2015
To: His Worship the Mayor and Members of City Council
First Name: Carolyn
Last Name: Jones
Address: 230 Main Street
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0B5
Email: carrots1@sasktel.net

Comments: I am disturbed about the 9th Street directional closure and wish to note my opposition to it. If the objective is to appease the residents on 9th Street, how will the increase in traffic be justified to the residents of 8th Street? In addition, north and south bound vehicles cannot meet on McPherson Ave. because of parking being allowed on both sides of the street. You must pull over in a vacant spot, if there is one, to allow an opposing vehicle to pass. Furthermore, McPherson at 8th Street is a pedestrian crossing used by many school children. Increased traffic on 8th Street increases the risk to them and all pedestrians and further restricts the flow of traffic to the freeway. I respectfully ask that you reconsider this closure.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/45652>

From: Rob Young <Karen.dyck@heartunfolding.com>
Sent: October 16, 2015 6:11 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Friday, October 16, 2015 - 06:11
Submitted by anonymous user: 71.17.246.142
Submitted values are:

Date: Friday, October 16, 2015
To: His Worship the Mayor and Members of City Council
First Name: Rob
Last Name: Young
Address: 203-212 10th Street EAst
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 2T6
Email: Karen.dyck@heartunfolding.com

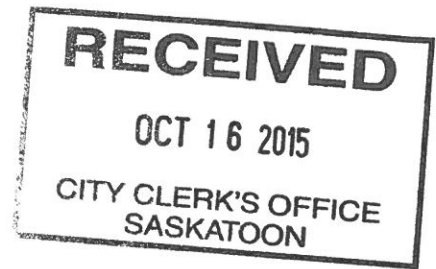
Comments:

I would like to keep the 9th Street exit onto Idylwyld open...
with it closed the only other option for me is to travel up McPherson which is already slowed down to one lane traffic most of the time because of parked cars on each side of the street, this combined with people exiting off of 8th creates even more congestion.
The 9th street exit, is wide and easily accommodates 2 lane traffic which makes for better traffic flow, and less potential of congestion and collisions.

I am grateful for your attention to reversing this decision.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/45650>

From: Karen Dyck <Karen.dyck@heartunfolding.com>
Sent: October 16, 2015 6:05 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Friday, October 16, 2015 - 06:05
Submitted by anonymous user: 71.17.246.142
Submitted values are:

Date: Friday, October 16, 2015
To: His Worship the Mayor and Members of City Council
First Name: Karen
Last Name: Dyck
Address: 203-212 10th Street EAst
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 2T6
Email: Karen.dyck@heartunfolding.com

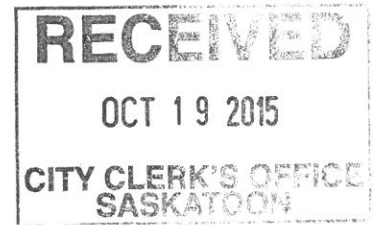
Comments:

I AM writing to state that I AM opposed to the 9th Street Directional Closure. This particular street is one of the streets in the Nutana area that is actually wide enough for 2 vehicles to pass each other without it moving down into single lane traffic which is the situation now that we have been forced to use McPherson to get onto 8th street, there is always a build up of people attempting to come off 8th onto McPherson while I AM attempting to come on to 8th and with the parked cars on the street makes for congestion.

I thank you for considering this situation before making a definite closure of this avenue for traffic flow.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/45649>

From: Mark Radu <m.radudio@gmail.com>
Sent: Sunday, October 18, 2015 5:07 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Sunday, October 18, 2015 - 17:06
Submitted by anonymous user: 64.110.196.101
Submitted values are:

Date: Sunday, October 18, 2015
To: His Worship the Mayor and Members of City Council
First Name: Mark
Last Name: Radu
Address: 303-10th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 2T6
Email: m.radudio@gmail.com

Comments:

I am writing to state my opposition to the 9th Street Directional Closure currently being considered by City Council.

In the 9 years in which I have lived at the corner of McPherson Ave and 10th Street, it has been my experience that McPherson Ave, from 8th St. and northbound to Sask. Cr., has always been a bottleneck reduced to single lane one-way traffic. This is due to the the narrowness of the street combined with vehicles parked bumper to bumper on both sides of the street from 8th St. northward to Sask Cres.

Turning off of 8th St. to travel the 3 blocks to my home has always been somewhat stressful as it necessitates scanning far ahead to ensure no oncoming traffic will block the street before I reach the intersection at 9th street, then scan again to reach the Main Street intersection and again to reach my destination at 10th and McPherson. It's necessary to reverse this in order to drive safely southbound back to 8th St.

This congestion and narrowness along McPherson can be difficult on a normal day to day basis in good weather but is compounded by winter snow and ruts and whenever events are staged at Rotary Park. This turns McPherson into a parking lot of gridlock with vehicles attempting to both enter and exit riverbank events.

The one small saving grace for me, has been the option of exiting to the freeway (Idylwyld dr.) northbound via the 9th St. exit. This exit permits me to avoid the last southbound portion of McPherson and any confrontations with traffic entering McPherson off of 8th St.

To permanently close this access will increase the traffic both entering and exiting the intersection of 8th St and McPherson as well as creating more congestion on the very narrow single lane open to vehicular traffic.

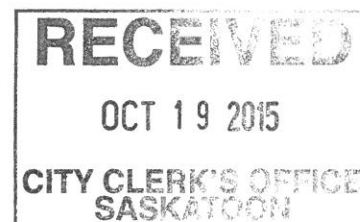
I urge City Council to keep the 9th Street exit/ entrance open.

Respectfully submitted,

Mark (and Laurie-Ann) Radu

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/45880>

From: Bob Trach <batrach@shaw.ca>
Sent: Sunday, October 18, 2015 9:34 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Sunday, October 18, 2015 - 21:33
Submitted by anonymous user: 70.64.65.22
Submitted values are:

Date: Sunday, October 18, 2015
To: His Worship the Mayor and Members of City Council
First Name: Bob
Last Name: Trach
Address: 132 Main St.
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0B2
Email: batrach@shaw.ca
Comments:
Re: 9th St. Directional Closure

After receiving the letters from the engineering dept, I have yet to find a reason as to why the access to Lorne avenue is being closed. In my 33 years of living in this area, it seems to have worked as it was designed. During the last few years, the density of people has increased and with the Iron Bridge closed, will another outlet closed actually help. Macpherson avenue is barely passable in winter with all the residential parking.

I don't think the extra traffic it will have to accommodate is going to be good for the people who live there.

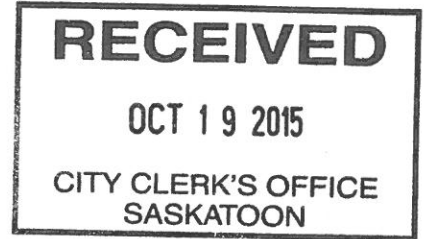
I hope you will reconsider this move and leave 9th St. open for thru traffic.

Thank-you
Bob Trach

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/45912>

6320-1

From: City Council
To: Marie
Subject: RE: Form submission from: Write a Letter to Council



-----Original Message-----

From: Marie Jensen [<mailto:mariejensen@sasktel.net>]
Sent: Monday, October 19, 2015 11:28 AM
To: City Council
Subject: Form submission from: Write a Letter to Council

Submitted on Monday, October 19, 2015 - 11:28
Submitted by anonymous user: 207.47.240.69
Submitted values are:

Date: Monday, October 19, 2015
To: His Worship the Mayor and Members of City Council
First Name: Marie
Last Name: Jensen
Address: 235-11th.st.e.
City: Saskatoon
Province: Saskatchewan
Postal Code: S7NOE5
Email: mariejensen@sasktel.net
Comments:
Mayor and city councilors
Closing 9th. St. E at the freeway entrance.
I am not in favor of this closure.

Reasons - fixes a problem when there isn't a problem.

Makes for very heavy traffic on macpherson ave...very busy street..now even busier...waiting your turn because it becomes one lane.

Waste of taxpayers money....

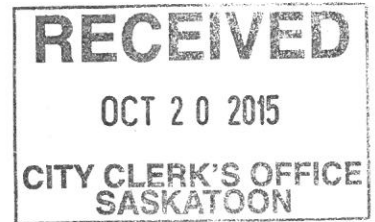
I am asking you to please vote "yes" to keep 9th. St. open to the freeway.

Thank you Marie Jensen. 306-653-2500

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/45971>

6320-1

From: Charles Foster <chfoster@shaw.ca>
Sent: Monday, October 19, 2015 5:56 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Monday, October 19, 2015 - 17:55
Submitted by anonymous user: 70.64.71.219
Submitted values are:

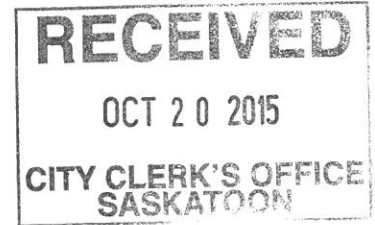
Date: Monday, October 19, 2015
To: His Worship the Mayor and Members of City Council
First Name: Charles
Last Name: Foster
Address: 1018 McPherson Ave
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0Y5
Email: chfoster@shaw.ca

Comments: Regarding 9th St. directional closure-I live on the NW corner of 8th Street and McPherson Ave. I've been here for 4 years so have had an opportunity to observe the traffic flow at this intersection. It is a busy intersection that sees a lot of traffic - particularly at rush hour times of the day. The problems at this intersection become worse when vehicles park on both sides of McPherson Ave - choking off McPherson Ave to one lane. - I am writing to state that I do not agree with the closure of 9th street access to the freeway as this will force even more traffic south on McPherson to 8th st. - making an already dangerous intersection even worse. .Respectfully submitted - Charlie Foster

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46071>

6350-1

From: Derek and Nora Potts <derek.potts@sasktel.net> on behalf of Derek and Nora Potts <derek.potts@sasktel.net>
Sent: Monday, October 19, 2015 11:24 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Monday, October 19, 2015 - 23:24
Submitted by anonymous user: 206.163.236.27
Submitted values are:

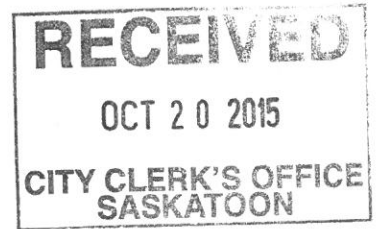
Date: Monday, October 19, 2015
To: His Worship the Mayor and Members of City Council
First Name: Derek and Nora
Last Name: Potts
Address: 218 - 8th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7H 0P3
Email: derek.potts@sasktel.net

Comments: As long time residents of Buena Vista, we strongly urge city council to revisit the decision to proceed with a permanent closure of the westbound access to the freeway from 9th Street East. We do not believe that enough was done to seek the input of the many residents who will be negatively affected by this decision. McPherson Avenue is too narrow to accommodate the increased traffic that could be so much easier directed through the much wider 9th Street East. Hopefully logic, regard for safety, and a duty to spend our tax dollars wisely will prevail. Thank you.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46113>

6320-1

From: Randall and Suzanne Winnitowy <rwinnitowy@sasktel.net>
Sent: Monday, October 19, 2015 8:35 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Monday, October 19, 2015 - 20:34
Submitted by anonymous user: 204.83.77.243
Submitted values are:

Date: Monday, October 19, 2015
To: His Worship the Mayor and Members of City Council
First Name: Randall and Suzanne
Last Name: Winnitowy
Address: 425 9th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N0A7
Email: rwinnitowy@sasktel.net

Comments:

I am providing comments in SUPPORT of the Notice of Motion to RESCIND the approval to install the directional closure.

As a long time resident of the 400 block of 9th Street, recent decisions regarding traffic movement in the neighbourhood suggest vehicular traffic is unwelcome and perhaps unnecessary in our neighbourhood.

As a resident who regularly walks, bikes and drives a vehicle, the use of the vehicle is becoming more and more restricted. The complete closure of the Traffic Bridge and the 9th Street Directional Closure will leave the only direct vehicular access to downtown via the Broadway Bridge.

As a regular user of what downtown Saskatoon has to offer, especially in the evening and in the winter, these restrictions are difficult to rationalize. If the argument for the 9th Street Directional Closure is based on safety and security, the traffic circle on 9th Street and Melrose appears to be a highly effective mechanism. If the argument for the Closure is based on pedestrian and cyclist safety, there are plenty of mechanisms (ie. crosswalks, yield signs) that are already in place to address this concern.

Vehicle movement is fundamental to life in any city. As a regular walker and cyclist, the City has done an excellent job of accommodating these needs. But as a vehicle driver, it appears the City is trying to accommodate all other interests in our neighbourhood except for the need to drive a vehicle and access other parts of this wonderful city.

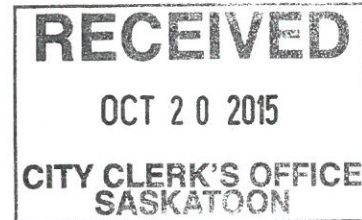
There are many cul-de-sacs in the city. They have been established to limit traffic circulation. A street is designed to move traffic and provide access to minor and major arteries. Ninth Street does not need to become a cul-de-sac.

Based on these arguments, the City should SUPPORT the Notice of Motion to RESCIND the approval to install the directional closure.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46101>

LISA KOZMYK

209 9th Street East, Saskatoon, SK, S7N 0A3
Tel: (306) 652-5876 Cell: (306) 270-0669
E-mail: lisa.kozmyk@gmail.com



October 20, 2015

Dear Council,

I am voicing my concern over the permanent closure of 9th street to the on ramp of the Idlywyld Freeway. The round about that had been put in place was an effective deterrant to thru traffic and I feel that the closure will create dangerous conditions by forcing outgoing traffic down Melrose Avenue to 8th Street.

With the on street parking we have in our older neighbourhoods, Melrose Avenue, especially between 8th and 9th Street becomes a one lane street. During the winter, this becomes more evident and it becomes very dangerous because of the ruts in our roads. Given the city's policy to not grade ruts until they hit a certain depth, the ability to manoeuvre out of the way of cars entering Melrose Avenue from 8th is severely diminished. Since more cars will now be forced down Melrose Avenue to access the Freeway, I believe we will see an increase in incidents on this block. As it is, I know personally of two people have had there cars side swipt on that block and both incidents occurred during the winter.

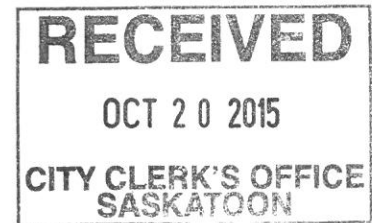
I would please ask my councillor Charlie Clark to take this letter as equivant to my presence at a meeting to discuss this closure. I was not aware of the meeting that took place previously and I feel that my written objections should have as much weight as attending a meeting.

Regards,

A handwritten signature in blue ink, appearing to read "L. Kozmyk".

Lisa Kozmyk

From: Gerard Rodier <g.rodier@shaw.ca>
Sent: October 20, 2015 7:41 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Tuesday, October 20, 2015 - 07:40
Submitted by anonymous user: 71.17.218.121
Submitted values are:

Date: Tuesday, October 20, 2015
To: His Worship the Mayor and Members of City Council
First Name: Gerard
Last Name: Rodier
Address: 138 Main Street
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N0B2
Email: g.rodier@shaw.ca

Comments:

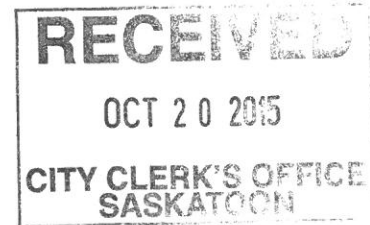
I am opposed to the closing of the 9th Street at the freeway entrance. I use this access on a daily basis since the Victoria (Traffic) bridge has not been rebuilt. I am very disappointed about the progress on this bridge as well.

Please do not close 9th Street.

Thank you

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46132>

From: Vincent Martin <vincemartin@shaw.ca>
Sent: Tuesday, October 20, 2015 3:39 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Tuesday, October 20, 2015 - 03:38
Submitted by anonymous user: 70.64.92.130
Submitted values are:

Date: Tuesday, October 20, 2015
To: His Worship the Mayor and Members of City Council
First Name: Vincent
Last Name: Martin
Address: 714 McPherson Avenue
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N0X8
Email: vincemartin@shaw.ca
Comments:
His Worship the Mayor and Members of City Council:

Re: We are opposed the closure of 9th street at the freeway on-ramp

This letter is written in opposition to the closure of 9th street and the on-ramp to the freeway. This is an important intersection that allows residents of the area access to the on-ramp without having to further congest the tight intersection at McPherson and 8th Street. With the closure of the traffic bridge, now more than ever we have noticed an increase of traffic in front of our home. No doubt a good deal of this traffic is heading to the Sid Buckwold Bridge to cross the river. Once the traffic bridge is re-opened we feel quite confident that traffic on my street and on 9th will decrease.

Questions for City Council:

Who does this closure serve?

How many people are served by this closure and how many are inconvenienced by it?

Why does 9th Street, a cul-de-sac have the right of way?

How does this slow traffic on 9th Street?

If this neighbourhood closure is being addressed by council, will all side street access or exits be closed to meet the design aspects of the day?

When we wrote our city councillor about this closure he responded by saying that if we were planning a city today, we would not have an access to the freeway directly from a side street. Note: This is access to an on-ramp, not directly to a freeway!) Now Charlie Clark may be correct on the design aspect, however this street and neighbourhood are not being designed today. They were designed some 40 years ago. 9th street is a nice wide street that allows cars to access and exit the on-ramp. Closing this access is not in the best interest of the residents of the area. It seems to just be making problems elsewhere.

Suggestions:

To reduce speed on 9th Street, make the intersection at McPherson and 9th a four way stop

To further reduce traffic speed on 9th Street, install a large wide speed bump with signs similar to what the city of Calgary did in areas like Killarney to slow traffic on residential side streets
To reduce traffic speed on McPherson, install a four way stop at 10th Street (note: this is 2 blocks away from 9th Street as Main Street run parallel to and is between 9th and 10th).

We have lived on McPherson Avenue since 2001 and have seen several changes to our area. Most good, and we applaud our Mayor and the members of city council in general for a job well done. We feel Charlie Clark has served us well, with the exception of on this issue (truly no hard feelings Charlie). It is not easy serving a growing city like Saskatoon or maintaining a balance between special interests and the greater good.

We trust city council will see the value in re-opening this access and looking at the suggestions above as well as those from the other residents before carving a decision in stone.

Please submit this letter for consideration by City Council for the review meeting being held on Monday October 26th, 2015 to discuss a motion to rescind the approval for the directional closure.

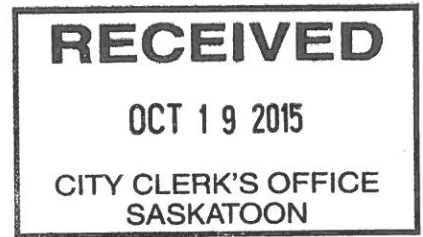
Thank you for considering our points of view

Vincent Martin
Joseph Pfeifer
101009226 SK Ltd, owners of 804 McPherson Avenue and 716 Melrose Avenue, Two small apartment building for a combined 27 apartment homes.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46121>

6320-1

From: Lyndsay Zelinski <lyndsayzelinski@hotmail.com>
Sent: October 20, 2015 9:29 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Tuesday, October 20, 2015 - 09:29
Submitted by anonymous user: 66.128.91.100
Submitted values are:

Date: Tuesday, October 20, 2015
To: His Worship the Mayor and Members of City Council
First Name: Lyndsay
Last Name: Zelinski
Address: 1003 McPherson Avenue
City: Saskatoon
Province: Saskatchewan
Postal Code: s7k 3j8
Email: lyndsayzelinski@hotmail.com

Comments: I have noticed an increased traffic flow on McPherson due to closure of 9th St. (which is very narrow on the 1000 block), allowing one car at a time to pass. Drivers use McPherson to access 8th St. from 9th to enter onto the freeway. Drivers also will use 1000 Block McPherson to access Rotary Park due to the closure. Also, I work downtown (5 to 6 days a week), and live on the east side of McPherson which has been an inconvenience. But foremost, I believe the closure of 9th St. is a safety concern that City Council must reconsider.

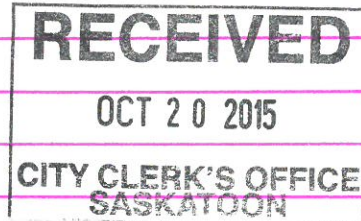
The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46151>

6320-1

20 October 2015 9:00 Am.

Subject: 9th Street East Directional Closure.

City of Saskatoon
City Clerk
Saskatoon City Hall



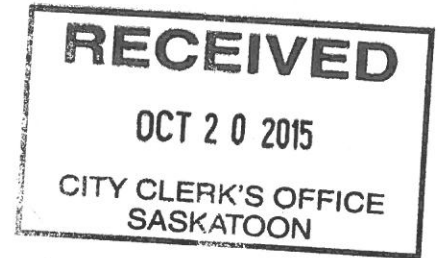
Dear Mayor and Councillors:

As a resident in the area of Nutana affected by closure of 9th St East @ Lorne Avenue Freeway intersection. I would like to add my voice and opinion to keeping the roadway at this intersection open to all traffic.

Given the freeway closes off all West Bound roads from 11th East up to Main Street East, this road to the freeway is necessary for traffic leaving the area. I request you to reconsider + listen to us.

Yours truly,
L. Neault.

From: Neale Buettner <neale.buettner@producer.com>
Sent: October 20, 2015 10:52 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Tuesday, October 20, 2015 - 10:51
Submitted by anonymous user: 198.169.210.253
Submitted values are:

Date: Tuesday, October 20, 2015
To: His Worship the Mayor and Members of City Council
First Name: Neale
Last Name: Buettner
Address: 306-208 Saskatchewan Cres. E.
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0K6
Email: neale.buettner@producer.com
Comments:

I am writing in response to communications I have had with your Transportation Department in regard to the Traffic Restrictions currently in place in Nutana, at 9th St & MacPherson Avenue, and 9th St and Idylwyld Drive.

The duty of all elected officials, and by logical extension, city employees is to ensure that the City of Saskatoon is managed in the best interests of its citizens. I believe the duty of the Transportation Department should be to ensure that Traffic moves in as efficient pattern as is possible.

The elimination of a perfectly good freeway entrance is, in my opinion, counterproductive to moving traffic smoothly in the Nutana area. Initiated as a Traffic 'calming' tactic, the effort was misplaced as traffic on this street does not need to be calmed. I drive through the area 2 to 3 times each day, and I very, very rarely see another vehicle on 9th Street. Furthermore the current set-up has the effect of re-routing traffic to 8th Street, and Idylwyld, and Lorne Ave, which is perhaps the most technically difficult intersection in the city, next only to 5 Corners.

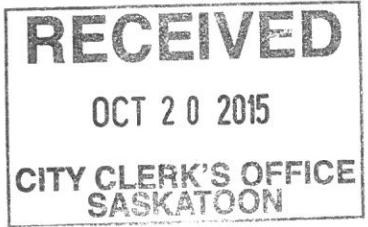
Traffic counting studies conducted by your department indicated no problem with Traffic counts, or excessive speed in the area. At that point, the street should have been restored to its' initial set-up with freeway access at 9th and Idylwyld, and stop signs at east and west-bound 9th St and MacPherson.

I would also like to suggest that Councillor Charlie Clark recuse himself from this vote, as he is clearly in a conflict of interest with home address on 9th Street. The pressure he applies on the Transportation Department obviously renders them unable to make a logical, sound engineering based decision in this matter as was exhibited by the ridiculous measure of installing an in-intersection round about at 9th & MacPherson.

Thank you
Neale Buettner
Taxpayer, and Concerned Citizen

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46167>

From: Eric Cline <ericcline2012@gmail.com>
Sent: Tuesday, October 20, 2015 12:18 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Tuesday, October 20, 2015 - 12:18
Submitted by anonymous user: 207.195.31.194
Submitted values are:

Date: Tuesday, October 20, 2015
To: His Worship the Mayor and Members of City Council
First Name: Eric
Last Name: Cline
Address: 221-11th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0E5
Email: ericcline2012@gmail.com

Comments:
(CORRECTED VERSION)

I read concerning the closure of 100 block 9th Street East ("9th") as a route of access to the freeway and that version of freeway access traffic to 1000 block McPherson ("McPherson"). I believe that this decision is seriously flawed, for the following reasons:

1. Context: 9th, as an access Street to the freeway, takes approximately 1/2 of the freeway traffic from Northwest Nutana origins, which is the only place it would come from to that access route. This decision, apparently to ease the burden for residents on 9th, doubles the burden for residents on McPherson.
2. McPherson is 66% as wide as 9th.
3. 9th is 150% as wide as McPherson.
4. 9th is wide enough to accommodate parking on both sides, and 2 lanes of traffic in the middle. While it may have existed before the freeway, it could have been designed for the purpose it has served for almost 50 years.
5. McPherson is not wide enough to comfortably accommodate the above and is already difficult to navigate with 50% of the traffic in question.
6. There are 21 residential units on 9th. There are 25 residential units including those in an apartment building on McPherson.
7. There is an acceleration lane from 9th onto the freeway.
8. There is no acceleration lane from McPherson to 8th Street, where traffic is going just as fast if not faster than traffic that has just turned onto the freeway.
9. 9th goes in the proper direction to enter the freeway from Northwest Nutana. McPherson goes the wrong way, requiring another block south to be driven, then one block west on 8th in busy traffic once that traffic is entered, then and added unnecessary block north to be driven, which is nonsensical in terms of traffic planning, safety and the environment.
10. 9th has been a freeway access for approximately 48 years. Everyone who lives there has purchased their property with that knowledge, reflected in the purchase price.
11. McPherson has not been a route for access to the freeway, while it has been a route for exit from the freeway. People who have moved there have purchased homes and moved there with that knowledge, reflected in the purchase price.

12. If it is an unfair burden to the residents of 9th, presently with one half of the relevant traffic on their wide roadway, why is it a fair burden to the residents of McPherson to take on 100% of the relevant traffic on their narrow roadway, not to mention the inconvenience to the driving public/civic taxpayers? 9th was not designed or sold as a cul-de-sac. Why is it fair to effectively make it into one for residents of 21 units while doubling the burden for the residents of 25 units on McPherson and inconveniencing the general public?

13. 9th does not take traffic onto a busy thoroughfare without an acceleration lane, as indicated above. The traffic on 9th adding to the freeway merges with freeway traffic only. The traffic on 8th after turning off McPherson can be freeway, Lorne Avenue or through traffic.

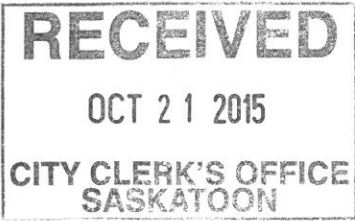
14. This indefensible plan is not only unnecessary, but completely nonsensical and, therefore, a waste of taxpayers money.

This choice seems very odd, even in this ward.

Sincerely,
Eric Cline

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46194>

From: Ksenija Smiljic <ksenija@sasktel.net>
Sent: Tuesday, October 20, 2015 9:47 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Tuesday, October 20, 2015 - 21:46
Submitted by anonymous user: 198.245.115.24
Submitted values are:

Date: Tuesday, October 20, 2015
To: His Worship the Mayor and Members of City Council
First Name: Ksenija
Last Name: Smiljic
Address: 616 McPherson Ave
City: Saskatoon
Province: Saskatchewan
Postal Code: S7N 0X6
Email: ksenija@sasktel.net
Comments:

As a member of the Nutana community, I do not support the 9th St. directional closure because I don't feel that it benefits the residents of the area in addressing the negative issues we are all facing with parking, speeding and volume of vehicles driving through our whole neighborhood. It simply relocates it to another street. This will force many vehicles to go up McPherson which only has 1 lane for driving & 2 parking lanes, to 8th St. E. (unlike 9th St which has 2 driving lanes as well as 2 parking lanes). The 8th St/McPherson intersection is a very busy intersection with almost constant heavy traffic. So much so that the City installed pedestrian lights to allow people to safely cross this very busy street. I drive this intersection every day & know how difficult it can be to turn left to go east on 8th St. I usually have to wait for a few minutes before I can make that left hand turn safely. Visibility to the east can be very limited by parked cars. Cars behind me that want to turn right on 8th St. cannot proceed until I have made my turn as McPherson is a narrow street. There are always cars parked on both sides of the street which only allows for 1 moving vehicle, whether that vehicle wants to go north or south.

McPherson is unique in that it is only 1 of 2 streets that actually goes all the way to Saskatchewan Cr/the River (& the Sid Buckwold Bridge) & Eastlake is the other, up by Nutana Collegiate & the Broadway Bridge. Rotary Park is at the bottom of McPherson along the river & it hosts some sort of event. festival, race, concert etc almost every weekend in the summer. Hundreds of people drive through our area (especially down the 600 block McPherson where I live as it is the last block before Sask Cr.) & park everywhere like for example during the Fire Works Festival. It can get difficult to get to & from our home & in and out of our driveway (which we built after purchasing the house as we realized on street parking was scarce, that was 20 years ago, It has steadily gotten much worse). We accept this as we understand that it is the price we pay for living in an area so close to the river, downtown and Broadway. There has been such a large increase in the number of vehicles that drive through our area for so many different reasons but that can be said of the entire city as well. We should be focusing more on safety and moving traffic efficiently. It's unfortunate that the traffic circle was taken out at 9th St & McPherson as it was a great way to slow traffic down and yet keep it moving from all 4 directions. A sign that actually said "yield to the left" would have helped alleviate the issue of people not knowing what to do at the circle and the pedestrian crossing lines needed to be prominently visible which they weren't. It is ironic that the circle was replaced with a stop sign for

McPherson Ave traffic creating this wide long speedway along 9th St from Idylwyld all the way to Victoria Ave. We also don't know what the impact the reopening of the traffic bridge will have on traffic flows and until we do it wouldn't make sense to make any permanent changes. I believe the 9th St. access to Idylwyld plays a vital part of sharing the burden of having too many vehicles and/or speeding vehicles up and down our streets. Closing 9th St sends traffic onto other streets that weren't built as wide as 9th St which also exacerbates the issue. The number of vehicles will never decrease so we need to develop strategies that will make sure that people are not racing through our neighborhood to get to that Idylwyld access and I believe closing 9th St will not fix that problem. The cars will still come but they will create bigger issues on other streets, streets that are already suffering traffic issues. It doesn't seem fair or right. I thank you for your time and consideration. Ksenija Smiljic

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/46317>



Office of the City Clerk
City Hall, Saskatoon

October 9, 2015

Attention: Joanne Sproule
City Clerk

On September 15, 2015 I submitted to your office, a Letter to Council attached to a number of petitions opposed to the closure of the 9th St freeway access.

At the time it was not able to be presented, however this matter is now to be heard by Council on October 26th through a motion to rescind. I request that my original submission now be included along with the attached cover letter as a presentation to Council at the October 26th meeting.

Respectfully yours,

A handwritten signature in cursive script that reads "Laurel Beaumont".

Laurel Beaumont
1012 McPherson Ave
Saskatoon, Sask. S7N 0Y5
(306)717-0305

On September 15, 2015 I presented the attached letter along with petitions from 97 people opposed to the closure of 9th Street.

Since that time, a temporary closure of 9th St was installed at the freeway access in lieu of the originally proposed permanent closure to measure its effects on the surrounding area. Traffic counts were also obtained before and after the closure.

One of two outcomes will have been recorded. Either diverted traffic backed-up and created problems on the narrow avenues that run adjacent to 9th, or, no problems resulted which would prove that there wasn't adequate traffic volume to justify the exorbitant cost of the closure.

In addition, the traffic circle was removed and stop signs were installed on McPherson Ave, 9th St was left uncontrolled. This configuration of signage proved to be unsafe due to obstructed sight lines caused by hedges and trees on corner properties and vehicles parked on 9th. It also failed to address the concern of speed on 9th St.

McPherson Ave has always had a constant and heavy flow of traffic in both directions as it is the only direct access between 8th St and Sask Cres (Rotary Park). As this is the case, I'm certain that traffic counts will have justified the installation of a 4-way stop at this intersection, a solution which would be the most logical and cost effective way to calm traffic.

I also want to bring your attention to what I feel is an underestimation of the cost of construction for the installation of a permanent turn-a-round. Table 5-1 of The Nutana Traffic Review estimates that it would be \$30,000.00, the same amount was stated for the installation of curb extensions at another location.

At any rate, it is an outrageous amount of public funds to allot to a project that does not have majority support of the neighbourhood residents who actually use the street in question. There is almost an equal number of area residents opposed to this closure as there were people in attendance at the general meetings, people who may or may not live near or be directly affected by the closure. There is simply not enough evidence to support the expenditure.

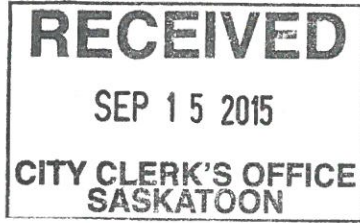
In closing, I request that Council consider the logic of the situation, acknowledge the area residents opposed to this closure and vote to allow the 9th St freeway access to remain open.

Respectfully yours,



Laurel Beaumont
1012 McPherson Ave
Saskatoon, Sask. S7N 0Y5
(306)717-0305

6330-1
x 6295-1



Letter to Saskatoon City Council:

September 15, 2015

The decision to close the 9th St Freeway on-ramp access was made without direct consultation with the residents of the neighborhood that would be directly affected by the closure. Many, if not most of us were unaware of the "area meetings" that resulted in this recommendation to Council, and as such did not attend. For most of us, the first we heard of Council's decision was the article in the Star Phoenix.

A road closure is a major and drastic decision that should not be based solely on the opinions of a few residents who attended a general area meeting. The people who actually use the road in question should have been surveyed to obtain their input. Logistics also dictates that the constraints of the surrounding streets and avenues where traffic will be shifted needs to be taken into account. For example, McPherson Ave is not wide enough to accommodate two-way traffic and vehicles must pull over wherever possible to allow oncoming vehicles to pass before continuing. As a result, any southbound vehicles trying to turn right onto 8th street to access the only remaining freeway access will completely block any other traffic on the avenue. It will be impossible for cars to turn right onto MacPherson from 8th street, causing traffic issues on 8th.

Additionally, you should be aware that traffic counts on 9th street are outdated, and no traffic counts have been done on the affected side streets.

9th Street is primarily a neighborhood route which not only alleviates the pressure off the 8th St freeway access but also access onto 8th St from the surrounding avenues. By leaving 9th St open you allow for a safer and more efficient distribution of traffic at peak periods, a goal which should be considered on all traffic corridors.

Furthermore, since council voted to close this access, several additional factors have come to light, which will doubtless increase the estimated building cost. For example, it is rumored that an access gate for emergency vehicle access will have to be added, and a majestic American Elm Tree may have to be removed. The tree would be a great loss to the urban forest at a significant cost, as would the gate, and consequential snow clearing for access to said gate. It is our understanding these unplanned costs could double the project's estimated cost. A project we believe the majority of the residents are opposed to.

I am taking the liberty of enclosing a sampling of the neighborhood residents opposed to the closure of 9th Street. These are people who actually use this route and don't want to lose this important corridor.

Now that official opposition to this street closure has been presented to you and you have been familiarized with the area, we request that Council reconsider their initial decision and vote to allow 9th Street to remain open.

Respectfully yours,

Laurel Beaumont
1012 McPherson Ave
Saskatoon, Sask. S7N 0Y5
(306) 717-0305

busstop@sasktel.net

Please contact to advise route taken.
306 (717) 0305
306 (653) 0456 mess mgr
busstop@sasktel.net

To Saskatoon City Council:

As an affected resident of the proposed closure of the 9th street freeway access, I wish my view to be known:

I completely disagree with the proposed closure and respectfully request Council reverse it's decision.

I do not believe sufficient consultation with affected residents was employed and respectfully request Council to review it's decision, negate the previous decision and initiate a new vote, following reasonable neighborhood consultation.

Name: Alaura Blackstone and Mohammad Abushaar

Address: #577-212 10th St. East, Saskatoon, SK.

Alle

akb710@mail.usask.ca

*I also heard the big tree would be cut down =
It seems the only ones benefitting are the people living in those houses, to bring up property value.*

11

To Saskatoon City Council:

As an affected resident of the proposed closure of the 9th street freeway access, I wish my view to be known:

I completely disagree with the proposed closure and respectfully request Council reverse it's decision.

I do not believe sufficient consultation with affected residents was employed and respectfully request Council to review it's decision, negate the previous decision and initiate a new vote, following reasonable neighborhood consultation.

Name: Vince Martin

Address: 714 McPherson Ave

306-717-3945.

Willing to assist in the
effort to keep this access
open.

[Handwritten signature]

To Saskatoon City Council:

As an affected resident of the proposed closure of the 9th street freeway access, I wish my view to be known:

I completely disagree with the proposed closure and respectfully request Council reverse it's decision.

I do not believe sufficient consultation with affected residents was employed and respectfully request Council to review it's decision, negate the previous decision and initiate a new vote, following reasonable neighborhood consultation.

Name: LOUIS GOSSNER

Address: 303-11th ST EAST

ALSO, SIMPLY MOVING TRAFFIC FROM ONE RESIDENTIAL STREET TO ANOTHER IS A WASTE OF TAXPAYER'S \$. THIS IS ONLY MOVING WHAT MAY PERCEIVED AS AN IRRITATION FOR ONE GROUP OF RESIDENTS INTO THE LAPS OF ANOTHER GROUP OF RESIDENTS.

Thank you,


To Saskatoon City Council:

As an affected resident of the proposed closure of the 9th street freeway access, I wish my view to be known:

I completely disagree with the proposed closure and respectfully request Council reverse it's decision.

I do not believe sufficient consultation with affected residents was employed and respectfully request Council to review it's decision, negate the previous decision and initiate a new vote, following reasonable neighborhood consultation.

Name: Iris Haluke

Address: #521 - 212 10th St. E.

Comment:

I would think that city council would have a more important and useful place to spend \$30,000.00.

To Saskatoon City Council:

As an affected resident of the proposed closure of the 9th street freeway access, I wish my view to be known:

I completely disagree with the proposed closure and respectfully request Council reverse it's decision.

I do not believe sufficient consultation with affected residents was employed and respectfully request Council to review it's decision, negate the previous decision and initiate a new vote, following reasonable neighborhood consultation.

Name: ~~Margi Corbett~~ Margi Corbett

Address: 420 10th St East, Saskatoon

Name: ~~Margi Corbett~~ Sheldon Corbett

Address: 420 10th St East, Saskatoon

(As neighbourhood residents, we are very concerned about the process by which this decision was made. Thanks, Laurel!)

To Saskatoon City Council:

As an affected resident of the proposed closure of the 9th street freeway access, I wish my view to be known:

I completely disagree with the proposed closure and respectfully request Council reverse it's decision.

I do not believe sufficient consultation with affected residents was employed and respectfully request Council to review it's decision, negate the previous decision and initiate a new vote, following reasonable neighborhood consultation.

Name: Shirley Duce

Address: 219-10th St E.

Residents who bought houses along 9th St were aware it was a through street to the freeway. So dumb! People other than people in the area I'm sure are not going out of their way to use 9th St access to freeway. My guess is its intermittent local traffic only. If you want to live in a cul-de-sac, then that's where you need to buy your house!

To Saskatoon City Council:

As an affected resident of the proposed closure of the 9th street freeway access, I wish my view to be known:

I completely disagree with the proposed closure and respectfully request Council reverse it's decision.

I do not believe sufficient consultation with affected residents was employed and respectfully request Council to review it's decision, negate the previous decision and initiate a new vote, following reasonable neighborhood consultation.

Name:

KEVIN QUINLAN

Address:

121 MAIN ST.

SASKATOON, S7N0B3

- ENTRANCE AT 9TH ST. GOOD AS IS
GOOD LONG APPROACH LANE PROVIDED FOR MERGING
- CLOSURE WOULD RESULT IN ^{MORE} TRAFFIC
JAMS ON McPHERSON
- 8TH ST. IS REGULAR RADAR TRAP AT McPHERSON
BECAUSE CARS TEND TO SPEED THERE
SAFETY CONCERN INCREASED WITH MORE
CARS TURNING IN FRONT OF SPEEDERS.

To Saskatoon City Council:

As an affected resident of the proposed closure of the 9th street freeway access, I wish my view to be known:

I completely disagree with the proposed closure and respectfully request Council reverse it's decision.

I do not believe sufficient consultation with affected residents was employed and respectfully request Council to review it's decision, negate the previous decision and initiate a new vote, following reasonable neighborhood consultation.

Name: Sandy Martens. Sandy Martens
Address: 121 Main Street

McPherson is not designed as a through street, it's bad enough trying to access 8th Street on a good day.

To Saskatoon City Council:

As an affected resident of the proposed closure of the 9th street freeway access, I wish my view to be known:

I completely disagree with the proposed closure and respectfully request Council reverse it's decision.

I do not believe sufficient consultation with affected residents was employed and respectfully request Council to review it's decision, negate the previous decision and initiate a new vote, following reasonable neighborhood consultation.

Name: Derek Potts & Nora Potts

Address: 218-8th St. East

Dear Council

D Potts

As long time residents of this neighbourhood (30 plus years) we urge city council to revisit this decision. We have witnessed increased traffic on our end of 8th Street over the past few years, making it much more difficult to make a right hand turn from McPherson Avenue onto 8th Street. Having one more access onto the freeway in a neighbourhood with narrow streets is very helpful to traffic flow. Closing ^{an} access that has worked very well for many years without thoroughly consulting with the people most affected is not logical. Thank you for your consideration. We urge you to reconsider this decision. *Derek & Nora Potts*

9th Street at Lorne Avenue Traffic Restriction Update

Recommendation

That the information be received.

Topic and Purpose

The purpose of this report is to provide information on the 9th Street at Lorne Avenue traffic restriction.

Report Highlights

1. Daily traffic data collection was completed and is presented.
2. Weekday peak hour intersection turning movements are presented.

Strategic Goal

Neighbourhood Traffic Reviews support the Strategic Goal of Moving around by providing a plan to guide the implementation of pedestrian infrastructure and intersection improvements.

Background

City Council, at its Regular Public Meeting held on May 25, 2015, resolved:

- “1. That the Neighbourhood Traffic Review for the Nutana neighbourhood be adopted as the framework for future traffic improvements in the area, to be undertaken as funding is made available through the annual budget process;
2. That a directional closure be installed at 9th Street and Lorne Avenue (Idylwyld Drive) to restrict westbound vehicular usage; and
3. That a full closure be installed on 14th Street between Lansdowne Avenue and Temperance Street to restrict all vehicular movements.”

Councillor Olauson, at the Regular Business Meeting of City Council held on September 28, 2015, provided the following Notice of Motion:

“Take notice that at the next Regular Business Meeting of City Council I will move the following motion:

‘That the following May 25, 2015 resolution of City Council be rescinded:

“That a directional closure be installed at 9th Street and Lorne Avenue/Idylwyld Drive to restrict westbound vehicular usage.””

Report

The traffic restriction at 9th Street and Lorne Avenue/Idylwyld Drive was installed in a temporary fashion on Friday, October 2, 2015.

9th Street at Lorne Avenue Traffic Restriction Update

Traffic data including average daily traffic volumes and weekday peak hour turning volumes was collected before and after the installation to quantify the change in traffic volumes.

Daily Traffic Information

The daily traffic information collected, including the date of collection, the daily traffic, and the 85th percentile speed is presented below.

Location	Before			After		
	Date	Daily Traffic	85 th Percentile Speed	Date	Daily Traffic	85 th Percentile Speed
McPherson Ave between 9 th St and Main St	Wed, Sep 30	1,293	40.33	Mon, Oct 5	1,159	39.71
	Thu, Oct 1	1,311	38.27	Tue, Oct 6	1,118	39.36
	Average:	1,302	39.30	Average:	1,138	39.53
McPherson Ave between 9 th St and 8 th St	Fri, Sep 25	1,342	50.18	Fri, Oct 16	1,473	35.64
	Mon, Sep 28	1,205	50.44	Mon, Oct 19	1,398	36.62
	Tue, Sep 29	1,207	53.11	Tue, Oct 20	1,336	37.07
	Average:	1,251	51.24	Average:	1,402	36.44 ¹
9th Street between McPherson Ave and Melrose Ave	Wed, Sep 30	698	47.80	Mon, Oct 5	520	45.79
	Thu, Oct 1	648	46.04	Tue, Oct 6	501	45.59
	Average:	673	46.92	Average:	510	45.69
9th Street between McPherson Ave and Lorne Ave/Idylwyld Dr	Fri, Sep 25	655	44.66	Fri, Oct 16	337	44.79
	Mon, Sep 28	767	44.22	Mon, Oct 19	291	47.14
	Tue, Sep 29	766	46.87	-	-	-
	Average:	729	45.25	Average:	314	45.96

1 – The measurement device was placed too close to 8th Street for this measurement to be accurate.

Preliminary analysis of the traffic data is as follows:

- The daily traffic volumes have dropped on McPherson Avenue between 9th Street and Main Street by 12%; on 9th Street between McPherson Avenue and Melrose Avenue by 24%; and on 9th Street between McPherson Avenue and Lorne Avenue by 57%.
- The daily traffic volumes have increased by 12% on McPherson Avenue between 9th and 8th Streets.
- The 85th percentile speeds appear to have remained consistent.

Peak Hour Traffic Information

The weekday peak hour intersection turning movements are presented in Attachment 1. Prior to the traffic restriction being installed the intersection turning movement counts were collected on September 23 and 24, 2015, and the after counts were collected on October 20, 2015. Preliminary analysis indicates:

- North, east, and west of the 9th Street/McPherson Avenue intersection, the weekday peak hour traffic has dropped.
- On McPherson Avenue between the 9th Street and 8th Street intersections, the weekday AM peak hour traffic has increased from 58 to 100 trips.
- On McPherson Avenue between the 9th Street and 8th Street intersections, the weekday PM peak hour traffic has increased very slightly from 125 to 128 trips.

9th Street at Lorne Avenue Traffic Restriction Update

- Some of the southbound traffic on McPherson Avenue that used to turn right onto 9th Street continued south on McPherson Avenue after October 2, 2015.
- None of the westbound traffic on McPherson Avenue that used to continue west on 9th Street through the McPherson Avenue intersection now turns left southbound onto McPherson Avenue.

The Administration also observed the intersection of 9th Street/McPherson Avenue and McPherson Avenue south to 8th Street on Tuesday, October 6, 2015 and Tuesday, October 13, 2015 from 7:30 AM to 8:30 AM. The following comments are provided:

- On October 6, 2015, four vehicles westbound on 9th Street illegally turned right onto Lorne Avenue/Idylwyld Ramp. On October 13, 2015 two vehicles completed the same illegal movement. Saskatoon Police Service enforcement may be required to enforce the new restriction.
- On October 6, 2015, three vehicles westbound on 9th Street completed legal U-turns at the Lorne Avenue/Idylwyld Ramp. On October 13, 2015, one vehicle completed this U-turn movement.
- The longest queue of southbound traffic on McPherson Avenue at 8th Street was only two vehicles. This queue seldom occurred.

The Administration will continue to monitor the traffic impacts of the restriction and make modifications to the design, as required, prior to permanent construction.

Public and/or Stakeholder Involvement

In October 2013, a public meeting was held to discuss traffic concerns and identify potential solutions. The feedback was used to develop the neighbourhood traffic plan which was presented at a follow up public meeting in September 2014. An additional public meeting was held in January 2015 to address the outstanding issues, including the matter of restricting traffic at 9th Street and Lorne Avenue. Feedback from the community was incorporated into the Neighbourhood Traffic Review.

Financial Implications

The Nutana Neighbourhood Traffic Plan identified an amount of \$30,000 for the installation of a permanent traffic restriction.

Environmental Implications

The overall impact of the recommendations on traffic characteristics including the impacts on greenhouse gas emissions is not known at this time.

Other Considerations/Implications

There are no communications, options, policy, privacy or CPTED considerations or implications.

Due Date for Follow-up and/or Project Completion

A follow-up report or project completion is not required.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Attachment

1. Weekday AM and PM Intersection Turning Volumes

Report Approval

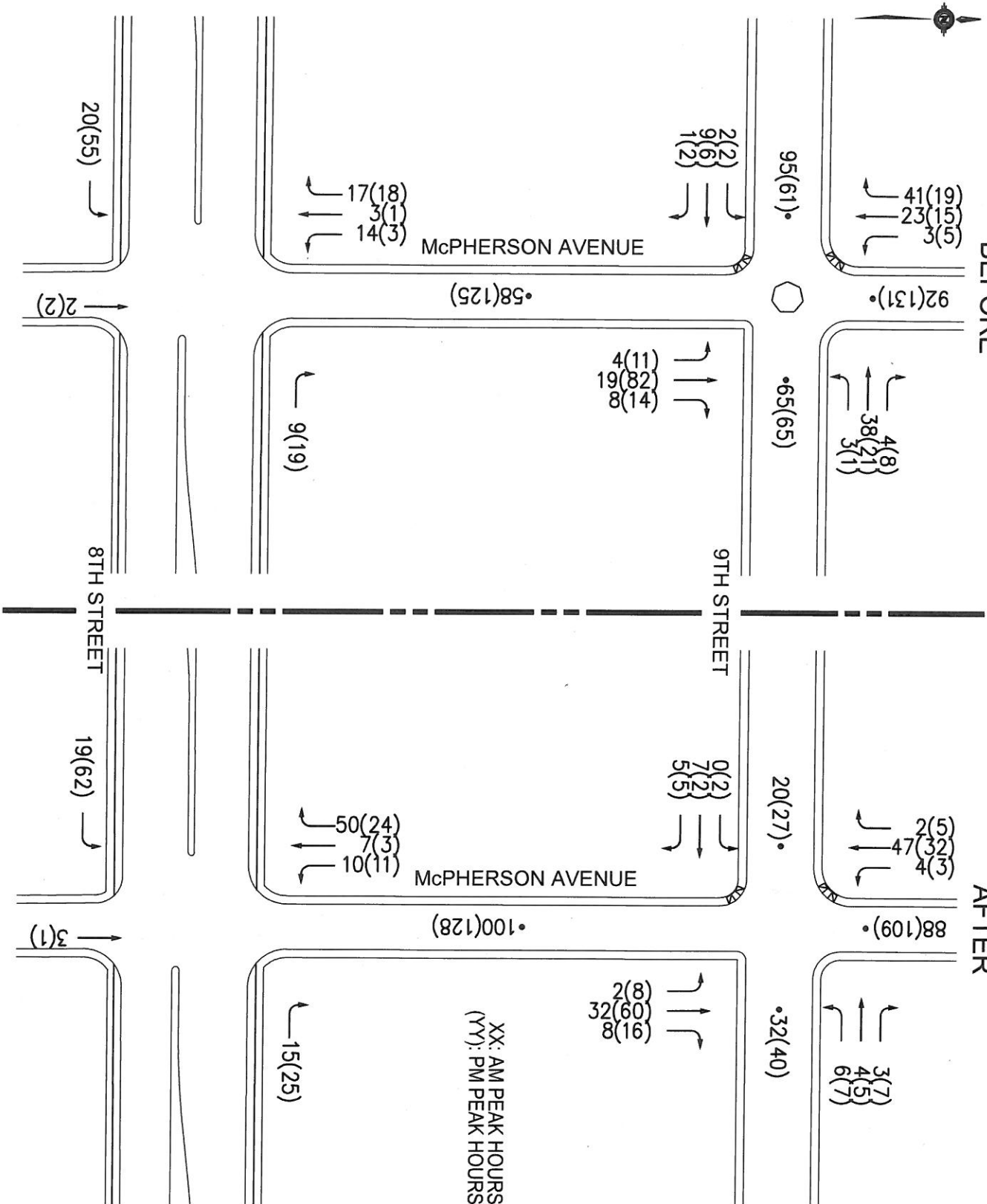
Written by: Jay Magus, Engineering Section Manager, Transportation
Reviewed by: Angela Gardiner, Director, Transportation
Approved by: Jeff Jorgenson, General Manager, Transportation & Utilities
Department

Council JM – 9th Street at Lorne Avenue Traffic Restriction Update

WEEKDAY AM & PM INTERSECTION TURNING VOLUMES

BEFORE

AFTER



8th Street East and Lorne Avenue Sanitary Chamber Installation – Emergency Design-Build

Recommendation

1. That the Administration prepare a contract with Hamm Construction Ltd. for the supply and installation of a sanitary chamber, and upgrade of sanitary and water mains at the intersection of 8th Street East and Lorne Avenue for a total estimated value of \$660,000 (including GST); and
2. That the City Solicitor be requested to prepare the appropriate agreement and that His Worship the Mayor and the City Clerk be authorized to execute the agreement under the Corporate Seal.

Topic and Purpose

The purpose of this report is to obtain approval for an emergency design-build contract for the supply and installation of a new sanitary chamber, and upgrade of adjacent sanitary and water mains at the intersection of 8th Street East and Lorne Avenue to Hamm Construction Ltd. (Hamm Construction).

Report Highlights

1. A contract to install the sanitary chamber and adjacent sanitary and water main upgrades was tendered twice and received no bids. The work is considered urgent.
2. The Administration approached Hamm Construction, who have agreed to perform the work and complete the project before the end of 2015 on a time-and-expense basis.
3. Administration and Hamm Construction have been working together on a modified design and construction approach given the complicated nature of the project.
3. The work is expected to significantly lower sewer gas emissions in the area; reduce infrastructure damage due to sewer gas; improve hydraulics of the sanitary main; and replace aging infrastructure in a coordinated approach.

Strategic Goals

The recommendations support the Strategic Goals of Sustainable Growth and Asset and Financial Sustainability as the work done under this contract will improve the sanitary and water main infrastructure and resolve existing problematic conditions.

The recommendations also support the Strategic Goal of Quality of Life as the installation of a new sanitary chamber and improvements to the adjacent infrastructure will help alleviate odour issues that area residents have been experiencing.

Background

The Construction and Design division prepared a tender for the supply and installation of a sanitary chamber, and upgrade adjacent sanitary and water mains at the intersection of 8th Street East and Lorne Avenue. The tender was issued upon completion of the University Bridge rehabilitation work. No bids were received for either tender call.

Report

Transportation & Utilities enacted a multi-divisional task force to coordinate a solution at this location using information gathered from operational, engineering and design, wastewater treatment, customer advocacy and communications personnel. Advice was also sought from the contracting and consulting communities. This work led to the design ultimately tendered.

Scope of Work

The tender includes replacing a problematic manhole that is hydraulically inefficient, prone to surcharging, and results in discharge of sewer gas. Sewer gas is odourous and corrosive to infrastructure. The manhole will be replaced with a new engineered sanitary chamber as well as extensive replacement of adjacent piping infrastructure. This solution will improve the hydraulic conditions and significantly reduce the levels of corrosive sewer gas at this location. The project involves extensive bypassing and re-routing of sewage during construction.

The primary and local water mains that run through the intersection will be replaced at the same time. This is an opportune time to replace adjacent aging infrastructure and minimize future costs and traffic disruption at this intersection.

The proposed upgrades at 8th Street East and Lorne Avenue are considered urgent due to a number of reasons. Current temporary bypassing of upstream sewage is not a long-term solution. Further, construction work is planned in the surrounding area, including on Broadway Avenue, in summer 2016. The coinciding traffic changes and restrictions will result in increased traffic at the 8th and Lorne intersection.

Design and Construction Approach

Due to the urgent nature of this rehabilitation work, Hamm Construction was approached in order to explore the possibility of entering into a design-build contract to complete the work. Hamm Construction have indicated that they would be able to offer the services of their most experienced personnel in order to complete the proposed manhole and piping upgrades in 2015.

Because of the lead time needed to construct the new chamber, in order to complete the project in 2015, the Administration has authorized Hamm Construction to have the chamber itself constructed and is therefore committed to that cost. The construction contract for on-site work will be on a time-and-expenses basis. The Administration believes that this design-build approach, with payment on a time-and-expense basis,

will result in a lower cost for the City than if the City transferred all risks to the contractor. This is considered an emergency sole-source contract.

Options to the Recommendation

There is the option of not approving the proposed construction portion of the contract with Hamm Construction. This would prevent the necessary work from being completed in 2015. Administration would need to re-tender the work in 2016, however, not until approximately September when the Broadway Avenue project is expected to be completed.

Public and/or Stakeholder Involvement

Residents located near the intersection of 8th Street East and Lorne Avenue have brought odour complaints to the attention of the Administration. Administration has consulted with the affected residents and conducted various tests to determine the root cause of the odour issues.

Communication Plan

Project details and updates will be shared with affected residents via letter, the City's website and Community Association communication forums (i.e. website, newsletter, and social media). Appropriate communication materials will be prepared to give advanced warning of road restrictions. These may include advanced road signage, Public Service Announcements, Daily Road Report, Traffic Detour Service Alerts, the Road Restrictions and Construction Projects Interactive Map and social media.

Financial Implications

Details of the estimated project cost are as follows:

Sanitary Chamber Installation:

Base Estimate for Chamber	\$336,469.12
GST	<u>16,823.46</u>
Total Estimated Cost	\$353,292.58
Less GST Rebate	<u>(16,823.46)</u>
Subtotal for Chamber	<u>\$336,469.12</u>

Adjacent Infrastructure Upgrades:

Base Estimate for Upgrades	\$291,554.33
GST	<u>14,577.72</u>
Total Estimated Cost	\$306,132.05
Less GST Rebate	<u>(14,577.72)</u>
Subtotal for Upgrades	<u>\$291,554.33</u>

Estimated Project Total:

Subtotal for Chamber	\$336,469.12
Subtotal for Upgrades	291,554.33
GST	<u>31,401.17</u>
Total Estimated Cost	\$659,424.62
Less GST Rebate	<u>(31,401.17)</u>
Net Estimated Cost to the City	<u>\$628,023.45</u>

There is sufficient funding allocated for this program within approved Capital Project #1618 – Wastewater Trunks Preservation and Capacity Programs, and Capital Project #0778 – Stonebridge Trunk Sewer.

Environmental Implications

The recommendation will result in the consumption of natural resources and the generation of greenhouse gas emissions once construction proceeds. Construction activities will require an estimated 19,200 L of diesel fuel, contributing to estimated greenhouse gas emissions of 58 tonnes CO₂e. The work will reduce odour emissions.

Other Considerations/Implications

There are no policy, privacy, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

Administration will submit a follow-up report to City Council including the final cost of the project once the work has been completed.

Public Notice

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

Report Approval

Written by: Caleb Ripley, Engineer-in-Training, Project Engineer
Reviewed by: Sohrab Khan, Senior Project Management Engineer
Reviewed by: Celene Anger, Director of Construction and Design
Reviewed by: Reid Corbett, Director of Saskatoon Water
Approved by: Jeff Jorgenson, General Manager, Transportation & Utilities Department



REVISED AGENDA PUBLIC HEARING MEETING OF CITY COUNCIL

Monday, October 26, 2015, 6:00 p.m.
Council Chamber, City Hall

Pages

1. **CALL TO ORDER**

2. **CONFIRMATION OF AGENDA**

8 - 11

Recommendation

1. That the attached letter from Jonathan Naylor submitting comments, dated October 25, 2015 be considered during item 5.1.3;
2. That the attached letter from Len Achs, Mid-West Group of Companies dated October 22, 2015, requesting to speak be received and considered during item 5.2.1;
3. That the attached letter from Rich Gabruch submitting comments, dated October 26, 2015, be considered during Item 5.2.1. and
4. That the agenda be confirmed as amended.

3. **DECLARATION OF PECUNIARY INTEREST**

4. **ADOPTION OF MINUTES**

Recommendation

That the minutes of the Public Hearing Meeting of City Council held on Monday, September 28, 2015 be approved.

5. **PUBLIC HEARINGS**

5.1 **Land Use, etc.**

5.1.1 Discretionary Use Application – Residential Care Home (Maximum Ten Residents) – 134 Avenue O South [File No. CK 4355-015-002]

12 - 20

Copies of the following are provided:

- Report of the General Manager, Community Services Department dated September 29, 2015; and
- Letter from the Committee Assistant, Municipal Planning Commission dated October 14, 2015.

The City Planner has advised that the notification has been placed on site and letters have been sent to all adjacent landowners within 75 metres of the site.

Recommendation

That the application submitted by Sanctum Care Group requesting permission to operate a Residential Care Home – Type II, with a maximum of ten residents, at 134 Avenue O South, be approved, subject to the following conditions:

1. That a building permit be obtained to convert the existing one-unit dwelling to a Residential Care Home - Type II;
2. That the applicant obtain a Development Permit and all other relevant permits and licenses; and
3. That the final plans submitted be substantially in accordance with the plans submitted in support of this Discretionary Use Application.

5.1.2 Proposed Rezoning by Agreement – From R1A to B1B by Agreement and M2 by Agreement – 3200 Preston Avenue South - Proposed Bylaw No. 9315 [File No. CK 4351-015-016 and PL 4350-Z18/15]

21 - 59

Copies of the following are provided:

- Proposed Bylaw No. 9315;
- Report of the General Manager, Community Services Department dated October 9, 2015;
- Letter from the Committee Assistant, Municipal Planning Commission dated October 14, 2015; and
- Notice that appeared in the local press on October 10 and 11, 2015

Recommendation

That permission be granted to introduce Bylaw No. 9315, and give same its FIRST reading.

Recommendation

That the submitted report and correspondence be received.

Recommendation

That the hearing be closed.

Recommendation

That Bylaw No. 9315 now be read a SECOND time.

Recommendation

That permission be granted to have Bylaw No. 9315 read a third time at this meeting.

Recommendation

That Bylaw No. 9315 now be read a THIRD time, that the bylaw be passed and the Mayor and the City Clerk be authorized to sign same and attach the corporate seal thereto.

5.1.3 Proposed Rezoning by Agreement – From B2 to B1B by Agreement – 418 Cumberland Avenue North – Varsity View - Proposed Bylaw No. 9316 [File No. CK 4351-015-017]

60 - 88

Copies of the following are provided:

- Proposed Bylaw No. 9316;
- Report of the General Manager, Community Services Department dated September 29, 2015;
- Letter from the Committee Assistant, Municipal Planning Commission dated October 14, 2015; and
- Notice that appeared in the local press on October 10 and 11, 2015

Recommendation

That permission be granted to introduce Bylaw No. 9316 and give same its FIRST reading.

Recommendation

That the submitted report and correspondence be received.

Recommendation

That the hearing be closed.

Recommendation

That Bylaw No. 9316 now be read a SECOND time.

Recommendation

That permission be granted to have Bylaw No. 9316 read a third time at this meeting.

Recommendation

That Bylaw No. 9316 now be read a THIRD time, that the bylaw be passed and the Mayor and the City Clerk be authorized to sign same and attach the corporate seal thereto.

5.1.4 Proposed Rezoning from R1A to R1B and RM3 – Lewin Crescent and Stonebridge Common – Stonebridge - Proposed Bylaw No. 9317 [File No. CK 4351-015-015]

89 - 97

Copies of the following are provided:

- Proposed Bylaw No. 9317;
- Report of the General Manager, Community Services Department dated September 29, 2015;
- Letter from the Committee Assistant, Municipal Planning Commission dated October 14, 2015; and
- Notice that appeared in the local press on October 10 and 11, 2015

Recommendation

That permission be granted to introduce Bylaw No. 9317, and give same its FIRST reading.

Recommendation

That the submitted report and correspondence be received.

Recommendation

That the hearing be closed.

Recommendation

That Bylaw No. 9317 now be read a SECOND time.

Recommendation

That permission be granted to have Bylaw No. 9317 read a third time at this meeting.

Recommendation

That Bylaw No. 9317 now be read a THIRD time, that the bylaw be passed and the Mayor and the City Clerk be authorized to sign same and attach the corporate seal thereto.

5.2 Public Notice Matters

5.2.1 Proposed Closure of Right-of-Way – 22nd Street and 2nd

98 - 108

Avenue North – Central Business District [File No. CK 6295-015-007 and IS 6295-1]

Recommendation

1. That a portion of the north-south lane adjacent to the 200 block of 22nd Street East be closed;
2. That portions of the proposed closure be sold to Nexus Holdings Inc. for \$35,005.50 plus GST and to Derek Morris for \$35,005.50 plus GST;
3. That all costs associated with the closure be paid for by the applicants, including Solicitor's fees and disbursements; and
4. That permission be granted to introduce Bylaw No. 9314, The Street Closing Bylaw, 2015 (No. 6), and give same its FIRST reading.

Recommendation

That the submitted report and correspondence be received.

Recommendation

That the hearing be closed.

Recommendation

That Bylaw No. 9314 now be read a SECOND time.

Recommendation

That permission be granted to have Bylaw No. 9314 read a third time at this meeting.

Recommendation

That Bylaw No. 9314 now be read a THIRD time, that the bylaw be passed and the Mayor and the City Clerk be authorized to sign same and attach the corporate seal thereto.

5.2.2 Proposed Closure of Right-of-Way – Portion Road Allowance South of Chappell Drive – Montgomery Place [File No. CK 6295-015-006 and TS 6295-1]

109 - 114

Recommendation

1. That portion of road allowance south of Chappell Drive be closed;
2. That after closure, this land be transferred to Canadian National Railway Company and consolidated with adjacent property;
3. That all costs associated with the closure be paid for by the applicants, including Solicitor's fees and disbursements;

- and
4. That permission be granted to introduce Bylaw No. 9319, The Street Closing Bylaw, 2015 (No.8), and give same its FIRST reading.

Recommendation

That the submitted report and correspondence be received.

Recommendation

That the hearing be closed.

Recommendation

That Bylaw No. 9319 now be read a SECOND time.

Recommendation

That permission be granted to have Bylaw No. 9319 read a third time at this meeting.

Recommendation

That Bylaw No. 9319 now be read a THIRD time, that the bylaw be passed and the Mayor and the City Clerk be authorized to sign same and attach the corporate seal thereto.

5.2.3 Proposed Closure of Right-of-Way, North-South Lane North of Packham Place – Sutherland Neighbourhood [File No. CK 6295-015-008 and TS 6295-1]

115 - 120

Recommendation

- That the north-south lane north of Packham Place be closed and sold to 101022891 Saskatchewan Ltd. (Acadia Construction) for \$10,242.78 plus GST;
- That all costs associated with the closure be paid for by the applicant, including Solicitor's fees and disbursements; and
- That permission be granted to introduce Bylaw No. 9318, The Street Closing Bylaw, 2015 (No.7), and give same its FIRST reading.

Recommendation

That the submitted report and correspondence be received.

Recommendation

That the hearing be closed.

Recommendation

That Bylaw No. 9318 now be read a SECOND time.

Recommendation

That permission be granted to have Bylaw No. 9318 read a third time at this meeting.

Recommendation

That Bylaw No. 9318 now be read a THIRD time, that the bylaw be passed and the Mayor and the City Clerk be authorized to sign same and attach the corporate seal thereto.

6. PROCLAMATIONS AND FLAG RAISINGS

Recommendation

1. That City Council approve all proclamations and flag raising requests as set out in section 6; and
2. That the City Clerk be authorized to sign proclamations, in the standard form, on behalf of City Council.

- 6.1 Myra Potter - United Way of Saskatoon and Area - November 2015 - 'United Way Month' - Flag Raising - November 5, 2015 [File No. CK 205-5, CK 205-1] 121 - 122**

Proclamation and Flag Raising Request

- 6.2 Joelle Foster - Futurpreneur Canada - November 16-22, 2015 - 'Global Entrepreneurship Week' - Flag Raising - November 17, 2015 [File No. CK 205-1] 123**

Flag Raising Request

- 6.3 Wilf Keller - Ag-West Bio - October 30 to November 6, 2015 - 'Biotech Week' [File No. CK 205-5] 124 - 125**

Proclamation Request

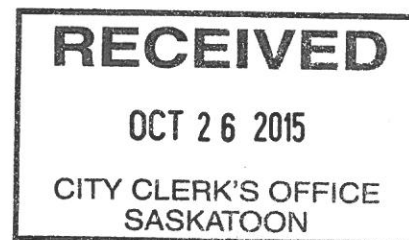
- 6.4 Omer Al-Katib - Honorary Consulate of Turkey in Saskatchewan - October 29, 2015 - 'Republic Day' - Flag Raising - October 29, 2015 [File No. CK 205-5, CK 205-1] 126 - 127**

Proclamation and Flag Raising Request

7. URGENT BUSINESS

8. ADJOURNMENT

From: Jonathan Naylor <jon.naylor@usask.ca>
Sent: Sunday, October 25, 2015 9:06 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Sunday, October 25, 2015 - 21:05
Submitted by anonymous user: 108.60.168.207
Submitted values are:

Date: Sunday, October 25, 2015
To: His Worship the Mayor and Members of City Council
First Name: Jonathan
Last Name: Naylor
Address: 1414 14th St E
City: Saskatoon
Province: Saskatchewan
Postal Code: S7H0A8
Email: jon.naylor@usask.ca
Comments:

I write this letter in the interest of clarity.

The communities concerns about the rezoning of 418 Cumberland Avenue North Varsity View (File No. CK 4351-015-017) were discussed at the public information meeting on June 18. The Varsity View Community Association also sent a letter outlining its concern regarding parking and safety to the Municipal Planning Commission.

The proposed rezoning of 418 Cumberland Ave is in direct conflict with the first goal of the Varsity View Local Area Plan: Addressing Neighborhood Parking Concerns (Varsity View Local Area Plan: Executive Summary, page iii). Normally, a development with a restaurant and 6 residential units is required to have 11 parking places for the use of tenants, their guests, employees, and clients. The proposed rezoning has only 2 parking spaces with a potential limited term commitment to 2 additional spaces in the University Parking lot (these are likely to become unavailable as College Quarter builds out). If a reasonable person concludes that the inadequacies of this arrangement are likely to exacerbate parking problems in Varsity View it is open to legal challenge on two grounds:

- It is not consistent with the Official Community Plan and the Local Area Plan (Bylaw No. 8769, Sections 5.2.2, 5.5, 18.1 and 18.2)
- It will injuriously affect neighboring properties (Planning and Development Act, 2007, sections 221, 226)

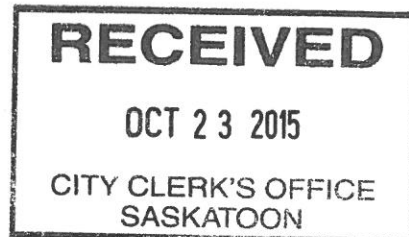
I look forwards to hearing Council's decision.

Sincerely,
Jonathan Naylor
1414 14th St E, Saskatoon, S7H 0A8

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47150>



October 22, 2015



His Worship, Mayor Don Atchison
and members of City Council
City Hall
222 3rd Avenue North
Saskatoon SK S7K 0J5

Via email city.clerks@saskatoon.ca

Dear Mayor Atchison and Council members:

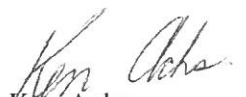
Re: Proposed Closure of Right-of-Way – 22nd Street and 2nd Avenue North – Central Business District [File No. CK 6295-015- 007 and IS 6295-1]

This letter is my request to speak at City Council on Monday October 26 regarding the above-referenced agenda item.

As most of you must know, we have been trying to complete the purchase of the lane at 22nd Street and 2nd Avenue for well over two years. This process is holding up a major development in the downtown core

I hope the City will support growth in our downtown core and expedite this process and allow the purchase of the lane to go forward.

Yours truly,


Ken Achs
President and CEO
midwest@mwdc.ca

KA/lp

cc Joan Thomas, office of Derek Morris via email joan@tcwest.ca



RECEIVED

OCT 23 2015

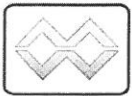
**CITY CLERK'S OFFICE
SASKATOON**

From: Lise Paquette <lise.paquette@mwdc.ca>
Sent: Friday, October 23, 2015 8:20 AM
To: Web E-mail - City Clerks
Cc: 'Joan Thomas'; Trevor Jacek
Subject: Proposed Closure of Right-of-Way – 22nd Street and 2nd Avenue North – Central Business District File CK 6295-015- 007
Attachments: CoS-let3(Mayor Atchison and City Council) re purchase of lane.pdf

Good morning,

Please find herewith, a letter from Ken Achs requesting to speak at City Council Monday October 26 regarding the above-referenced matter.

Regards,



Lise Paquette

Executive Assistant to Ken Achs

Mid-West Group of Companies

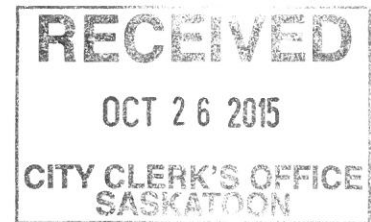
T 306.933.4838 | F 306.933.4121 | lise.paquette@mwdc.ca

100-450 2nd Avenue North, Saskatoon SK S7K 2C3 | www.midwestgroup.ca

Saskatoon's best commercial landlord—we always do our best in keeping our tenants happy, comfortable and safe!

PRIVILEGED AND CONFIDENTIAL: This communication, including attachments, is for the exclusive use of addressee and may contain proprietary, confidential and/or privileged information. If you are not the intended recipient, any use, copying, disclosure, dissemination or distribution is strictly prohibited. If you are not the intended recipient, please notify the sender immediately by return e-mail, delete this communication and destroy all copies.

From: Rich Gabruch <r.gabruch@gabruchlegal.ca>
Sent: Monday, October 26, 2015 9:59 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Monday, October 26, 2015 - 09:58
Submitted by anonymous user: 142.165.205.63
Submitted values are:

Date: Monday, October 26, 2015
To: His Worship the Mayor and Members of City Council
First Name: Rich
Last Name: Gabruch
Address: 154 Skeena Crescent
City: Saskatoon
Province: Saskatchewan
Postal Code: A7K4G7
Email: r.gabruch@gabruchlegal.ca

Comments: Regarding proposed lane closure between 2nd and 3rd Avenue abutting 22nd street. We operate our office and the lanes allow us access to our underground parking. Daily there are parking abuses in the laneways and we need alternate access points such as the proposed lane to access our parking garage. We therefore oppose its closure.... unless you steppe dup enforcement of illegal laneway parking to hourly patrols.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47214>

Discretionary Use Application – Residential Care Home (Maximum Ten Residents) – 134 Avenue O South

Recommendation

That a report be forwarded to City Council, at the time of the public hearing, recommending that the application submitted by Sanctum Care Group requesting permission to operate a Residential Care Home – Type II, with a maximum of ten residents, at 134 Avenue O South, be approved, subject to the following conditions:

1. That a building permit be obtained to convert the existing one-unit dwelling to a Residential Care Home - Type II;
2. That the applicant obtain a Development Permit and all other relevant permits and licenses; and
3. That the final plans submitted be substantially in accordance with the plans submitted in support of this Discretionary Use Application.

Topic and Purpose

The purpose of this report is to consider a Discretionary Use Application from Sanctum Care Group to operate a Residential Care Home - Type II, with a maximum of ten residents, at 134 Avenue O South.

Report Highlights

1. A Residential Care Home is proposed at 134 Avenue O South to accommodate up to ten residents.
2. This proposal meets all relevant Zoning Bylaw No. 8770 (Zoning Bylaw) requirements.
3. The proposal is not anticipated to significantly impact the surrounding land uses.

Strategic Goal

This application supports the City of Saskatoon's (City) Strategic Goal of Quality of Life as the proposal continues to promote and facilitate the development of supportive housing forms in all areas of the city.

Background

The property at 134 Avenue O South is a one-unit dwelling located in the Pleasant Hill neighbourhood and is zoned R2 District under the Zoning Bylaw (see Attachment 1). A Residential Care Home - Type II is considered a discretionary use in the R2 District. Sanctum Care Group has submitted an application requesting City Council's approval to convert the existing one-unit dwelling to a Residential Care Home - Type II, for those requiring transitional care from hospital to community, with a maximum of ten residents.

Discretionary Use Application – Residential Care Home (Maximum Ten Residents) – 134 Avenue O South

Report

Zoning Bylaw Requirements

A “Residential Care Home” means a licensed or approved group care home governed by provincial regulations that provides, in a residential setting, 24 hour care of persons in need of personal services, supervision, or assistance essential for sustaining the activities of daily living or for the protection of the individual.

A “Residential Care Home – Type II” means a residential care home in which the number of residents, excluding staff, is more than 5 and not more than 15.

The Residential Care Home will be served by four full-time staff during the day and two overnight. The parking requirement for this Residential Care Home is five spaces. Plans submitted by the applicant indicate the required parking will be provided in the attached garage and in the rear yard. The required parking in the rear yard will be hard-surfaced (see Attachment 2).

No exterior alterations will be undertaken that would be inconsistent with the residential character of the existing properties. This proposal meets all relevant Zoning Bylaw requirements.

Comments from Other Divisions

No concerns were noted by other divisions with respect to this proposal. Refer to Attachment 3 for the full remarks.

Conclusion

The proposed Residential Care Home – Type II at 134 Avenue O South, accommodating ten residents, meets all relevant Zoning Bylaw provisions and is not anticipated to have any significant impact on surrounding land uses.

Options to the Recommendation

City Council could deny this Discretionary Use Application. This option is not recommended as the proposal complies with all relevant Zoning Bylaw requirements and has been evaluated as a discretionary use, subject to the provisions of Section 4.7 of the bylaw.

Public and/or Stakeholder Involvement

Notices to property owners within a 75 metre radius of the site were mailed out in June 2015 to solicit feedback on the proposal. The Pleasant Hill Community Association was also advised, and indicated support for the proposal.

A Public Information Meeting was held on August 26, 2015, and was attended by the proponents, individuals associated with the project, and the Ward Councillor. The proponents and the Ward Councillor spoke briefly about the proposal (see Attachment 4).

Discretionary Use Application – Residential Care Home (Maximum Ten Residents) – 134 Avenue O South

Communication Plan

No further communication is planned beyond the stakeholder involvement noted above and the required notice for the public hearing.

Other Considerations/Implications

There are no policy, financial, environmental, privacy, or CPTED implications or considerations.

Due Date for Follow-up and/or Project Completion

No follow-up is required.

Public Notice

Public notice is required for consideration of this matter, pursuant to Section 11 (b) of Public Notice Policy No. C01-021.

Once this application has been considered by the Municipal Planning Commission, a date for a public hearing will be set. The Community Services Department will give notice by mail to assessed property owners within 75 metres of the subject site and to the Pleasant Hill Community Association. Notification posters will also be placed on the subject site.

Attachments

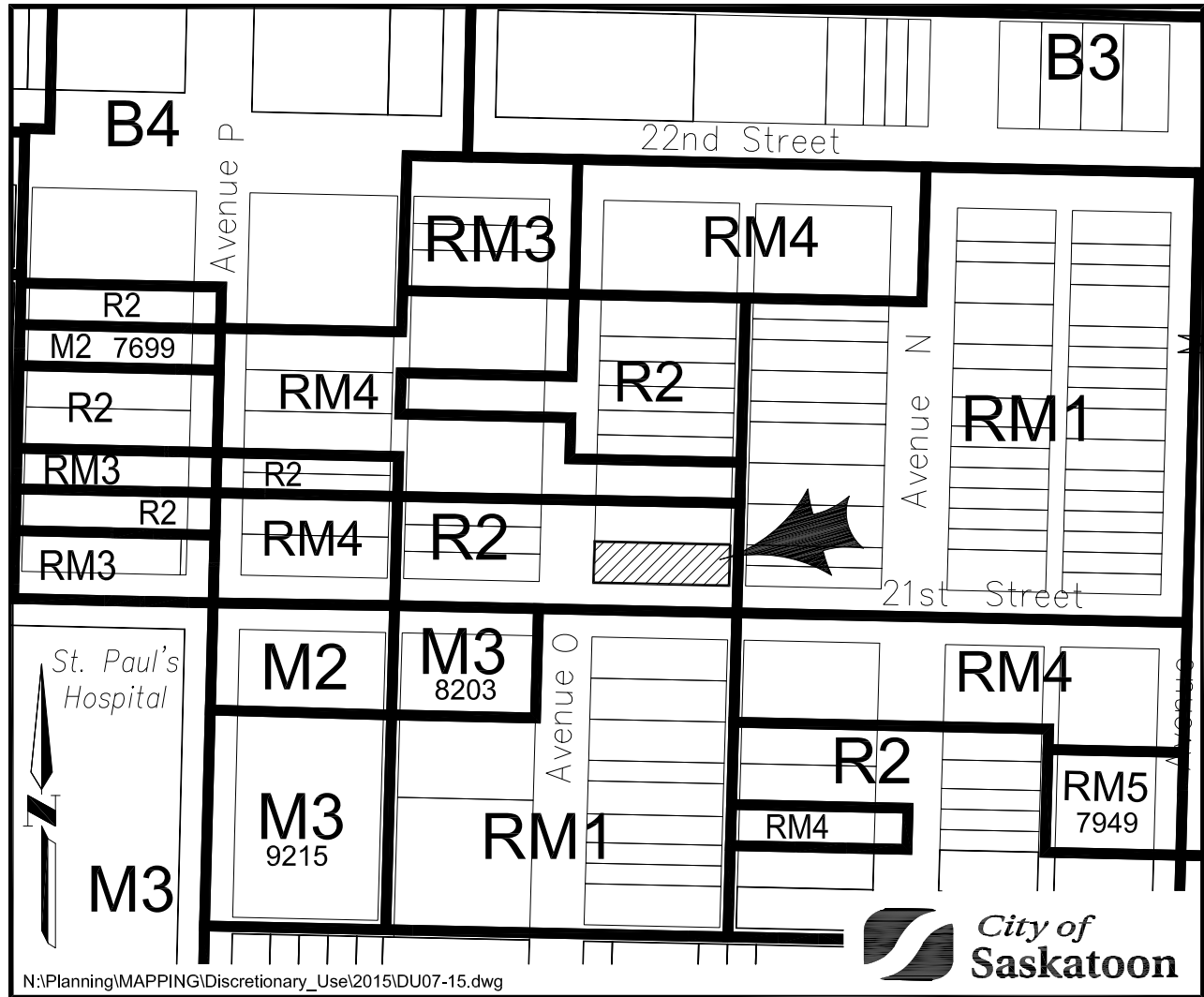
1. Location Plan – 134 Avenue O South
2. Site Plan – 134 Avenue O South
3. Comments From Other Divisions
4. Community Engagement Summary

Report Approval

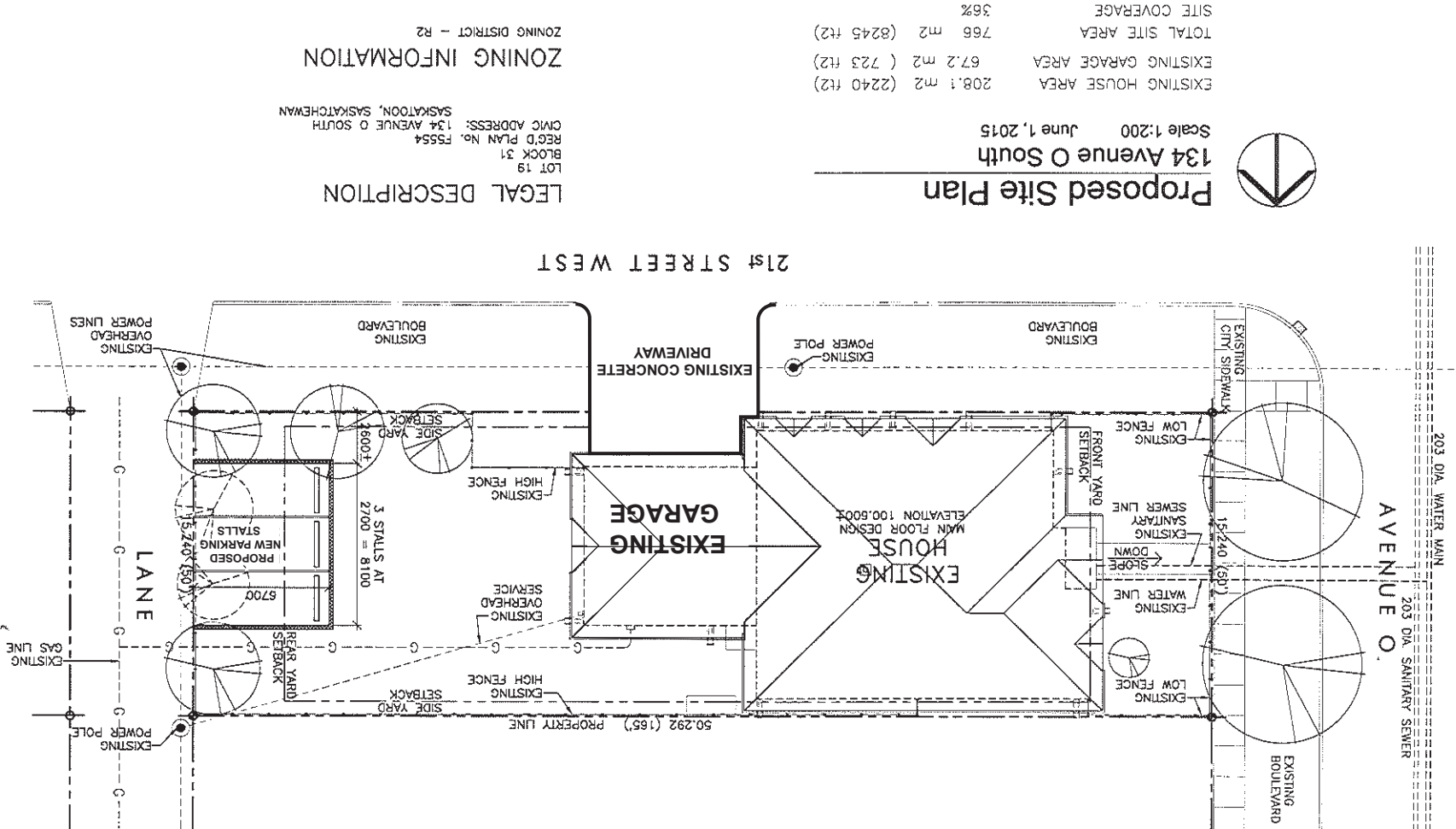
Written by: Daniel McLaren, Planner, Planning and Development
Reviewed by: Alan Wallace, Director of Planning and Development
Approved by: Randy Grauer, General Manager, Community Services Department

S/Reports/DS/2015/MPC – Discretionary Use Application – Residential Care Home (Maximum Ten Residents) – 134 Avenue O South/lc

Location Plan - 134 Avenue O South



Site Plan - 134 Avenue O South



EXISTING HOUSE AREA	208.1 m ² (2240 ft ²)
EXISTING GARAGE AREA	67.2 m ² (723 ft ²)
TOTAL SITE AREA	766 m ² (8245 ft ²)
SITE COVERAGE	36%

Proposed Site Plan
 134 Avenue O South
 Scale 1:200
 June 1, 2015

LEGAL DESCRIPTION
 LOT 19
 BLOCK 31
 REG'D PLAN No. F5554
 CMC ADDRESS: 134 AVENUE O SOUTH
 SASKATOON, SASKATCHEWAN

ZONING INFORMATION
 ZONING DISTRICT - R2

MARCH SCHAFTEL ARCHITECTS LTD. 202 avenue B north saskatoon saskatchewan S7L 1E2 306.931.3033 fax: 306.374.1661	Drawn By: Sack: Date: JUNE 1, 2015 File No: 20-333	BVR 1-200 134 Avenue O South Saskatoon, SK	PROPOSED SITE PLAN	P1
	203 DIA WATER MAIN 203 DIA SANITARY SEWER AVENUE O			

Comments From Other Divisions

1. Transportation and Utilities Department, Comments
The proposed Discretionary Use Application has been received and reviewed by the Transportation and Utilities Department. Following are comments from this department:
 - a) Requirements prior to Approval of the Discretionary Use:
 - None.
 - b) Requirements after Discretionary Use Approval and Prior to Building Permit Approval:
 - There is no capacity available for additional storm water from this site. This site is designed for 25 % imperviousness; if there is any proposed increase to the current site imperviousness, on-site storage will be required in order to match the original site storm water flows for a two-year storm event.
 - Planning and Development Division to determine if sprinklers are required.
 - c) Additional Comments:
 - Water and sanitary services are available.

2. Neighbourhood Planning Section, Community Services Department, Comments
The Neighbourhood Planning Section has no concerns with the Discretionary Use Application requesting approval for a proposed Residential Care Home for as many as ten residents at 134 Avenue O South.

3. Saskatoon Transit, Transportation and Utilities Department, Comments
Saskatoon Transit has no comments regarding the referenced property.

4. Building Standards Division, Community Services Department, Comments
The Building Standards Division of the Community Services Department has no objection to the proposed Discretionary Use Application provided that:
 - a) Separate building permits are obtained for the Residential Care Home Type II;
 - b) A Residential Care Home is permitted to be classified as a residential occupancy provided the home does not provide sleeping accommodations for more than ten persons, including caregivers; and
 - c) The building shall be protected by an automatic fire suppression system if any occupant is not capable of self-preservation.

Note: The applicant has been informed of, and agrees to, the above requirements.



Shaping Saskatoon

Bridging to Tomorrow... for a 21st Century City



Community Engagement Summary Public Information Meeting for Proposed Discretionary Use 134 Avenue O South to be Used for a Residential Care Home- Type II with a Maximum of Ten Residents (Sanctum Care Group)

Project Description

A public information meeting was held regarding a proposed Residential Care Home – Type II, located at 134 Avenue O South. The meeting provided residents of Pleasant Hill, specifically those within 75 metres of the subject site, the opportunity to learn more about the proposed development and the discretionary use process, and to have the opportunity to comment on the proposal and ask any questions that they may have.

The meeting was held at St. George's Seniors Hall on August 26, 2015, at 7 p.m.

Community Engagement Strategy

Notice to residents within a 75 metre radius of the subject site were sent out on July 30, 2015. Letters, along with the public meeting notice, were also sent to the Pleasant Hill Community Association, Ward Councillor, and Community Consultant.

The purpose of the meeting was to inform, and consult with, the nearby residents. Interested or concerned individuals were provided with an opportunity to learn more about the proposal and to provide perspective and comments for consideration.

The public information meeting provided an opportunity to listen to a presentation by the applicant and create a dialogue between the applicant and nearby residents. City of Saskatoon (City) staff were also available to answer questions regarding the discretionary use process and general zoning regulations.

Summary of Community Engagement Feedback

The proponents, individuals associated with the project, and the Ward Councillor attended the public meeting. No members of the general public attended the meeting. The Pleasant Hill Community Association President was made available by teleconference, and expressed support for the proposal. The proponents and the Ward Councillor spoke briefly regarding the proposal.

Next Steps

Feedback from the meeting will be summarized and presented as part of the report to the Municipal Planning Commission and City Council.



Once this application has been considered by the Municipal Planning Commission, a date for a public hearing will be set, and notices will be sent, by mail, to property owners within 75 metres of the subject site and to the Pleasant Hill Community Association. Notification posters will also be placed on the subject site. No other public engagement is planned.

ACTION	ANTICIPATED TIMING
Planning and Development Division prepares and presents to Municipal Planning Commission (MPC). MPC reviews proposal and recommends approval or denial to City Council.	September 29, 2015
Public Notice - Community Consultant, Ward Councillor, and all participants that attended the Public Information Meeting, will be provided with direct notice of the Public Hearing, as well as all residents who were notified previously. A notification poster sign will be placed on site.	October 6 to 26, 2015
Public Hearing – Public Hearing conducted by City Council, with an opportunity provided to interested persons or groups to present. Proposal considered together with the reports of the Planning and Development Division, Municipal Planning Commission, and any written or verbal submissions received by City Council.	October 26, 2015
Council Decision - may approve or deny proposal.	October 26, 2015

Prepared by:
Daniel McLaren, Planner
Planning and Development
September 1, 2015

October 14, 2015

City Clerk

Dear City Clerk:

**Re: Municipal Planning Commission Report for Public Hearing
Discretionary Use Application - Residential Care Home
(Maximum Ten Residents) - 134 Avenue O South
[File No. CK 4355-015-002]**

The Municipal Planning Commission, at its meeting held on October 9, 2015 considered a report of the General Manager, Community Services Department, dated September 29, 2015, on the above application and supports the following recommendation of the Community Services Department:

That a report be forwarded to City Council, at the time of the public hearing, recommending that the application submitted by Sanctum Care Group requesting permission to operate a Residential Care Home – Type II, with a maximum of ten residents, at 134 Avenue O South, be approved, subject to the following conditions:

1. That a building permit be obtained to convert the existing one-unit dwelling to a Residential Care Home - Type II;
2. That the applicant obtain a Development Permit and all other relevant permits and licenses; and
3. That the final plans submitted be substantially in accordance with the plans submitted in support of this Discretionary Use Application.

The Commission respectfully requests that the above report be considered by City Council at the time of the public hearing with respect to the above proposed application.

Yours truly,



Holly Thompson, Committee Assistant
Municipal Planning Commission

HT:sj

BYLAW NO. 9315

The Zoning Amendment Bylaw, 2015 (No. 27)

The Council of The City of Saskatoon enacts:

Short Title

1. This Bylaw may be cited as *The Zoning Amendment Bylaw, 2015 (No. 27)*.


Purpose

2. The purpose of this Bylaw is to authorize the rezoning agreements which are annexed hereto as Appendix "B" and "C".

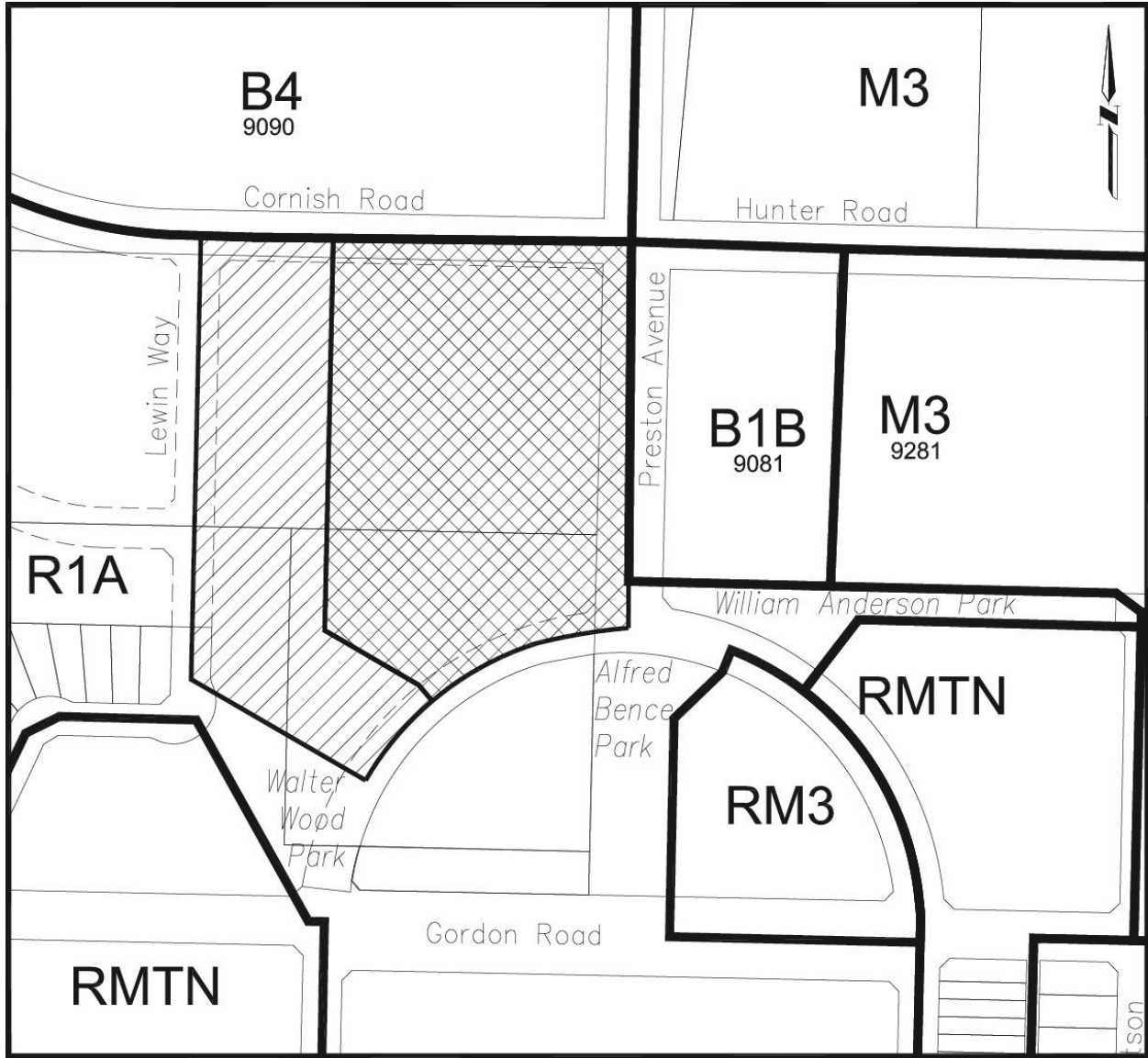
Zoning Bylaw Amended

3. Zoning Bylaw No. 8770 is amended in the manner set forth in this Bylaw.



R1A District to M2 District

4. The Zoning Map, which forms part of Bylaw No. 7800, is amended by rezoning the lands described in this Section and shown as  on Appendix "A" to this Bylaw from an R1A District to an M2 District subject to the provisions of the Agreement annexed as Appendix "B" to this Bylaw:
 - (a) Civic Address: 3200 Preston Avenue South
Portion of Surface Parcel No. 120563704
Legal Land Description: Blk/Par A, Plan 62S15086, Ext 1
As described on Certificate of Title 69S07597;
 - (b) Portion of Surface Parcel No. 120563658
Legal Land Description: Blk/Par C, Plan 66S07349, Ext 1
As described on Certificate of Title 66S08042; and
 - (c) Portion of Surface Parcel No. 203116665
Legal Land Description: NE 10-36-05-3, Ext 10.

Appendix "A"



ZONING AMENDMENT

-  From R1A to M2 by Agreement
-  From R1A to B1B by Agreement

Rezoning Agreement

This Agreement made effective this ____ day of _____, 2015.

Between:

The City of Saskatoon, a municipal corporation pursuant to *The Cities Act*, S.S. 2002 Chapter C-11.1 (hereinafter referred to as “the City”)

- and -

Pinnacle Developments Inc., a body corporate incorporated under the laws of the Province of Saskatchewan with Offices in the City of Saskatoon in the Province of Saskatchewan (hereinafter referred to as “the Owner”)

- and -

Dream Asset Management Corporation, an extra-provincial corporation carrying on business in the City of Saskatoon, in the Province of Saskatchewan (the “Owner”)

Whereas:

1. The Owner is the registered owner of the land described as follows:
 - (a) Civic Address: 3200 Preston Avenue South
Portion of Surface Parcel No. 120563704
Legal Land Description: Blk/Par A, Plan 62S15086, Ext 1
As described on Certificate of Title 69S07597;
 - (b) Surface Parcel No. 120563658
Reference Land Description: Blk/Par C, Plan 66S07349, Ext 1
As described on Certificate of Title 66S08042; and
 - (c) Portion of Surface Parcel No. 203116665
Reference Land Description: NE 10-36-05-3, Ext 10

(hereinafter referred to as “the Land”);
2. The Owners have applied to the City for approval to rezone the Land from an R1A District to an M2 District to allow the development of the proposal specified in this Agreement;

3. The City has an approved Development Plan which, pursuant to Section 69 of *The Planning and Development Act, 2007*, contained guidelines respecting the entering into of agreements for the purpose of accommodating requests for the rezoning of land; and
4. The City has agreed, pursuant to the provisions of Section 69 of *The Planning and Development Act, 2007*, to rezone the Land from an R1A District to an M2 District, subject to this Agreement.

Now therefore this Agreement witnesseth that the Parties hereto covenant and agree as follows:

Land to be Used in Accordance with Agreement

1. The Owners agree that, upon the Land being rezoned from an R1A District to an M2 District, none of the Land shall be developed or used except in accordance with the terms and conditions set out in this Agreement.

Use of Land

2. The Owner agrees that the use of the Land being rezoned will be restricted to the construction of a dwelling group consisting of multiple unit dwellings. The buildings and the site must be constructed and developed in substantial compliance with the site plan and building elevations attached as Schedule “A” to this Agreement.

Development Standards

3. The development standards applicable to the Land shall be those applicable to an M2 District except as follows:
 - (a) Amenity Space: minimum of 14 square metres per unit.
 - (b) Front Yard Setback: minimum of 4.0 metres.

Pedestrian Walkway

4.
 - (1) Pedestrian walkways must be provided throughout the site providing connections to adjacent sites and public roadways.
 - (2) Where pedestrian walkways cross a driving aisle, a raised or textured crosswalk must be provided with a minimum width of 1.2 metres.

Bicycle Parking

5. Bicycle parking shall be provided and located in areas that are visible and well lit and in close proximity to building entrances.

Parking

6. Parking shall be provided at a rate of 1.3 spaces per dwelling unit and 0.125 visitor spaces per dwelling unit.

Site Access

7. No vehicular traffic shall be permitted to enter the site from Lewin Way.

New Legal Description

8. The Owner has undertaken a subdivision application to create a new title for the Land. Upon creation of the new title, a schedule, being Schedule "B", will be added to this Agreement containing the legal description of the Land as described in the new title. Upon the addition of Schedule "B", the legal description of the Land for the purposes of this Agreement shall be as described in Schedule "B" in replacement of the legal description contained in paragraph 1 of the Recitals.

Application of Zoning Bylaw

9. The Owners covenant and agree that any sale, lease or other disposition or encumbrance of the Land or part thereof shall be made subject to the provisions of this Agreement.

Compliance with Agreement

10. The Owners covenant and agree not to develop or use the Land unless such development, use and construction complies with the provisions of this Agreement.

Dispositions Subject to Agreement

11. The Owners covenant and agree that any sale, lease or other disposition or encumbrance of the Land or part thereof shall be made subject to the provisions of this Agreement.

Definitions

12. Any word or phrase used in this Agreement which is defined in Zoning Bylaw No. 8770 shall have the meaning ascribed to it in that Bylaw.

Departures and Waivers

13. No departure or waiver of the terms of this Agreement shall be deemed to authorize any prior or subsequent departure or waiver, and the City shall not be obliged to continue any departure or waiver or permit subsequent departure or waiver.

Severability

14. If any covenant or provision of this Agreement is deemed to be void or unenforceable in whole or in part, it shall not be deemed to affect or impair the validity of any other covenant or provision of this Agreement.

Governing Law

15. This Agreement shall be governed and interpreted in accordance with the laws of the Province of Saskatchewan.

Effective Date of Rezoning

16. It is understood by the Owners that the Land shall not be effectively rezoned from an R1A District to an M2 District until:
 - (a) The Council of The City of Saskatoon has passed a Bylaw to that effect; and
 - (b) This Agreement has been registered by the City, by way of Interest Registration, against the Title to the Land.

Use Contrary to Agreement

17. (1) The Council of The City of Saskatoon may declare this Agreement void where any of the Land or buildings thereon is developed or used in a manner which is contrary to the provisions of this Agreement, and upon the Agreement being declared void, the Land shall revert to the district to which it was subject to before rezoning.

- (2) If this Agreement is declared void by the Council of The City of Saskatoon, the City shall not, by reason thereof, be liable to the Owners or to any other person for any compensation, reimbursement or damages on account of loss or profit, or on account of expenditures, or on any other account whatsoever in connection with the Land.

Registration of Interest

- 18. (1) The Parties hereto acknowledge that this Agreement is made pursuant to Section 69 of *The Planning and Development Act, 2007* and the Owners agree that this Agreement shall be registered by way of an Interest Registration against the Title to the Land. As provided in Section 236 of *The Planning and Development Act, 2007*, Section 63 of *The Land Titles Act, 2000* does not apply to the Interest registered in respect of this Agreement.
- (2) This Agreement shall run with the Land pursuant to Section 69 of *The Planning and Development Act, 2007*, and shall bind the Owners, its successors and assigns.

Enurement

- 19. This Agreement shall enure to the benefit of and be binding upon the Parties hereto and their respective heirs, executors, administrators, successors and assigns.

The City of Saskatoon

Mayor

c/s

City Clerk

Pinnacle Developments Inc.

c/s

Dream Development Corporation

c/s

Affidavit Verifying Corporate Signing Authority

Canada)
Province of Saskatchewan)
To Wit:)

I, _____, of the City of Saskatoon, in the
(Name)
Province of Saskatchewan, _____, make oath and say:
(Position Title)

1. I am an officer or director of the corporation named in the within instrument.
2. I am authorized by the corporation to execute the instrument without affixing a corporate seal.

Sworn before me at the City of Saskatoon,)
in the Province of Saskatchewan, this ____)
day of _____, 2015.)
)
_____)

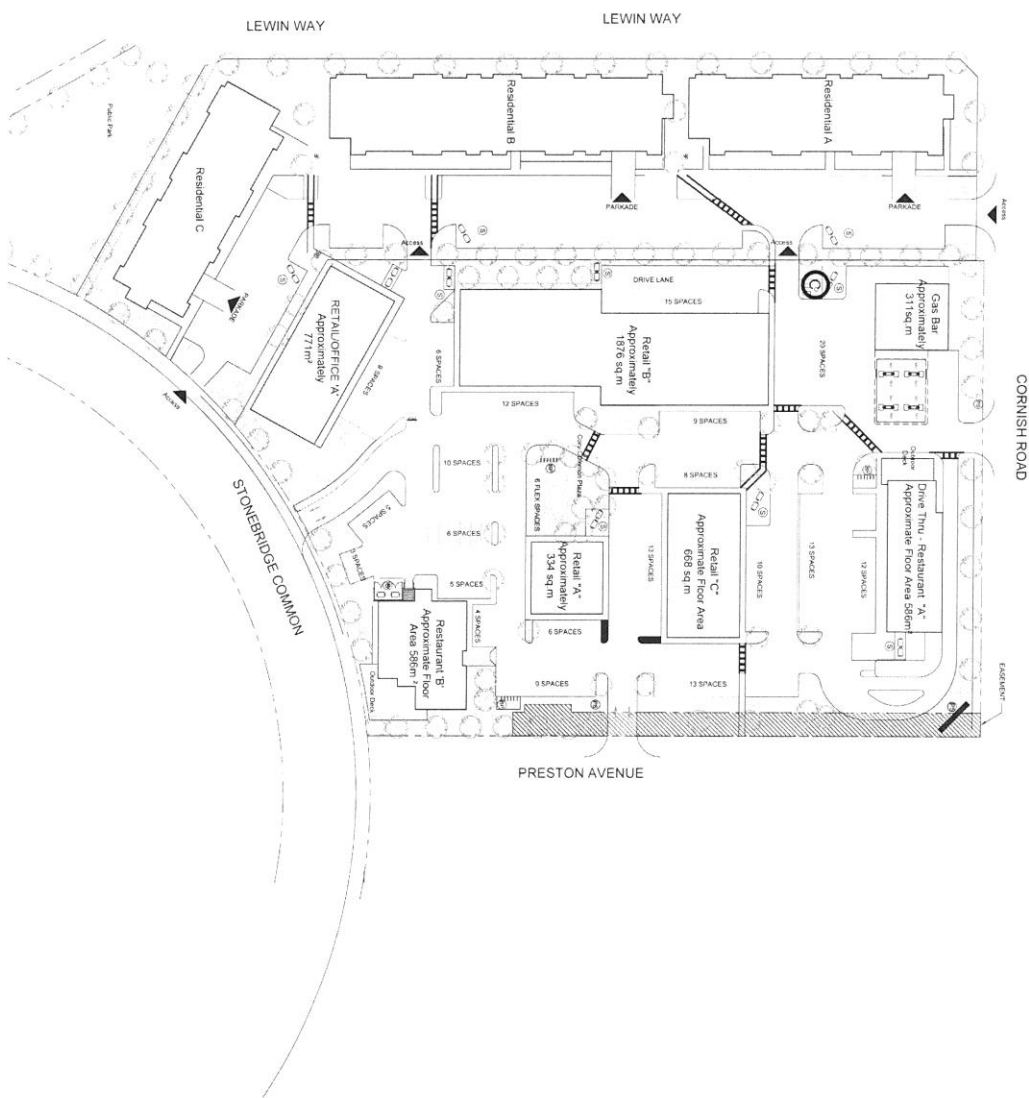
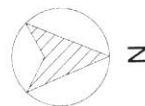
A Commissioner for Oaths for Saskatchewan.)
My commission expires _____.)
(or) Being a solicitor.)

(Signature)

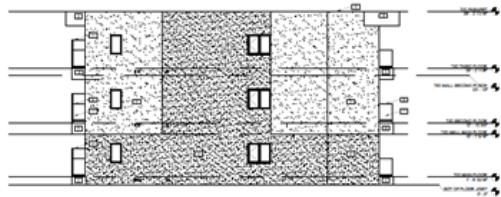
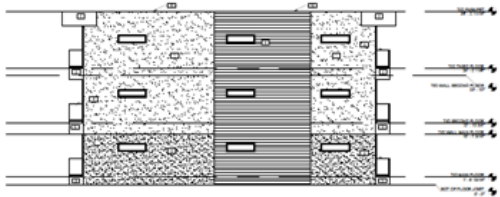
Schedule "A"

SITE PLAN, 3000 PRESTON AVENUE
1:500

KEY
 (C) CELL TOWER AND CABINETS BY SASKTEL



Typical Elevations – M2 by Agreement



Legend	
[Symbol]	Window
[Symbol]	Door
[Symbol]	Wall
[Symbol]	Roof
[Symbol]	Foundation
[Symbol]	Other

Rezoning Agreement

This Agreement made effective this ____ day of _____, 2015.

Between:

The City of Saskatoon, a municipal corporation pursuant to *The Cities Act*, S.S. 2002 Chapter C-11.1 (hereinafter referred to as “the City”)

- and -

Pinnacle Developments Inc., a body corporate incorporated under the laws of the Province of Saskatchewan with Offices in the City of Saskatoon in the Province of Saskatchewan (hereinafter referred to as “the Owner”)

Whereas:

1. The Owner is the registered owner of the land described as follows:
 - (a) Civic Address: 3200 Preston Avenue South
Portion of Surface Parcel No. 120563704
Legal Land Description: Blk/Par A, Plan 62S15086, Ext 1
As described on Certificate of Title 69S07597; and
 - (b) Surface Parcel No. 120563658
Reference Land Description: Blk/Par C, Plan 66S07349, Ext 1
As described on Certificate of Title 66S08042

(hereinafter referred to as “the Land”);
2. The Owners have applied to the City for approval to rezone the Land from an R1A District to a B1B District to allow the development of the proposal specified in this Agreement;
3. The City has an approved Development Plan which, pursuant to Section 69 of *The Planning and Development Act, 2007*, contained guidelines respecting the entering into of agreements for the purpose of accommodating requests for the rezoning of land; and
4. The City has agreed, pursuant to the provisions of Section 69 of *The Planning and Development Act, 2007*, to rezone the Land from an R1A District and a B1B District, subject to this Agreement.

Now therefore this Agreement witnesseth that the Parties hereto covenant and agree as follows:

Land to be Used in Accordance with Agreement

1. The Owners agree that, upon the Land being rezoned from an R1A District a B1B District, none of the Land shall be developed or used except in accordance with the terms and conditions set out in this Agreement.

Use of Land

2.
 - (1) The use of the land is limited to a shopping centre that may include those uses permitted in the B1B District.
 - (2) The use of the land may also include a gas bar as well as lounges operating in conjunction with and attached to a restaurant.
 - (3) The buildings and the site must be constructed and developed in substantial compliance with the site plan attached as Schedule “A” to this Agreement.
 - (a) Minor variances in the size, location and number of buildings are permissible provided that the maximum building area and maximum area for retail and restaurant uses do not exceed the limits prescribed and as approved by the Development Officer.

Development Standards

3. The development standards applicable to the Land shall be those applicable to a B1B District except as follows:
 - (a) The total maximum building floor area for all retail and restaurant uses shall not exceed 3,725 square metres (40,000 square feet);
 - (b) A maximum of two retail uses are permitted to have a maximum building floor area of up to 930 square metres (10,000 square feet);
 - (c) All other retail uses shall not exceed a maximum building floor area of 325 square metres (3,500 square feet) each;
 - (d) A maximum of two restaurants are permitted to have a maximum building floor area of 586 square metres (6,308 square feet) each provided they have an outdoor amenity space or patio which shall be located so as to not cause a nuisance to nearby residential uses;

- (e) For all other restaurants shall not exceed a maximum building floor area of 325 square metres (3,500 square feet) each;
- (f) The floor area of a lounge in conjunction with and attached to any restaurant shall not exceed 50% of the public assembly floor area of the adjoining restaurant or 100 square metres (1,076 square feet), whichever is less. The total building floor area of a restaurant and lounge cannot exceed maximum building floor area as provided in this Agreement;
- (g) Any accessory convenience store associated with the gas bar shall not exceed a maximum building floor area of 325 square metres (3,500 square feet); and
- (h) Rear yard setback shall be a minimum of 0.9 metres for Retail Office Building “A” as shown on Schedule “A”.

Landscaping

- 4. In addition to the landscaping requirements for a B1B District, landscaping of traffic islands and other interior areas on the site shall be provided whenever possible and to the satisfaction of the Development Officer.

Outdoor Plaza

- 5.
 - (1) An outdoor plaza must be developed on-site as indicated on the site plan, attached as Schedule “A” to this Agreement, providing for use by the general public and commercial tenants of the site.
 - (2) The outdoor plaza must include hard and soft landscaping elements such as decorative paving treatments, seating and attractive planting.
 - (3) Any garbage or loading areas directly adjacent to the outdoor plaza must be suitably screened from view with a constructed wall that incorporates finishes so as to appear as part of the building associated to it.

Outdoor Lighting

- 6. All outdoor lighting must be of an appropriate arrangement and intensity that does not unduly interfere with the adjacent land uses or with the safe operation of nearby roadways and traffic control devices.

Pedestrian Walkways

7. (1) Pedestrian walkways must be provided throughout the site providing connections to adjacent sites and public roadways.
- (2) Where pedestrian walkways cross a driving aisle, a raised or textured crosswalk must be provided with a minimum width of 1.2 metres.

Bicycle Parking

8. (1) A minimum of one bicycle parking space must be provided for every 300 square metres of gross floor area devoted to commercial use.
- (2) Bicycle parking shall be provided and located in areas that are visible, well lit and in close proximity to building entrances.

Street Interface

9. Building faces that front onto Preston Avenue must, to the extent possible, provide interaction with the street including, but not limited to, glazing, principal entrances and articulation of the building facade through the use of building materials, colours, textures and other architectural features to the satisfaction of the Development Offices.

Signage

10. Signs shall be permitted on the Land in accordance with the regulations applicable to Signage Group No. 2 contained in The Sign Regulations, being Schedule “A” to Zoning Bylaw No. 7800 except as follows:
 - (a) A maximum of three freestanding signs are permitted for the site subject to the provisions of 3.2.3.2 of The Sign Regulations;
 - (b) Freestanding signs cannot be within 55 metres from any other freestanding sign on the site measured linearly along the property line;
 - (c) One area identification sign is permitted subject to the following:
 - (i) the sign must not exceed 3.0 metres in height;
 - (ii) the size of any single face area of the sign must not exceed 6.0 square metres;
 - (iii) the total face area must not exceed 12.0 metres; and

- (iv) the sign must not consist of any advertising.
- (d) Signage can only be illuminated during the hours of operation applicable to the particular permitted land use; and
- (e) Maximum of 3 portable signs shall be permitted on the site at any one time.

New Legal Description

- 11. The Owner has undertaken a subdivision application to create a new title for the Land. Upon creation of the new title, a schedule, being Schedule “B”, will be added to this Agreement containing the legal description of the Land as described in the new title. Upon the addition of Schedule “B”, the legal description of the Land for the purposes of this Agreement shall be as described in Schedule “B” in replacement of the legal description contained in paragraph 1 of the Recitals.

Application of Zoning Bylaw

- 12. The Owners covenant and agree that any sale, lease or other disposition or encumbrance of the Land or part thereof shall be made subject to the provisions of this Agreement.

Compliance with Agreement

- 13. The Owners covenant and agree not to develop or use the Land unless such development, use and construction complies with the provisions of this Agreement.

Dispositions Subject to Agreement

- 14. The Owners covenant and agree that any sale, lease or other disposition or encumbrance of the Land or part thereof shall be made subject to the provisions of this Agreement.

Definitions

- 15. Any word or phrase used in this Agreement which is defined in Zoning Bylaw No. 8770 shall have the meaning ascribed to it in that Bylaw.

Departures and Waivers

16. No departure or waiver of the terms of this Agreement shall be deemed to authorize any prior or subsequent departure or waiver, and the City shall not be obliged to continue any departure or waiver or permit subsequent departure or waiver.

Severability

17. If any covenant or provision of this Agreement is deemed to be void or unenforceable in whole or in part, it shall not be deemed to affect or impair the validity of any other covenant or provision of this Agreement.

Governing Law

18. This Agreement shall be governed and interpreted in accordance with the laws of the Province of Saskatchewan.

Effective Date of Rezoning

19. It is understood by the Owners that the Land shall not be effectively rezoned from an R1A District to a B1B District until:
 - (a) The Council of The City of Saskatoon has passed a Bylaw to that effect; and
 - (b) This Agreement has been registered by the City, by way of Interest Registration, against the Title to the Land.

Use Contrary to Agreement

20. (1) The Council of The City of Saskatoon may declare this Agreement void where any of the Land or buildings thereon is developed or used in a manner which is contrary to the provisions of this Agreement, and upon the Agreement being declared void, the Land shall revert to the district to which it was subject to before rezoning.
- (2) If this Agreement is declared void by the Council of The City of Saskatoon, the City shall not, by reason thereof, be liable to the Owners or to any other person for any compensation, reimbursement or damages on account of loss or profit, or on account of expenditures, or on any other account whatsoever in connection with the Land.

Registration of Interest

- 21. (1) The Parties hereto acknowledge that this Agreement is made pursuant to Section 69 of *The Planning and Development Act, 2007* and the Owners agree that this Agreement shall be registered by way of an Interest Registration against the Title to the Land. As provided in Section 236 of *The Planning and Development Act, 2007*, Section 63 of *The Land Titles Act, 2000* does not apply to the Interest registered in respect of this Agreement.
- (2) This Agreement shall run with the Land pursuant to Section 69 of *The Planning and Development Act, 2007*, and shall bind the Owners, its successors and assigns.

Enurement

- 22. This Agreement shall enure to the benefit of and be binding upon the Parties hereto and their respective heirs, executors, administrators, successors and assigns.

The City of Saskatoon

Mayor

c/s

City Clerk

Pinnacle Developments Inc.

c/s

Affidavit Verifying Corporate Signing Authority

Canada)
Province of Saskatchewan)
To Wit:)

I, _____, of the City of Saskatoon, in the
(Name)
Province of Saskatchewan, _____, make oath and say:
(Position Title)

1. I am an officer or director of the corporation named in the within instrument.
2. I am authorized by the corporation to execute the instrument without affixing a corporate seal.

Sworn before me at the City of Saskatoon,)
in the Province of Saskatchewan, this ____)
day of _____, 2015.)
)
_____)

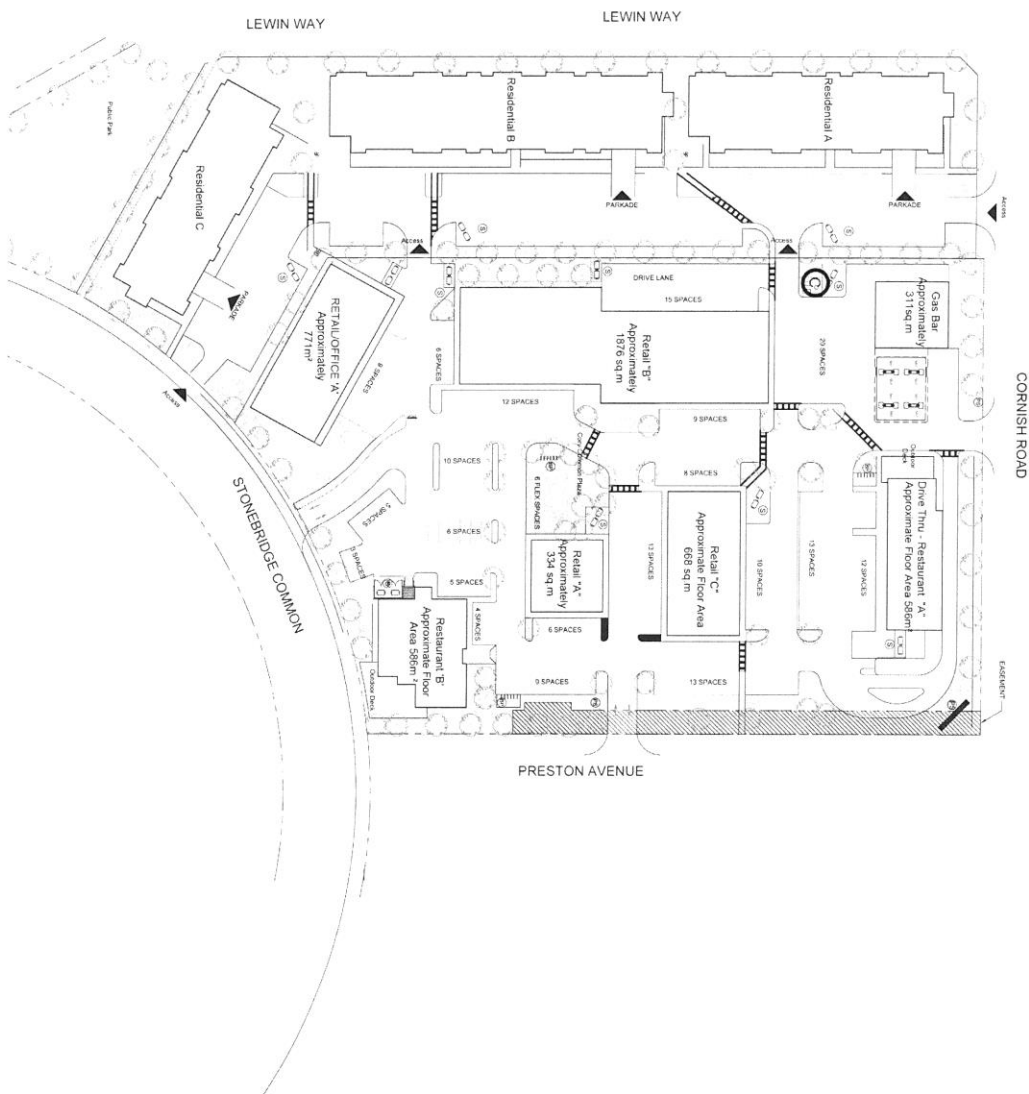
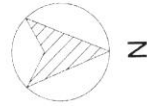
A Commissioner for Oaths for Saskatchewan.)
My commission expires _____.)
(or) Being a solicitor.)

(Signature)

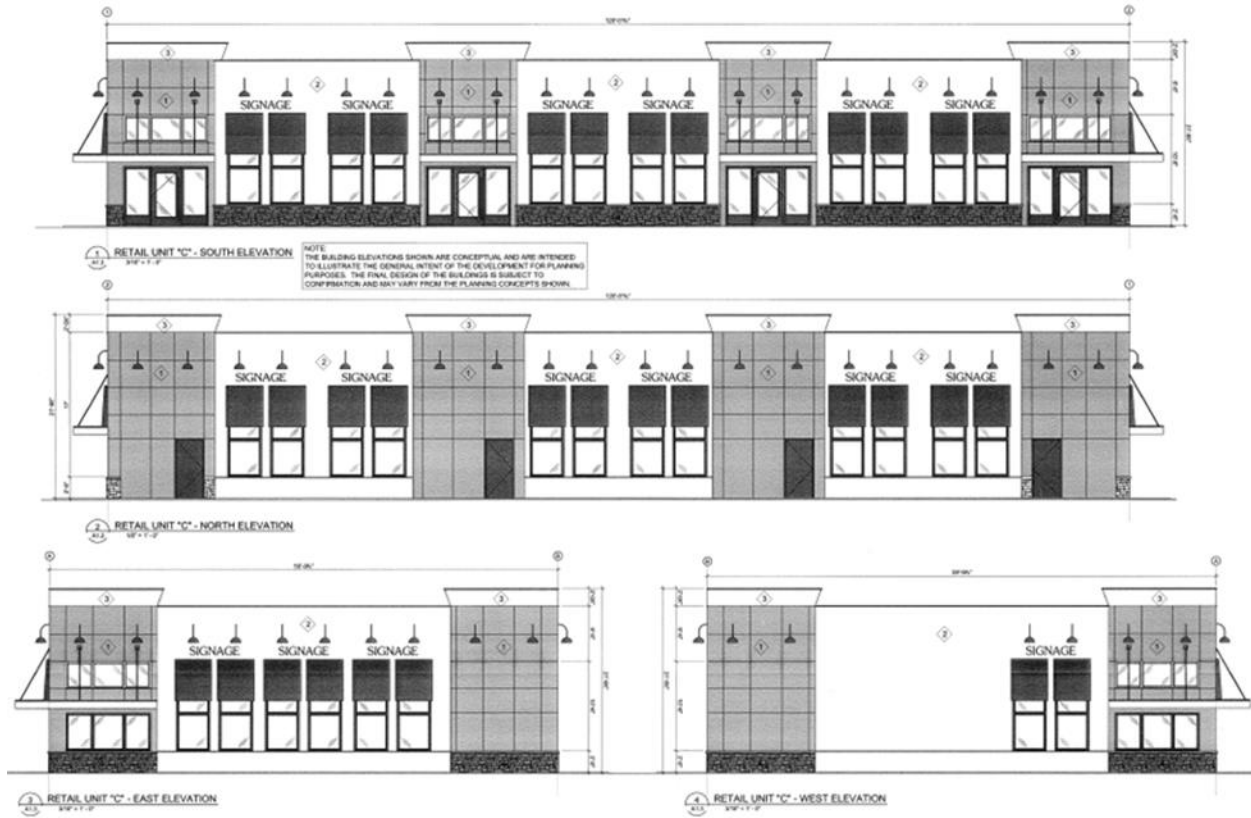
Schedule "A"

1 SITE PLAN, 3000 PRESTON AVENUE
1:500

KEY
C CELL TOWER AND CABINETS BY SASKTEL



Typical Elevations – B1B by Agreement



Proposed Rezoning by Agreement – From R1A to B1B by Agreement and M2 by Agreement – 3200 Preston Avenue South

Recommendation

That a copy of this report be submitted to City Council recommending that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone 3200 Preston Avenue South, as outlined in this report, be approved.

Topic and Purpose

An application has been submitted by Pinnacle Developments Inc. (Pinnacle Developments) and Innovative Residential Inc. (Innovative Residential) requesting to rezone 3200 Preston Avenue South from R1A – One-Unit Residential District (R1A District) to B1B – Neighbourhood Commercial – Mixed-Use District (B1B District) and M2 – Community Institutional Service District (M2 District), subject to Rezoning Agreements (Agreement).

The rezoning will provide for the development of a mixed-use site incorporating residential, commercial, and institutional uses, consistent with the Stonebridge Neighbourhood Concept Plan (Concept Plan).

Report Highlights

1. The Concept Plan identifies the subject site for mixed-use development.
2. A mixed-use development combining residential, commercial, and institutional uses is proposed.
3. This application will apply zoning to the area that is consistent with the Concept Plan and allow development to proceed.
4. Rezoning Agreements will ensure that a variety of features of the development are provided that contribute to its fit within the Stonebridge neighbourhood.

Strategic Goal

This report supports the Strategic Goal of Sustainable Growth by supporting the creation of “complete community” neighbourhoods, which includes development that combines a mix of uses.

Background

The Concept Plan was originally approved by City Council in 2005 (see Attachment 1). The Concept Plan identified the subject site as “mixed use” and as part of a village centre that combines multiple-unit- residential uses in an appropriate mix with retail, commercial, office, and institutional uses.

Proposed Rezoning by Agreement – From R1A to B1B by Agreement and M2 by Agreement – 3200 Preston Avenue South

This application will apply zoning to this area that is consistent with the land uses envisioned by the Concept Plan.

Report

Proposed Development of 3200 Preston Avenue South

Pinnacle Developments and Innovative Residential are proposing to develop 3200 Preston Avenue South as a mixed-use development that incorporates a horizontal mix of multiple-unit residential, commercial, and institutional uses (see Attachment 2).

On the west side of the site, a dwelling group consisting of 3, three-storey, multiple-unit dwellings is proposed with approximately 150 units in total. A combination of underground and surface parking will serve the development.

On the east side of the site, a neighbourhood commercial development containing approximately 5,000 m² of leasable space over seven buildings is proposed that will provide for commercial, retail, office, and institutional uses.

Connectivity between the two components of the development is provided through roadway connections and a pedestrian walkway system through the site. The approximate centre of the commercial component of the site contains a plaza that will provide for flexible use by commercial tenants and the community. This feature aligns with the vision of the Concept Plan for this site as part of a village centre for Stonebridge.

See Attachment 3 for a proposed site plan, Attachment 4 for building elevations of the residential component, and Attachment 5 for building elevations of the commercial component.

M2 District by Agreement

It is proposed that a zoning designation of M2 – Community Institutional Service District, subject to an Agreement, be applied to the residential component of the site. The Agreement will provide for the development of a dwelling group as proposed.

B1B District by Agreement

It is proposed that a zoning designation of B1B District, subject to an Agreement, be applied to the commercial component of the site. The Agreement will provide for the development of a neighbourhood commercial site that is of an appropriate scale for the area.

As part of the Agreement, provision will be made for a limited number of retail stores and restaurants to exceed the 325 m² maximum limit on gross floor area (GFA) that exists in the B1B District. Two retail stores may have a maximum GFA of 930 m², while two restaurants may have a maximum GFA of 586 m². These restrictions are necessary to ensure that the development remains as a neighbourhood scale mixed-use site.

Proposed Rezoning by Agreement – From R1A to B1B by Agreement and M2 by Agreement – 3200 Preston Avenue South

Other aspects of the Agreement will ensure that:

- a) buildings fronting on to Preston Avenue appropriately address and interact with the street;
- b) a pedestrian walkway system through the site and connecting to adjacent streets is provided to promote walkability;
- c) an outdoor plaza will be constructed to provide amenity space;
- d) bicycle parking will be provided;
- e) outdoor lighting that will not unduly interfere with adjacent properties;
- f) landscaping of the site is provided, including internally within the site; and
- g) signage is of an appropriate scale for the area.

See Attachment 6 for the complete list of terms for each of the two Agreements.

Comments from Other Divisions

No concerns were received through the administrative referral process that precludes this application from proceeding to the public hearing. Please refer to Attachment 7 for complete comments.

Options to the Recommendation

City Council could choose to deny this application. This decision is not recommended as the proposed development is consistent with the Concept Plan.

Public and/or Stakeholder Involvement

A public information meeting regarding this proposal was held on July 14, 2015, with 18 members of the general public and the local City Councillor in attendance.

Discussion at the meeting specific to the development proposal generally related to:

- a) the design, height, and size of proposed buildings;
- b) concerns relating to the commercial component, including traffic, noise, and effects from outdoor lighting; and
- c) potential uses in the commercial component.

A general discussion about issues with the development process in Stonebridge also took place. See Attachment 8 for a record of the questions, comments, and discussion.

Other Considerations/Implications

There are no policy, financial, environmental, privacy, or CPTED implications or considerations. A communication plan is not required at this time.

Due Date for Follow-up and/or Project Completion

No follow-up is required.

Public Notice

Public notice is required for consideration of this matter, pursuant to Section 11(a) of Public Notice Policy No. C01-021. Once this application has been considered by the Municipal Planning Commission, it will be advertised in accordance with

**Proposed Rezoning by Agreement – From R1A to B1B by Agreement and M2 by Agreement –
3200 Preston Avenue South**

Public Notice Policy No. C01-021, and a date for a public hearing will be set. The Planning and Development Division will notify all property owners within 75 metres of the subject site of the public hearing date, by letter. A notice will be placed in The StarPhoenix two weeks prior. Notice boards will be placed on the site.

Attachments

1. Stonebridge Neighbourhood Concept Plan
2. Location Map
3. Proposed Site Plan
4. Proposed Building Elevations – Residential
5. Proposed Building Elevations – Commercial (Sample Elevation)
6. Provisions of Rezoning Agreements
7. Comments from Other Divisions
8. Community Engagement Summary

Report Approval

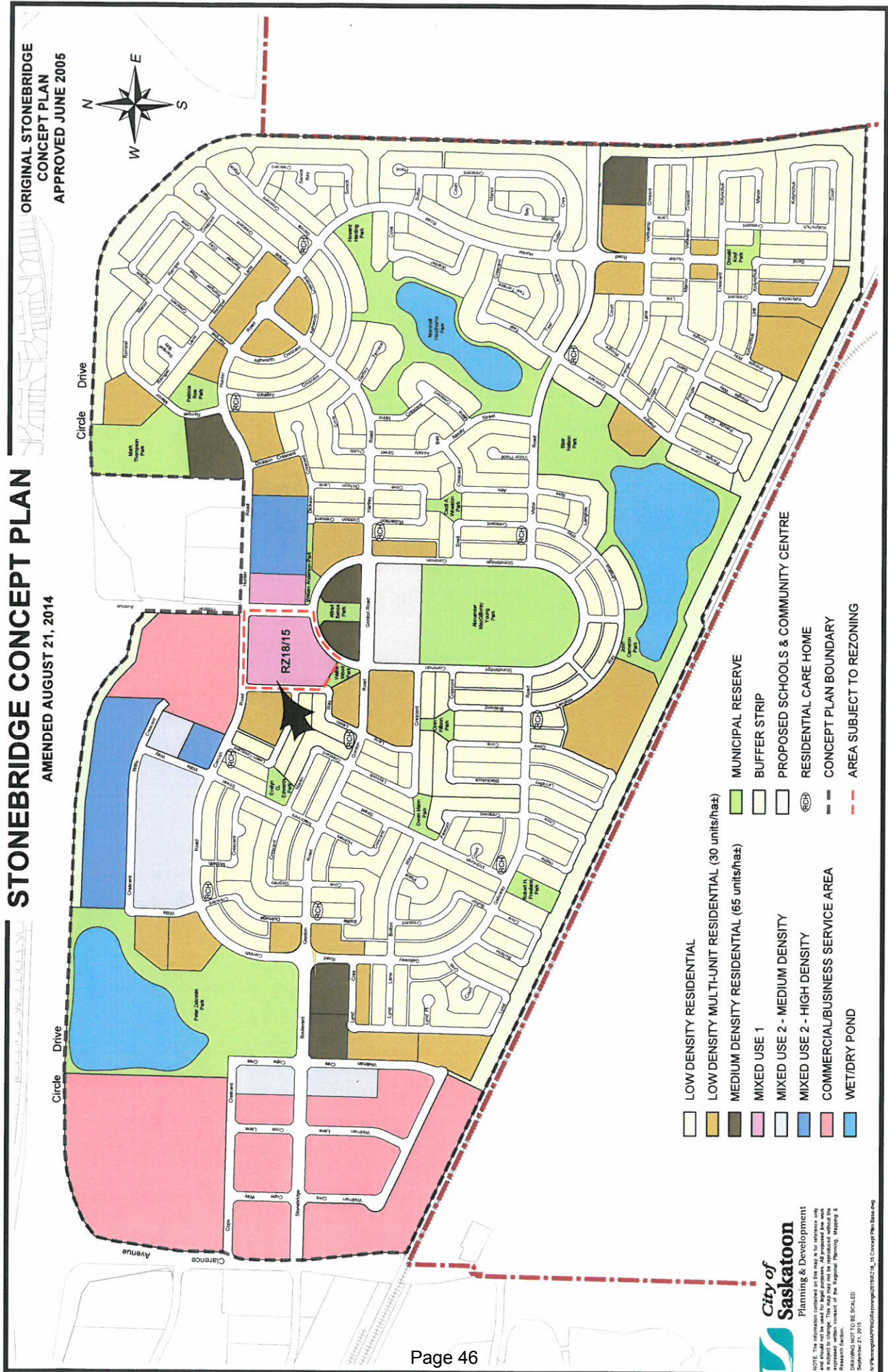
Written by: Brent McAdam, Planner, Planning and Development
Reviewed by: Alan Wallace, Director of Planning and Development
Approved by: Randy Grauer, General Manager, Community Services Department

S:\Reports\DS\2015\MPC-Proposed Rezoning by Agreement – From R1A to B1B by Agreement and M2 by Agreement – 3200
Preston Avenue South\kt
FINAL/APPROVED – R. Grauer – Sept 21/15

STONEBRIDGE CONCEPT PLAN

AMENDED AUGUST 21, 2014

ORIGINAL STONEBRIDGE
CONCEPT PLAN
APPROVED JUNE 2005



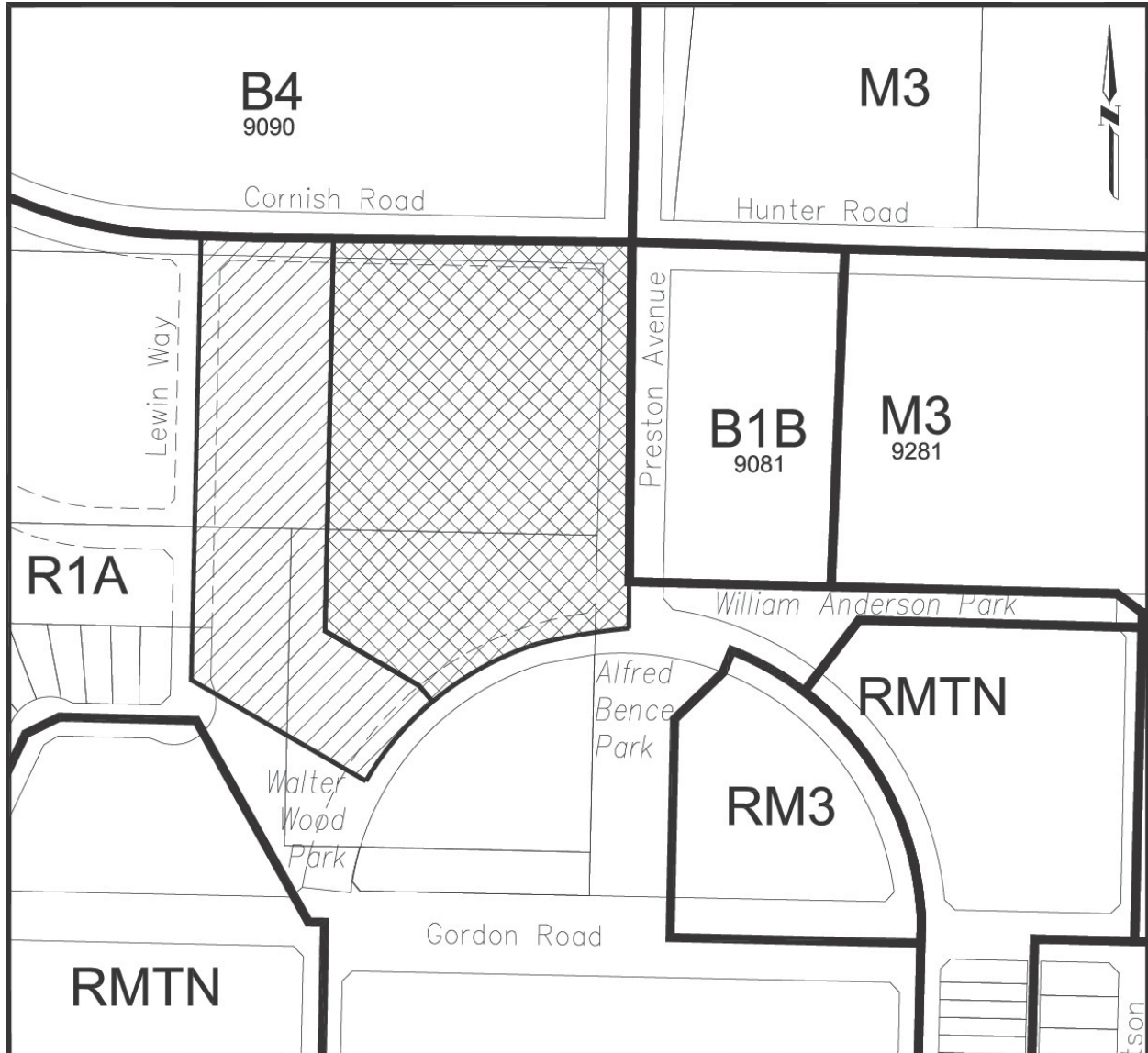
- LOW DENSITY RESIDENTIAL
- LOW DENSITY MULTI-JUNIT RESIDENTIAL (30 units/ha±)
- MEDIUM DENSITY RESIDENTIAL (65 units/ha±)
- MIXED USE 1
- MIXED USE 2 - MEDIUM DENSITY
- MIXED USE 2 - HIGH DENSITY
- COMMERCIAL/BUSINESS SERVICE AREA
- WET/DRY POND
- MUNICIPAL RESERVE
- BUFFER STRIP
- PROPOSED SCHOOLS & COMMUNITY CENTRE
- RESIDENTIAL CARE HOME
- CONCEPT PLAN BOUNDARY
- AREA SUBJECT TO REZONING

**City of
Saskatoon**
Planning & Development



NOTE: The information contained on this map is for reference only and should not be used for legal purposes. All proposed line work is subject to change without notice. The information is not intended to be used in any way without the express written consent of the Regional Planning, Mapping & Research Section.

REVISED BY: [Name]
DATE: [Date]
SCALE: [Scale]

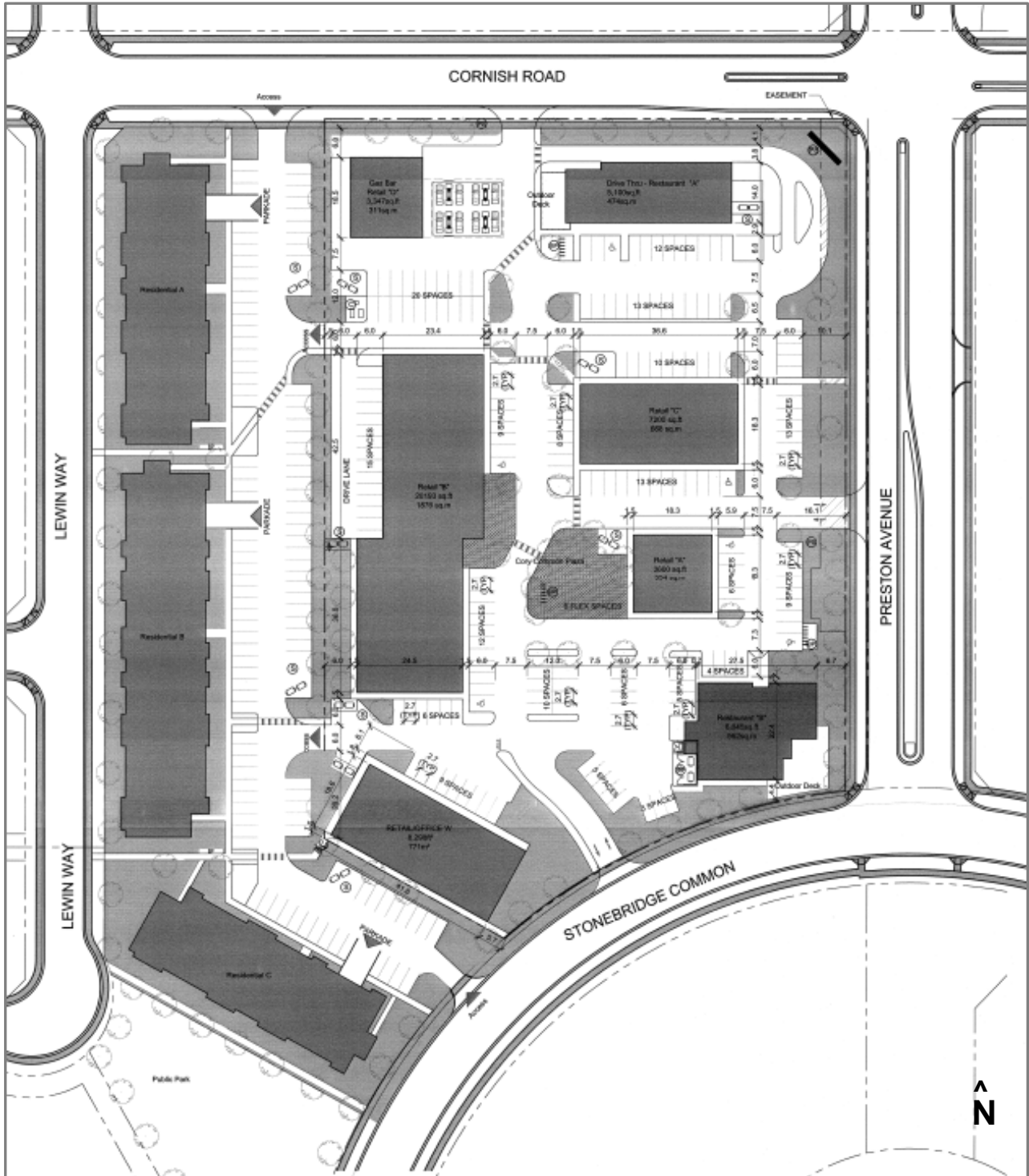
Location Map

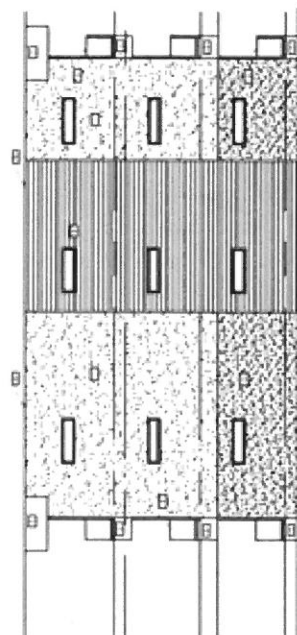
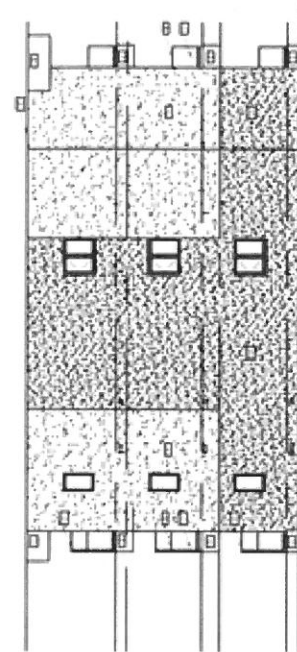
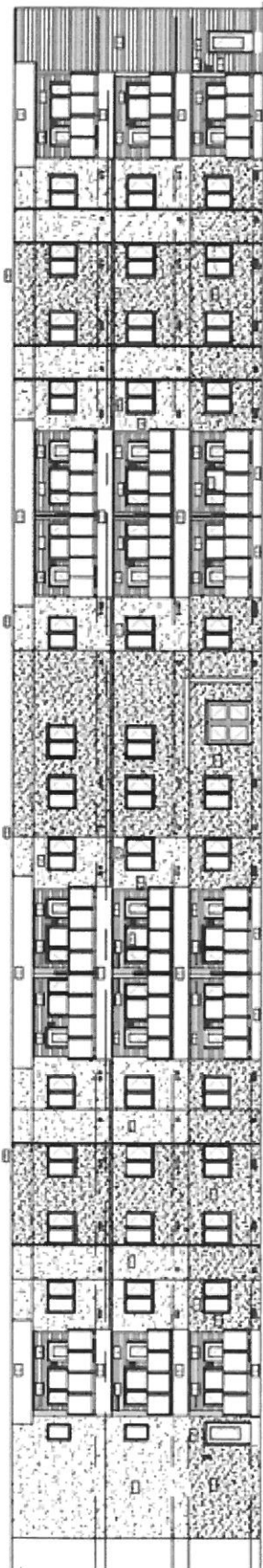


ZONING AMENDMENT

-  From R1A to M2 by Agreement
-  From R1A to B1B by Agreement

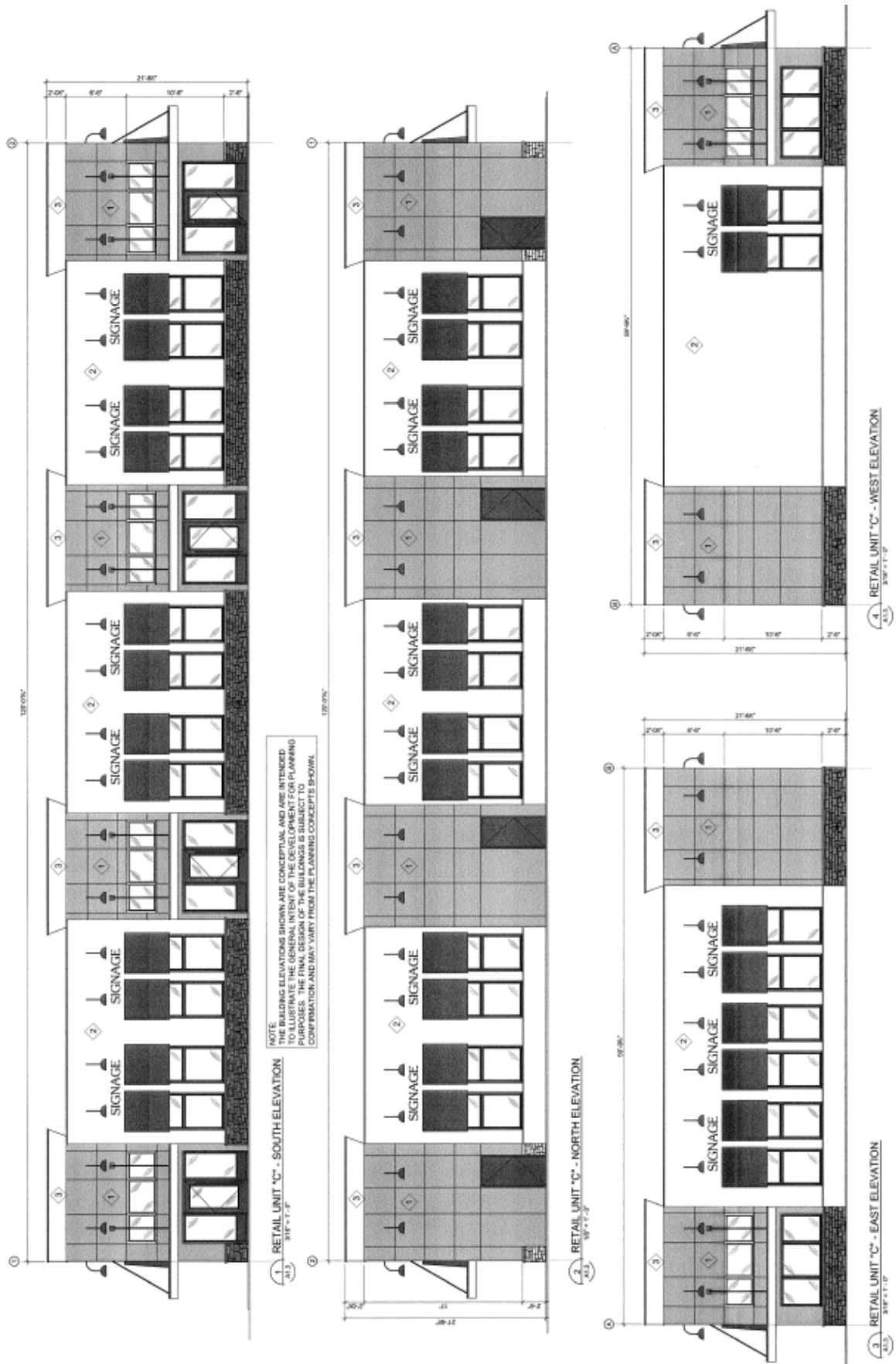
Proposed Site Plan





Proposed Building Elevations – Commercial
(Sample Elevation)

ATTACHMENT 5



Provisions of Rezoning Agreements

B1B by Agreement

- **Use of Land:** A shopping centre that may include those uses permitted in the B1B District, and lounges in conjunction with and attached to a restaurant, and a gas bar.
- **Development Standards:**
 - Total maximum building floor area for all retail uses shall not exceed 3,725 m² (40,000 square feet).
 - A maximum of two retail uses are permitted to have a maximum building floor area up to of 930 m² (10,000 square feet).
 - All other retail uses shall not exceed a maximum building floor area of 325 m² (3,500 square feet).
 - A maximum of two restaurants are permitted to have a maximum building floor area of 586 m² (6,308 square feet) each provided they have an outdoor amenity space or patio. Outdoor amenity space or patio must be located so as to not cause a nuisance with nearby residential uses.
 - All other restaurants shall not exceed a maximum building floor area of 325 m² (3,500 square feet).
 - The floor area of a lounge in conjunction with and attached to any restaurant shall not exceed 50% of the public assembly floor area of the adjoining restaurant or 100 m² (1,076 ft²), whichever is less.
 - Any accessory convenience store associated with the gas bar shall not exceed a maximum building floor area of 325 m² (3,500 square feet).
 - Rear yard setback of 0.9 metres (The building located in the southwest corner of the site has a rear yard setback of 0.9 metres).
 - The site must be developed substantially in accordance with the site plan attached to this Agreement. Minor variations in the size, location and number of buildings are permissible provided that the maximum building area and maximum area for retail and restaurant uses do not exceed the limits prescribed.
- **Street Interface:** Building faces that front on to Preston Avenue shall, to the extent that is possible, provide interaction with the street including, but not limited to: glazing, principal entrances, and articulation of the building façade through the use of building materials, colour, textures, and other architectural features.
- **Pedestrian Walkways:**
 - Pedestrian walkways shall be provided throughout the site providing connections to adjacent public roadways and to adjacent sites.
 - Where pedestrian walkways cross a driving aisle, a raised or textured crosswalk shall be provided with a minimum width of 1.2 metres.

- **Outdoor Plaza:** An outdoor public plaza shall be developed on-site as indicated on the site plan that provides for use by the general public and commercial tenants of the site. Hard and soft landscaping elements shall be used, including decorative paving treatments, seating, and attractive plantings. Any garbage or loading areas directly adjacent to the plaza shall be suitably screened from view with a constructed wall that incorporates finishes so as to appear as part of the building associated to it.
- **Bicycle Parking:**
 - Shall be provided in locations that are readily visible, well lit and in close proximity to building entrances.
 - A minimum of one bicycle parking space shall be provided for every 300 m² of gross floor area.
- **Landscaping:**
 - As per the requirements of the B1B District, and to the satisfaction of the Development Officer.
 - Landscaping of traffic islands and other interior areas on the site shall be provided wherever possible and to the satisfaction of the Development Officer.
- **Outdoor Lighting:** All outdoor lighting shall be of an appropriate arrangement and intensity that does not unduly interfere with the adjacent land uses or interfere with the safe operation of nearby roadways and traffic control devices.
- **Signage:** Signage shall comply with Signage Group 2 of the Sign Regulations with the following provisions:
 - A maximum of three freestanding signs are permitted for the site, and are subject to the regulations in Signage Group 2 for shopping centres.
 - Freestanding signs are subject to a minimum spacing requirement of 55 metres from any other freestanding sign on site measured linearly along the property line.
 - One area identification sign shall be permitted and shall not exceed 3.0 metres in height. The size of any single face area shall not exceed 6.0 metres square and the total face area shall not exceed 12.0 metres. No advertising is permitted on this sign.
 - Signage shall be illuminated only during hours of operation of the uses located on the land.
 - A maximum of three portable signs shall be permitted on the site at any one time.

All other provisions of the B1B District shall apply.

M2 by Agreement

- **Use of Land:** A dwelling group.
- **Parking:**
 - Shall be provided at a rate of 1.3 spaces per dwelling unit plus 0.125 visitor spaces per unit.
 - No vehicular access shall be permitted from Lewin Way.
- **Amenity Space:** A minimum of 14 m² per unit.
- **Front Yard Setback:** A minimum of 4.0 metres.
- **Bicycle Parking:**
 - Shall be provided in locations that are readily visible, well lit and in close proximity to building entrances.
- **Pedestrian Walkways:**
 - Pedestrian walkways shall be provided throughout the site providing connections to adjacent public roadways and to adjacent sites.
 - Where pedestrian walkways cross a driving aisle, a raised or textured crosswalk shall be provided with a minimum width of 1.2 metres.

All other provisions of the M2 District shall apply.

Comments From Other Divisions

Transportation and Utilities Department

The proposed amendments, as noted in the report, are acceptable to the Transportation and Utilities Department, with the following comments:

1. The sanitary sewer model suggests having the apartments tie-in to Gordon Road/Stonebridge Common and the commercial tie-in to Preston Avenue. No connections to Cornish Road will be permitted.
2. The storm model identified a need for on-site storage to accommodate the volume in a 2-year event.
3. There is sufficient water flow to the site.
4. The Traffic Impact Study (TIS) submitted in relation to this proposal and its recommendations are acceptable.

COMMUNITY ENGAGEMENT SUMMARY

PUBLIC INFORMATION MEETING FOR PROPOSED REZONING BY AGREEMENT 3200 PRESTON AVENUE SOUTH

Applicant: Pinnacle Developments Inc.
File: PL 4350 – Z18/15

Project Description

A public information meeting was held regarding a proposed rezoning by agreement of 3200 Preston Avenue South

The meeting was held at Circle Drive Alliance Church – Fireside Lounge on July 14, 2015, at 7:00 p.m.

Community Engagement Strategy

Purpose:

To inform and consult. Residents were provided with an overview of the applicant's proposal and provided the opportunity to ask questions and provide comments. Written comments (email/comment sheets) were accepted following the meeting.

Form of community engagement used:

Public Information Meeting – Residents are provided an opportunity to listen to a presentation by the applicant, participate in a question and answer session, and speak directly with the applicant and/or City staff following the formal portion of the meeting. City staff were in attendance to provide an overview of the rezoning process and the next steps following the meeting.

Level of input or decision making required from the public:

Comments, concerns, and opinions were sought from the public.

Who was involved:

- Internal stakeholders: The standard referral process was followed, and relevant internal divisions of the City were contacted for comments. Councillor Loewen was also contacted.
- External stakeholders: A flyer with details of the meeting was sent to property owners within an approximate 75 metre radius of the subject site in advance of the meeting (a total of 74 notices). The Stonebridge Community Association was also provided notice.
- Eighteen members of the general public attended the meeting, in addition to Councillor Loewen.

Summary of Community Engagement Feedback

Following introductory remarks of the rezoning process by City staff and an overview of the proposed rezoning by the applicant, a question and answer period and general discussion followed. Concerns raised, questions, and general points of discussion related to:

- Preference for a pitched roof on the apartment buildings instead of a flat roof as proposed.

- Concern over proposed height and number of stories of residential buildings adjacent to single family homes.
- Concerns over issues experienced with other commercial development in the area; how can such problems be avoided with this development?
 - Issues arising from a nearby patio to the north regarding its size, noise levels, and lighting
- Questions, concerns, and comments regarding the commercial/institutional component of the development related to:
 - How will the lighting on the commercial site be arranged to not interfere with adjacent residences?
 - Discussion around the aesthetics and design of the commercial buildings.
 - Height of the buildings
 - Proposed size of stores
 - Bars or taverns are not wanted
 - Concern over large amounts of outdoor seating directly adjacent to residential and noise impacts.
 - Concern over increased traffic in the area
 - Concern over garbage and litter
- Will there be vehicle access to the site from Lewin Way? It was confirmed that no accesses are proposed from Lewin Way.
- What is the impact of traffic on Lewin Way when the roadway is complete from Gordon Rd to Cornish Rd and the area is developed.
 - Lewin being used for short-cutting and vehicles are travelling at high speeds down the residential streets
- Inadequate signage is in place in the area to direct drivers who are trying to shortcut away from deadend streets and construction areas
- Discussion of upcoming neighbourhood-wide traffic study in approximately 2 years when development in Stonebridge will be substantially complete and traffic patterns will have normalized.
- Concern regarding how neighbourhood concept plans and related information is shared with homeowners in a new neighbourhood, including:
 - How does the City work with builders and realtors to ensure that accurate information about a neighbourhood's development and intended land uses is shared with homeowners?
 - Some felt misled about future plans for the area from their builder or realtor.
 - Expressions of surprise and frustration that the area would not be developed as single family homes.
 - Discussion regarding the fact that the parcel has been designated as "Mixed Use" on the Concept Plan since its inception, and that such information is shared openly by the City.
- What is the zoning changing from?
 - Explanation that the area is currently zoned R1A District, dating to a practice of "blanket-zoning" new neighbourhoods as R1A and introducing other zoning

designations compatible with the concept plan over time as the neighbourhood builds out.

- Blanket-zoning is no longer used due to the diversity of land uses present in our new neighbourhoods.
- Expressions of confusion and frustration with the R1A zoning designation, suggesting that it would be developed as single family housing; feelings of being misled.
- The proposed rezoning is to implement the “Mixed Use” land designation identified on the Stonebridge Concept Plan.

To date, no written comments have been received by our office.

Next Steps

ACTION	ANTICIPATED TIMING
Planning and Development prepares and presents to Municipal Planning Commission (MPC). MPC reviews proposal and recommends approval or denial to City Council.	September 29, 2015
Public Notice - Community Consultant, Ward Councillor, as well as all participants that attended the public information meeting will be provided with direct notice of the Public Hearing, as well as all residents who were notified previously. A notification poster sign will be placed on site. Advertisements prepared and placed in <u>The StarPhoenix</u> , City Page (as per the City’s Public Notice Policy).	October 5 to 10, 2015
Public Hearing – Public Hearing conducted by City Council, with opportunity provided to interested persons or groups to present. Proposal considered together with the reports of the Planning and Development Division, Municipal Planning Commission, and any written or verbal submissions received by City Council.	October 26, 2015
Council Decision - may approve or deny proposal.	October 26, 2015

Prepared by: Brent McAdam, Planner, Planning and Development
August 11, 2015

October 14, 2015

City Clerk

Dear City Clerk:

**Re: Municipal Planning Commission Report for Public Hearing
Proposed Rezoning by Agreement - From R1A to B1B by Agreement and
M2 by Agreement - 3200 Preston Avenue South [File No. CK 4351-015-016]**

The Municipal Planning Commission, at its meeting held on October 9, 2015 considered a report of the General Manager, Community Services Department, dated September 29, 2015, on the above application and supports the following recommendation of the Community Services Department:

That a copy of this report be submitted to City Council recommending that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone 3200 Preston Avenue South, as outlined in this report, be approved.

The Commission respectfully requests that the above report be considered by City Council at the time of the public hearing with respect to the above proposed application.

Yours truly,



Holly Thompson, Committee Assistant
Municipal Planning Commission

HT:sj

**ZONING NOTICE
STONEBRIDGE NEIGHBOURHOOD**

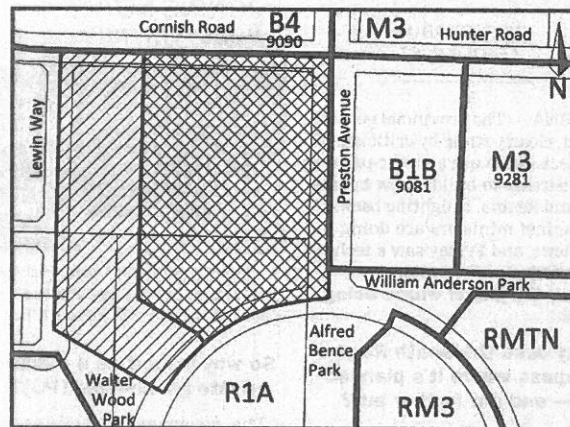
PROPOSED ZONING BYLAW AMENDMENT – BYLAW NO. 9315

Saskatoon City Council will consider amendments to the City's Zoning Bylaw (No. 8770) regarding 3200 Preston Avenue South in Stonebridge, submitted by Pinnacle Developments Inc and Innovative Residential Inc.

By way of Bylaw No. 9315, The Zoning Amendment Bylaw, 2015 (No. 27), the following zoning designations are proposed to be applied to the subject property, consistent with the approved Stonebridge Neighbourhood Concept Plan:

- B1B – Neighbourhood Commercial – Mixed Use District, subject to a Rezoning Agreement; and
- M2 – Community Institutional Service District, subject to a Rezoning Agreement.

LEGAL DESCRIPTION – Plan of Proposed Subdivision of Parcel A, Plan No. 62S15086; Parcels A and B, Plan No. 102097009; Parcel C, Plan No. 66S07349; Parcel XX, Plan No. 102191712; and Part of NE ¼ Sec 10 – Twp 36 – Rge 5 – W3M



PROPOSED ZONING AMENDMENT

- From R1A to M2 by Agreement
- From R1A to B1B by Agreement

File No. R218-2015

REASON FOR THE AMENDMENT – The Stonebridge Neighbourhood Concept Plan, approved in 2005, identified the subject land for mixed-use development. This zoning application will facilitate the development of a mixed-use site incorporating residential, commercial, and institutional uses, consistent with the Concept Plan.

The agreements will provide regulations to address the appropriate uses, site features and scale of development for this neighbourhood mixed use site.

INFORMATION – Questions regarding the proposed amendment or requests to view the proposed amending Bylaw, the City of Saskatoon Zoning Bylaw and Zoning Map may be directed to the following without charge:
Community Services Department, Planning and Development
Phone: 306-975-7621 (Paula Kotasek-Toth)

PUBLIC HEARING – City Council will hear all submissions on the proposed amendment, and all persons who are present at the City Council meeting and wish to speak on **Monday, October 26, 2015 at 6:00 p.m. in City Council Chamber, City Hall, Saskatoon, Saskatchewan.**

All written submissions for City Council's consideration must be forwarded to:
His Worship the Mayor and Members of City Council
c/o City Clerk's Office, City Hall
222 Third Avenue North, Saskatoon SK S7K 0J5

All submissions received by the City Clerk by **10:00 a.m. on Monday, October 26, 2015** will be forwarded to City Council. City Council will also hear all persons who are present and wish to speak to the proposed Bylaw.

BYLAW NO. 9316

The Zoning Amendment Bylaw, 2015 (No. 28)

The Council of The City of Saskatoon enacts:

Short Title

1. This Bylaw may be cited as *The Zoning Amendment Bylaw, 2015 (No. 28)*.


Purpose

2. The purpose of this Bylaw is to authorize the rezoning agreement which is annexed hereto as Appendix "B".

Zoning Bylaw Amended

3. Zoning Bylaw No. 8770 is amended in the manner set forth in this Bylaw.

B2 District to B1B District

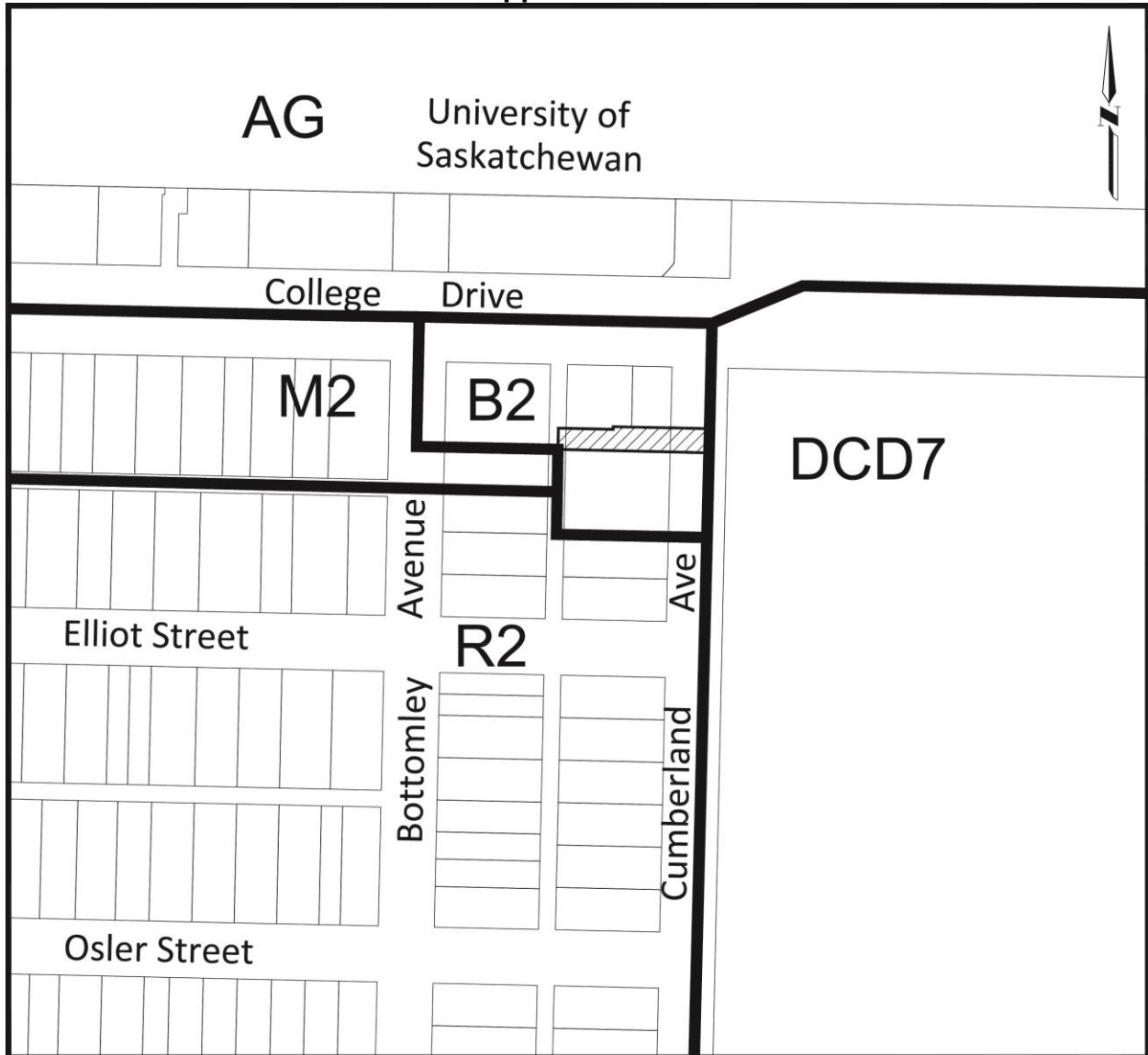
4. The Zoning Map, which forms part of Bylaw No. 7800 is amended by rezoning the lands described in this Section and shown as  on Appendix "A" to this Bylaw from a B2 District to a B1B District subject to the provisions of the Agreement annexed as Appendix "B" to this Bylaw:

- (a) Civic Address: 418 Cumberland Avenue North
Surface Parcel No. 131617414
Legal Land Description: Lot C, Plan G798, Ext 0
As described on certificate of Title 01SA25255; and
- (b) Surface Parcel No. 131617425
Legal Land Description: Lot D, Plan G798, Ext 0
As described on certificate of Title 01SA25255.

Execution of Agreement Authorized

5. The Mayor and Clerk are authorized to execute the Agreement annexed as Appendix "B" to this Bylaw.

Appendix "A"



ZONING AMENDMENT



From B2 to B1B by Agreement

Rezoning Agreement

This Agreement made effective this ____ day of _____, 2015.

Between:

The City of Saskatoon, a municipal corporation pursuant to *The Cities Act*, S.S. 2002 Chapter C-11.1 (hereinafter referred to as the “City”)

- and -

Slade Properties Inc., a body corporate incorporated under the laws of the Province of Saskatchewan with Offices in the City of Saskatoon in the Province of Saskatchewan (hereinafter referred to as the “Owner”)

Whereas:

1. The Owner is the registered owner of the land described as follows:
 - (a) Civic Address: 418 Cumberland Avenue North
Surface Parcel No. 131617414
Legal Land Description: Lot C, Plan G798 Ext 0
As described on Certificate of Title 01SA25255; and
 - (b) Surface Parcel No. 131617425
Legal Land Description: Lot D, Plan G798 Ext 0
As described on Certificate of Title 01SA25255;

(hereinafter referred to as the “Land”);
2. The Owner has applied to the City for approval to rezone the Land from a B2 District to a B1B District to allow development of the proposal specified in the Agreement;
3. The City has an approved Official Community Plan which, pursuant to Section 69 of *The Planning and Development Act, 2007*, contains guidelines respecting the entering into of agreements for the purpose of accommodating requests for the rezoning of land; and
4. The City has agreed, pursuant to the provisions of Section 69 of *The Planning and Development Act, 2007*, to rezone the Land from a B2 District to a B1B District, subject to this Agreement.

Now therefore this Agreement witnesseth that the Parties hereto covenant and agree as follows:

Land to be Used in Accordance with Agreement

1. The Owner agrees that, upon the Land being rezoned from a B2 District to a B1B District, none of the Land shall be developed or used except in accordance with the terms and conditions set out in this Agreement.

Use of Land

2. The Owner agrees that the use of the Land being rezoned will be restricted to the construction of a mixed-use building consisting of a main floor restaurant and multi-unit dwellings on the upper floors with a minimum of five dwelling units and a maximum of six dwelling units. The building and the site must be constructed and developed in substantial compliance with the site plan attached as Schedule “A” to this Agreement.

Development Standards

3. The development standards applicable to the Land shall be those applicable to a B1B District except as follows:
 - (a) Building Height: maximum of 12.25m.
 - (b) Front Yard Setback: minimum of 6.0m.

Parking

4. A minimum of two parking spaces shall be provided. Parking shall not be permitted in the front yard.

Outdoor Plaza

5. The required front yard shall be developed as an outdoor plaza including, but not limited to, soft and hard landscaping elements such as paving stones and plantings, seating areas and bicycle parking.

Street Interface

6. The north building elevation shall be developed in such a manner as to provide visual interest from the street through the use of design finishes that may include different building materials, colours and textures.

Signage

7. Signs shall be permitted on the Land in accordance with the regulations applicable to Signage Group No. 2 contained in The Sign Regulations, being Appendix “A” to Zoning Bylaw No. 7800 except as follows:
 - (a) One wall sign shall be permitted on both the primary and secondary building face.

Garbage Pickup Area

8. A space to be used exclusively for garbage storage and pickup having minimum dimensions of 2.1 m by 6.7 shall be provided.

Application of Zoning Bylaw

9. The Owner covenants and agrees that, except to the extent otherwise specified in this Agreement, the provisions of The City of Saskatoon Zoning Bylaw No. 8770 as amended from time to time shall apply.

Compliance with Agreement

10. The Owner covenants and agrees not to develop or use the Land unless such development, use and construction complies with the provisions of this Agreement.

Dispositions Subject to Agreement

11. The Owner covenants and agrees that any sale, lease or other disposition or encumbrance of the Parcel or part thereof shall be made subject to the provisions of this Agreement.

Definitions

12. Any word or phrase used in this Agreement which is defined in Zoning Bylaw No. 8770 shall have the meaning ascribed to it in that Bylaw.

Departures and Waivers

13. No departure or waiver of the terms of this Agreement shall be deemed to authorize any prior or subsequent departure or waiver, and the City shall not be obliged to continue any departure or waiver or permit subsequent departure or waiver.

Severability

14. If any covenant or provision of this Agreement is deemed to be void or unenforceable in whole or in part, it shall not be deemed to affect or impair the validity of any other covenant or provision of this Agreement.

Governing Law

15. This Agreement shall be governed and interpreted in accordance with the laws of the Province of Saskatchewan.

Effective Date of Rezoning

16. It is understood by the Owner that the Land shall not be effectively rezoned from a B2 District to a B1B District until:
 - (a) the Council of The City of Saskatoon has passed a Bylaw to that effect; and
 - (b) this Agreement has been registered by the City, by way of Interest Registration, against the Title to the Land.

Use Contrary to Agreement

17. (1) The Council of The City of Saskatoon may declare this Agreement void where any of the Land or buildings thereon is developed or used in a manner which is contrary to the provisions of this Agreement, and upon the Agreement being declared void, the Land shall revert to the district to which it was subject to before rezoning.
- (2) If this Agreement is declared void by the Council of The City of Saskatoon, the City shall not, by reason thereof, be liable to the Owner or to any other person for any compensation, reimbursement or damages on account of loss or profit, or on account of expenditures, or on any other account whatsoever in connection with the Land.

Registration of Interest

- 18. (1) The Parties hereto acknowledge that this Agreement is made pursuant to Section 69 of *The Planning and Development Act, 2007* and the Owner agrees that this Agreement shall be registered by way of an Interest Registration against the Title to the Land. As provided in Section 236 of *The Planning and Development Act, 2007*, Section 63 of *The Land Titles Act, 2000* does not apply to the Interest registered in respect of this Agreement.
- (2) This Agreement shall run with the Parcel pursuant to Section 69 of *The Planning and Development Act, 2007*, and shall be bind the Owner, its successors and assigns.

Enurement

- 19. This Agreement shall enure to the benefit of and be binding upon the Parties hereto and their respective heirs, executors, administrators, successors and assigns.

The City of Saskatoon

Mayor

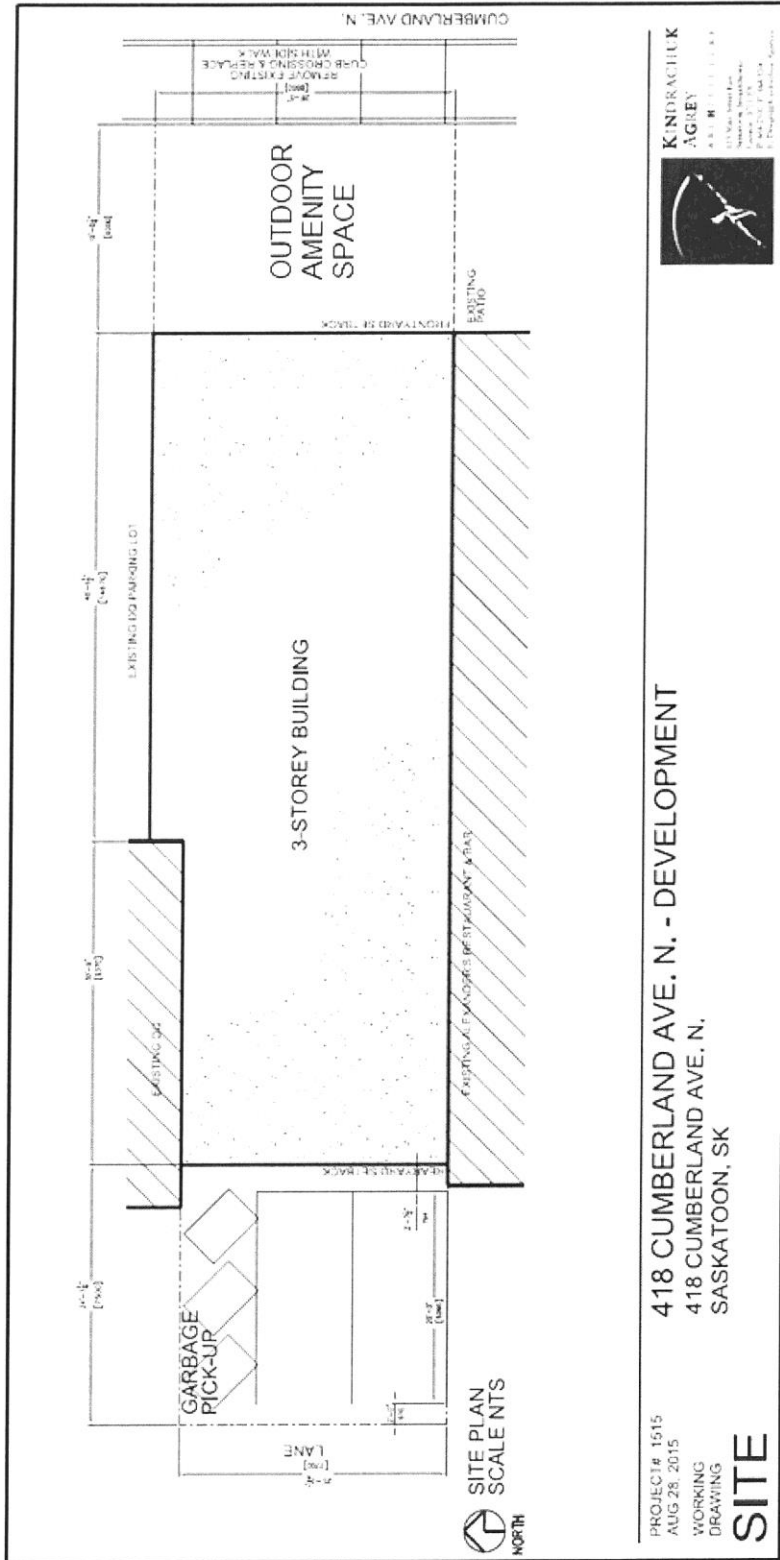
c/s

City Clerk

Slade Properties Inc.

c/s

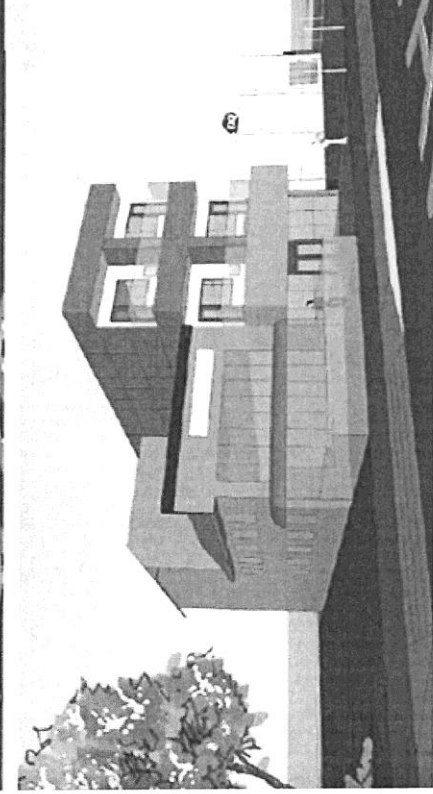
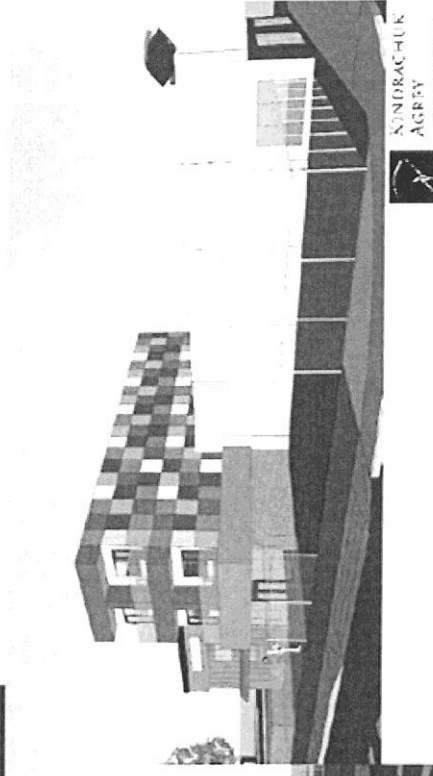
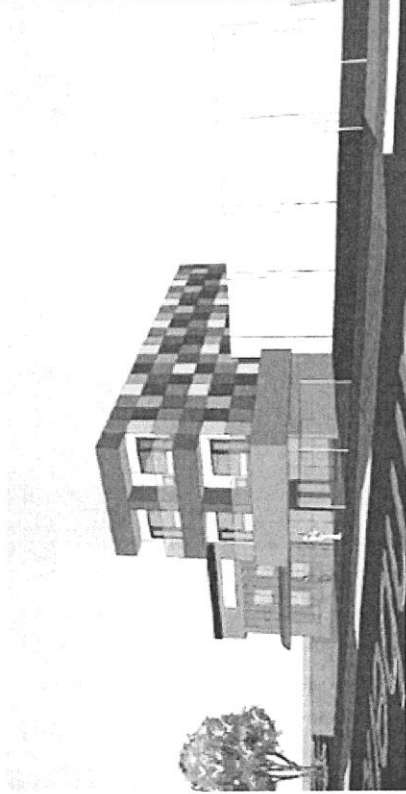
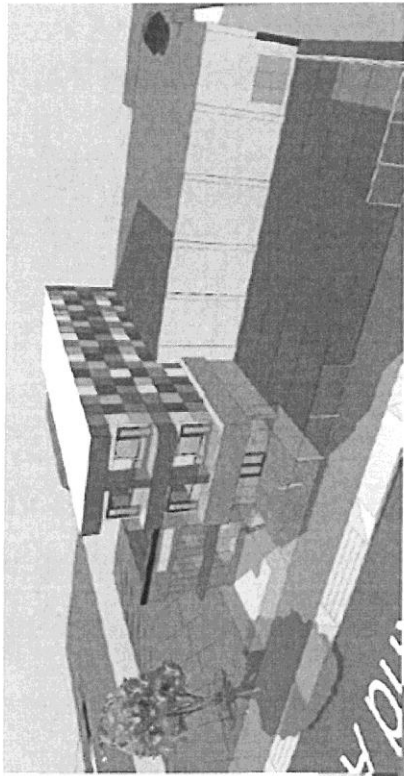
Schedule "A"



**KINDRACHUK
 AGREY**
 ARCHITECTS
 111 Main Street East
 Saskatoon, Saskatchewan
 S7N 1A1
 Tel: (306) 975-1111
 Fax: (306) 975-1112

418 CUMBERLAND AVE. N. - DEVELOPMENT
 418 CUMBERLAND AVE. N.
 SASKATOON, SK

PROJECT # 1515
 AUG 28, 2015
 WORKING
 DRAWING
SITE



KONDRACHUK
AGREY
ARCHITECTS

Proposed Rezoning by Agreement – From B2 to B1B by Agreement – 418 Cumberland Avenue North – Varsity View

Recommendation

That a copy of this report be submitted to City Council recommending that at the time of the public hearing, City Council consider the Administration's recommendation that 418 Cumberland Avenue North be rezoned from B2 – District Commercial to B1B – Neighbourhood Commercial Mixed Use District, subject to a Rezoning Agreement, be approved.

Topic and Purpose

An application from Slade Properties Inc. has been submitted requesting that 418 Cumberland Avenue North be rezoned from B2 – District Commercial to B1B - Neighbourhood Commercial Mixed Use District, subject to a Rezoning Agreement. The rezoning will accommodate the development of a mixed-use building.

Report Highlights

1. The subject property is proposed to be redeveloped with a three-storey mixed-use building consisting of a main floor restaurant, and the second and third floors containing three residential units each.
2. A Rezoning Agreement will address land use, building form, and the development of public amenity space on site.
3. The development proposal aligns with relevant policies and planning initiatives, such as the Varsity View Local Area Plan (LAP), Growth Plan to Half a Million (Growth Plan), and College Quarter Concept Plan.

Strategic Goal

This report supports the Strategic Goal of Sustainable Growth. The proposed rezoning will permit the construction of a mixed-use infill project.

Background

The subject property, 418 Cumberland Avenue North, is zoned B2 – District Commercial, and is located near the intersection of Cumberland Avenue and College Drive, immediately south of the University of Saskatchewan (U of S) (see Attachment 1).

The site is currently developed with a two-storey building containing a restaurant on the main floor and one dwelling unit on the second floor. The applicant has indicated that the building is at the end of its serviceable life.

Report

Proposed Redevelopment

Slade Properties Inc. is proposing to demolish the existing building at 418 Cumberland

Proposed Rezoning by Agreement – From B2 to B1B by Agreement – 418 Cumberland Avenue North – Varsity View

Avenue North and develop a three-storey building with a restaurant on the main floor, and a total of six dwelling units on the second and third floors.

The main floor restaurant will have a gross floor area of approximately 170 m² (1,830 ft²). The dwelling units will consist of bachelor suites ranging in area from approximately 40 m² (425 ft²) to 55 m² (590 ft²). The front yard is proposed to be developed as outdoor public amenity space, with seating, landscaping, and bicycle parking, intended to serve restaurant patrons and to improve the aesthetics and quality of the pedestrian realm in the area. See Attachment 2 for a site plan of the proposal, and Attachment 3 for perspectives of the building.

Parking

Two parking spaces are proposed to be provided. Regular parking requirements in the B1B District would require approximately 11 parking spaces in conjunction with this development; 1 space per dwelling unit, and an estimated 5 spaces for the restaurant.

The Administration supports a reduction in required parking for the following reasons:

- The proximity to the U of S is likely to attract tenants that do not own vehicles and restaurant patrons that arrive by alternate means.
- This area is located within the Varsity View Residential Parking Permit Zone. As per Residential Parking Program Bylaw, 1999, No. 7862, residents of multiple-unit dwellings with five or more units are not eligible to receive a parking permit. As a consequence, prospective tenants will be compelled to either not have a vehicle or make alternate parking arrangements. Otherwise, tenants will be subject to on-street parking restrictions and monetary fines.
- Alternate parking arrangements are available in the area, such as the U of S Stadium Parkade, approximately 150 metres directly to the east. This parkade is currently under-utilized.
- The area is well served by transit routes, and is adjacent to a route being considered for improved service.
- The removal of the existing driveway on Cumberland Avenue will return a parking space to the street.
- Bike parking and improvements to the pedestrian realm on-site will encourage and support active transportation.

The development is in close proximity to the U of S, and the area experiences high pedestrian foot traffic. Phase I of the City Centre Plan measured pedestrian movement in this area in 2010. In summer, a daily average of 4,840 pedestrians pass through the area, comparable with downtown locales such as 1st Avenue between 21st and 22nd Streets (4,944), and 2nd Avenue between 22nd and 23rd Streets (4,910). Pedestrian traffic is higher in winter, at 7,184, when the U of S is in session, exceeding all other count areas included in the study, except for 21st Street between 1st and 2nd Avenues (7,988).

Rezoning by Agreement

A rezoning from B2 – District Commercial to B1B – Neighbourhood Commercial Mixed Use District, subject to a Rezoning Agreement, is proposed to accommodate the

Proposed Rezoning by Agreement – From B2 to B1B by Agreement – 418 Cumberland Avenue North – Varsity View

development as proposed. Terms of the Rezoning Agreement will address land use, building height, front yard setback, parking, development of the outdoor public amenity space, and appearance of the façade. See Attachment 4 for a complete list of the proposed terms.

Varsity View LAP

The Varsity View LAP was approved by City Council in 2014, and identifies several land use goals for the neighbourhood. This proposal is felt to align with the spirit and intent of these goals, as discussed in Attachment 5.

Other Planning Initiatives

This proposal aligns with other relevant planning initiatives. These initiatives contribute to a long-term vision for Cumberland Avenue, in which it transitions to becoming a higher density, mixed-use, and pedestrian-oriented corridor.

Growth Plan: A key component of the Growth Plan is the development of a BRT system. College Drive has been identified as a prime candidate for a BRT route, with preliminary plans showing a stop at College Drive and Cumberland Avenue, approximately 25 metres north of this site. Along with the BRT system, increased density and mixed uses are intended along major corridors to support the viability of public transit. Both College Drive and Cumberland Avenue have been identified as priority corridors for such growth.

College Quarter Concept Plan: The College Quarter Concept Plan identifies a vision for the long-term redevelopment of U of S lands directly to the east of this site. The area directly opposite is intended as a “mixed-use village” concept that combines commercial, residential, and institutional uses. Mixed-use, higher density development on the west side of Cumberland Avenue will serve as an appropriate interface with development on College Quarter, and increase connectivity to the Varsity View neighbourhood.

Comments from Other Divisions

No concerns were received through the administrative referral process that precludes this application from proceeding to the Public Hearing. Please refer to Attachment 6 for complete comments.

Options to the Recommendation

City Council could choose to deny this application. This decision would maintain the current B2 zoning and not permit this proposal to be constructed as proposed.

Public and/or Stakeholder Involvement

A Public Information Meeting regarding this proposal was held on June 18, 2015, at Brunskill School, with ten members of the general public and the Ward Councillor in attendance. A full summary of the comments, questions, and discussion at the meeting is included in Attachment 7.

To date, our office has received one letter of objection from an adjacent commercial property owner (see Attachment 8). The primary concern expressed relates to existing

Proposed Rezoning by Agreement – From B2 to B1B by Agreement – 418 Cumberland Avenue North – Varsity View

parking pressures in the area, and the number of parking spaces proposed in conjunction with this development.

In response, and as discussed earlier in this report, it is proposed that a provision be included in the Rezoning Agreement that requires a minimum number of residential units for this project. The purpose of this is to make building residents ineligible to receive an on-street parking permit, which would further exacerbate parking issues in the area. Other reasons why the Administration supports reducing parking requirements at this particular location are discussed previously in the report.

Other Considerations/Implications

There are no policy, financial, environmental, privacy, or CPTED implications or considerations. A communication plan is not required at this time.

Due Date for Follow-up and/or Project Completion

No follow-up is required.

Public Notice

Public notice is required for consideration of this matter, pursuant to Section 11 (a) of Public Notice Policy No. C01-021. Once this application has been considered by the Municipal Planning Commission, it will be advertised in accordance with Public Notice Policy No. C01-021, and a date for a Public Hearing will be set. The Planning and Development Division will notify all property owners within 75 metres of the subject site of the Public Hearing date, by letter. A notice will be placed in The StarPhoenix two weeks prior. Notice boards will be placed on the site.

Attachments

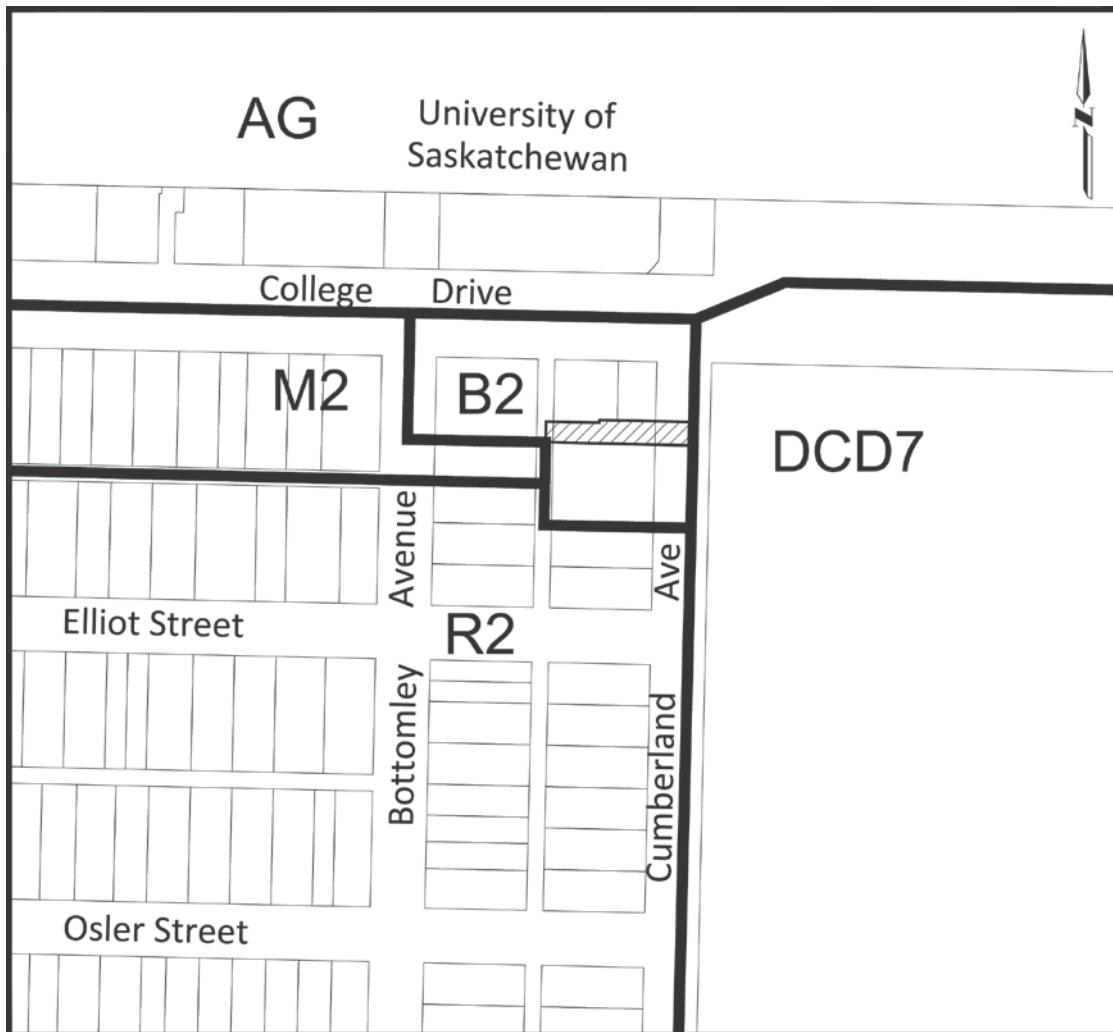
1. Location Map
2. Proposed Site Plan
3. Building Perspectives
4. Provisions of Rezoning Agreement
5. Alignment with Varsity View Local Area Plan
6. Comments from Other Divisions
7. Community Engagement Summary
8. Letter from Adjacent Property Owner

Report Approval


Written by: Brent McAdam, Planner, Planning and Development
Reviewed by: Alan Wallace, Director of Planning and Development
Approved by: Randy Grauer, General Manager, Community Services Department

S/Reports/DS/2015/MPS – Proposed Rezoning by Agreement – From B2 to B1B by Agreement – 418 Cumberland Avenue North – Varsity View/lc

Location Map



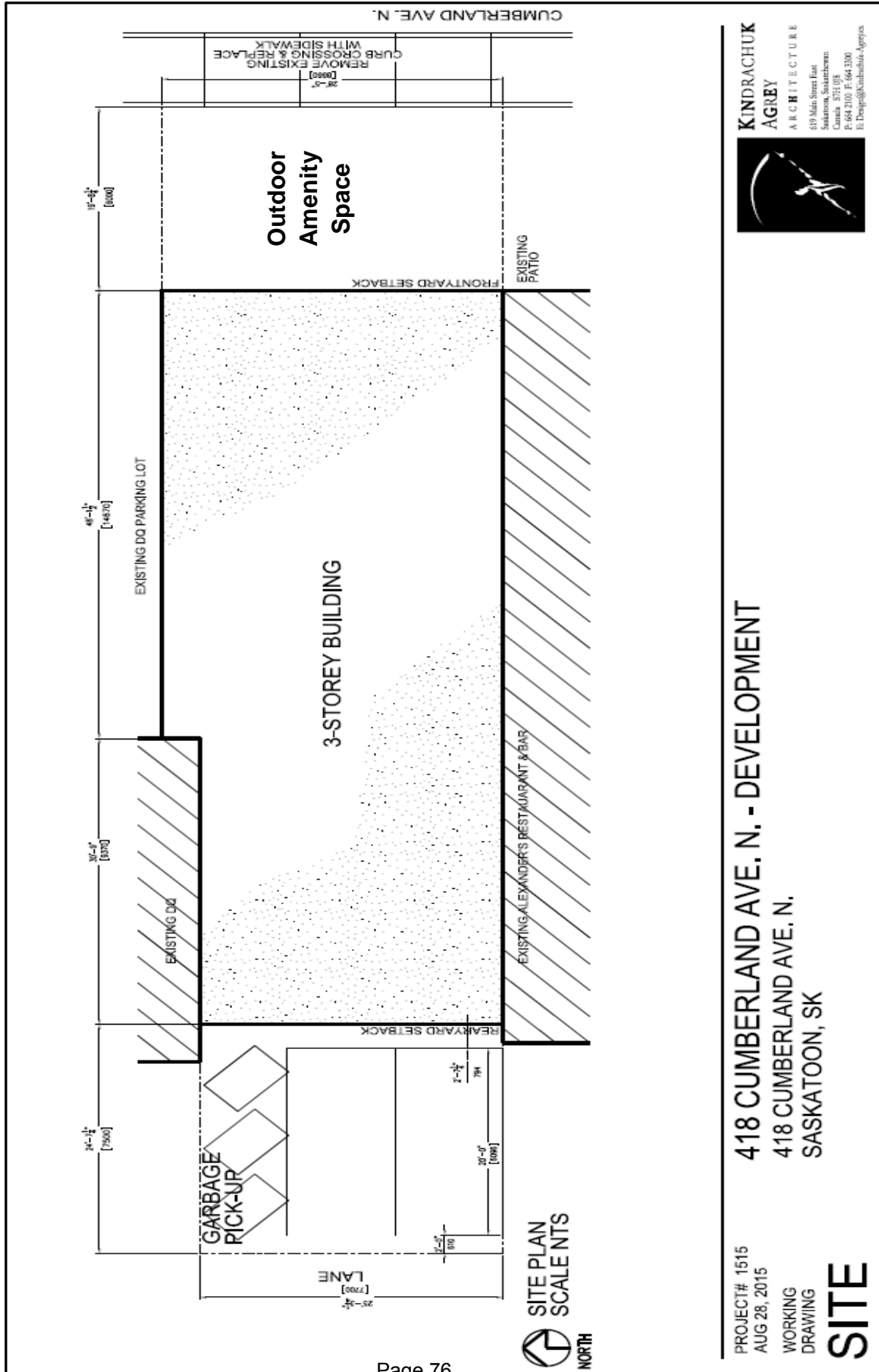
ZONING AMENDMENT

 From B2 to B1B by Agreement

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Proposed Site Plan



**KINDRACHUK
AGREY**
ARCHITECTURE
419 Main Street East
Saskatoon, Saskatchewan
Canada S7H 0H8
P: 664 2100 F: 664 3300
E: Design@Kindrachuk-Agreys



418 CUMBERLAND AVE. N. - DEVELOPMENT
418 CUMBERLAND AVE. N.
SASKATOON, SK

PROJECT# 1515
AUG 28, 2015
WORKING
DRAWING

SITE

Building Perspectives



KINDRACHUK
AGREY
ARCHITECTURE



Provisions of Rezoning Agreement

- **Use of Land:** A restaurant and a multiple-unit dwelling with a minimum of five dwelling units and a maximum of six.
- **Building Height:** A maximum of 12.25 metres.
- **Front Yard Setback:** A minimum of 6.0 metres.
- **Parking:** A minimum of two parking spaces shall be provided. Parking shall not be permitted in the front yard.
- **Outdoor Plaza:** The required front yard shall be developed as an outdoor plaza including, but not limited to, soft and hard landscaping elements such as paving stones and plantings, seating areas, and bicycle parking.
- **North Elevation:** The north building elevation shall be developed in such a manner as to provide visual interest from the street through the use of design finishes that may include different building materials, colours, and textures.
- **Signage:** In addition to one wall sign being permitted on the primary building face, one wall sign on one secondary building face shall be permitted as well. All other provisions of the Signage Group 2 of the Sign Regulations shall otherwise apply.
- **Garbage Space:** One garbage space with minimum dimensions of 2.1 m x 6.7 m shall be required.
- All other provisions of the B1B District shall apply.

Alignment with Varsity View Local Area Plan

Land Use Goals	
Local Area Plan Statement	Planning and Development Comment
To encourage sensitive land redevelopment along major and upcoming traffic corridors east/west and north/south	The proposed rezoning supports the redevelopment of a property along a major traffic corridor.
That redevelopment should enhance neighbourhood amenities, greenspaces, and public areas. Varsity View welcomes redevelopment in appropriate locations with the expectation of net gain for residents.	The proposal provides an enhancement to the neighbourhood through the provision of outdoor amenity space which supports an improved public realm.
That Varsity View recognizes the need to increase density of neighbourhoods in Saskatoon through thoughtful, considered development.	The total increase in density resulting from this project is relatively minor.
That development occurring along the borders of Varsity View promotes pedestrian connectivity and urban design integration with the University of Saskatchewan and adjacent neighbourhoods.	This project is located in close proximity to the U of S campus. Outdoor amenity space provided supports an improved public realm and will enhance the attractiveness of walking in the area.
Considerations for Cumberland Avenue	
Development along Cumberland Avenue is expected to transition over time as College Quarter builds out. The following elements are identified for consideration with any new development on Cumberland Avenue directly opposite the College Quarter development.	
Local Area Plan Statement	Planning and Development Comment
Higher intensity uses are scaled down from College Drive to 14 th Street in three zones that parallel uses in the College Quarter Master Plan: <ul style="list-style-type: none"> • College Drive to Elliott Street – Mixed Use 	This mixed-use proposal is opposite the proposed “mixed-use village” of the College Quarter, which will incorporate commercial, residential, and institutional uses.
Two-storey buildings are the most desirable; however, density may be increased with bonuses of public amenity space, underground parking, etc.	A three-storey building is proposed; however, public amenity space is being provided.
Mixed-use developments are desirable with small scale commercial at-grade and residential above.	The proposal incorporates a small restaurant at-grade with two floors of residential units above.
Amenities provided by any commercial uses are aimed at the neighbourhood level services to complete the neighbourhood, not city-wide attractions, such as nightclubs or bars.	Only a restaurant is permitted as a commercial use on the main floor as per the Rezoning Agreement.
Front yard setbacks may be accommodated for those creating patio, outdoor spaces, and/or common public gathering places only.	A front yard setback will be permitted to accommodate patio/amenity space.
No front yard setbacks for parking.	The Rezoning Agreement will prohibit parking in the front yard.

Comments From Other Divisions

Transportation and Utilities Department

The proposed rezoning, as noted in the report, is acceptable to the Transportation and Utilities Department, with the following comments:

1. The closest fire hydrant is at the intersection of Cumberland Avenue North and College Drive. The available fire flow at this hydrant is 167 L/S, while the required fire flow for B1B and for this proposed development is 220 L/S. This hydrant is approximately 25 metres from the proposed development. The developer is required to provide a fire flow assessment report from a qualified engineer for the actual fire flow requirements for the development.

Planning and Development Comment: The Applicant acknowledges and accepts this requirement and will address it in the detailed design stage prior to the issuance of a building permit.

2. The storm water sewers are already full and there is no extra capacity available for a two-year storm. If the developer intends to increase the imperviousness of the lot, they will be required to provide on-site storage.

Planning and Development Comment: The Applicant acknowledges and accepts this requirement and will address it in the detailed design stage prior to the issuance of a building permit.

3. The current sanitary system is able to accommodate the additional sewage flow that would be generating from this proposed development. The dry weather flow sanitary model shows no capacity issues for this development.

COMMUNITY ENGAGEMENT SUMMARY

PUBLIC INFORMATION MEETING FOR PROPOSED REZONING BY AGREEMENT 418 CUMBERLAND AVENUE NORTH

Applicant: Slade Properties Inc.
File: PL 4350 – Z14/15

Project Description

A public information meeting was held regarding a proposed rezoning by agreement of 418 Cumberland Avenue North.

The meeting was held at the Brunskill School Library on June 18, 2015, at 7 p.m.

Community Engagement Strategy

Purpose:

To inform and consult. Residents were provided with an overview of the applicant's proposal, and given the opportunity to ask questions and provide comments. Written comments (email/comment sheets) were accepted following the meeting.

Form of Community Engagement Used:

Public Information Meeting – Residents were provided an opportunity to listen to a presentation by the applicant, participate in a question and answer session, and speak directly with the applicant and/or City of Saskatoon (City) staff following the formal portion of the meeting. City staff were in attendance to provide an overview of the rezoning process and the next steps following the meeting.

Level of Input or Decision Making Required from the Public:

Comments, concerns, and opinions were sought from the public.

Who was Involved:

- Internal stakeholders: The standard referral process was followed, and relevant internal divisions of the City were contacted for comments. Councillor Clark was also contacted.
- External stakeholders: A flyer with details of the meeting was sent to property owners within an approximate 75 metre radius of the subject site in advance of the meeting (a total of 18 notices). The Varsity View Community Association was also provided notice.
- Ten members of the general public attended the meeting, in addition to Councillor Clark.

Summary of Community Engagement Feedback

Following introductory remarks of the rezoning process by City staff, and an overview of the proposed rezoning by the applicant, a question and answer period and general

discussion followed. Concerns raised, questions, and general points of discussion were:

- Concern expressed over parking issues in area being exacerbated by new development, especially with additional residential suites being proposed.
 - Discussion followed regarding Residential Parking Permit Bylaw, 1999, No. 7862, that restricts the issuance of resident parking permits to buildings with four units or less.
 - This proposal has six units, meaning that permits could not be obtained by tenants, and their vehicles would be subject to the parking restrictions in the area.
 - Discussion of past tenants in the existing building; an even split did and did not have their own vehicles.
 - It was noted that alternate arrangements for a parking spot can be made at the nearby Stadium Parkade.
- Adjacent business experiencing issues with other business' customers parking in their lot.
- Further stress on existing commercial properties with respect to parking demand.
- Discussion of how the development ties in with the Varsity View Local Area Plan.
- Discussion regarding the proposed building design:
 - Expression of the desire for a pitched roof with gables versus the flat roof proposed; it was noted that other commercial buildings on that corner, and generally throughout Saskatoon, have flat roofs.
 - Architecture that is consistent with the neighbourhood is desired.
 - What kind of green space and landscaping will be provided?
 - Preference expressed for a two-storey building instead of a three-storey building.
 - Preference expressed for fewer residential units; three instead of six.
 - Some expressed that they liked the proposed building.
 - Ideas for treatment of the north-facing blank wall, which could accommodate a mural or public art project.
- Discussion from the restaurant franchisee at this location who estimates that 90% of their business is from walk-in customers.
- Questions regarding how drainage to the rear lane would be affected.
- Discussion of costs and viability of three-storey construction over two-storey construction.

To date, one letter of objection has been received from an adjacent property owner regarding parking issues in the area.

Next Steps

ACTION	ANTICIPATED TIMING
Planning and Development prepares and presents proposal to Municipal Planning Commission (MPC). MPC reviews proposal and recommends approval or denial to City Council.	September 29, 2015
Public Notice – Attendees of the public meeting will be provided with notice of the Public Hearing, as well as all others who were notified previously. A notification poster will be placed on site. An advertisement is prepared and placed in <u>The StarPhoenix</u> .	October 10 to 16, 2015
Public Hearing – Occurs at City Council, with the opportunity for interested parties to present. Proposal considered together with the reports of Planning and Development, Municipal Planning Commission, and any written or verbal submissions received.	October 26, 2015
Council Decision – may approve or deny proposal.	October 26, 2015

**Prepared by: Brent McAdam, Planner, Planning and Development,
August 12, 2015.**

Letter from Adjacent Property Owner

Alexander's Restaurant
 Nestor Hlynsky
 Harold Swanson
 414 Cumberland Ave. N.
 Saskatoon, SK
 S7H 2Z7
 PH: 306-956-7777
 alexandrsaskatoon@gmail.com

July 15, 2015.

RE: Response to a proposed rezoning development at 418 Cumberland Avenue North in a B2 commercial district on College Drive and Cumberland Avenue North, Saskatoon.

We support and welcome a new development to replace the existing structure, but object to the size of the proposed structure on a very small lot with a strong emphasis on residential units, its consequent parking implications, and impact on existing businesses. The following will reflect on some key characteristics and issues.

The proposal on a 25 foot frontage lot, "is for a 3 storey building, with a main floor restaurant and 6 residential suites on two floors above." The current building has a main floor Subway restaurant with one rental unit on the second floor. The tenant occupying the second floor currently has an on street Varsity View 24/7 parking permit - one of only four parking spaces on the 400 block on Cumberland Ave. N.

In this B2 zoning district there is absolutely no parking on College Drive, nor the east side of Cumberland Ave. N.

The proposed rezoning is to have a much larger seating capacity restaurant with virtually no parking for staff or customers. The proposed addition of 6 suites also has no parking capacity for tenants or visitors. The building's proposed height of 12 metres (3 floors) exceeds the current by-law restriction of 7.5 metres (2 floors), a variance of approximately 35%.

Our central issue with the proposal is that there is no reasonable provision to address the extreme lack of parking capacity for an enlarged restaurant with 6 suites. Should 6 suites be permitted, tenants would not be able to get on street parking permits. If, however, 4 suites are developed then tenants would be able to apply for on street parking permits - essentially all of the parking spaces on the 400 block of Cumberland Ave. N. could be occupied by tenants vehicles rather than customers vehicles for commercial establishments.

Theoretically, should the proposed 6 suites be reduced to 4 suites with more bedrooms

per suite, then tenants, as it is now, would be permitted to receive on street parking permits. That is, there could be re-design changes to the proposal once construction begins or alterations or adaptations at some future date. Six, eight, ten, or twelve tenants could apply for on street parking permits. So too, where would their guests or visitors park? This situation would exacerbate an already very limited parking area. The stress factors for all businesses, Starbucks, Dairy Queen, confectionary, Subway, Alexander's, and other businesses would be further economically impacted.

At Alexander's we provide sufficient parking for staff and customers. Far too often, however, and on a daily basis, customers going to other businesses will use our parking lot. We understand the situation and have a level of tolerance. It is during busier hours that it becomes a concern, for example, customers calling us on their cell phones cancelling a reservation because of a lack of parking stalls or just driving through a full lot and moving on. We constantly observe vehicles parking on our lot and going to other businesses or services - they do this because of the extreme lack of parking capacity in the area and on the street.

We have addressed and enforced parking issues over the years with due diligence, placing restrictive parking signs on our lot, as other businesses have done. Despite many efforts, parking infractions continue. City parking enforcement could well attest to the myriad of parking issues in this area.

Given these issues, we suggest that to avoid current and future stress impacts on businesses in their particular B2 zoning district that no residential tenants be permitted to have on street parking permits issued. This B2 district was intended more so as a commercial zone than for residential units.

There, perhaps, should be an agreement with this rezoning development that restricts the issuance of on street parking permits to residential tenants on the square block surrounding this B2 district. All businesses in this B2 area have significant investments that should not be weakened by the issuance of on street parking permits.

This area is a smaller commercial hub that supports the neighbourhood and the university community.

October 14, 2015

City Clerk

Dear City Clerk:

**Re: Municipal Planning Commission Report for Public Hearing
Proposed Rezoning by Agreement - From B2 to B12 by Agreement
418 Cumberland Avenue North - Varsity View [File No. CK 4351-015-017]**

The Municipal Planning Commission, at its meeting held on October 9, 2015 considered a report of the General Manager, Community Services Department, dated September 29, 2015, on the above application and supports the following recommendation of the Community Services Department:

That a copy of this report be submitted to City Council recommending that at the time of the public hearing, City Council consider the Administration's recommendation that 418 Cumberland Avenue North be rezoned from B2 – District Commercial to B1B - Neighbourhood Commercial Mixed Use District, subject to a Rezoning Agreement, be approved.

The Commission respectfully requests that the above report be considered by City Council at the time of the public hearing with respect to the above proposed application.

Yours truly,



Holly Thompson, Committee Assistant
Municipal Planning Commission

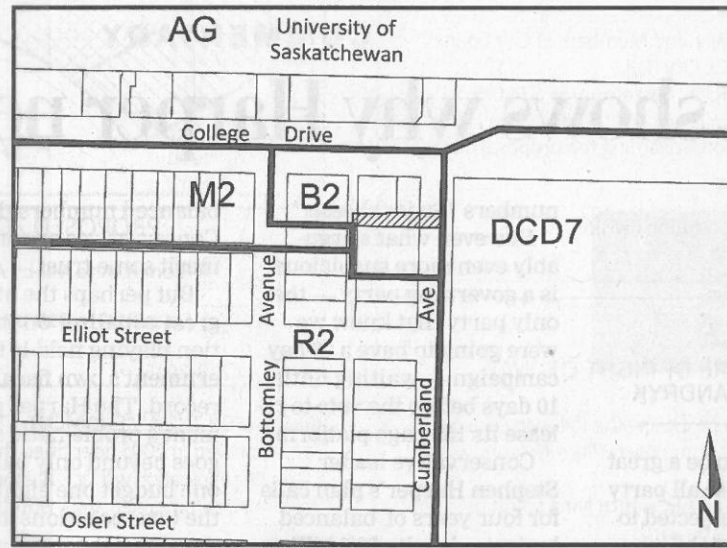
HT:sj

**ZONING NOTICE
VARSITY VIEW NEIGHBOURHOOD**


PROPOSED ZONING BYLAW AMENDMENT – BYLAW NO. 9316

Saskatoon City Council will consider an amendment to the City's Zoning Bylaw (No. 8770), submitted by Slade Properties. By way of Bylaw No. 9316, The Zoning Amendment Bylaw, 2015 (No. 28), 418 Cumberland Avenue North is proposed to be rezoned from B2 – District Commercial District to B1B – Neighbourhood Commercial – Mixed Use District, subject to a Rezoning Agreement.

LEGAL DESCRIPTION – Lot C and D, Plan No. G798



PROPOSED ZONING AMENDMENT

 From B2 to B1B by Agreement

File No. RZ14-2015

REASON FOR THE AMENDMENT – The proposed rezoning will permit the redevelopment of the property as a three-storey mixed-use building with a restaurant on the main floor and a total of six residential units on the upper floors.

The Rezoning Agreement will control the use of land and provide regulations on building height, front yard setback, parking, and development of outdoor public amenity space, specific to this project.

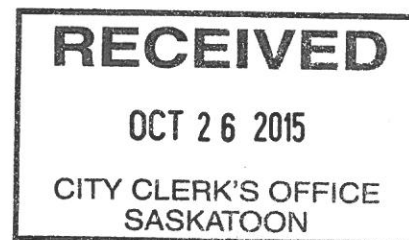
INFORMATION – Questions regarding the proposed amendment or requests to view the proposed amending Bylaw, the City of Saskatoon Zoning Bylaw and Zoning Map may be directed to the following without charge:
Community Services Department, Planning and Development
Phone: 306-975-7621 (Paula Kotasek-Toth)

PUBLIC HEARING – City Council will hear all submissions on the proposed amendment, and all persons who are present at the City Council meeting and wish to speak on **Monday, October 26, 2015 at 6:00 p.m. in City Council Chamber, City Hall, Saskatoon, Saskatchewan.**

All written submissions for City Council's consideration must be forwarded to:
His Worship the Mayor and Members of City Council
c/o City Clerk's Office, City Hall
222 Third Avenue North, Saskatoon SK S7K 0J5

All submissions received by the City Clerk by **10:00 a.m. on Monday, October 26, 2015** will be forwarded to City Council. City Council will also hear all persons who are present and wish to speak to the proposed Bylaw.

From: Jonathan Naylor <jon.naylor@usask.ca>
Sent: Sunday, October 25, 2015 9:06 PM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Sunday, October 25, 2015 - 21:05
Submitted by anonymous user: 108.60.168.207
Submitted values are:

Date: Sunday, October 25, 2015
To: His Worship the Mayor and Members of City Council
First Name: Jonathan
Last Name: Naylor
Address: 1414 14th St E
City: Saskatoon
Province: Saskatchewan
Postal Code: S7H0A8
Email: jon.naylor@usask.ca
Comments:

I write this letter in the interest of clarity.

The communities concerns about the rezoning of 418 Cumberland Avenue North Varsity View (File No. CK 4351-015-017) were discussed at the public information meeting on June 18. The Varsity View Community Association also sent a letter outlining its concern regarding parking and safety to the Municipal Planning Commission.

The proposed rezoning of 418 Cumberland Ave is in direct conflict with the first goal of the Varsity View Local Area Plan: Addressing Neighborhood Parking Concerns (Varsity View Local Area Plan: Executive Summary, page iii). Normally, a development with a restaurant and 6 residential units is required to have 11 parking places for the use of tenants, their guests, employees, and clients. The proposed rezoning has only 2 parking spaces with a potential limited term commitment to 2 additional spaces in the University Parking lot (these are likely to become unavailable as College Quarter builds out). If a reasonable person concludes that the inadequacies of this arrangement are likely to exacerbate parking problems in Varsity View it is open to legal challenge on two grounds:

- It is not consistent with the Official Community Plan and the Local Area Plan (Bylaw No. 8769, Sections 5.2.2, 5.5, 18.1 and 18.2)
- It will injuriously affect neighboring properties (Planning and Development Act, 2007, sections 221, 226)

I look forwards to hearing Council's decision.

Sincerely,
Jonathan Naylor
1414 14th St E, Saskatoon, S7H 0A8

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47150>

BYLAW NO. 9317

The Zoning Amendment Bylaw, 2015 (No. 29)

The Council of The City of Saskatoon enacts:

Short Title

1. This Bylaw may be cited as *The Zoning Amendment Bylaw, 2015 (No. 29)*.

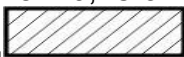
Purpose

2. The purpose of this Bylaw is to amend the Zoning Bylaw to rezone the lands described in the Bylaw from an R1A District to an R1B District and an RM3 District respectively.


Zoning Bylaw Amended

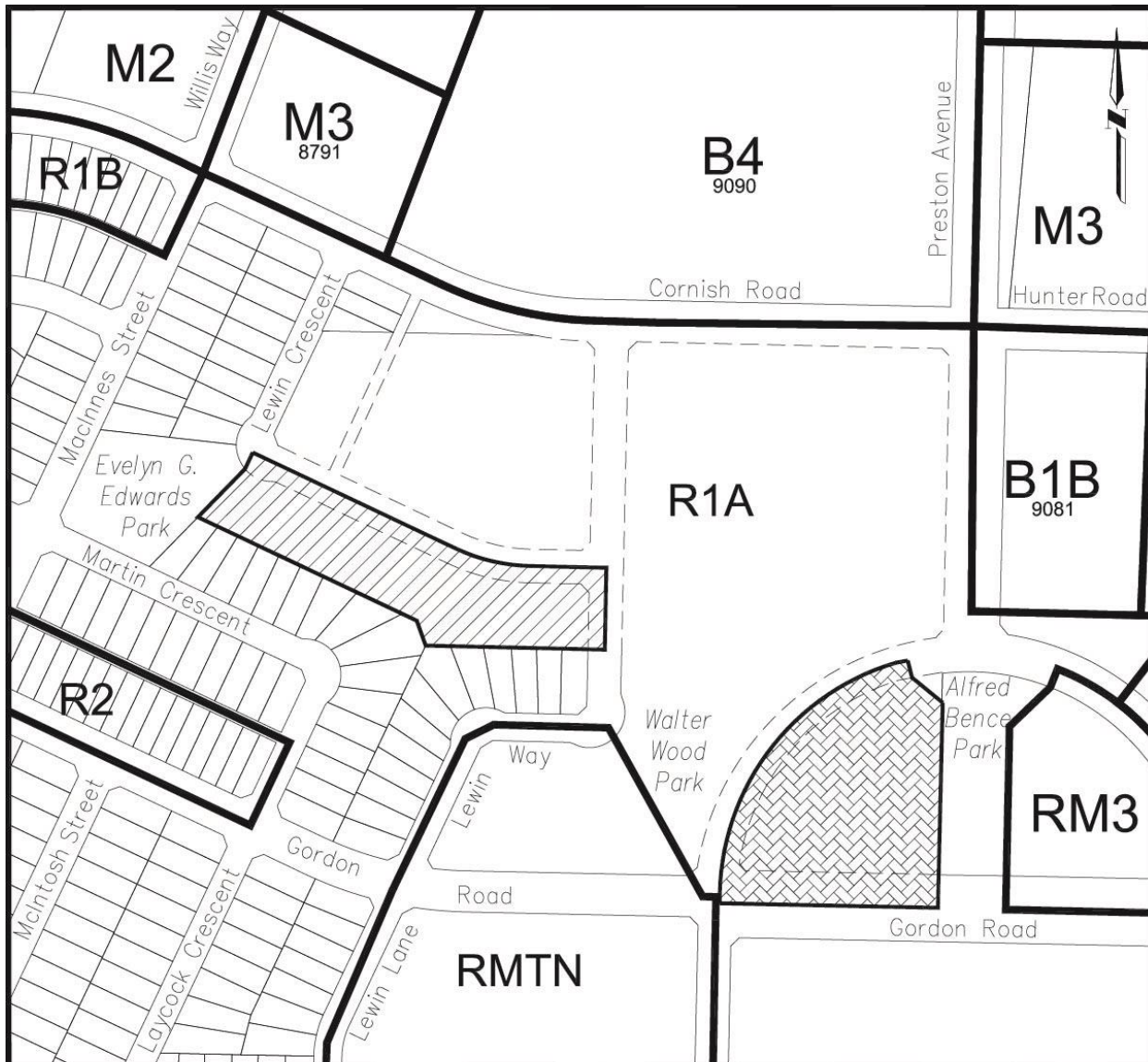
3. Zoning Bylaw No. 8770 is amended in the manner set forth in this Bylaw.

R1A District to R1B District

4. The Zoning Map, which forms part of Bylaw No. 8770, is amended by rezoning the lands described in this Section and shown  on Appendix "A" to this Bylaw from an R1A District to an R1B District:
 - (a) Lots 30 to 47 inclusive of Block 185, as shown on a Plan of Proposed Subdivision of Parcel A, Reg'd Plan No. 62S15086, Parcels A and B, Plan No. 102097009, and Parcel C, Reg'd Plan No. 66S07349, and Parcel XX, Plan No. 102191712 and Part of N.E. ¼ Sec. 10 Twp. 36, Rge. 5, W. 3rd Mer. Preston Avenue, Cornish Road and Lewin Crescent, Saskatoon, Saskatchewan by T.R. Webb, S.L.S. dated August 19, 2015.

R1A District to RM3 District

5. The Zoning Map, which forms part of Bylaw No. 8770, is amended by rezoning the lands described in this Section and shown  on Appendix "A" to this Bylaw from an R1A District to an RM3 District.



ZONING AMENDMENT

 From R1A to R1B

 From R1A to RM3

Proposed Rezoning from R1A to R1B and RM3 – Lewin Crescent and Stonebridge Common – Stonebridge

Recommendation

That at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone land in the Stonebridge neighbourhood, as outlined in this report, be approved.

Topic and Purpose

An application has been submitted by Dream Development requesting to rezone land in the Stonebridge neighbourhood from R1A – One-Unit Residential District (R1A District) to R1B – Small Lot One-Unit Residential District (R1B District) and RM3 – Medium-Density Multiple-Unit Dwelling District (RM3 District) (see Attachment 1). The rezoning will facilitate single-family and medium-density residential development consistent with the Stonebridge Neighbourhood Concept Plan (Concept Plan).

Report Highlights

1. This application is consistent with the approved Concept Plan for the Stonebridge neighbourhood.
2. No issues or concerns were raised through the administrative review process.

Strategic Goal

Under the City of Saskatoon's (City) Strategic Goal of Sustainable Growth, providing a mix of housing types and densities within our neighbourhoods supports the priority to create "complete community" neighbourhoods.

Background

The Concept Plan, originally approved by City Council in 2005, identifies the subject lands for low- and medium-density residential (see Attachment 2).

By legislation, the City is required to advertise all proposed rezonings and hold a public hearing.

Report

Concept Plan

This application proposes to apply zoning designations to the areas identified that are consistent with the Concept Plan.

Zoning Bylaw No. 8770

Development of the subject areas will be required to comply with the requirements of the R1B District and RM3 District, respectively.

Proposed Rezoning from R1A to R1B and RM3 – Lewin Crescent and Stonebridge Common – Stonebridge

The purpose of the R1B District is to provide for small lot residential development in the form of one-unit dwellings, as well as related community uses. This rezoning will accommodate 18 residential units.

The purpose of the RM3 District is to provide for a variety of residential developments in a medium-density form, as well as related community uses.

Comments from Other Divisions

No concerns were identified through the administrative referral process.

Options to the Recommendation

City Council could choose to deny this application. This option is not recommended as it is consistent with the Concept Plan.

Public and/or Stakeholder Involvement

Extensive public consultation was conducted in conjunction with the development of the Concept Plan. As this application is consistent with the Concept Plan, a public information meeting was not held.

Other Considerations/Implications

There are no policy, financial, environmental, privacy, or CPTED implications or considerations. A communication plan is not required at this time.

Due Date for Follow-up and/or Project Completion

No follow-up is required.

Public Notice

Public notice is required for consideration of this matter, pursuant to Section 11 (a) of Public Notice Policy No. C01-021.

Once this application has been considered by the Municipal Planning Commission, it will be advertised in accordance with Public Notice Policy No. C01-021, and a date for a public hearing will be set. The Planning and Development Division will notify all property owners within a 75 metre (246 feet) buffer of the proposed site of the public hearing date, by letter. A notice will be placed in The StarPhoenix two weeks prior to the public hearing.

Attachments

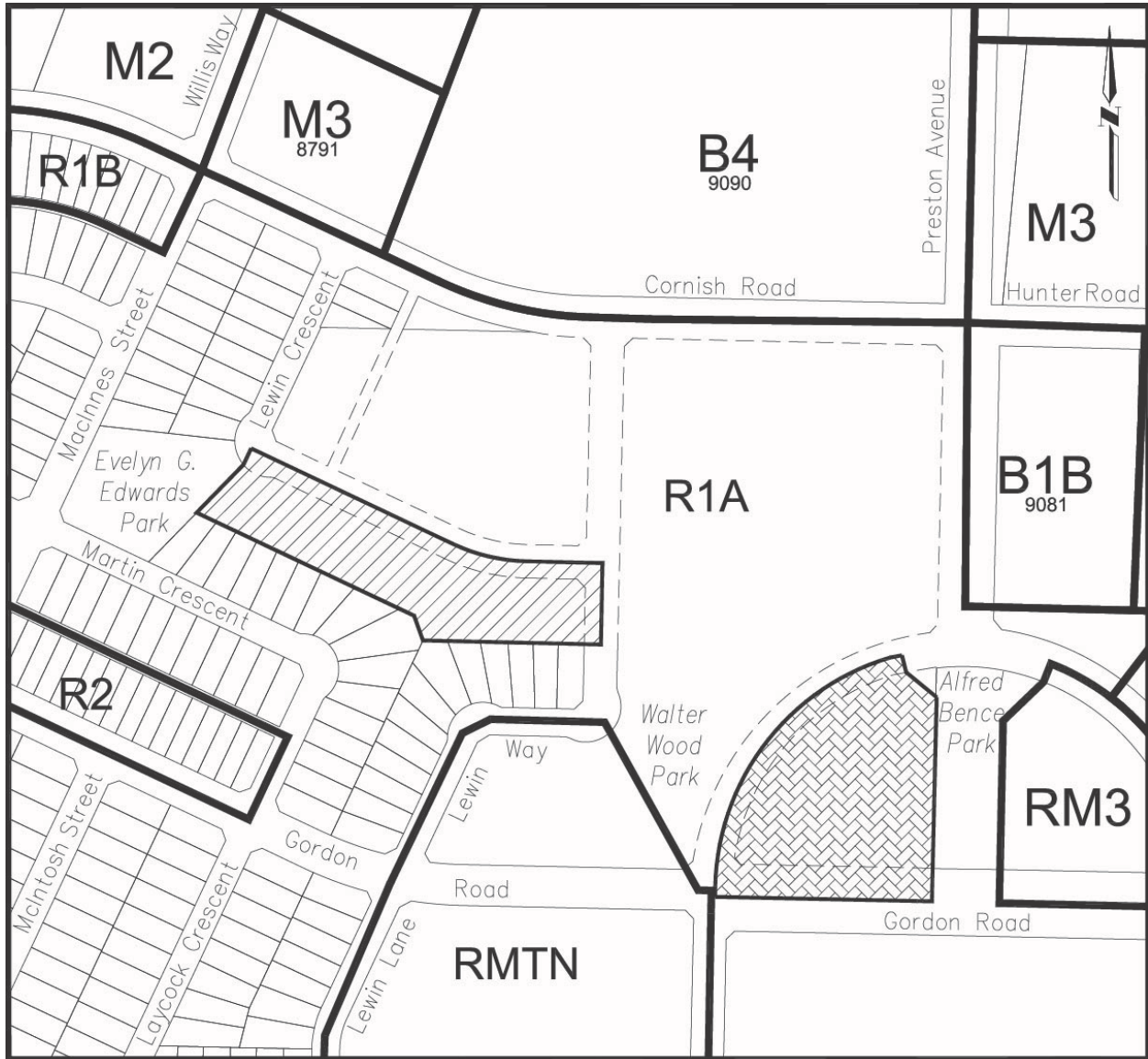
1. Location Map
2. Stonebridge Neighbourhood Concept Plan

Report Approval

Written by: Brent McAdam, Planner, Planning and Development
Reviewed by: Alan Wallace, Director of Planning and Development
Approved by: Randy Grauer, General Manager, Community Services Department

S:\Reports\DS\2015\MPC – Proposed Rezoning from R1A to R1B and RM3 – Lewin Crescent and Stonebridge Common – Stonebridge\kt

Location Map



ZONING AMENDMENT

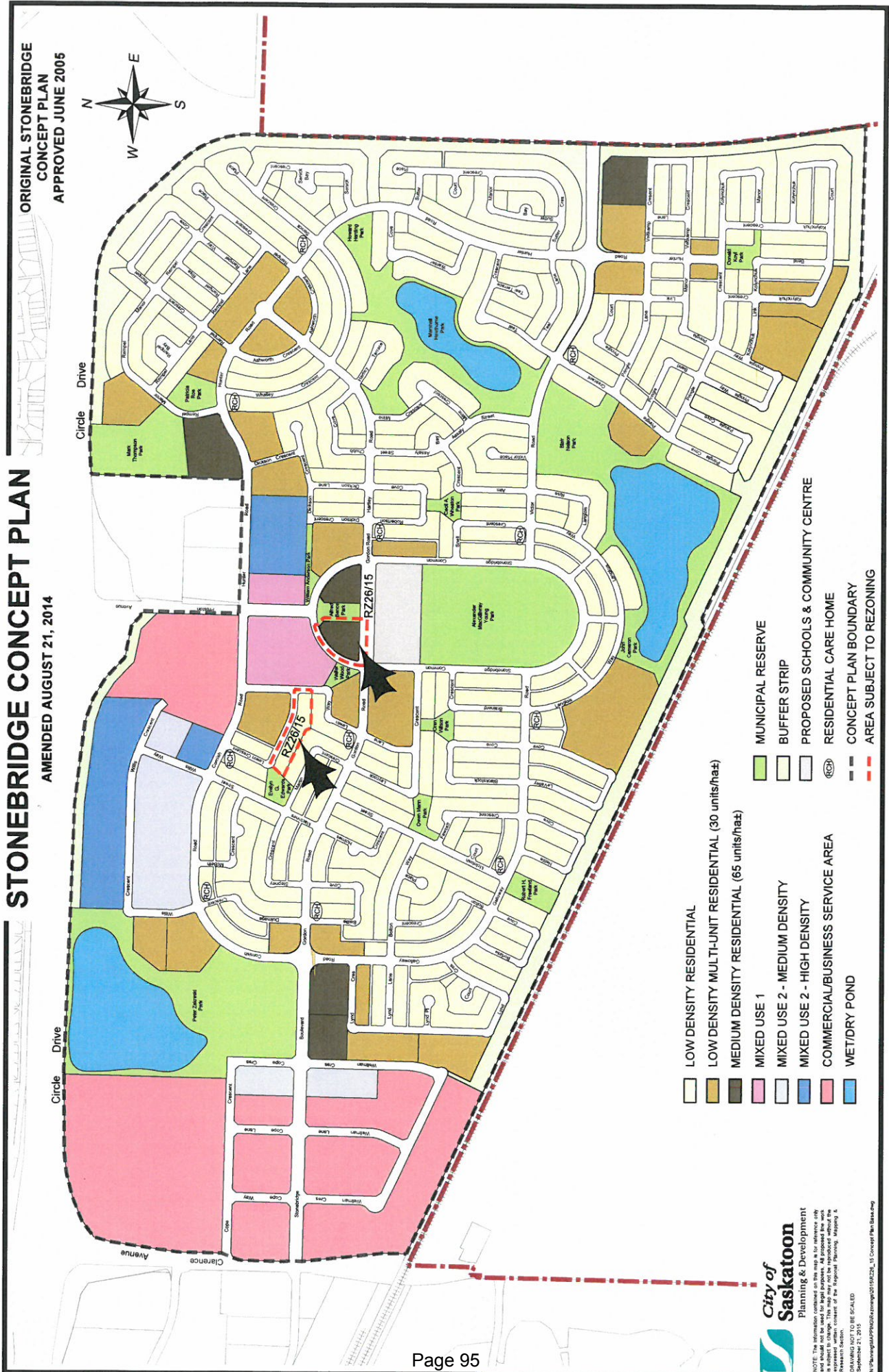
 From R1A to R1B

 From R1A to RM3

STONEBRIDGE CONCEPT PLAN

AMENDED AUGUST 21, 2014

ORIGINAL STONEBRIDGE
CONCEPT PLAN
APPROVED JUNE 2005



- LOW DENSITY RESIDENTIAL
- LOW DENSITY MULTI-UNIT RESIDENTIAL (30 units/ha±)
- MEDIUM DENSITY RESIDENTIAL (65 units/ha±)
- MIXED USE 1
- MIXED USE 2 - MEDIUM DENSITY
- MIXED USE 2 - HIGH DENSITY
- COMMERCIAL/BUSINESS SERVICE AREA
- WET/DRY POND
- MUNICIPAL RESERVE
- BUFFER STRIP
- PROPOSED SCHOOLS & COMMUNITY CENTRE
- RESIDENTIAL CARE HOME
- CONCEPT PLAN BOUNDARY
- AREA SUBJECT TO REZONING

**City of
Saskatoon**
Planning & Development

NOTE: The information contained on this map is for reference only and is not to be used for legal purposes. All proposed line work is subject to change. This map is prepared in accordance with the expressed intent of the Regional Planning, Mapping & GIS Department. It is not intended to be used for legal purposes. It is not a warranty, representation, or agreement of any kind. It is not to be used for legal purposes. It is not a warranty, representation, or agreement of any kind. It is not to be used for legal purposes. It is not a warranty, representation, or agreement of any kind.

DATE: AUGUST 21, 2014
PROJECT: STONEBRIDGE CONCEPT PLAN

October 14, 2015

City Clerk

Dear City Clerk:

**Re: Municipal Planning Commission Report for Public Hearing
Proposed Rezoning from R1A to R1B and RM3 - Lewin Crescent
and Stonebridge Common - Stonebridge [CK 4351-015-015]**

The Municipal Planning Commission, at its meeting held on October 9, 2015, considered a report of the General Manager, Community Services Department, dated September 29, 2015, on the above application and supports the following recommendation of the Community Services Department:

That at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone land in the Stonebridge neighbourhood, as outlined in this report, be approved.

The Commission respectfully requests that the above report be considered by City Council at the time of the public hearing with respect to the above proposed application.

Yours truly,



Holly Thompson, Committee Assistant
Municipal Planning Commission

HT:sj

**ZONING NOTICE
STONEBRIDGE NEIGHBOURHOOD**

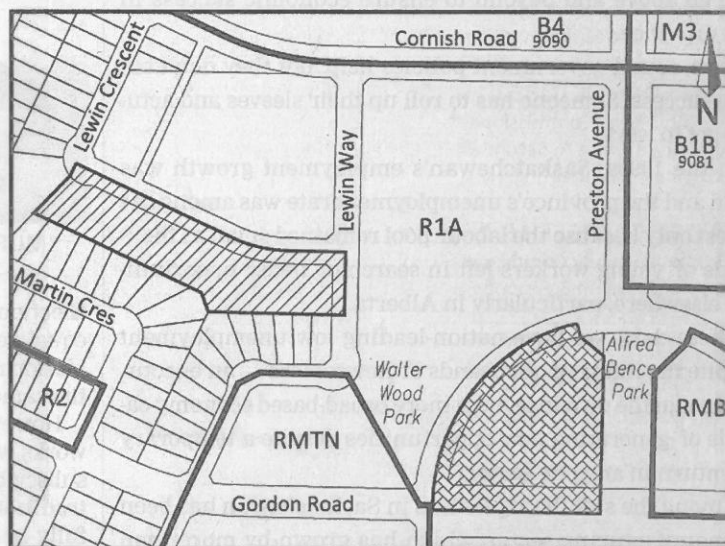
PROPOSED ZONING BYLAW AMENDMENT – BYLAW NO. 9317

Saskatoon City Council will consider amendments to the City's Zoning Bylaw (No. 8770) regarding land located on Lewin Crescent and Stonebridge Common in Stonebridge, submitted by Dream Development.



By way of Bylaw No. 9317, The Zoning Amendment Bylaw, 2015 (No. 29), the following zoning designations are proposed to be applied to the subject lands, consistent with the approved Stonebridge Neighbourhood Concept Plan:

- R1B – Small Lot One-Unit Residential District; and
- RM3 – Medium Density Multiple-Unit Dwelling District.

LEGAL DESCRIPTION – Plan of Proposed Subdivision of Parcel A, Plan No. 62S15086; Parcels A and B, Plan No. 102097009; Parcel C, Plan No. 66S07349; Parcel XX, Plan No. 102191712; and Part of NE ¼ Sec 10 – Twp 36 – Rge 5 – W3M.



PROPOSED ZONING AMENDMENT

-  From R1A to R1B
-  From R1A to RM3

File No. RZ26-2015

REASON FOR THE AMENDMENT – This rezoning will provide for the development of single family and medium density multiple-unit residential development, consistent with the approved Stonebridge Neighbourhood Concept Plan.

INFORMATION – Questions regarding the proposed amendment or requests to view the proposed amending Bylaw, the City of Saskatoon Zoning Bylaw and Zoning Map may be directed to the following without charge:
Community Services Department, Planning and Development
Phone: 306-975-7621 (Paula Kotasek-Toth)

PUBLIC HEARING – City Council will hear all submissions on the proposed amendment, and all persons who are present at the City Council meeting and wish to speak on **Monday, October 26, 2015 at 6:00 p.m. in City Council Chamber, City Hall, Saskatoon, Saskatchewan.**

All written submissions for City Council's consideration must be forwarded to:
His Worship the Mayor and Members of City Council
c/o City Clerk's Office, City Hall
222 Third Avenue North, Saskatoon SK S7K 0J5

All submissions received by the City Clerk by **10:00 a.m. on Monday, October 26, 2015** will be forwarded to City Council. City Council will also hear all persons who are present and wish to speak to the proposed Bylaw.

Proposed Closure of Right-of-Way – 22nd Street and 2nd Avenue North – Central Business District

Recommendation

1. That a portion of the north-south lane adjacent to the 200 block of 22nd Street be closed;
2. That portions of the proposed closure are sold to Nexus Holdings Inc. for \$35,005.50 plus GST and to Derek Morris for \$35,005.50 plus GST;
3. That all costs associated with the closure be paid for by the applicants, including Solicitor's fees and disbursements; and
4. That City Council consider Bylaw No. 9314, The Street Closing Bylaw, 2015 (No. 6).

Topic and Purpose

This report requests approval to close, and subsequently sell, a portion of the north – south lane immediately north of 22nd Street East and east of 2nd Avenue North in the Central Business District. The Administration has agreed to sell portions of the lane to Nexus Holdings Inc. for \$35,005.50 plus GST and to Derek Morris for \$35,005.50 plus GST pending City Council Approval.

Report Highlights

If approved, the closure of a portion of the north – south lane immediately north of 22nd Street and east of 2nd Avenue North will be transferred to Nexus Holdings Inc. (owners of 102 and 112 – 2nd Avenue North) for payment of \$35,005.50 plus GST and to Derek Morris (owner of 212 – 22nd Street East) for payment of \$35,005.50 plus GST.

Strategic Goal

The report supports the Strategic Goal of Sustainable Growth as closure will allow for development in the Central Business District.

Background

On May 28, 2012, the Administration received a request from Nexus Holdings Inc. to purchase a portion of the lane and consolidate this land with their existing property. This process includes notifying other City Departments and land owners adjacent to the lane and requesting their review and comment.

Report

On May 31, 2013, Nexus Holdings Inc. received a letter from the Administration in agreement to sell a portion of the lane, and outlined a cost of \$75.00 per sq. ft., for a total price of \$35,005.50 plus GST.

On May 31, 2013, Derek Morris received a letter from the Administration in agreement to sell a portion of the lane, and outlined a cost of \$75.00 per sq. ft., for a total price of \$35,005.50 plus GST.

Webb Surveys has submitted a Plan of Proposed Lane Closure dated September 30, 2013 which outlines the right-of-way to be closed.

A proposed Subdivision No. 81/13 was approved by the General Manager, Community Services Department on October 16, 2013.

To complete this transaction, the Administration requires City Council to approve Bylaw No. 9314, The Street Closing Bylaw, 2015 (No. 6) (Attachment 1).

As the lane is not fully closed, this will create a dead end situation and typically a turnabout would be required. However, in this instance the lane is too narrow to create a turnabout given the adjacent buildings. Therefore, signage will be installed that indicates the lane is not a through lane. Parking will not be allowed in the lane and will be enforced according to Bylaw No. 7200, The Traffic Bylaw.

Public and/or Stakeholder Involvement

Other City departments were surveyed for feedback and have no objection to the proposed plan. Utility agencies have been contacted with respect to the closure and no easements are required.

Fifteen property owners in the immediate area were sent letters and asked to indicate their support or opposition of the lane closure. Five property owners indicated their support in writing.

Support or opposition to the lane closure by the owner of 101191060 Saskatchewan Ltd. (120 – 2nd Avenue North) is unclear to the Administration. The following is a summary of communication between the Administration and the property owner:

- On April 6, 2013 the Administration mailed a letter requesting an indication of support or opposition to the lane closure (the initial proposal included closure of the entire north-south lane).
- On April 11, 2013 the Administration received a phone call indicating an interest in purchasing a portion of the lane adjacent to their property. This portion is immediately north of the road closure area recommended in this report.
- On April 16, 2013 the Administration received a written response that did not support the lane closure.
- On May 31, 2013 the Administration provided a Letter of Agreement outlining three options describing how the lane could be divided between the three property owners (Nexus Holdings Inc., Derek Morris, and 101191060 Saskatchewan Ltd.).
- In early June of 2013 the Administration received a phone call from 101191060 Saskatchewan Ltd. in response to the Letter of Agreement.

- The owner indicated no interest in purchasing the portion of lane immediately adjacent to his property due to cost.
- On September 24, 2014 the Administration mailed a letter requesting confirmation of support for the lane closure. The proposed lane closure did not include any portion of lane adjacent to the property owned by 101191060 Saskatchewan Ltd.
- On December 17, 2014 the Administration received a written letter stating opposition to the lane closure.
- On March 23, 2015 the Administration met with the three property owners. After discussing several options, the owner of 101191060 Saskatchewan Ltd. indicated he required time to think about the lane closure, and that he would respond to the other property owners within a week. At the time the property owner had submitted an application to Building Standards to convert the second storey into a restaurant / lounge and the construction of an exterior exit stair off the back of the building. This issue, unresolved at the time, was the focus of the property owner.
- On June 4, 2015 the property owner received a Building and Development permit from the City of Saskatoon for the proposed work to convert the second storey of his building.
- Since June 4, 2015 the Transportation division has attempted to arrange a meeting with the property owner numerous times via telephone and e-mail to receive in writing his support or opposition. No meeting has occurred.
- On October 16, 2015 the Administration mailed a certified letter to the property owner again requesting written indication of support or opposition to the lane closure.

The Ministry of Highways and Infrastructure has confirmed that no compensation is required to the Crown.

Communication Plan

Communication activities are included with the requirement for Public Notice. No other communication activities are required.

Financial Implications

Funding from the closure will be deposited into the Dedicated Roadway Reserve.

Safety/Crime Prevention Through Environmental Design (CPTED)

Community Services Division provided the following comments regarding the closure of this lane:

- Closing only half the lane leaves the north end at a higher risk of experiencing inappropriate or criminal activity than may be experienced currently. With no natural surveillance, the closure may become an entrapment area and a potential crime generator, and there is less connectivity to the surrounding area.

- Presently, both 102 and 112 – 2nd Avenue North are vacant and sight lines are adequate during the day. At night, when the lots are full of vehicles, sightlines are not as evident. However, users have many exit points and there are no entrapment areas.
- It will be important to monitor this area as with any development on 102 and 112 – 2nd Avenue North, the access lane will become more like a tunnel, particularly if there is a fence erected between 102 and 112 – 2nd Avenue North and 212 – 22nd Street and at the north end of the closed lane. These areas will become dead ends and have the potential to become entrapment areas.
- The residual rear lane, behind 120 – 2nd Avenue North will become a dead end and although short, the lane remains an entrapment area and should be monitored to ensure it does not attract inappropriate or criminal behaviour.
- In reference to reducing safety concerns and Safe Growth/CPTED, reviews of future developments in this rear lane is to be included in any contract or agreement with the owners of all affected properties as a condition of sale. This will ensure that current and future owners and civic staff are aware of this requirement.

At this time it is unclear to the Administration if 101191060 Saskatchewan Ltd. has interest in purchasing portion of the lane adjacent to their property at 120 – 2nd Avenue North. CPTED comments will continue to be communicated with the adjacent property owners and the location will be monitored yearly until the remaining lane is purchased.

Other Considerations/Implications

There are no other options, policy, environmental or privacy considerations or implications.

Due Date for Follow-up and/or Project Completion

There will be no follow up report.

Public Notice

Public Notice is required for consideration of this matter, pursuant to Section 3b) of Policy C01-021, Public Notice Policy. The following notice was given:

- Advertised in The StarPhoenix on the weekend of October 10, 2015 (Attachment 2);
- Posted on the City Hall Notice Board on Thursday, October 8, 2015;
- Posted on the City of Saskatoon website on Thursday, October 8, 2015; and
- Public Notice sent to businesses adjacent to lane.

Attachments

1. Bylaw No. 9314, The Street Closing Bylaw, 2015 (No. 6)
2. Copy of Public Notice

Report Approval

Written by: Shirley Matt, Senior Transportation Engineer, Transportation
Reviewed by: Jay Magus, Engineering Section Manager, Transportation

Proposed Closure of Right-of-Way – 22nd Street and 2nd Avenue North – Central Business District

Reviewed by: Angela Gardiner, Director of Transportation
Approved by: Jeff Jorgenson, General Manager, Transportation & Utilities
Department

Council SM – Prop Clos ROW – 22nd St and 2nd Ave N – Central Business District.docx

“Approved by Jeff Jorgenson, GM of T & U Department, October 19, 2015”

BYLAW NO. 9314**The Street Closing Amendment Bylaw, 2015 (No. 6)**

The Council of The City of Saskatoon enacts:

Short Title

1. This Bylaw may be cited as *The Street Closing Amendment Bylaw, 2015 (No. 6)*.

Purpose

2. The purpose of this Bylaw is to close a portion of the north-south lane adjacent to 22nd Street and parallel to 2nd Avenue in the Central Business District, Saskatoon, SK.

Closure of Portion of Lane

3. A portion of the north-south lane adjacent to 22nd Street East and parallel to 2nd Avenue North, Saskatoon, Saskatchewan more particularly described as all that portion of Lane L1, Registered Plan No. F2392, lying South of the production South Easterly of the North Easterly boundary of Lot 43, Block 150, Registered Plan No. 99SA32572, as shown on a Plan of Proposed Lane Closure prepared by T.R. Webb, S.L.S. dated September 30, 2013 and attached as Schedule "A" to this Bylaw, is closed.

Coming into Force

4. This Bylaw comes into force on the day of its final passing.

Read a first time this _____ day of _____, 2015.

Read a second time this _____ day of _____, 2015.

Read a third time and passed this _____ day of _____, 2015.

Mayor

City Clerk

Schedule "A"

PLAN OF PROPOSED
CLOSURE OF PART OF
LANE L1,
REG'D PLAN NO F2392
NW 1/4 SEC 28-36-5-3
SASKATOON, SASK.
SCALE 1:1000

T.R. Webb September 20, 2013
Saskatchewan Land Surveyor

Dimensions shown are in metres and decimals thereof.
Portion of this plan to be approved is outlined with a bold, dashed line and contains 0.01± h (0.02± ac.)
Dimensions shown are approximate and may differ from the final plan of survey by 0.5± m

Approved under the provisions of
Bylaw No. 6537 of the
City of Saskatoon

Date _____
Community Services Department

Reg'd Plan	44
No	158
995412572	

3rd AVENUE NORTH

23rd STREET

Reg'd Plan	49	Reg'd Plan	35	36
No	48	No		995412572
	B			

Reg'd Plan	33	Reg'd Plan	37
No	34	No	38
	C195		
Reg'd Plan	150	Reg'd Plan	39
No		No	995412572
995412572	45		

Reg'd Plan	44	Reg'd Plan	150	41
No	366	No		995412572
	43			
Reg'd Plan	2379	Reg'd Plan	42	
No		No		
995412572				

Lane to be closed

22nd STREET

2nd AVENUE NORTH

Reg'd Plan	41
149	
No	
995412572	
50	

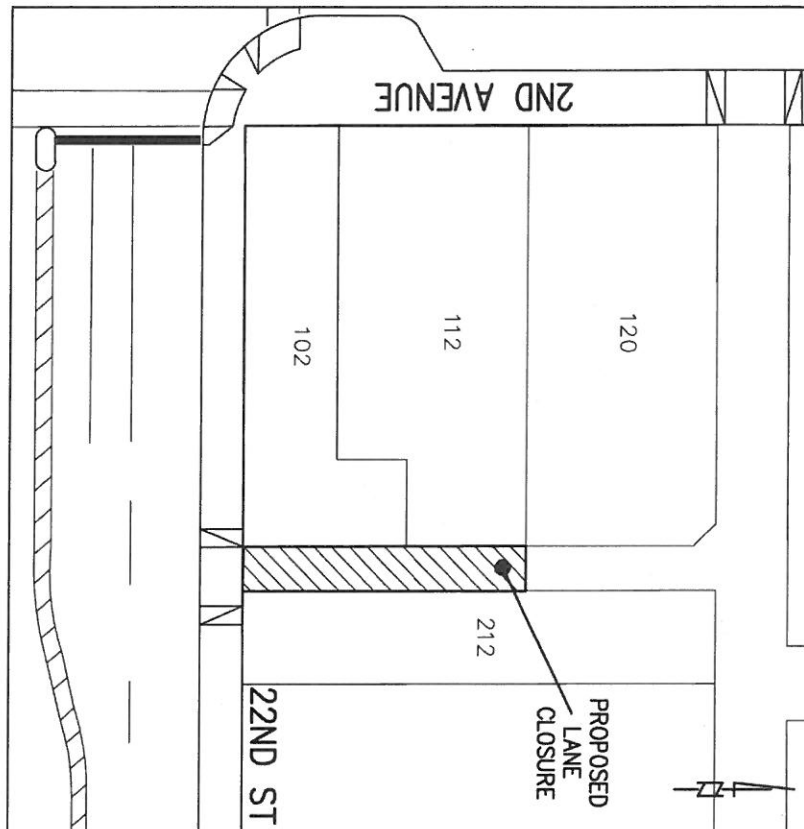
Prepared by
Robert Burnage
13-2038.m CAS

PUBLIC NOTICE

PROPOSED CLOSURE OF RIGHT-OF-WAY

The City of Saskatoon is proposing the following lane closure for the Central Business District.

**Location – Between 22nd Street and 2nd Avenue North,
Central Business District**



PUBLIC MEETING - City Council will consider the above matter and hear all persons present at the City Council meeting who wish to speak on Monday, October 26, 2015, at 6:00 p.m. in the Council Chambers, City Hall, Saskatoon, Saskatchewan.

WRITTEN SUBMISSIONS – If you wish to submit a letter on this matter for City Council’s consideration, it must be received by **Monday October 26, 2015 at 10:00 a.m.**

Mail to:

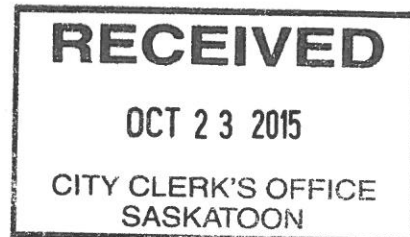
His Worship the Mayor and Members of City Council
c/o City Clerk’s Office, City Hall
222 Third Avenue North, Saskatoon, SK S7K 0J5

INFORMATION - Questions regarding the proposal may be directed to:

Shirley Matt, Transportation division
Phone 306-975-3145 Page 105



October 22, 2015



His Worship, Mayor Don Atchison
and members of City Council
City Hall
222 3rd Avenue North
Saskatoon SK S7K 0J5

Via email city.clerks@saskatoon.ca

Dear Mayor Atchison and Council members:

Re: Proposed Closure of Right-of-Way – 22nd Street and 2nd Avenue North – Central Business District [File No. CK 6295-015- 007 and IS 6295-1]

This letter is my request to speak at City Council on Monday October 26 regarding the above-referenced agenda item.

As most of you must know, we have been trying to complete the purchase of the lane at 22nd Street and 2nd Avenue for well over two years. This process is holding up a major development in the downtown core

I hope the City will support growth in our downtown core and expedite this process and allow the purchase of the lane to go forward.

Yours truly,


Ken Achs
President and CEO
midwest@mwdc.ca

KA/lp

cc Joan Thomas, office of Derek Morris via email joan@tcwest.ca



RECEIVED

OCT 23 2015

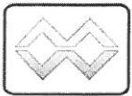
CITY CLERK'S OFFICE
SASKATOON

From: Lise Paquette <lise.paquette@mwdc.ca>
Sent: Friday, October 23, 2015 8:20 AM
To: Web E-mail - City Clerks
Cc: 'Joan Thomas'; Trevor Jacek
Subject: Proposed Closure of Right-of-Way – 22nd Street and 2nd Avenue North – Central Business District File CK 6295-015- 007
Attachments: CoS-let3(Mayor Atchison and City Council) re purchase of lane.pdf

Good morning,

Please find herewith, a letter from Ken Achs requesting to speak at City Council Monday October 26 regarding the above-referenced matter.

Regards,



Lise Paquette

Executive Assistant to Ken Achs

Mid-West Group of Companies

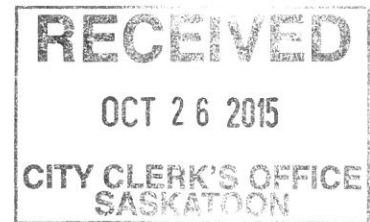
T 306.933.4838 | F 306.933.4121 | lise.paquette@mwdc.ca

100-450 2nd Avenue North, Saskatoon SK S7K 2C3 | www.midwestgroup.ca

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From: Rich Gabruch <r.gabruch@gabruchlegal.ca>
Sent: Monday, October 26, 2015 9:59 AM
To: City Council
Subject: Form submission from: Write a Letter to Council



Submitted on Monday, October 26, 2015 - 09:58
Submitted by anonymous user: 142.165.205.63
Submitted values are:

Date: Monday, October 26, 2015
To: His Worship the Mayor and Members of City Council
First Name: Rich
Last Name: Gabruch
Address: 154 Skeena Crescent
City: Saskatoon
Province: Saskatchewan
Postal Code: A7K4G7
Email: r.gabruch@gabruchlegal.ca

Comments: Regarding proposed lane closure between 2nd and 3rd Avenue abutting 22nd street. We operate our office and the lanes allow us access to our underground parking. Daily there are parking abuses in the laneways and we need alternate access points such as the proposed lane to access our parking garage. We therefore oppose its closure.... unless you steppe dup enforcement of illegal laneway parking to hourly patrols.

The results of this submission may be viewed at:
<https://www.saskatoon.ca/node/398/submission/47214>

Proposed Closure of Right-of-Way – Portion Road Allowance South of Chappell Drive – Montgomery Place

Recommendation

1. That portion of road allowance south of Chappell Drive is closed;
2. That after closure, this land be transferred to Canadian National Railway Company and consolidated with adjacent property;
3. That all costs associated with the closure be paid for by the applicants, including Solicitor's fees and disbursements; and
4. That City Council consider Bylaw No. 9319, The Street Closing Bylaw, 2015 (No.8).

Topic and Purpose

This report requests approval to close a portion of road allowance south of Chappell Drive in the Montgomery Place neighbourhood. The portion of road allowance was included in a Land Sale Agreement between the City of Saskatoon and Canadian National Railway Company (CNR) made effective on June 24, 2013.

Report Highlights

If approved, the closure of the portion of road allowance south of Chappell Drive will be transferred to CNR. The road allowance will be consolidated with adjacent land and will be used to facilitate the construction of a bypass track for the CNR Chappell Yard operations.

Strategic Goal

This report supports the Strategic Goal of Sustainable Growth as the closure will allow development in the Montgomery Place neighbourhood.

Background

On June 24, 2013 the City of Saskatoon and CNR made effective a Land Sale Agreement that included a portion of road allowance south of Chappell Drive. The land will be used by CNR to construct and operate an additional bypass track at their Chappell Yards facility. 0.062 hectares of the land sold was considered road allowance which requires a closure.

Report

Meridian Surveys Ltd. has submitted a Plan of Proposed Road Closure dated July 22, 2014 which outlines the road allowance to be closed. The area to be closed is 0.062 hectares.

A proposed Subdivision No. 14/15 was approved by the General Manager, Community Services Department on March 24, 2015.

Proposed Closure of Right-of-Way – Portion Road Allowance – South of Chappell Drive – Montgomery Place

To complete this transaction, the Administration requires City Council to approve Bylaw No. 9319, The Street Closing Bylaw, 2015 (No.8) (Attachment 1).

A dead end situation exists, and the road allowance closure will relocate the dead end approximately 31 metres to the south.

Public and/or Stakeholder Involvement

Utility agencies have been contacted with respect to the closure and have no objections to the closure subject to the following conditions: that SaskTel require easements.

Other than CPR and the City of Saskatoon, TransGas is the only property owner in the vicinity of the closure. A Public Notice was provided to TransGas and no opposition to the closure has been raised.

The Ministry of Highways and Infrastructure has confirmed that no compensation is required to the Crown.

Communication Plan

Communication activities are included with the requirement for Public Notice. No other communication activities are required.

Other Considerations/Implications

There are no options, policy, financial, environmental, privacy or CPTED considerations or implications.

Due Date for Follow-up and/or Project Completion

There will be no follow up report.

Public Notice

Public Notice is required for consideration of this matter, pursuant to Section 3b) of Policy No. C01-021, The Public Notice Policy. The following notice was given:

- Advertised in The StarPhoenix on the weekend of October 10, 2015 (Attachment 2);
- Posted on the City Hall Notice Board on Thursday, October 8, 2015;
- Posted on the City of Saskatoon website on Thursday, October 8, 2015; and
- Public notices will be mailed to all affected parties by October 9, 2015.

Attachments

1. Bylaw No. 9319, The Street Closing Bylaw, 2015 (No.8)
2. Copy of Public Notice

Report Approval

Written by: Shirley Matt, Senior Transportation Engineer, Transportation
Reviewed by: Jay Magus, Engineering Section Manager, Transportation
Reviewed by: Angela Gardiner, Director of Transportation

**Proposed Closure of Right-of-Way – Portion Road Allowance – South of Chappell Drive –
Montgomery Place**

Approved by: Jeff Jorgenson, General Manager, Transportation & Utilities
Department

Council - SM – Prop Closure Portion Rd Allowance – South of Chappell Dr – Montgomery PI

BYLAW NO. 9319**The Street Closing Bylaw, 2015 (No. 8)**

The Council of The City of Saskatoon enacts:

Short Title

1. This Bylaw may be cited as *The Street Closing Bylaw, 2015 (No. 8)*.

Purpose

2. The purpose of this Bylaw is to close a portion of road allowance south of Chappell Drive in Saskatoon, Saskatchewan.

Closure of Portion of Chappell Drive

3. A portion of road allowance south of Chappell Drive, Saskatoon, Saskatchewan, more particularly described as all that portion of the original road allowance adjoining the S.E. ¼ Sec. 23 and the S.W. ¼ Sec. 24 – 36 – 6 – 3 and lying north of a line connecting the S.E. corner of Parcel W to the S.W. corner of Parcel Y, Plan 102153855 as shown on plan of proposed road closure, as prepared by Meridian Surveys dated July 22, 2014, and attached as Schedule "A" to this Bylaw is closed.

Coming into Force

4. This Bylaw comes into force on the day of its final passing.

Read a first time this _____ day of _____, 2015.

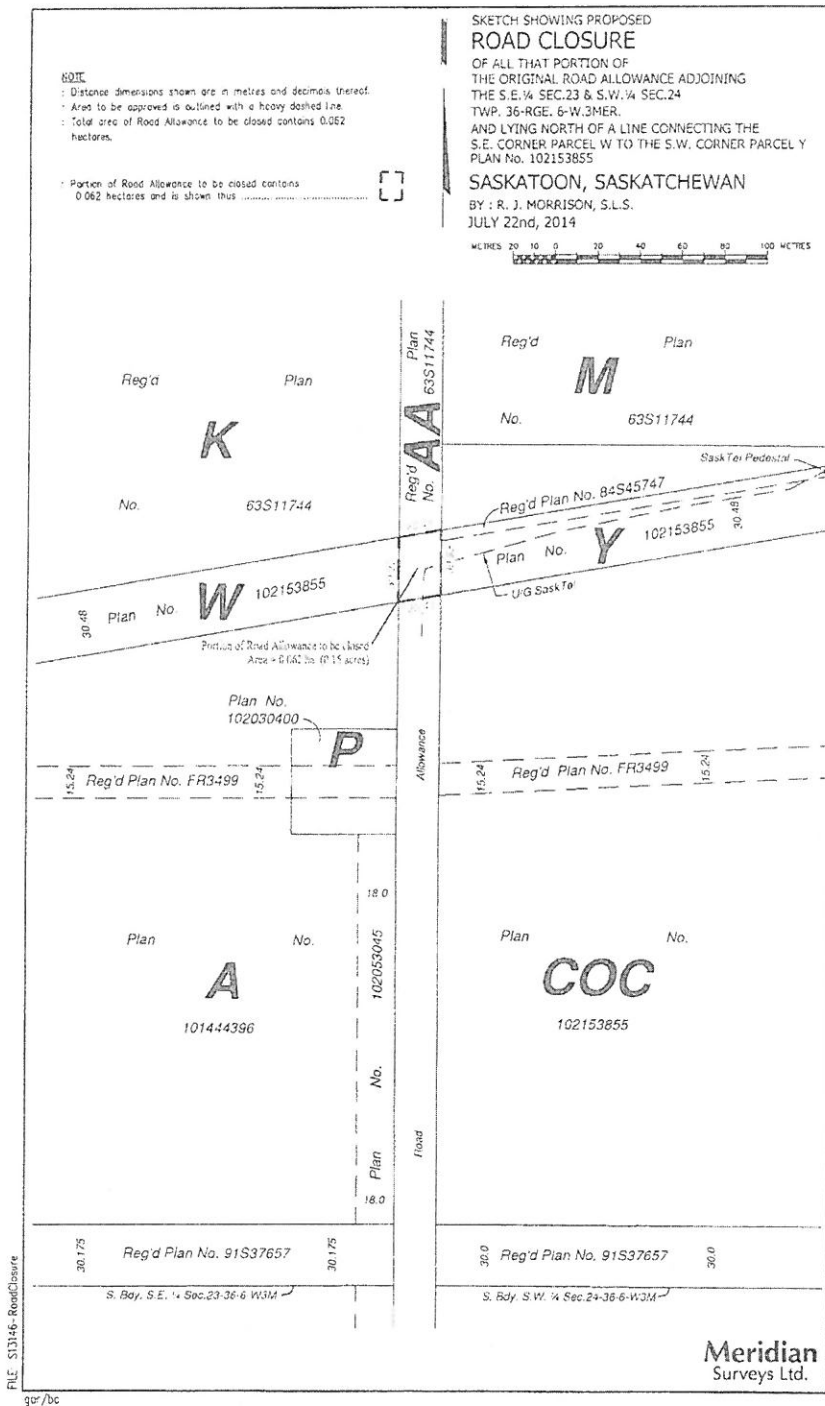
Read a second time this _____ day of _____, 2015.

Read a third time and passed this _____ day of _____, 2015.

Mayor

City Clerk

Schedule "A"

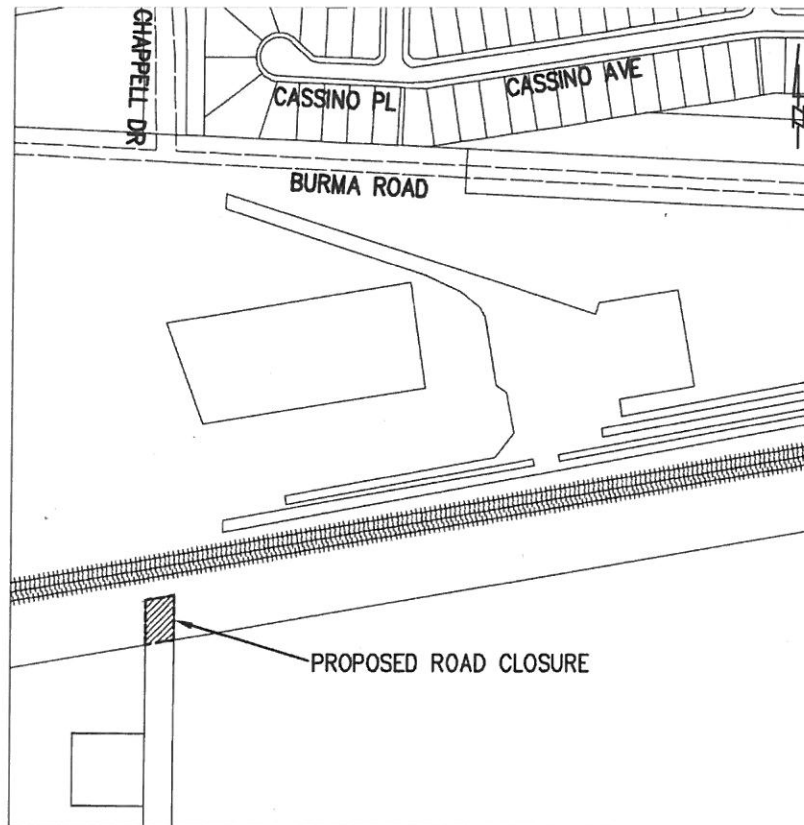


PUBLIC NOTICE

PROPOSED CLOSURE OF RIGHT-OF-WAY

The City of Saskatoon is proposing the following closure of Road Allowance in the Montgomery Place Neighbourhood

Location: Portion of Road Allowance lying south of the Canadian National Railway yards



PUBLIC MEETING - City Council will consider the above matter and hear all persons present at the City Council meeting who wish to speak on Monday, October 26, 2015, at 6:00 p.m. in the Council Chambers, City Hall, Saskatoon, Saskatchewan.

WRITTEN SUBMISSIONS – If you wish to submit a letter on this matter for City Council’s consideration, it must be received by **Monday October 26, 2015 at 10:00 a.m.**

Mail to:

His Worship the Mayor and Members of City Council
c/o City Clerk’s Office, City Hall
222 Third Avenue North, Saskatoon, SK S7K 0J5

INFORMATION - Questions regarding the proposal may be directed to:

Shirley Matt, Transportation division
Phone 306-975-3145

Proposed Closure of Right-of-Way, North-South Lane North of Packham Place – Sutherland Neighbourhood

Recommendation

1. That the north-south lane north of Packham Place be closed and sold to 101022891 Saskatchewan Ltd. (Acadia Construction) for \$10,242.78 plus GST;
2. That all costs associated with the closure be paid for by the applicant, including Solicitor's fees and disbursements; and
3. That City Council consider Bylaw No. 9318, The Street Closing Bylaw, 2015 (No.7).

Topic and Purpose

This report requests approval to close, and subsequently sell, the north-south lane north of Packham Place. The Administration has agreed to sell the lane to Acadia Construction for \$10,242.78 plus GST pending City Council Approval.

Report Highlights

If approved, the closure of the lane north of Packham Place will be transferred to Acadia Construction for payment of \$10,242.78 plus GST.

The lane north of Packham Place is not required by the Transportation division and the closure will permit Acadia Construction to add additional storage on the site.

Strategic Goal

This report supports the Strategic Goal of Sustainable Growth as the closure will allow development in the Sutherland neighbourhood.

Background

City Council, at its meeting held on July 15, 2002, adopted a proposed road closure for Packham Place:

- “1) that City Council give notice of its intention to consider the closing as shown on Closing Plan No. 242-0071-002r001;
- 2) that the City Solicitor be instructed to
 - a) take all necessary steps to bring the intended closing forward; and
 - b) complete the closing and obtain title in the name of the City of Saskatoon, should formal Council assent issue; and
- 3) that upon the City of Saskatoon obtaining title to the portion of right-of-way intended to be closed, that portion of right-of-way be sold to Acadia Construction for \$10,242.78 plus all legal costs for closing, to include Solicitors fees and disbursements.”

Proposed Closure of Right-of-Way, North-South Lane North of Packham Place – Sutherland Neighbourhood

The Administration has determined that the recommendations were not completed, including the creation of a Street Closing Bylaw for approval by City Council and the collection of funds from Acadia Construction.

In 2014, the City Bylaw Inspector was contacted in regards to personal property stored in the lane. Acadia Construction has blocked off the lane and used the lane as storage. As their subdivision application was never completed in 2002, they were advised by the Transportation division to proceed with the closure application and resubmit a subdivision application.

On March 13, 2014, the Administration received a formal request from Acadia Construction to purchase the lane and consolidate this land with their existing property. Subsequently, the Administration has proceeded with the lane closure process.

Report

On February 24, 2014, Acadia Construction received a letter from the Administration in agreement to sell the lane and outlined a total price of \$10,242.78 plus GST. The lane north of Packham Place is not required by the Transportation division and the closure will permit Acadia Construction to add additional storage on the site.

Webb Surveys has submitted a Plan of Proposed Lane Closure dated March 14, 2014 which outlines the lane to be closed.

A proposed Subdivision No.37/14 was approved by the General Manager, Community Services Department on September 2, 2014.

To complete this transaction, the Administration requires City Council to approve Bylaw No. 9318, The Street Closing Bylaw, 2015 (No.7) (Attachment 1).

A dead end situation will be created as a result of the proposed lane closure; however, a lane turnaround will be created to resolve this issue.

Public and/or Stakeholder Involvement

Other City departments were surveyed for feedback and have no objection to the proposed plan. Utility agencies have been contacted with respect to the closure and have no objections to the closure subject to the following conditions that: Transportation division, Saskatoon Light & Power, and SaskTel require easements.

All adjacent property owners have been notified and have not indicated any objection to the proposed lane closure.

The Ministry of Highways and Infrastructure has confirmed that the Minister's consent is not required nor is compensation to the Crown required pursuant to Sections 13(4) and 13(2)(a) of *The Cities Act*.

Communication Plan

Communication activities are included with the requirement for Public Notice. No other communication activities are required.

Financial Implications

Funding from the closure will be deposited into the Dedicated Roadway Reserve.

Other Considerations/Implications

There are no options, policy, environmental, privacy or CPTED considerations or implications.

Due Date for Follow-up and/or Project Completion

There will be no follow up report.

Public Notice

Public Notice is required for consideration of this matter, pursuant to Section 3b) of Policy C01-021, Public Notice Policy. The following notice was given:

- Advertised in The StarPhoenix on the weekend of October 10, 2015 (Attachment 2);
- Posted on the City Hall Notice Board on Thursday, October 8, 2015;
- Posted on the City of Saskatoon website on Thursday, October 8, 2015; and
- Public Notice sent out to businesses adjacent to the lane.

Attachments

1. Bylaw No. 9318, The Street Closing Bylaw, 2015 (No.7)
2. Copy of Public Notice

Report Approval

Written by: Shirley Matt, Senior Transportation Engineer, Transportation
Reviewed by: Jay Magus, Engineering Section Manager, Transportation
Reviewed by: Angela Gardiner, Director of Transportation
Approved by: Jeff Jorgenson, General Manager, Transportation & Utilities
Department

Council SM – Prop Clos of ROW - North-South Lane North of Packham Place – Sutherland

BYLAW NO. 9318**The Street Closing Bylaw, 2015 (No. 7)**

The Council of The City of Saskatoon enacts:

Short Title

1. This Bylaw may be cited as *The Street Closing Bylaw, 2015 (No. 7)*.

Purpose

2. The purpose of this Bylaw is to close a portion of the north-south lane north of Packham Place in Saskatoon, Saskatchewan.

Closure of Portion of Packham Place

3. A portion of the north-south lane north of Packham Place, Saskatoon, Saskatchewan, more particularly described as all that portion of Lane 3, Registered Plan No. 65S02405 lying adjacent to the Northeast boundary of Parcel A, Registered Plan No. 77S26886 as shown on a Plan of Proposed Lane Closure as prepared by T.R. Webb, S.L.S. dated March 14, 2014, and attached as Schedule "A" to this Bylaw, is closed.

Coming into Force

4. This Bylaw comes into force on the day of its final passing.

Read a first time this _____ day of _____, 2015.

Read a second time this _____ day of _____, 2015.

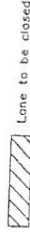
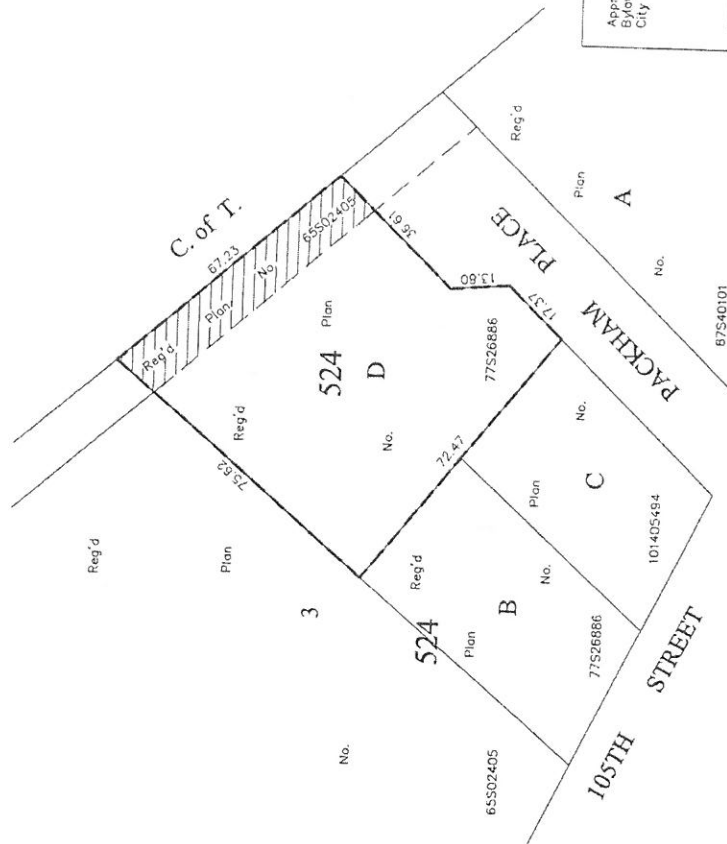
Read a third time and passed this _____ day of _____, 2015.

Mayor

City Clerk

Schedule "A"

PLAN OF PROPOSED
LANE CLOSURE OF
PART OF LANE 3,
REG'D PLAN NO. 65S02405
PACKHAM PLACE
SASKATOON, SASKATCHEWAN
BY T.R. WEBB, S.L.S.
SCALE 1:1000



Dimensions shown are in metres and decimals thereof.
Portion of this plan to be approved is outlined with a bold, dashed line and contains 0.47± ha (1.17± ac).

Distances shown are approximate and may vary from the final plan of survey by ± 0.5 m

T.R. Webb
T.R. Webb March 14, 2014
Saskatchewan Land Surveyor Seal

Approved under the provisions of
Bylaw No. 6537 of the
City of Saskatoon

Date _____
Community Services Department

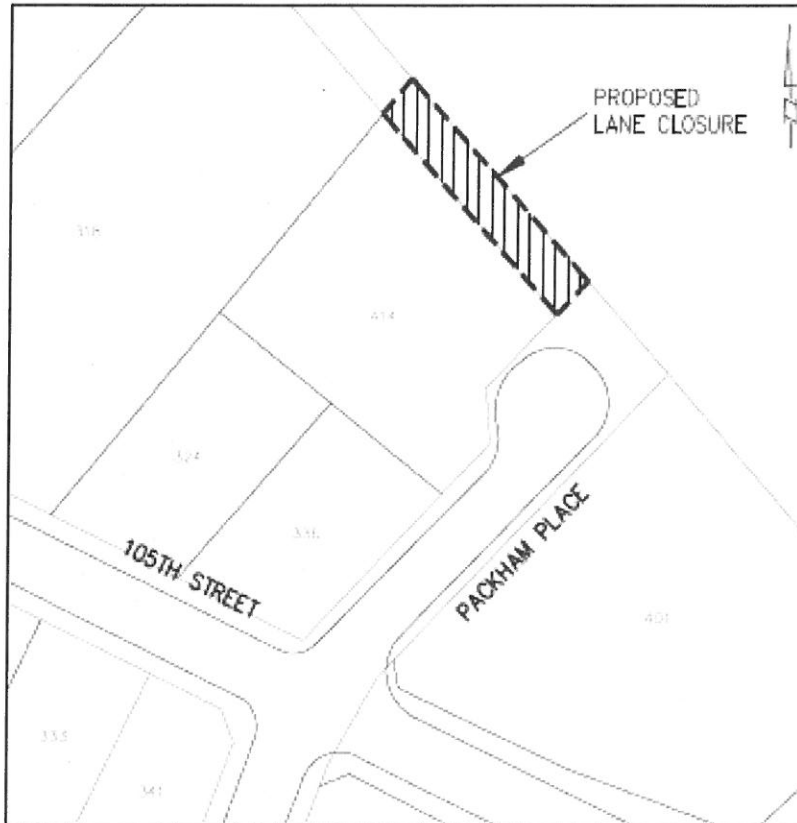
Prepared by
T.R. Webb
T.R. Webb
© 2014
02-1060sc NLD

PUBLIC NOTICE

PROPOSED CLOSURE OF RIGHT-OF-WAY

The City of Saskatoon is proposing the following closure of a north-south lane behind Packham Place in the Sutherland Neighbourhood.

Location: Lane north of Packham Place



PUBLIC MEETING - City Council will consider the above matter and hear all persons present at the City Council meeting who wish to speak on Monday, October 26, 2015, at 6:00 p.m. in the Council Chambers, City Hall, Saskatoon, Saskatchewan.

WRITTEN SUBMISSIONS – If you wish to submit a letter on this matter for City Council’s consideration, it must be received by **Monday October 26, 2015 at 10:00 a.m.**

Mail to:

His Worship the Mayor and Members of City Council
 c/o City Clerk’s Office, City Hall
 222 Third Avenue North, Saskatoon, SK S7K 0J5

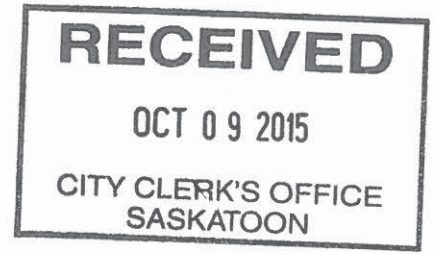
INFORMATION - Questions regarding the proposal may be directed to:

Shirley Matt, Transportation division
 Phone 306-975-3145 Page 120

205-5

From: Paige Gignac <pgignac@unitedwaysaskatoon.ca> on behalf of Paige Gignac
<pgignac@unitedwaysaskatoon.ca>
Sent: October 09, 2015 1:29 PM
To: City Council
Subject: Form submission from: Write a Letter to Council

205-1



Submitted on Friday, October 9, 2015 - 13:28
Submitted by anonymous user: 142.165.130.138
Submitted values are:

Date: Friday, October 09, 2015
To: His Worship the Mayor and Members of City Council
First Name: Paige
Last Name: Gignac
Address: 100-506 25th Street East
City: Saskatoon
Province: Saskatchewan
Postal Code: S7K 4A7
Email: pgignac@unitedwaysaskatoon.ca
Comments:
Your Worship the Mayor and Members of City Council:

Please accept this letter as a formal request from the Board of Directors for United Way of Saskatoon and Area. We ask that November be proclaimed United Way month in the City of Saskatoon. We would also request that the City of Saskatoon fly the United Way flag in honour of the community-wide effort by the citizens of Saskatoon to create opportunities for a better life for all.

As you are aware, November is the final month of the community's efforts to "ignite possibility and create a better life for everyone in our community" through United Way. Since 1959, individuals, corporations, labour groups, government, small business, employees and community groups have come together in a community-wide effort to improve lives, build community and bring about long-term improvements to the quality of life in Saskatoon and area.

The 2015 Community Campaign goal is \$6 million (and beyond!), and we are confident the citizens of Saskatoon will continue their generosity as they have in the past, to help us to meet our goal.

Thank you in advance for your consideration. To further discuss this request or to notify us of your decision, please contact Paige Gignac, Coordinator, Marketing & Communications with United Way of Saskatoon and Area by phone at 306-291-1313 or by email at pgignac@unitedwaysaskatoon.ca.

Sincerely,

Myra Potter
Interim CEO
United Way of Saskatoon & Area

Subject: Flag Raising Date - United Way

From: Paige Gignac [mailto:PGignac@unitedwaysaskatoon.ca]

Sent: Monday, October 19, 2015 3:48 PM

To: Thompson, Holly (Clerks)

Subject: RE: Flag Raising Date

Hi Holly,

Sorry for the delay. The date of the flag raising will be November 5th at 10:30am. I am filling out the application right now.

Paige

Paige Gignac

Coordinator, Marketing & Communications

pgignac@unitedwaysaskatoon.ca

United Way of Saskatoon & Area

Together, we are possibility



October 8, 2015

His Worship Donald J. Atchison
Office of the Mayor
222 Third Avenue North
Saskatoon, SK S7K 0J5

Dear Mr. Atchison:

Re: Global Entrepreneurship Week (GEW) 2015 Flag Raising

Global Entrepreneurship Week (GEW) is the world's largest celebration of entrepreneurship. This year, Global Entrepreneurship Week falls on **November 16-22, 2015**, and Futurpreneur Canada is proud to be the official Canadian host for the seventh consecutive year. On behalf of Futurpreneur Canada, I respectfully request your support of GEW by raising the GEW flag on November 17, 2015.

Global Entrepreneurship Week aims to inspire people around the world through local, national and international activities designed to help them explore their potential as self-starters and innovators. Global Entrepreneurship Week activities, from large-scale competitions and events to intimate networking gatherings, connect participants to potential collaborators, mentors and investors—introducing them to new possibilities and exciting opportunities to explore their entrepreneurial potential. About 10 million people across 160 countries are expected to take part.

This year, Futurpreneur Canada is looking for **Canada's Most Entrepreneurial City** through the 2015 City Battle. The winning Canadian city will be the city that hosts the most events per capita during Global Entrepreneurship Week (GEW) 2015. Each event must be registered online at www.gewcanada.com and the winner of the Most Entrepreneurial City title will be announced on Monday, November 23, 2015.

A GEW Ambassador will be travelling to cities across Canada to showcase entrepreneurial activity and a key stop on the journey would be a GEW Flag Raising event in Saskatoon to raise support and awareness of entrepreneurship in the city. Entrepreneurs are key to the economic and social fabric of Saskatoon and as the Mayor of Saskatoon, you are a champion of entrepreneurship in the city.

All Saskatonians play a role in increasing the prevalence and success of small businesses in the city and with your expressed support by raising a GEW flag provided to you by Futurpreneur Canada, we can harness this momentum.

Yours sincerely,

A handwritten signature in black ink, appearing to read "Joelle Foster".

Joelle Foster, Regional Director

cc: Fiona Wilson, Manager of Government Relations



September 25, 2015

Mayor Don Atchison
City of Saskatoon
Saskatoon, SK

RE: Proclamation of Biotech Week – October 30 to November 6, 2015
Invitation to Biotech & Beer at Boffins – November 5, 4 to 7 PM

Dear Mayor Atchison,

We would like to begin by thanking you for your support of Canada's National Biotech Week (NBW) by participating in events and by proclaiming Biotech Week in the City of Saskatoon in past years. We would be grateful if the City of Saskatoon would proclaim October 30 to November 6 Biotech Week for 2015.

This countrywide event celebrates the imagination, opportunities and success of biotechnology innovators across Canada. The Saskatchewan NBW steering committee has planned a wide range of exciting activities to showcase the province's achievements and encourage involvement in the life sciences and growth in our bioeconomy.

Your proclamation will complement a variety of events that promote and celebrate the biotechnology industry; Café Sci (October 27), the Amazing Biotech Race (November 2), a presentation by the CEO of Agri-Trend, Rob Saik (November 4), Walking Dead Dances at various elementary schools throughout the week, and the Seventh Annual Biotech and Beer at Boffins (4:00 to 7:00 PM, November 5) – which we would be delighted to have you attend.

Saskatchewan hosts a large percentage of Canada's agricultural biotechnology activities, with many other bioscience activities taking hold here, including in the environment and health sectors. Two university campuses, Innovation Place technology parks, and numerous research institutions and technical training centres create a significant science cluster - a great benefit to businesses, from start-ups to international success stories. Bioscience discoveries foster new business opportunities in the areas of biofuels and bioproducts, functional foods and natural health products. Outstanding science R&D infrastructure, a talented workforce, quality raw materials and a solid business environment make Saskatchewan the perfect place for bio-business!

For more information about Biotech Week activities in Saskatchewan visit <http://www.agwest.sk.ca/events/178> or contact Ag-West Bio Communications Director Jackie Robin by email: jackie.robin@agwest.sk.ca or at 306-668-2656. For information about activities across the country visit the BIOTECanada's National Biotech Week website: www.imagenation.ca.

Thank you again for your outstanding support for Saskatchewan's bioeconomy. We look forward to working with you to celebrate excellence in biotechnology in our province and country.

Sincerely,

Wilf Keller
President & CEO

305-5

From: Web E-mail - Mayor's Office
Sent: September 29, 2015 8:59 AM
To: City Clerk's Office
Subject: FW: Proclamation of Biotech Week - October 30 to November 6, 2015
Attachments: 2015 Biotech Week Letter - Mayor Atchison.pdf

Good morning,

Please see the proclamation request below.

Chanda Atwood tel 306.975.3202

Office of the Mayor
City of Saskatoon | 222 3rd Avenue North | Saskatoon SK S7K 0J5
mayors.office@saskatoon.ca
www.saskatoon.ca



From: Wilf Keller [<mailto:Wilf.Keller@agwest.sk.ca>]
Sent: Monday, September 28, 2015 4:02 PM
To: Web E-mail - Mayor's Office
Cc: Jackie Robin; Wilf Keller; Lana Mollard
Subject: RE: Proclamation of Biotech Week - October 30 to November 6, 2015

Ag-West Bio would be grateful if the City of Saskatoon would proclaim October 30 to November 6 Biotech Week for 2015. The proclamation from the City of Saskatoon would complement a variety of events that promote and celebrate the biotechnology industry as noted in the attached letter.

Thank you for your consideration and support.

Regards,

Wilf Keller

Wilf Keller
President and CEO
Ag-West Bio Inc.
#101 - 111 Research Drive
Saskatoon SK S7N 3R2
Telephone: 306.975.1939
Direct: 306.668.3571
Cell: 306.241.1466
Fax: 306.975.1966
Email: wilf.keller@agwest.sk.ca
www.agwest.sk.ca

From: Ömer Al-Katib <omer.alkatib@mfa.gov.tr>
Sent: October 01, 2015 2:59 PM
To: City Council
Subject: Recognition of Special Day in Turkey
Attachments: HCG Republic Day Flag Request letter.pdf

Good afternoon,

I am emailing to inquire regarding recognition of Republic Day in Turkey on October 29th. It would be very meaningful if His Worship would be so kind as to provide a letter recognizing This day to the Canadian Turkish community in Saskatoon.

As you may remember, your office was so kind in assisting to arrange a flag raising on October 29th, 2014 and in April 2015 to commemorate Children's Day/National Day in Turkey.

As well, I would like to request a flag raising. I have spoken to the City Communications office and they provided the form for Use of Civic Square to be completed, however I understand that an approval by Council is also required. I hope the letter attached provides sufficient detail for this special day in Turkey.

I am available to discuss or answer any questions that you may have.

Please find letter to His Worship attached.

Thank you in advance.

Regards

Omer Al-Katib
Honorary Consul General - Saskatchewan
Republic of Turkey
email: omer.alkatib@mfa.gov.tr
mobile: +(306) 222-4554
Facebook: <https://www.facebook.com/TurkeyConsulSask>

Bu e-posta mesajı ve ekinde bulunabilecek dosyalar yalnız mesajın alıcı hanesinde kayıtlı kullanıcı(lar) içindir. Mesajın alıcısı değilseniz, lütfen hemen göndericiyi uyarınız. Mesajı dağıtmayınız, kopyalamayınız, içeriğini açıklamayınız; çıktı almaksızın siliniz. Bu mesajda kayıtlı görüş ve düşünceler hiçbir şekilde Türkiye Cumhuriyeti Dışişleri Bakanlığına atfedilemeyeceği gibi, Bakanlık bakımından bağlayıcı değildir. Virüs ve kötü amaçlı yazılımların bu mesajda yerleşmesinin engellenmesi amacıyla gerekli tüm önlemler alınmış olsa da, bu mesajın sisteminizde yaratabileceği kayıp ve zararlardan dolayı Bakanlık hukuken sorumluluk kabul etmez.

Türk dış politikasına ilişkin gelişmeler hakkında bilgi almak için www.mfa.gov.tr adresine gidebilir, twitter ve facebook sayfamızı izleyebilir, seyahatleriniz için www.evisa.gov.tr sayfasını ziyaret edebilirsiniz.



**Honorary Consulate of Turkey in Saskatchewan
101-203 Stonebridge Blvd
Saskatoon, Saskatchewan Canada S7T 0G3**

October 1, 2015

His Worship Mayor Don Atchison
Mayor of Saskatoon

Your Worship,

As Canadian Turks, we are proud of being citizens of this free and democratic country which stands out as one of the most successful examples of multiculturalism in the world. We appreciate the opportunities provided by the City of Saskatoon to different ethnic communities including Canadian Turks.

We have a growing population of Canadian Turks in the Province of Saskatchewan and in the City of Saskatoon. For this reason, as Honorary Consul General of Turkey in Saskatchewan, I would respectfully wish to request an annual Proclamation to commemorate October 29th, Republic Day in Turkey

As well, through appropriate channels, I will contact City Hall to arrange a flag raising ceremony to commemorate this important day. Your Worship, I would certainly extend a personal invitation to you to join us on these days as we recognize these special events.

October 29th is truly an important day for Turkey which commemorates the founding of the modern Turkish Republic by Mustafa Kemal Atatürk in 1923. This event marked the end of the Ottoman Empire and started Turkey on the path to embark on a wide-ranging set of reforms in the political, economic and cultural aspects of Turkish society. These reforms have left a lasting legacy of which the peoples of Turkish heritage are proud: the conversion of the newly founded Republic into today's modern, democratic and secular Turkish state.

It would truly be meaningful to the community of Canadian Turks to have this special and important day recognized by their adopted community.

We would be happy to provide additional information should Your Worship deem it necessary.

Best regards,

Sincerely

Omer Al-Katib
Honorary Consul General of Turkey, Saskatchewan