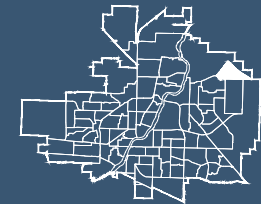




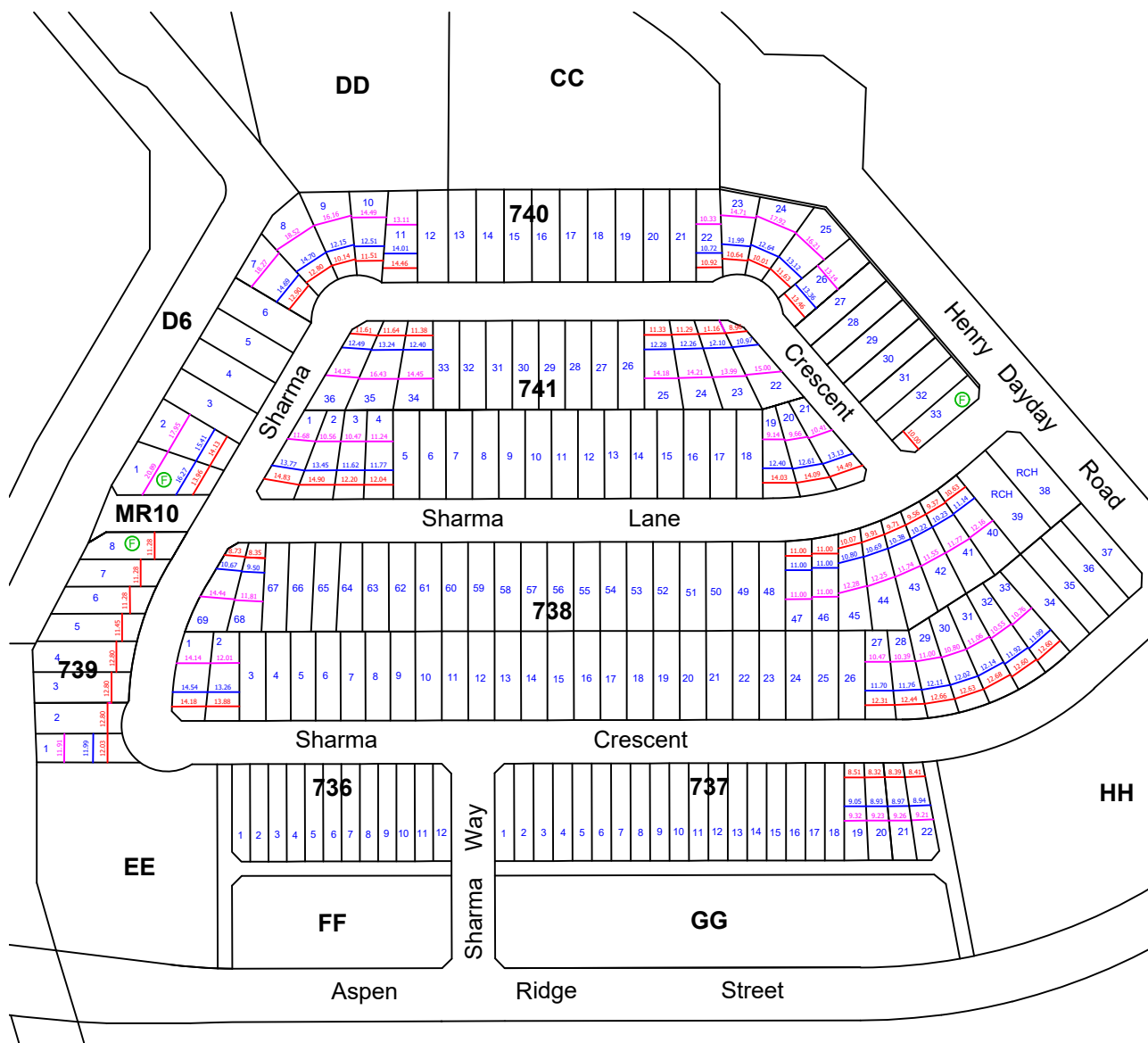
Phase 7 - Lot Information Map

116 Lots: Sharma Lane, Sharma Crescent, & Sharma Way



Legend:

- Single Unit Residential
- Highly Visible Lot
Additional Exterior Materials Required as per Architectural Control Req.
- Residential Care Home
- Corner Garage Location
- Easement 2m
- Easement 3m
- Easement 5m
- Walkout Basement
- Transitional Lot
- Transformer
- Service Pedestal
- Block Number
- Lot Number
- Civic Address
- Mail Box
- Fire Hydrant
- Decorative Fence
Fence 10' Within Rear Lot
- Masonry Fence
Masonry Fence 10' Within Rear Lot
- Transit Stop
- Vertical Curb
- Street Light
- Proposed Cell Tower



LEGEND

Note: The information contained on this map is for illustrative purposes only and should not be used for legal purposes. All the information was compiled from available sources and the line work is subject to change without notice. Parks and buffer renderings are for illustrative purposes only and does not represent what will be constructed.

- Conceptual 6m Setback
- Conceptual 12m Setback
- Conceptual 24m Setback
- Lot with side fence encroachment



Date:
NOV. 2023

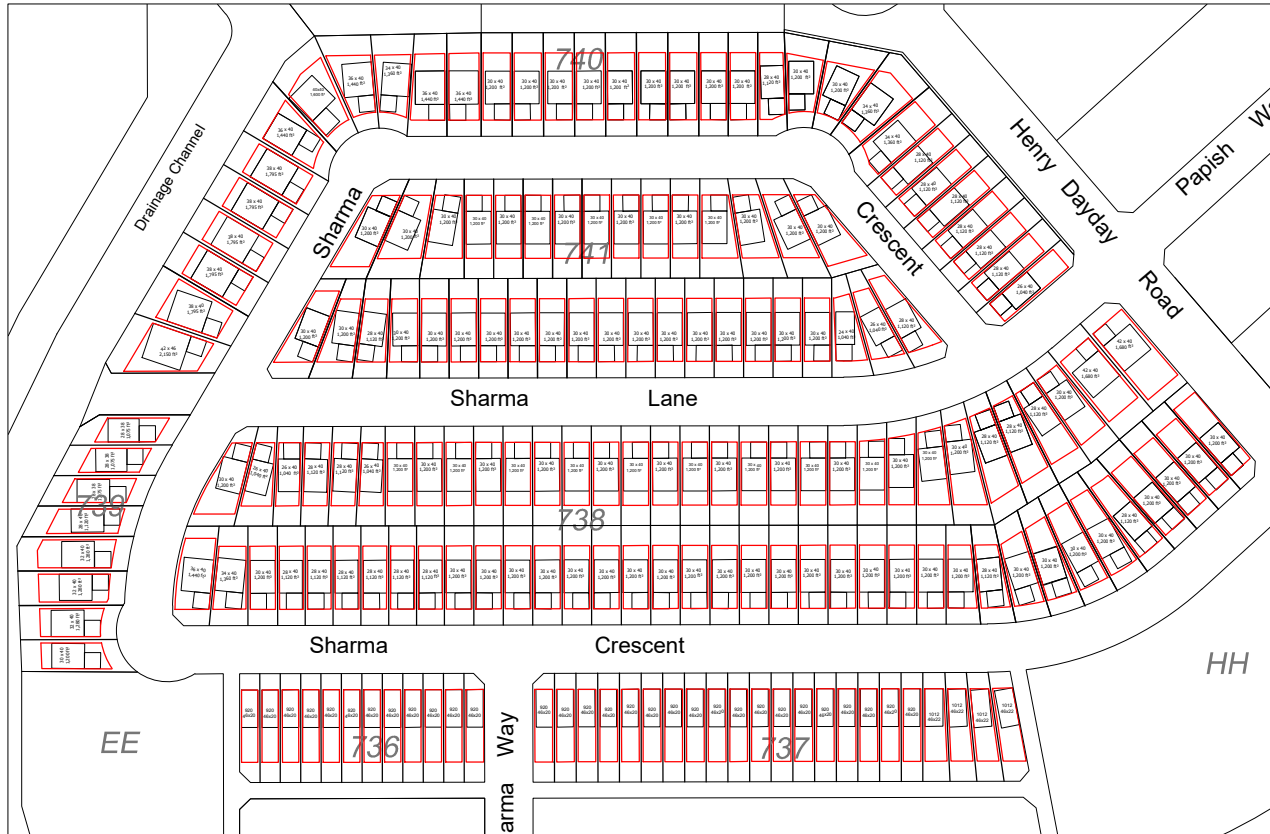
Scale:
NTS

Drawn By:
AMR

File No.

ASPEN RIDGE PHASE 7

HOUSE AND GARAGE SETBACK DIMENSIONS



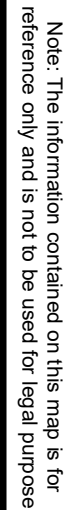
Note: The information contained on this map is for illustrative purposes only and should not be used for legal purposes. All the information was compiled from available sources and the line work is subject to change without notice. Parks and buffer renderings are for illustrative purposes only and does not represent what will be constructed.



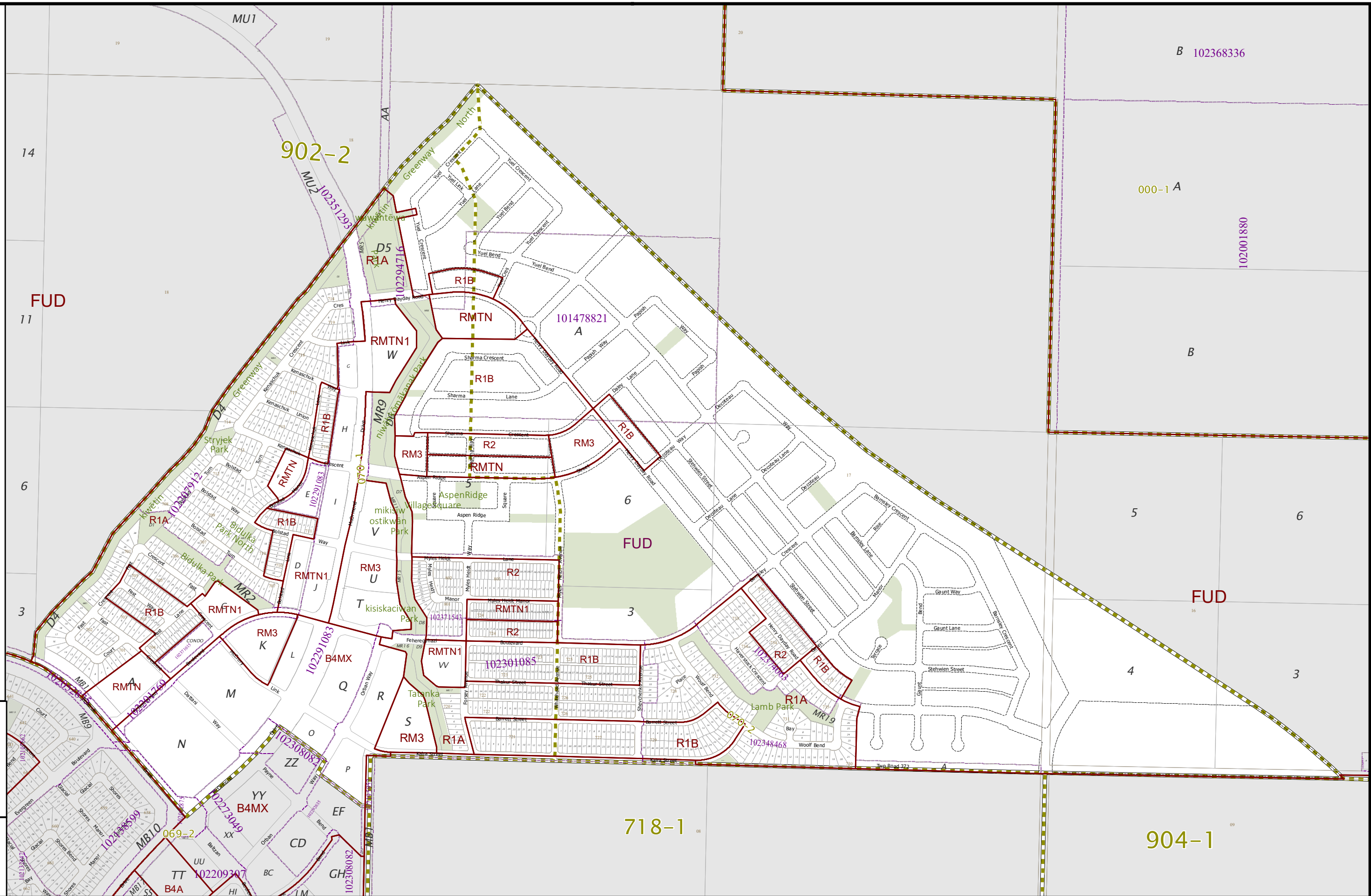
Date: Oct. 2023	Scale: NTS
Drawn By: AMR	File No.

ASPEN RIDGE PHASE 7

HOUSING FOOTPRINTS AND
ZONING BYLAW BUILDING AREA



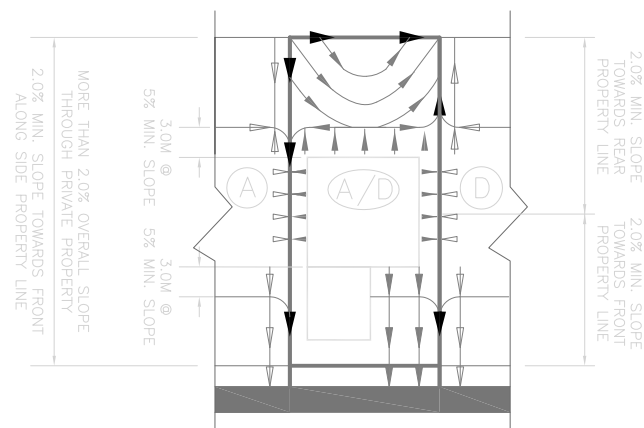
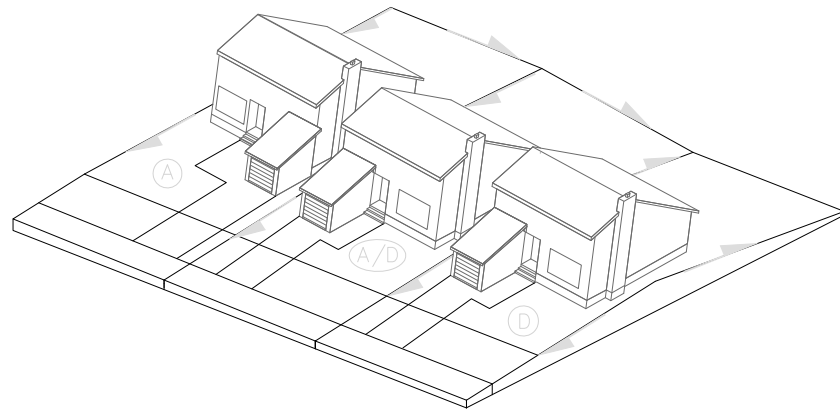
Bylaw 8770



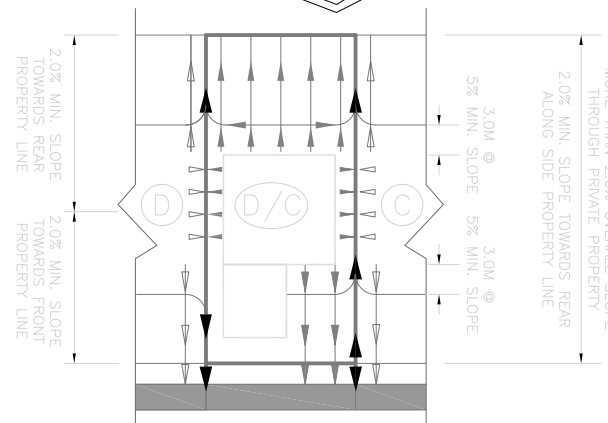
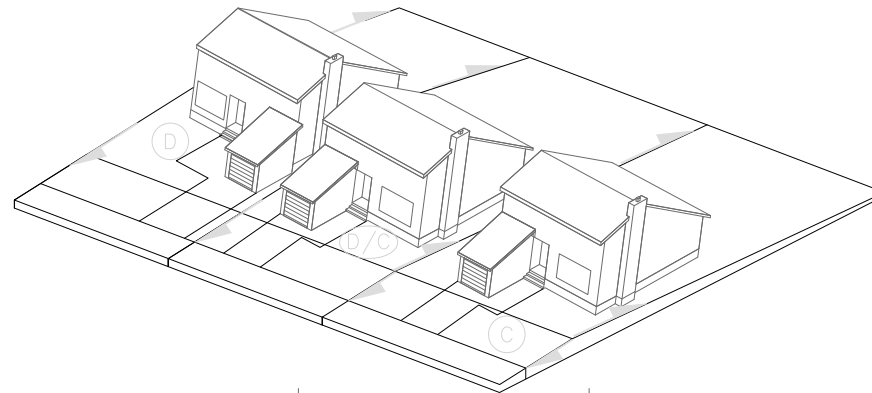
ASPEN RIDGE LAND USE CONCEPT PLAN



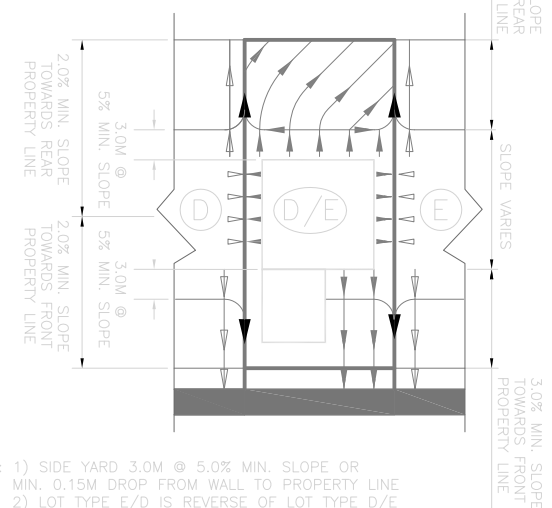
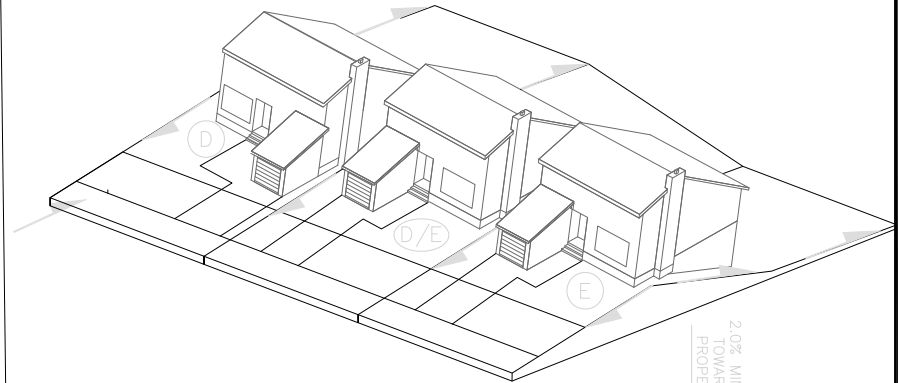
Lot Grading Types



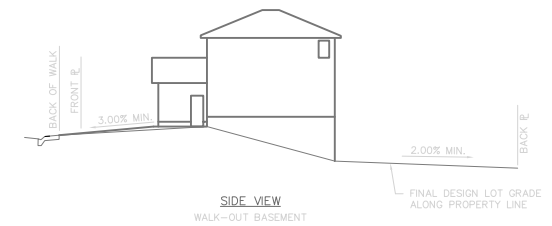
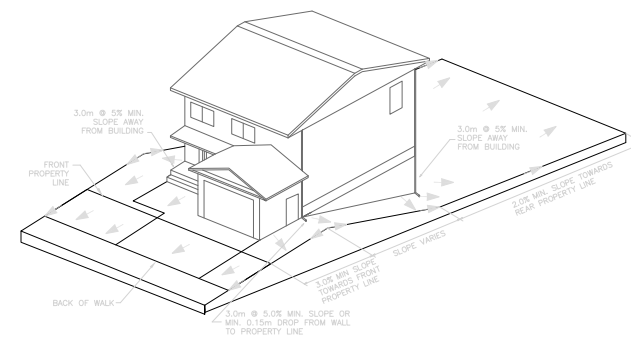
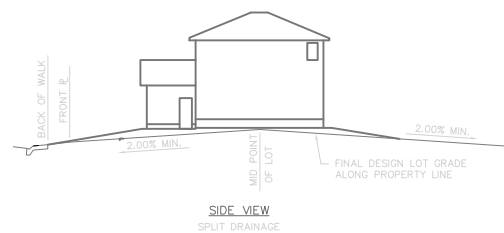
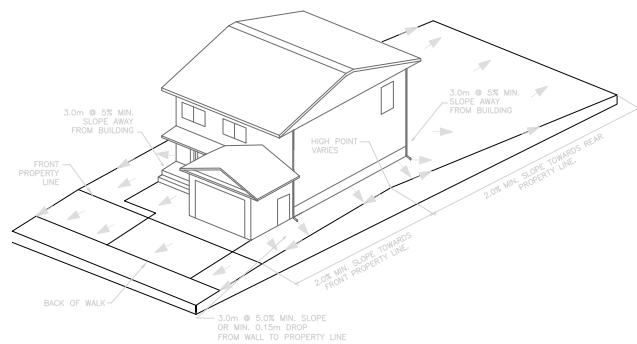
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2) LOT TYPE D/A IS REVERSE OF LOT TYPE A/D

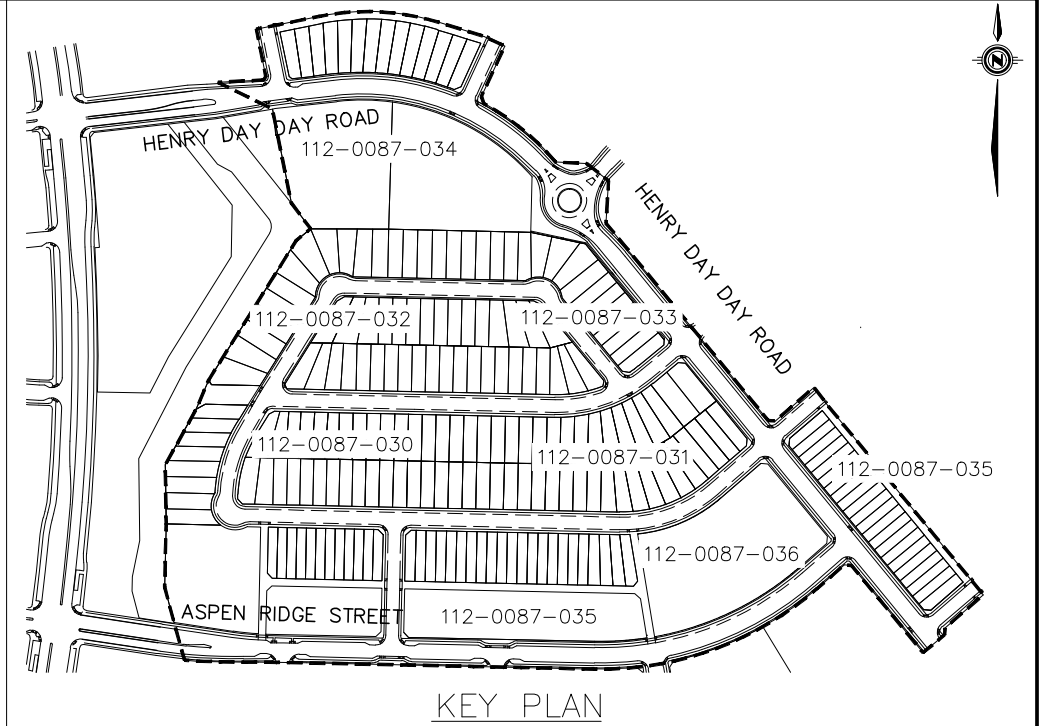
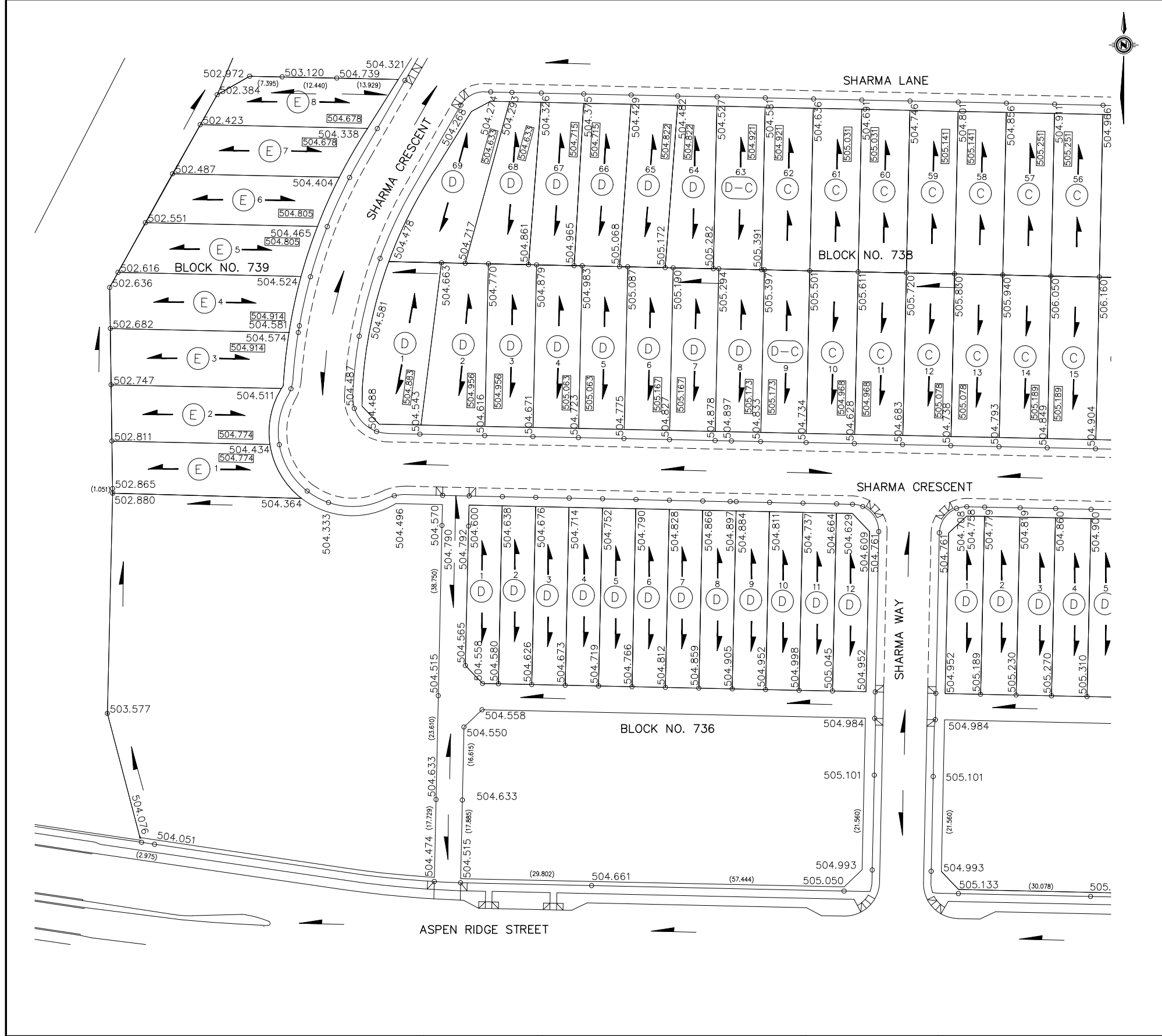


NOTE: 1) SIDE YARD 3.0M @ 5.0% MIN. SLOPE OR MIN. 0.15M DROP FROM WALL TO PROPERTY LINE
2) LOT TYPE C/D IS REVERSE OF LOT TYPE D/C

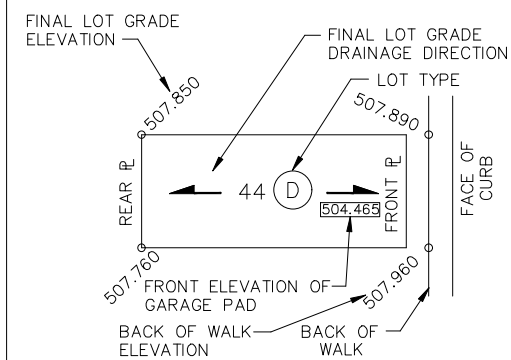


NOTE: 1) SIDE YARD 3.0M @ 5.0% MIN. SLOPE OR MIN. 0.15M DROP FROM WALL TO PROPERTY LINE
2) LOT TYPE E/D IS REVERSE OF LOT TYPE D/E





LOT DRAINAGE LEGEND

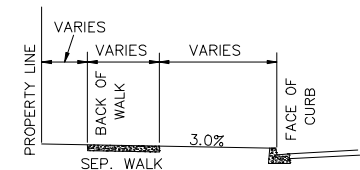


- NOTE:
1. FRONT AND SIDE FLANKAGE LOT GRADES REPRESENT THE DESIGN ELEVATION AT THE BACK OF WALK (B.O.W.)
 2. DIMENSIONS SHOWN IN BRACKETS ARE BETWEEN GRADE CHANGES.

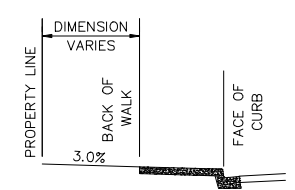
LOT DRAINAGE DETAILS

LOT TYPE 'D' - SEE DRAWING 102-0022-017
LOT TYPE 'E' - SEE DRAWING 102-0022-019

SEPARATE CURB & WALK



COMBINED CURB & WALK



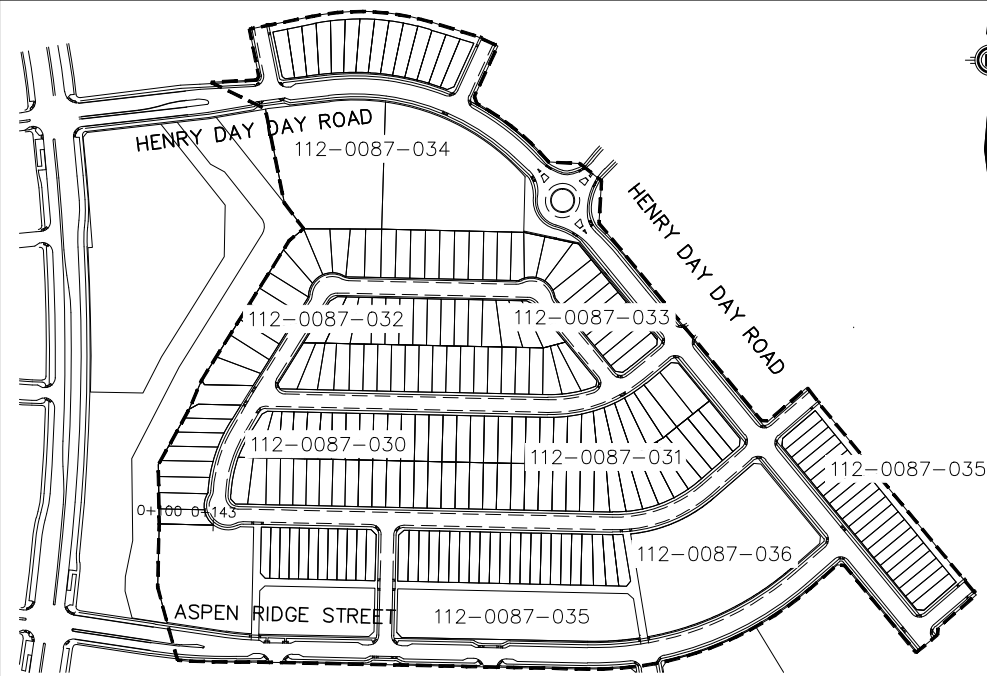
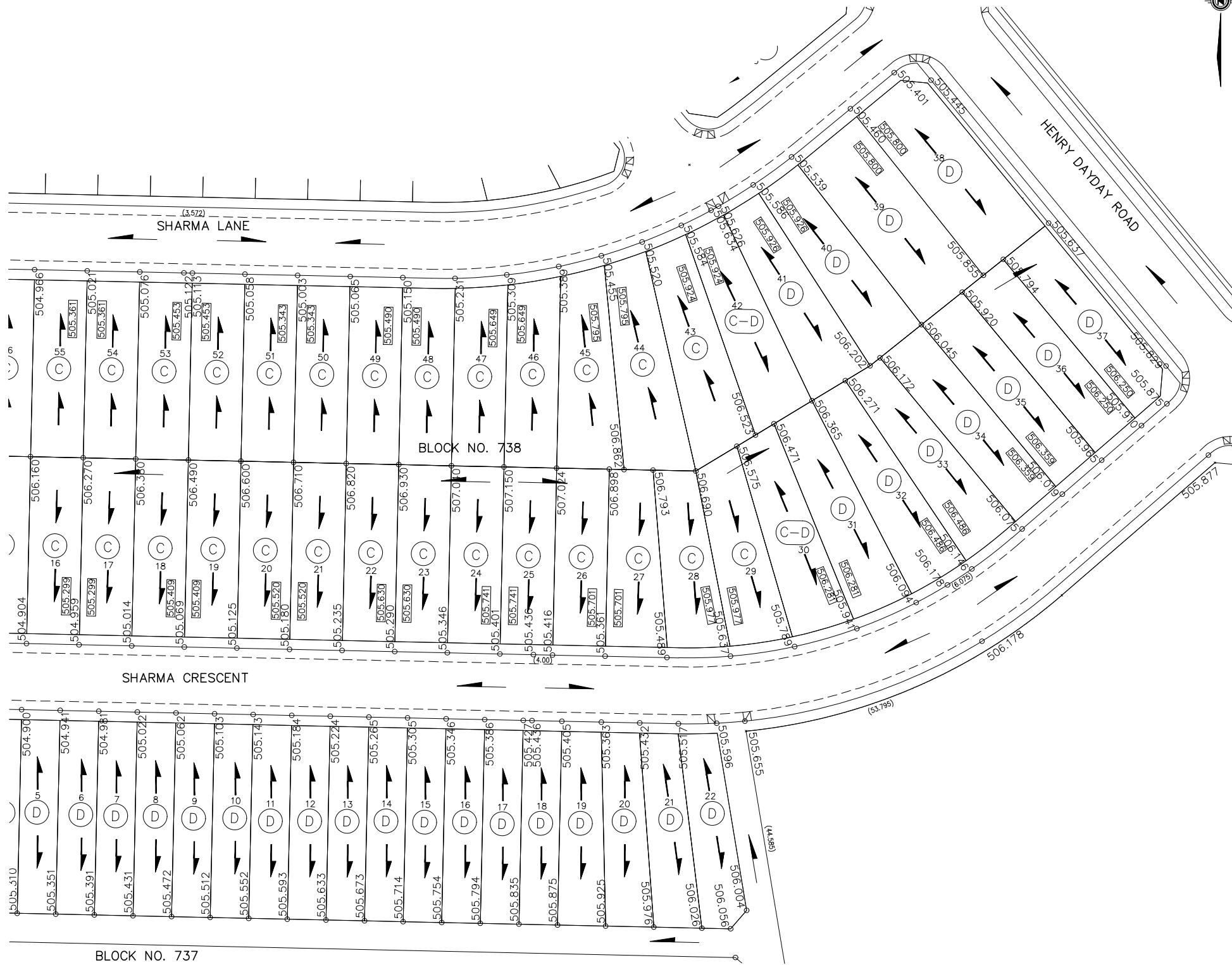
CURB & WALK LEGEND

- ROLLED CURB & WALK
- VERTICAL CURB & WALK
- ROLLED CURB ONLY
- VERTICAL CURB ONLY
- CONCRETE WALKWAY
- EASEMENT POINT NO.
- PROPERTY LINE
- DRAINAGE ARROW
- DRIVEWAY
- PHASE BOUNDARY

CONST.
PROJECT NO. 502-0087-030r001_CWL

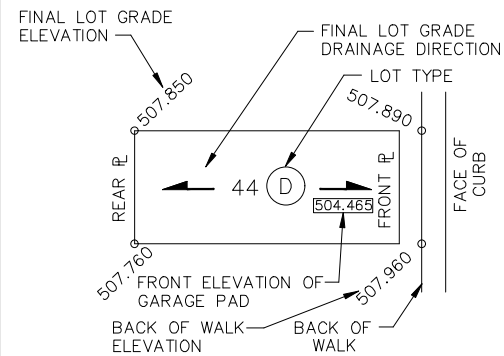
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CHECKED BY:	CHECKED BY:																							
SIGNATURE	SIGNATURE																							
NAME	NAME																							
DATE	DATE																							
DRAWN BY JVS	DWG STDS CHECKED BY																							
DATE 2023-OCT-13	DATE																							
					LOT GRADES		Dec 21, 2023																	
					ASPEN RIDGE STREET, SHARMA CRESCENT,		DATE																	
					SHARMA WAY & SHARMA LANE		SHEET NO. PLAN NO.																	
							112-0087-030r001																	

1	ISSUED FOR CONSTRUCTION	2023-OCT-13	JVS	SEAL
	PLAN DESCRIPTION/REVISION	DATE	BY	



KEY PLAN

LOT DRAINAGE LEGEND

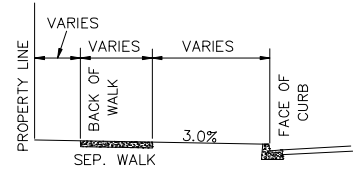


- NOTE:
1. FRONT AND SIDE FLANKAGE LOT GRADES REPRESENT THE DESIGN ELEVATION AT THE BACK OF WALK (B.O.W.)
 2. DIMENSIONS SHOWN IN BRACKETS ARE BETWEEN GRADE CHANGES.

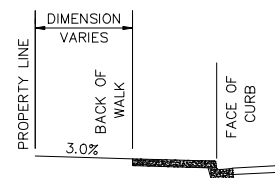
LOT DRAINAGE DETAILS

LOT TYPE 'D' - SEE DRAWING 102-0022-017
LOT TYPE 'E' - SEE DRAWING 102-0022-019

SEPARATE CURB & WALK



COMBINED CURB & WALK

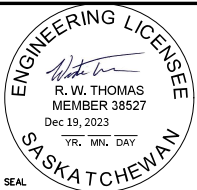


CURB & WALK LEGEND

- ROLLED CURB & WALK
- VERTICAL CURB & WALK
- ROLLED CURB ONLY
- VERTICAL CURB ONLY
- CONCRETE WALKWAY
- EASEMENT POINT NO.
- PROPERTY LINE
- DRAINAGE ARROW
- DRIVEWAY
- PHASE BOUNDARY

CONST.
PROJECT NO. 502-0087-308r001_CWL

PLAN DESCRIPTION/REVISION	DATE	BY	SEAL
1 ISSUED FOR CONSTRUCTION	2023-OCT-13	JVS	



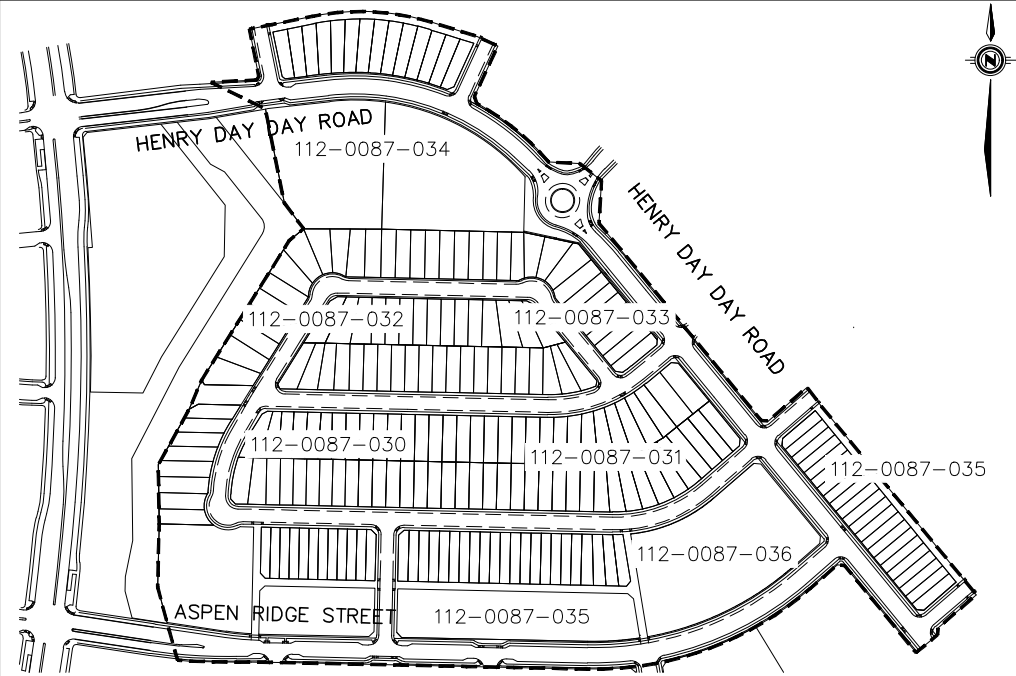
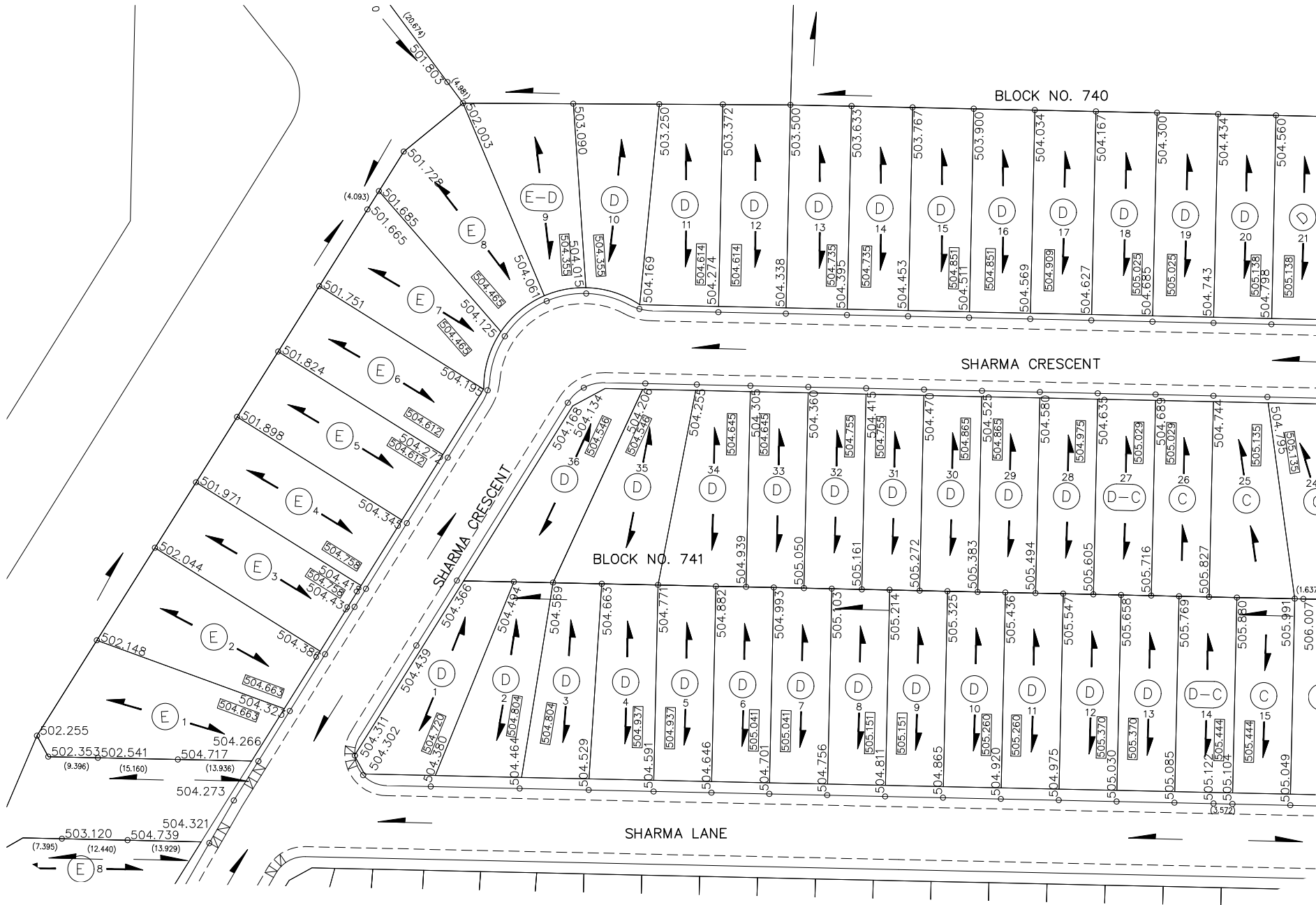
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DATE	DATE
DRAWN BY JVS DATE 2023-OCT-13	DWG STDS CHECKED BY DATE



ASPEN RIDGE LOT GRADES - PHASE D1B

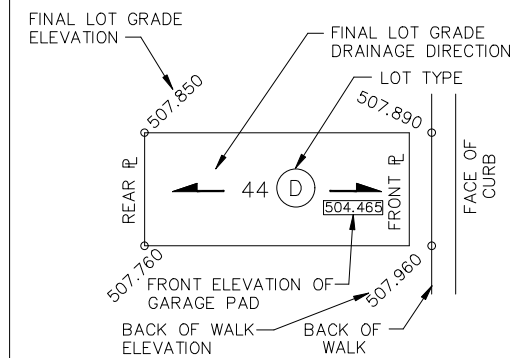
LOT GRADES
SHARMA LANE & SHARMA CRESCENT

ENGINEERING MANAGER	Dec 21, 2023
SCALES: HOR. 1:1000	DATE
VERT.	
SHEET NO.	PLAN NO.
	112-0087-031r001



KEY PLAN

LOT DRAINAGE LEGEND



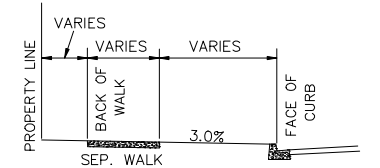
NOTE:

1. FRONT AND SIDE FLANKAGE LOT GRADES REPRESENT THE DESIGN ELEVATION AT THE BACK OF WALK (B.O.W.)
2. DIMENSIONS SHOWN IN BRACKETS ARE BETWEEN GRADE CHANGES.

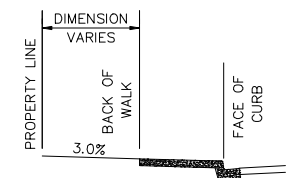
LOT DRAINAGE DETAILS

LOT TYPE 'D' - SEE DRAWING 102-0022-017
LOT TYPE 'E' - SEE DRAWING 102-0022-019

SEPARATE CURB & WALK



COMBINED CURB & WALK

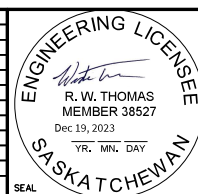


CURB & WALK LEGEND

- ROLLED CURB & WALK
- VERTICAL CURB & WALK
- ROLLED CURB ONLY
- VERTICAL CURB ONLY
- CONCRETE WALKWAY
- EASEMENT POINT NO.
- PROPERTY LINE
- DRAINAGE ARROW
- DRIVEWAY
- PHASE BOUNDARY

CONST.
PROJECT NO. 502-0087-308r001_CWL

PLAN DESCRIPTION/REVISION	DATE	BY	SEAL
1 ISSUED FOR CONSTRUCTION	2023-OCT-13	JVS	



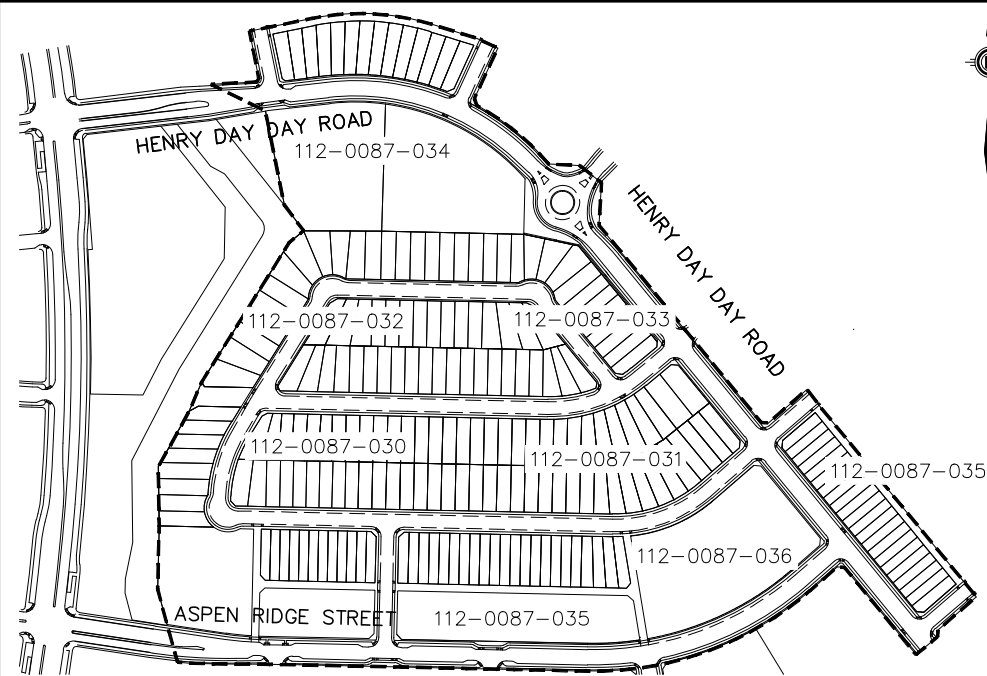
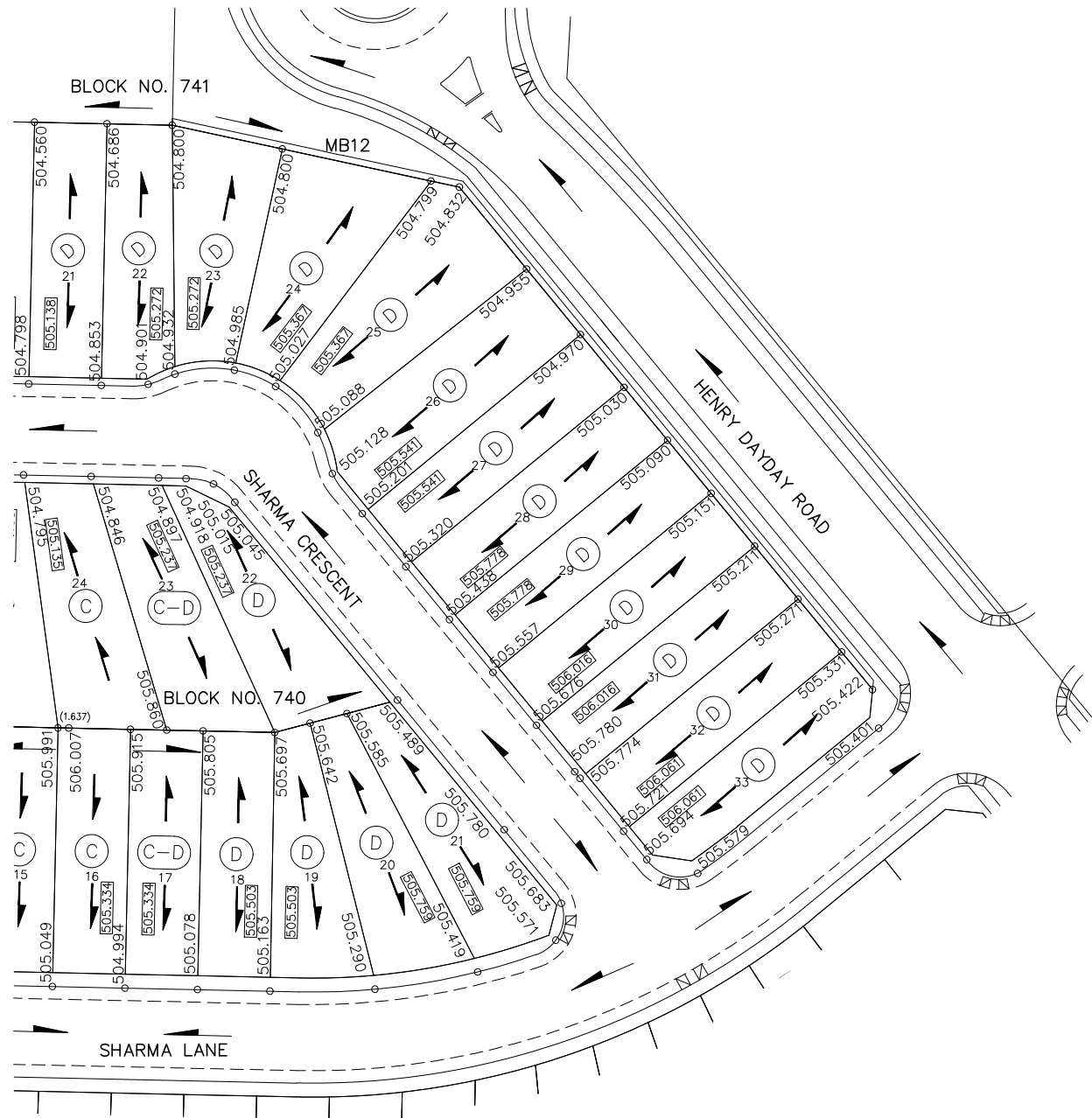
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SIGNATURE	SIGNATURE
NAME	NAME
DATE	DATE
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ASPEN RIDGE LOT GRADES - PHASE D1B

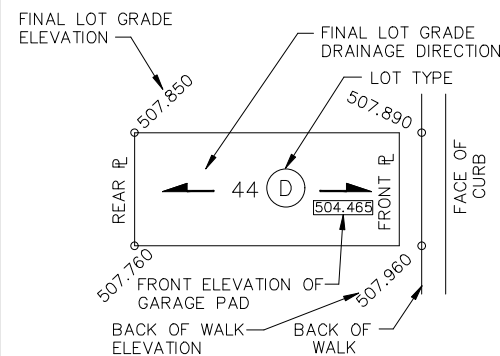
LOT GRADES
SHARMA CRESCENT & SHARMA LANE

ENGINEERING MANAGER	Dec 21, 2023
SCALES: HOR. 1:1000 VERT. 1:1000	DATE
SHEET NO. 112-0087-032r001	PLAN NO.



KEY PLAN

LOT DRAINAGE LEGEND



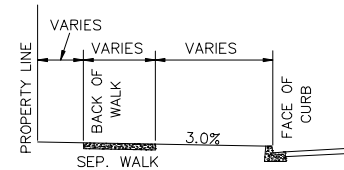
NOTE:

1. FRONT AND SIDE FLANKAGE LOT GRADES REPRESENT THE DESIGN ELEVATION AT THE BACK OF WALK (B.O.W.)
2. DIMENSIONS SHOWN IN BRACKETS ARE BETWEEN GRADE CHANGES.

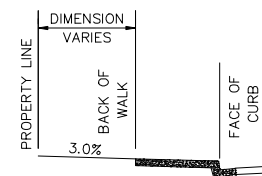
LOT DRAINAGE DETAILS

LOT TYPE 'D' - SEE DRAWING 102-0022-017
LOT TYPE 'E' - SEE DRAWING 102-0022-019

SEPARATE CURB & WALK



COMBINED CURB & WALK

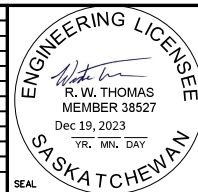


CURB & WALK LEGEND

- ROLLED CURB & WALK
- VERTICAL CURB & WALK
- ROLLED CURB ONLY
- VERTICAL CURB ONLY
- CONCRETE WALKWAY
- EASEMENT POINT NO.
- PROPERTY LINE
- DRAINAGE ARROW
- DRIVEWAY
- PHASE BOUNDARY

CONST.
PROJECT NO. 502-0087-308r001_CWL

PLAN DESCRIPTION/REVISION	DATE	BY	SEAL
1 ISSUED FOR CONSTRUCTION	2023-OCT-13	JVS	



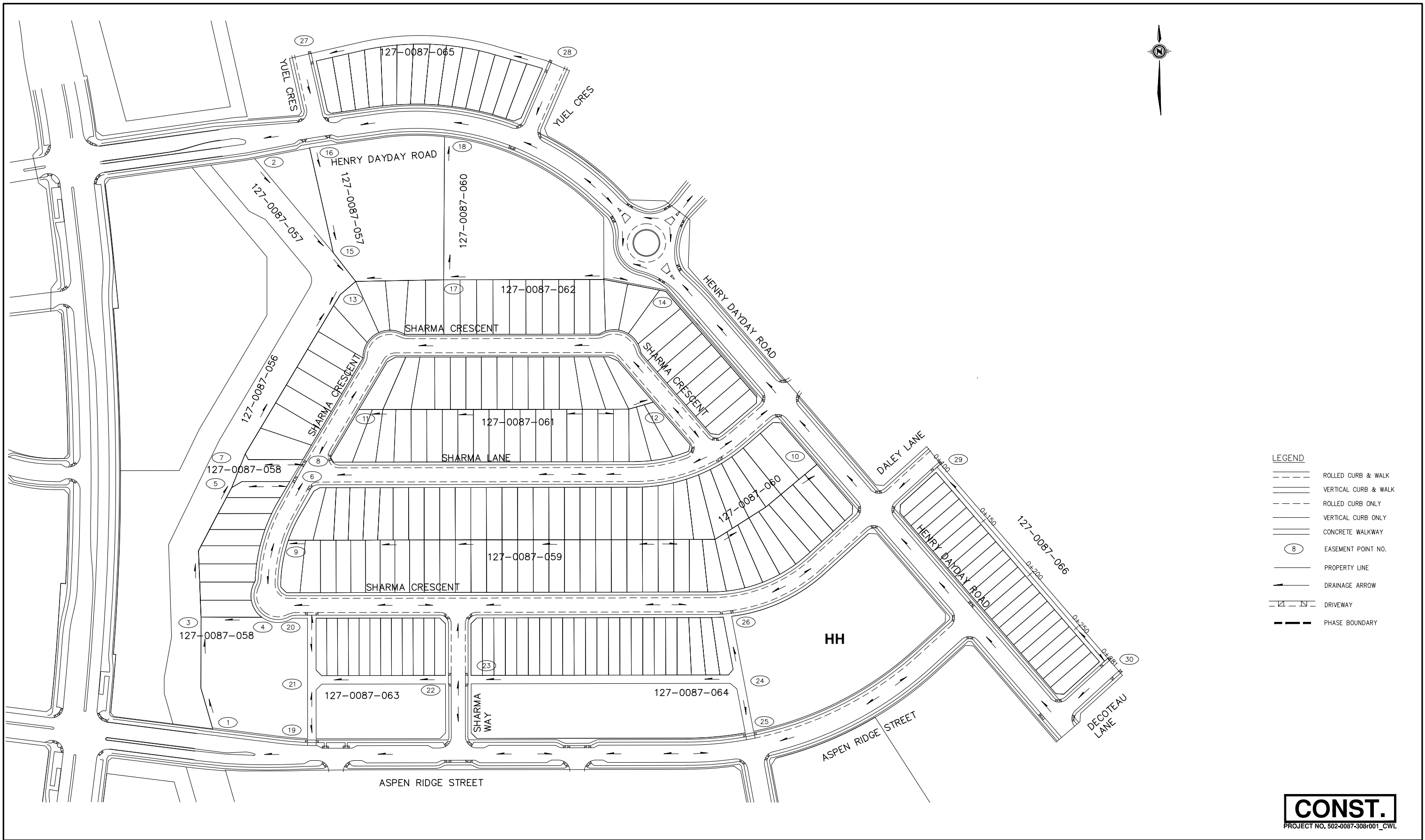
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SIGNATURE	SIGNATURE
NAME	NAME
DATE	DATE
DRAWN BY JVS DATE 2023-OCT-13	DWG STDS CHECKED BY DATE



ASPEN RIDGE LOT GRADES - PHASE D1B

LOT GRADES
SHARMA LANE & SHARMA CRESCENT

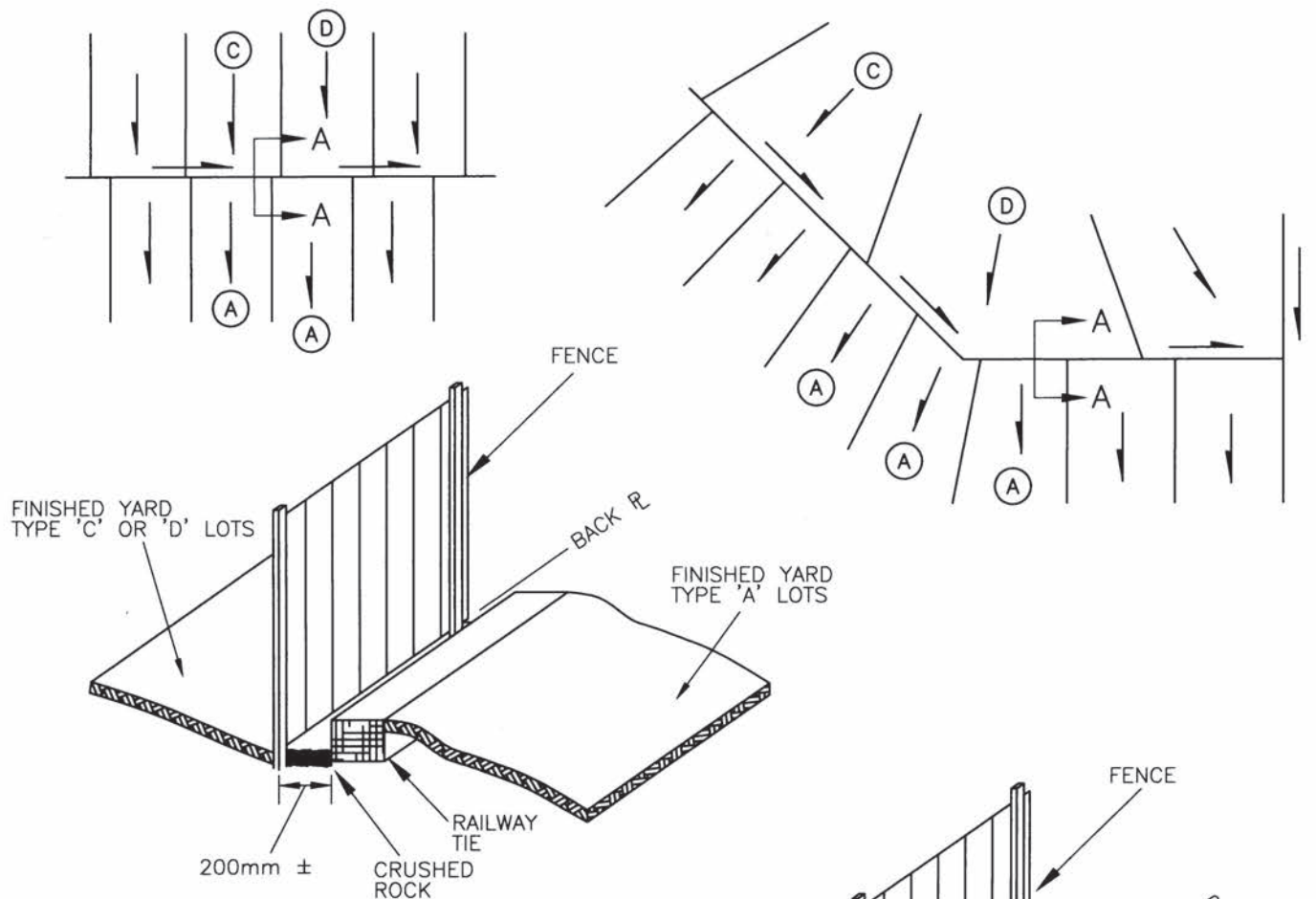
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SCALES: HOR. 1:1000 VERT. 1:1000	Dec 21, 2023 DATE
SHEET NO. 8	PLAN NO. 112-0087-033r001



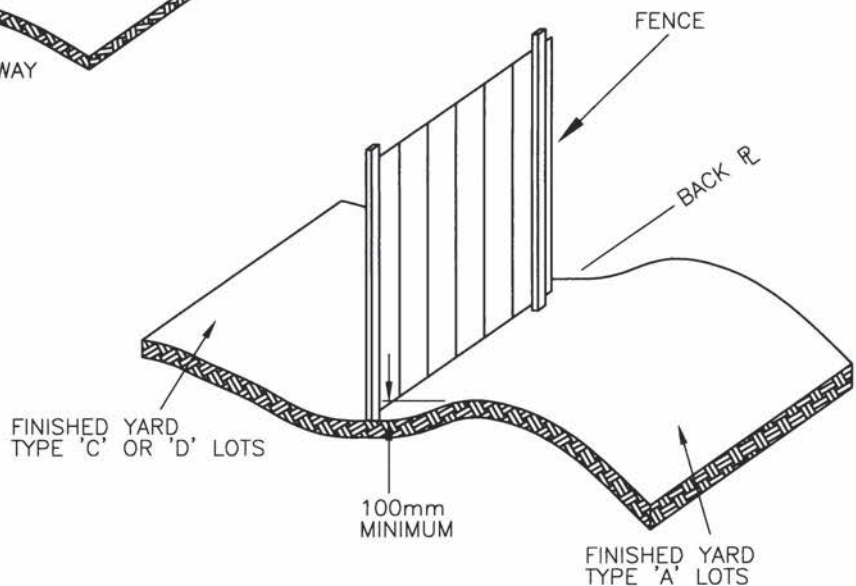
- LEGEND
- ROLLED CURB & WALK
 - VERTICAL CURB & WALK
 - ROLLED CURB ONLY
 - VERTICAL CURB ONLY
 - CONCRETE WALKWAY
 - 8 EASEMENT POINT NO.
 - PROPERTY LINE
 - DRAINAGE ARROW
 - DRIVEWAY
 - PHASE BOUNDARY

CONST.
PROJECT NO. 502-0087-308r001_CWL

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CHECKED BY:	CHECKED BY:																						
SIGNATURE	SIGNATURE																						
NAME	NAME																						
DATE	DATE																						
DRAWN BY JVS	DWG STDs CHECKED BY																						
DATE 2023-OCT-20	DATE																						
						EASEMENT SITE PLAN		Dec 21, 2023															
								DATE															
								SHEET NO. PLAN NO.															
								127-0087-055r001															
1 ISSUED FOR CONSTRUCTION		2023-OCT-13 JVS																					
PLAN DESCRIPTION/REVISION		DATE BY																					



SECTION A-A
OPTION 1



SECTION A-A
OPTION 2

THE PURPOSE OF THIS DRAINAGE SWALE IS TO ENSURE THAT DRAINAGE FROM THE BACK OF THE TYPE 'C' AND 'D' LOTS DOES NOT ENTER THE TYPE 'A' LOTS ON THE OTHER SIDE OF THE EASEMENT.

THE UTILITY KIOSKS LOCATED ALONG THE UTILITY EASEMENTS WILL HAVE THE ELEVATION MARKED TO SHOW THE FINAL GRADE AT THE BACK PROPERTY LINE.

THIS WOULD APPLY TO THE BACK OF ALL TYPE 'A' LOTS.

REVISIONS 1 2 3		CITY OF SASKATOON INFRASTRUCTURE SERVICES DEPARTMENT	APPROVED CITY ENGINEER P. ENG. ENGINEER ENGINEER	
DRAWN BY <u>MRH</u> DATE <u>13/06/02</u> CHECKED BY _____ DATE _____			DRAINAGE SWALE BACK OF LOT, 1 OF 2 SCALES : HOR. <u>N.T.S.</u> VERT. <u>N.T.S.</u> PLAN NO. 102-0022-005r001	

PHASE 1
D1B AREA GRADING



LIFT STATION

FOREBAY

HENRY DAYDAY ROAD

YUEL CRESCENT

YUEL CRESCENT

SHARMA CRESCENT

SHARMA LANE

SHARMA CRESCENT

SHARMA WAY

ASPEN RIDGE STREET



APPROVED FOR
TENDER

TRANSPORTATION & UTILITIES
CITY OF SASKATOON

REVISION No. 1
Feb 04, 2019

THE CONTRACTOR IS RESPONSIBLE
FOR DETERMINING THE LOCATION
AND ELEVATION OF ALL
UNDERGROUND UTILITIES.

CHECKED BY:

CHECKED BY:

DATE

DATE

DRAWN BY

DATE



**City of
Saskatoon**

Transportation & Utilities Department

ASPEN RIDGE LAND DEVELOPMENT PHASE D1B

AREA GRADING

DESIGN CONTOURS

CHIEF ENGINEER

SCALES :

HOR. 1:2500

VERT. 1:2500

SHEET NO.

PLAN NO.

113-0087-324r001

11				
10				
9				
8				
7				
6				
5				
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3				
2				
1	ISSUED FOR TENDER	2019-FEB-04	EDH	SEAL
	PLAN DESCRIPTION/REVISION	DATE	BY	

PHASE 1
D1B AREA GRADING

LEGEND:

- 0.25

0.25

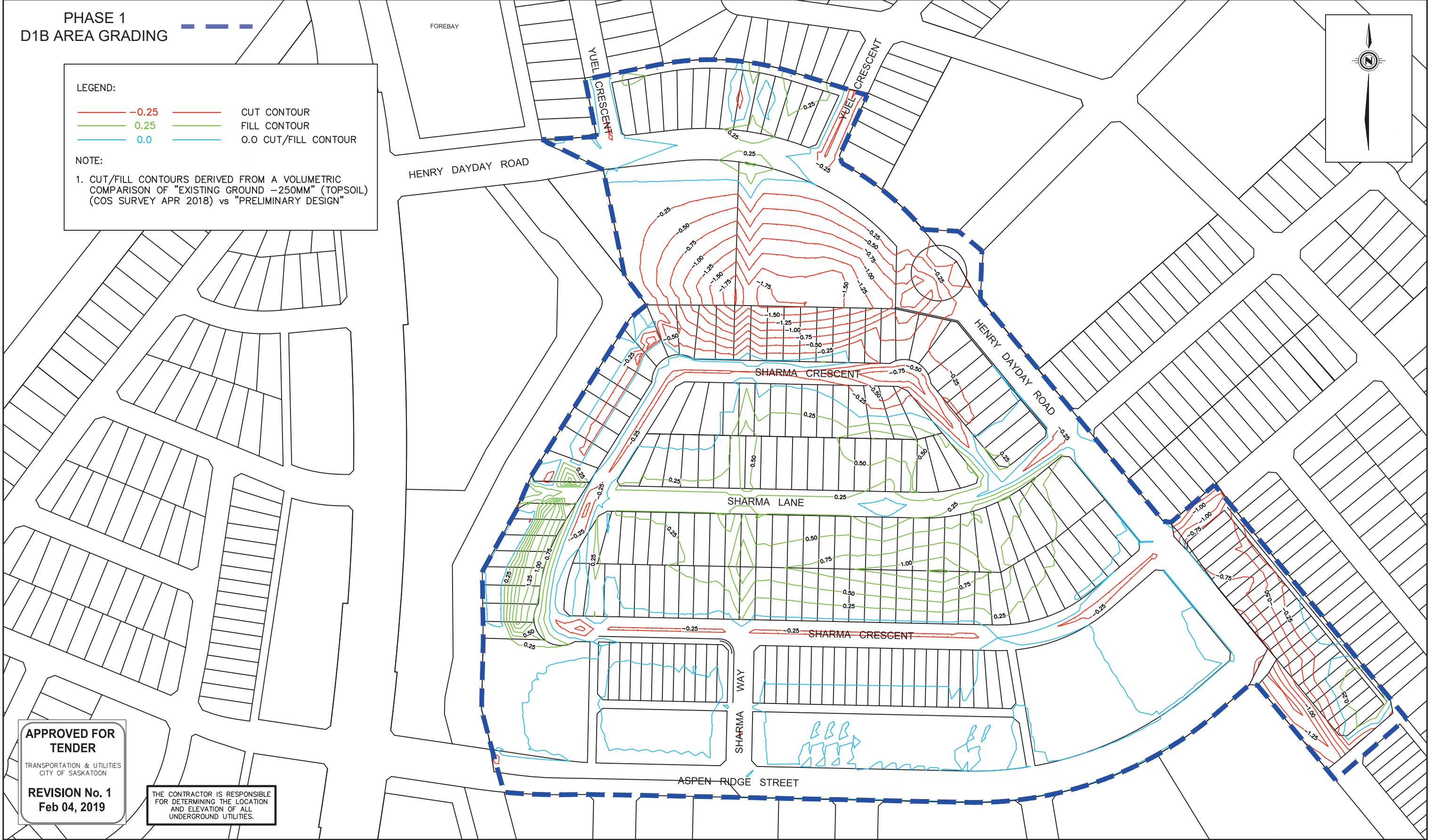
0.0
- CUT CONTOUR

FILL CONTOUR

0.0 CUT/FILL CONTOUR

NOTE:

1. CUT/FILL CONTOURS DERIVED FROM A VOLUMETRIC COMPARISON OF "EXISTING GROUND -250MM" (TOPSOIL) (COS SURVEY APR 2018) vs "PRELIMINARY DESIGN"



APPROVED FOR
TENDER

TRANSPORTATION & UTILITIES
CITY OF SASKATOON

REVISION No. 1
Feb 04, 2019

THE CONTRACTOR IS RESPONSIBLE
FOR DETERMINING THE LOCATION
AND ELEVATION OF ALL
UNDERGROUND UTILITIES.

11				
10				
9				
8				
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4				
3				
2				
1	ISSUED FOR TENDER	2019-FEB-04	EDM	
	PLAN DESCRIPTION/REVISION	DATE	BY	SEAL

CHECKED BY:	CHECKED BY:
DATE	DATE
DRAWN BY:	DATE



City of
Saskatoon

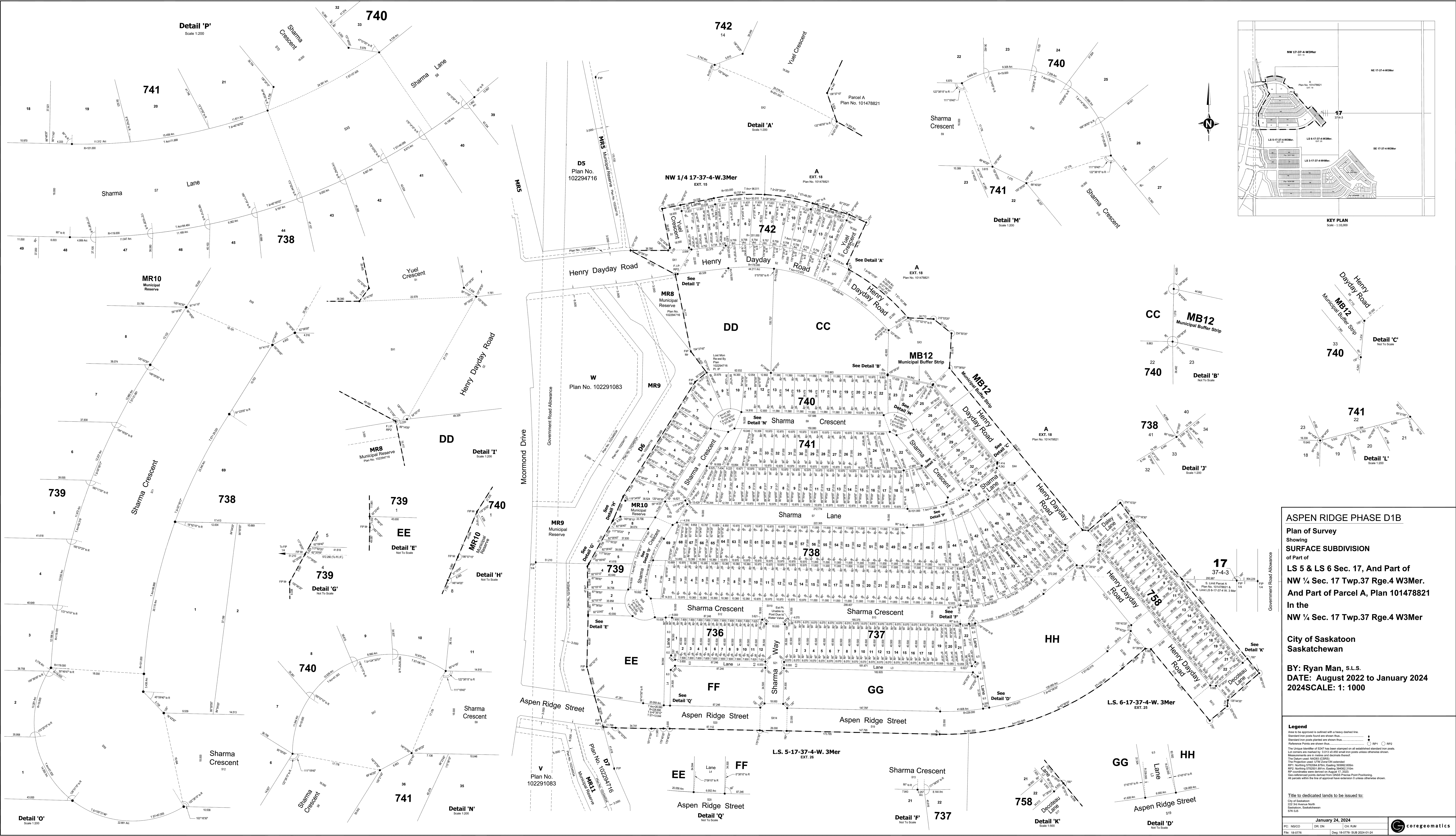
Transportation & Utilities Department

ASPEN RIDGE LAND DEVELOPMENT PHASE D1B

AREA GRADING

CUT & FILL CONTOURS

CHIEF ENGINEER	
SCALES:	
HOR. 1:2500	DATE
VERT. 1:2500	
SHEET NO.	PLAN NO.
	113-0087-325r001



ASPEN RIDGE PHASE D1B
Plan of Survey
SHOWING
SURFACE SUBDIVISION
of Part of
LS 5 & LS 6 Sec. 17, And Part of
NW ¼ Sec. 17 Twp.37 Rge.4 W3Mer.
And Part of Parcel A, Plan 101478821
In the
NW ¼ Sec. 17 Twp.37 Rge.4 W3Mer

City of Saskatoon
Saskatchewan

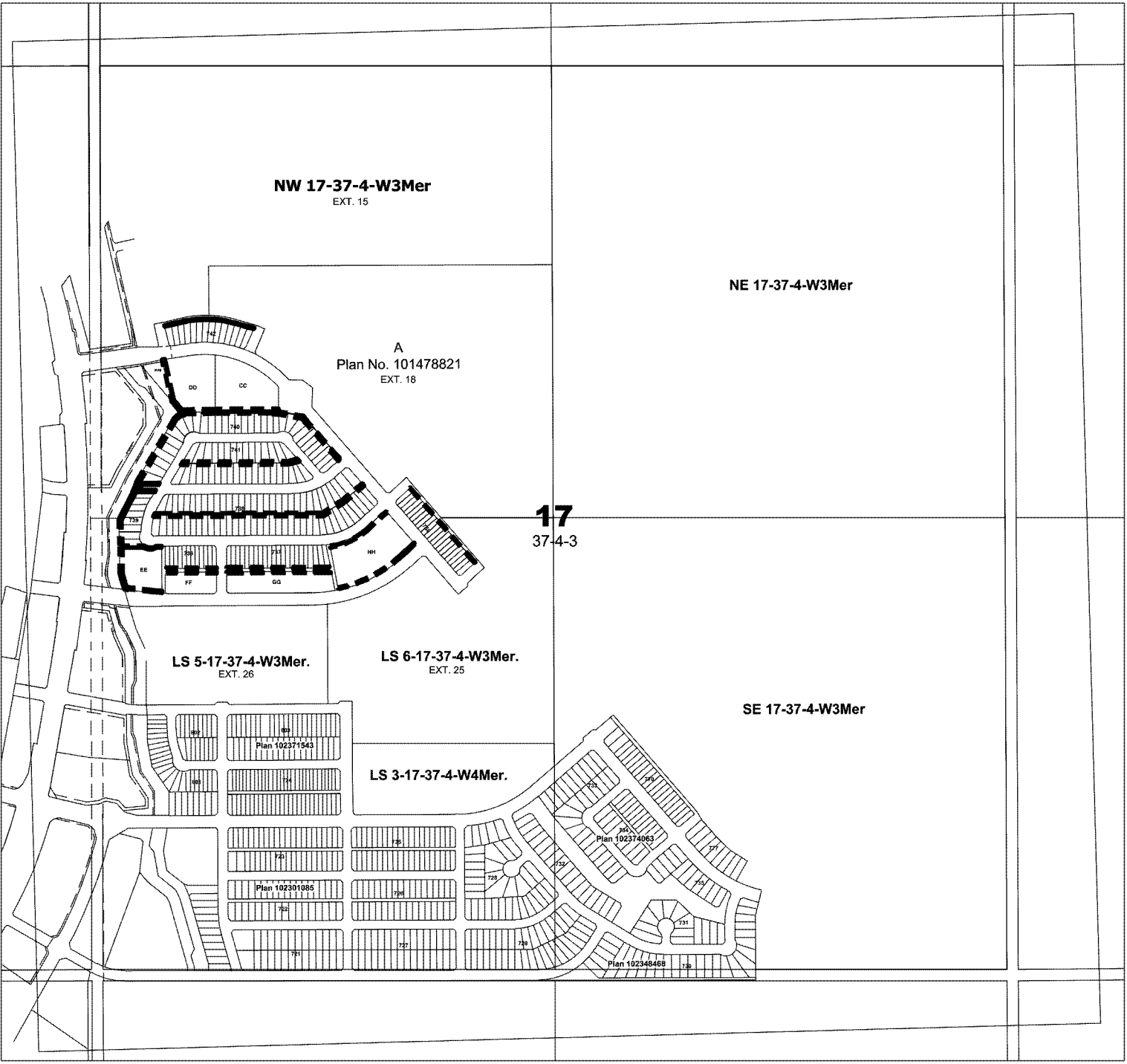
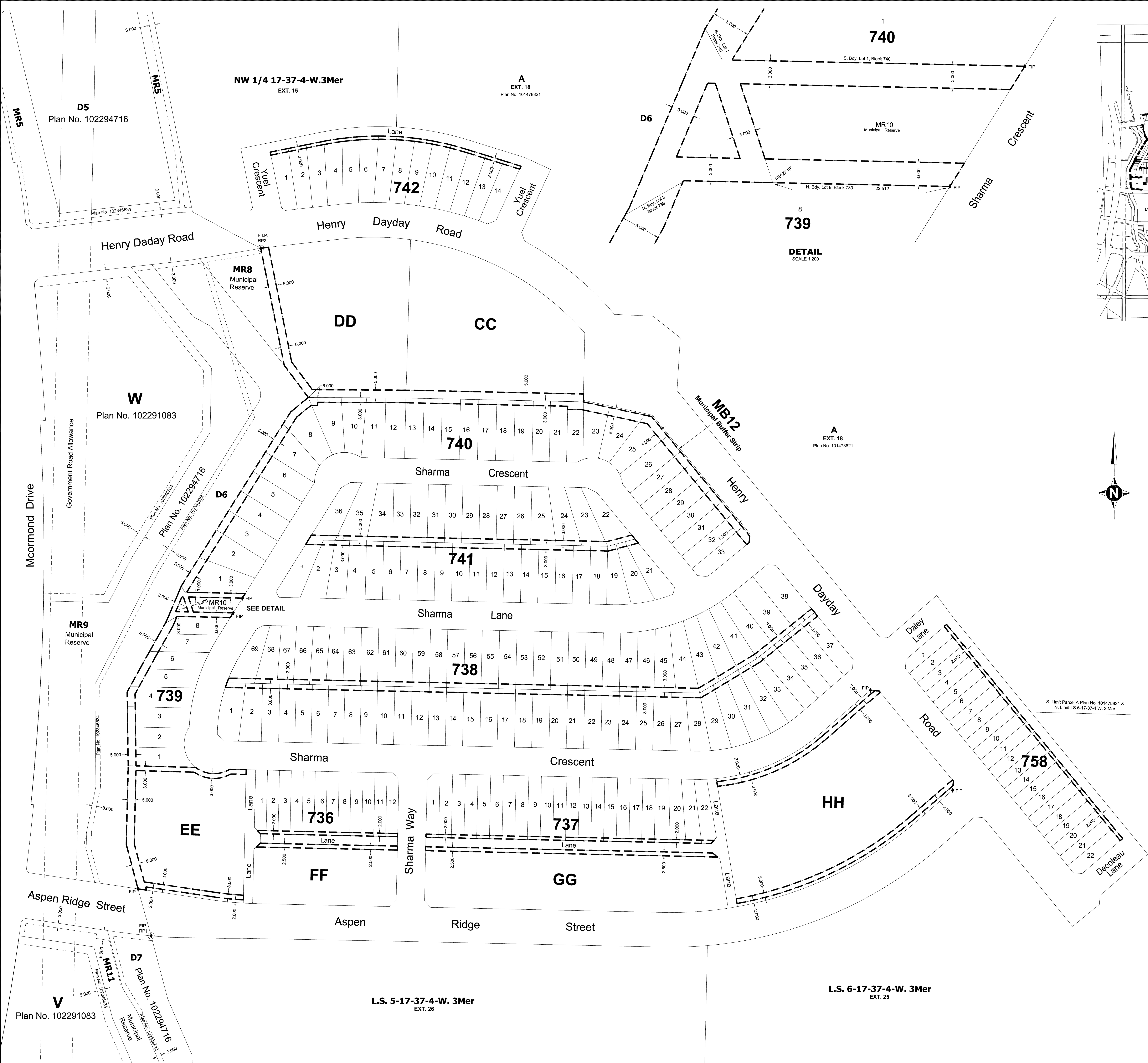
BY: Ryan Man, S.L.S.
DATE: August 2022 to January 2024
2024SCALE: 1: 1000

Legend
Area to be acquired is outlined with a heavy dashed line.
Standard iron posts found are shown thus:
Standard iron posts planted are shown thus:
Reference Points are shown thus: RP1 RP2
The Unique Identifier of 2247 has been stamped on all established standard iron posts.
Lot corners are marked by 0.013 or 400 small iron posts unless otherwise shown.
Measurements are in metres and decimal fractions thereof.
The datum used: NAD83 (CSRS).
The Projection used: UTM Zone 18N.
RP1: Northing 5702064.87m; Easting 309962.00m
RP2: Northing 5702061.80m; Easting 309602.00m
RP coordinates were derived on August 17, 2023.
Geo-referenced points derived from GNSS Precise Point Positioning.
All parcels within the line of approval have extension 0 unless otherwise shown.

Title to dedicated lands to be issued to:
City of Saskatoon
222 3rd Avenue North
Saskatoon, Saskatchewan
S7N 0Z5

January 24, 2024
PC: NS/CO DR: DN CH: RM
Fax: 16-0776 Draw: 16-0776-SUB 2024-01-24

coreometrics



ASPEN RIDGE PHASE D1B




DESCRIPTIVE PLAN - TYPE II

Showing
FEATURE UTILITY RIGHTS OF WAY

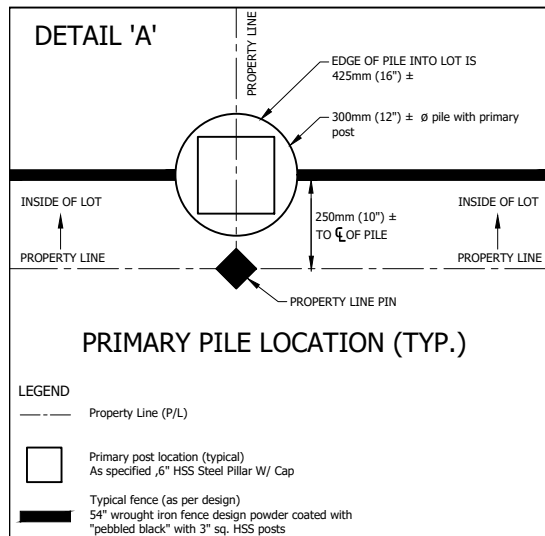
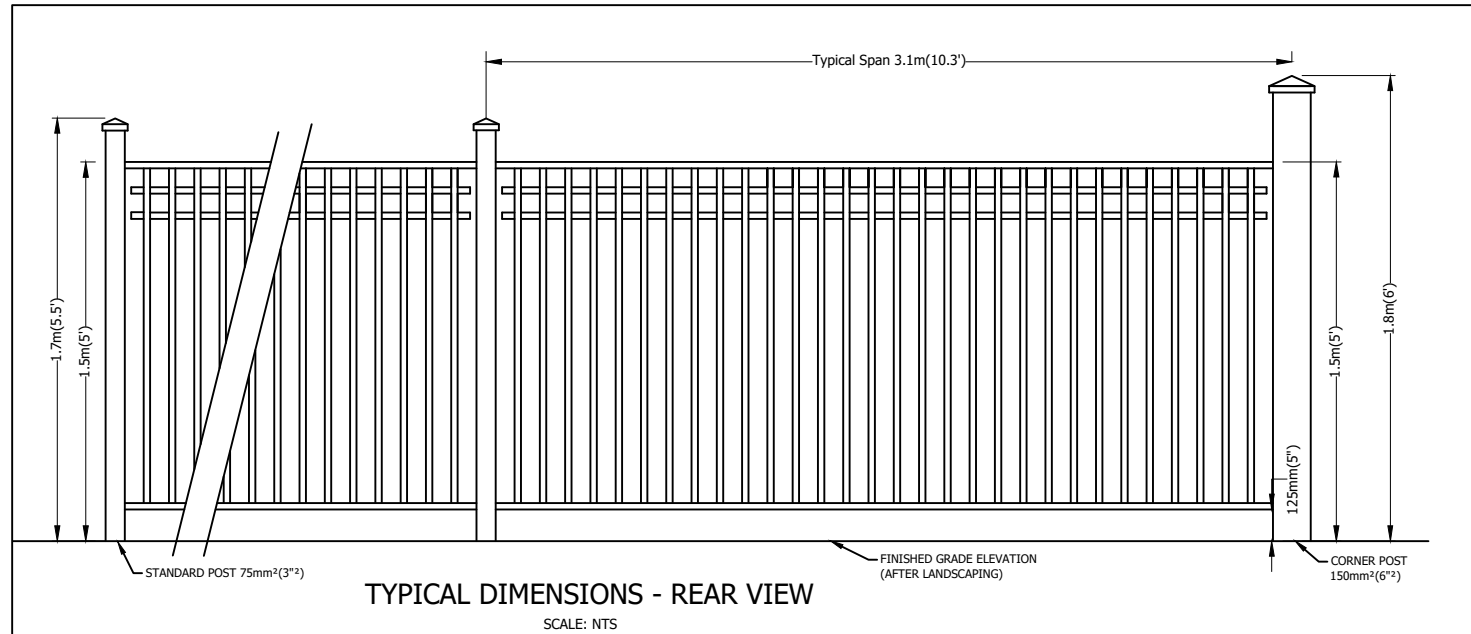
Within
Lots 1-12, Block 736 &
Lots 1-22, Block 737 &
Lots 1-4, Block 739 &
Lots 9-22 Block 758 &
Parcels EE, FF, GG & HH
Plan 102415812
In
S.W. 1/4 Sec.17 Twp. 37 Rge.4 W3Mer.
And
Lots 1-69, Block 738 &
Lots 1-9, Block 758 &
Lots 4-8, Block 738 &
Lots 1-33, Block 740 &
Lots 1-36, Block 741 &
Lots 1-14, Block 742 &
Municipal Reserve MR10 &
Parcels CC, DD and HH
Plan 102415812
In
N.W. 1/4 Sec.17 Twp. 37 Rge.4 W3Mer.

City of Saskatoon
Saskatchewan

BY: Ryan Man, S.L.S.
DATE: December 14, 2023
SCALE: 1: 1000

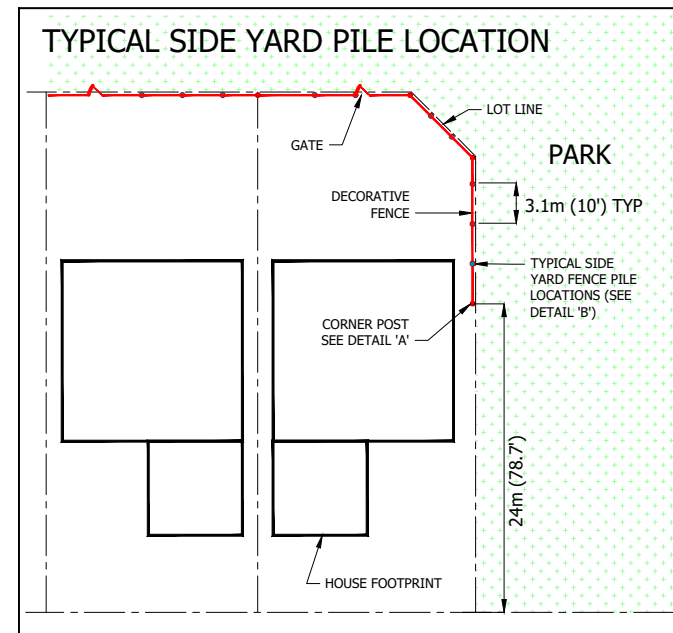
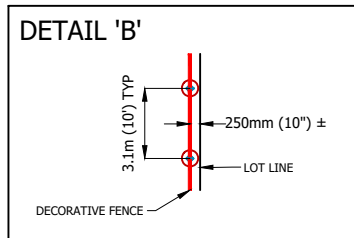
Legend
Area to be approved is outlined with a heavy dashed line.
Standard iron posts found are shown thus: 
Reference Points are shown thus:  RP1  RP2
Measurements are in metres and decimals thereof.
The Datum used: NAD83 (CSRS)
The Projection used: UTM Zone 13N extended
RP1: Northing 578264.87m; Easting 393992.805m
RP2: Northing 5782501.891m; Easting 394082.310m
RP coordinates were derived on August 17, 2023.
Geo-referenced points derived from GNSS Precise Point Positioning.
All parcels within the line of approval have extension 0 unless otherwise shown.

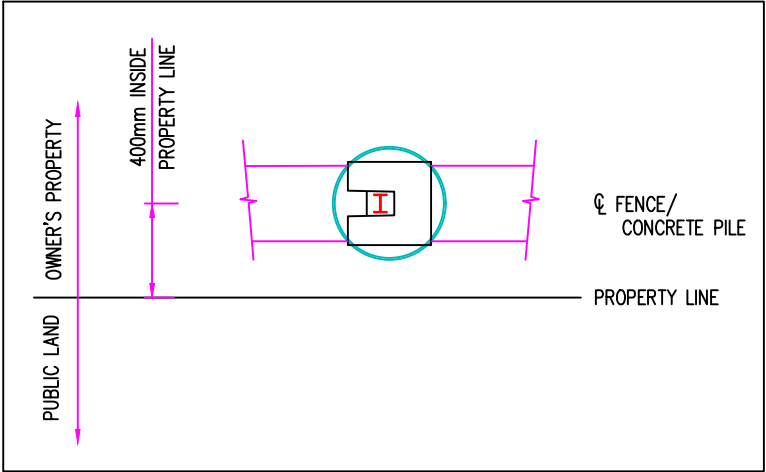
Aspen Ridge No Gate Decorative Fence Details



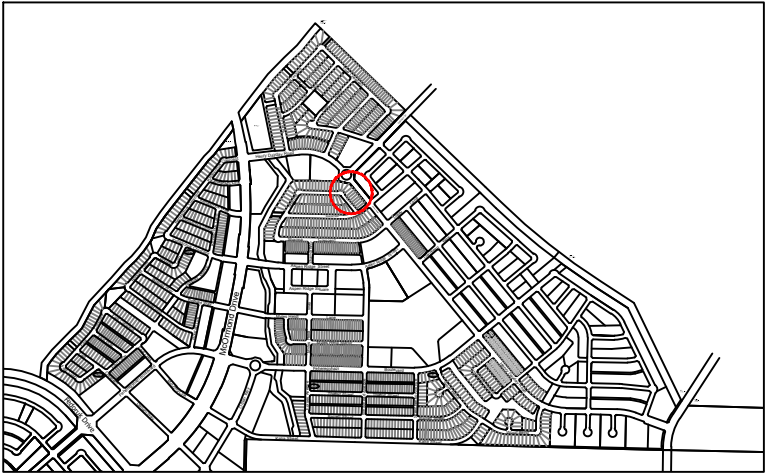
NOTES:

- DO NOT DISTURB fence piles during basement excavation
- Centerline of fence is APPROXIMATELY 250mm (10") ± inside of lot. THE ACTUAL DISTANCE MAY VARY
- SIDEYARD FENCE SITUATION:
Please determine exact location of fence prior to finalizing house width

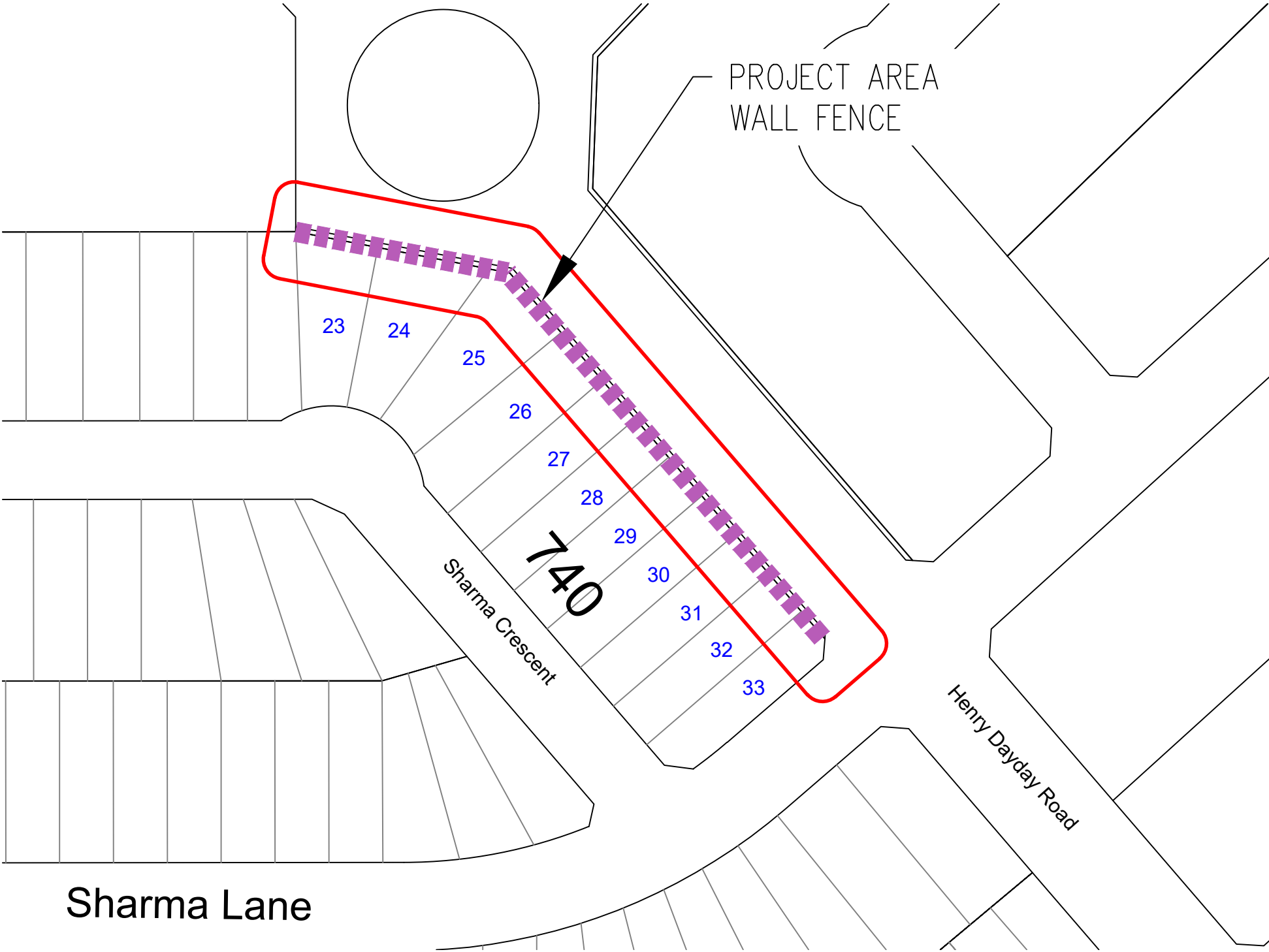




PROPERTY LINE DETAIL
SCALE: 1:20



ASPEN RIDGE LOCATION MAP
SCALE: NTS



SHARMA FENCE SITE PLAN
SCALE: 1:1000

LEGEND:

- WALL FENCE
- PROJECT AREA
- MUNICIPAL BUFFER (MB12)
- LOT AND BLOCK NUMBER

23

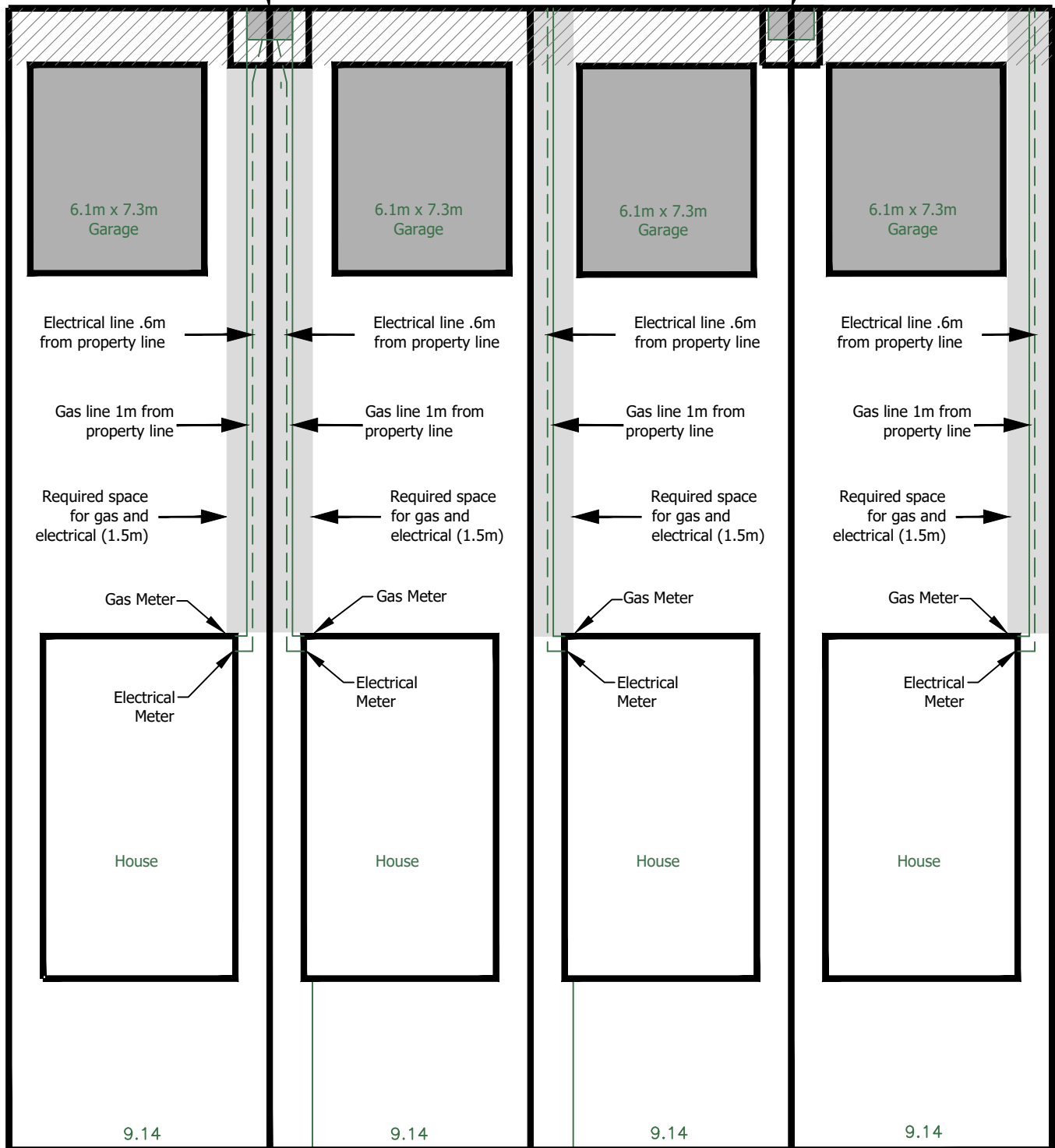
Note: The information contained on this map is for illustrative purposes only and should not be used for legal purposes. All the information was compiled from available sources and the line work is subject to change without notice. Parks and buffer renderings are for illustrative purposes only and does not represent what will be constructed.

Electrical Pedestal
SaskPower Easement Each Lot
1.375m x 2m

6 metre wide rear lane (paved)

Transformer (approx. one
perblock) SaskPower Easement
Each Lot 1m x 2m

Attachment 10



LEGEND



2m EASEMENT

Street Right-of-Way

Servicing From Lane SaskPower and SaskEnergy Underground Lines

Notes:

- This drawing is intended as a guide only. For more accurate information please contact the respective utilities and the City of Saskatoon Building Standards Branch.
- For the purpose of this illustration, each lot is shown as 9.14m wide and 40m deep.
- A garage, or other accessory building may not be built over the gas line or electrical service.
- The Land Branch takes no responsibility regarding the legality of the building layout on your lot.

Joint Servicing Initiative

January 2015



■ Background of the Joint Servicing Initiative

To support the thriving economy currently being experienced in Saskatchewan; SaskEnergy, SaskPower and SaskTel are working on refining and broadening the process of a “joint service installation” approach for urban residential home builders.

This process will provide a timely, cost-effective and coordinated service to home builders that includes the installation of gas, electric and communications in a common trench from the main line to the dwelling. This collaborative approach will achieve efficiencies for the installation of the urban service lines and address challenges associated with space constraints for new lots.



■ What You Need to Know About Joint Trench Lots

Joint Trench Lots are for shallow utility services in urban residential areas (single family homes) where the electric, gas, and communication lines are all installed in a common trench. Currently joint trench installation is only being done in Regina, Saskatoon, Warman, Martensville and Dalmeny.

If you have purchased a lot in any of these locations, a contract crew will be installing all shallow utilities in one trench from the main line at the back of the lot to the dwelling.

To prepare for the installation of your shallow utilities please refer to the Site Check Readiness list on the following page.



Index:

- Background
- Joint trench lots
- Site Check Readiness
- Procedures for applying for services

Site Check Readiness

To ensure installation of your service is completed when scheduled, your site must be ready for the crew on the date you indicate on the application (site ready date). Along with your application, please submit a plot plan for the address or addresses you are applying for.

Site readiness includes the following:

1. Provide a plot plan that indicates the location of the residence within the property.
2. House number must be visible from the street.
3. In order to secure the natural gas bracket, a 24" x 10" pressure treated board must be in place that does not contravene any natural gas codes.
4. You must maintain a .9 metre (3 ft) clearance around the natural gas service regulator with any exhaust vents, opening windows or doors. Please refer to your mechanical contractor for appropriate codes of other intake clearances.
5. The area around the house is backfilled and the lot is to within 150 mm (6") of finished grade. To ensure proper installation routing, customers are asked to ensure that property pins are in place and marked for easy locating by our field staff.
6. Utility access within the site must meet the following requirements:
 - i) Access is required for equipment to get into the yard(s) where the work needs to occur (trencher, mini hoe, etc.), clear of buildings, fences, decks, etc.
 - ii) A clear path is maintained for the trench route from the metering points to the takeoff points. The width needs to be enough to operate small trenchers and mini hoes at a minimum in ideal soil conditions, and larger equipment when frozen or rocky conditions exist. The trench is to be at least 0.6 metres (2 ft) off of the parallel property line (for fencing) and at least 0.6 metres (2 ft) wide to ensure separation of facilities in the trench. Further width is often required at surface to slope trench during installation for safe trenching rules. This will require approximately 2 metres (6.5 ft) clear access along the property line to the meter (electric and gas) boards to allow for construction of the facilities.
 - iii) Be aware that any pads or foundations near this route may slump with settling of the trench.
7. The trench is from the pedestal or pole to the meter location(s) (typically the closest corner from the pedestal or pole to the house). This service route must be clear of debris or obstructions, such as dirt piles and lumber.
 - i) SaskEnergy and SaskPower reserve the right to determine the meter location due to physical impediments that may restrict access for personnel and equipment. Alternate meter locations must be pre-approved prior to construction.
8. If separate trenches are utilized, the natural gas trench (SaskEnergy) must be at least 1 metre (3 ft) in distance from the SaskPower trench.
9. In instances where both gas, electric and communication cables are installed in the same trench (currently only in Regina, Saskatoon, Warman, Martensville and Dalmeny), you must leave a 1.2 metre (4 ft) corridor adjacent to the property line for utilities. If the minimum of 1.2 metres is not maintained, an encroachment over the gas service may occur and you will be responsible for reparation and subsequent cost.
10. Your electrician has attached an energization sticker on the meter socket indicating the service is ready for connection. The sticker ensures:
 - i) An electrical permit has been obtained
 - ii) The main panel is connected and in the open (off) position
 - iii) The service is grounded and ready to be connected to SaskPower's electrical system
11. To facilitate your service connection, please notify SaskPower once your electrician has affixed the energization sticker.





Urban Applications for Services

SaskEnergy and SaskPower have been working on a process to streamline our customers application experience. Beginning February 17, 2015, you will be able to make application for gas and electric urban services by utilizing either one of the Crowns' websites. The information will be shared between the two Crowns using a secure file transfer protocol.

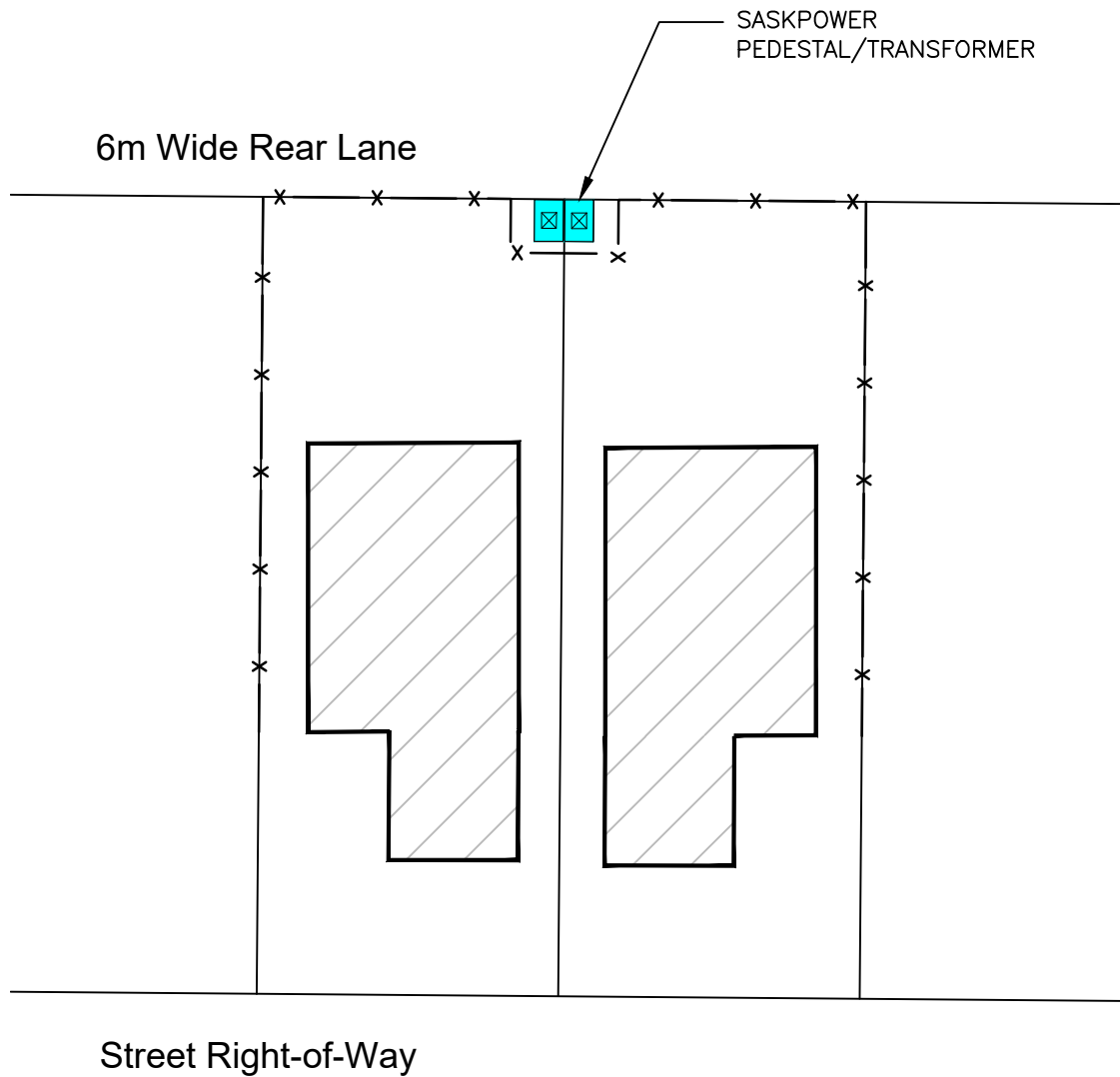
We encourage you to make application via SaskPower's website rather than SaskEnergy's. This will enable the Crowns to eliminate some manual processes that would otherwise occur when applying via SaskEnergy's website.

When you know the date that your property will be ready for servicing, you can make application to either SaskEnergy or SaskPower by the following methods:

- If you use SaskEnergy's application, please apply online at saskenergy.com
- If you use SaskPowers application, please apply online to saskpower.com or call 1-888-SKPOWER (1-888-757-6937) and select Option 4.
- For your telephone and cable providers (SaskTel, Shaw or Access) the conduit will be installed in the joint trench along with SaskPower and SaskEnergy. It is the responsibility of the homeowner to apply for telephone and cable service to one of these providers.

Even though Joint Installation is only being offered in Saskatoon, Regina, Warman, Martensville, and Dalmeny all urban applications for service will be shared between the crowns.





LEGEND

— x — FENCE LOCATION — UTILITY PEDESTALS AND TRANSFORMERS MUST BE ACCESSIBLE FROM REAR LANE AND BE LOCATED OUTSIDE FENCE AS SHOWN

Servicing From Lane
SaskPower and SaskEnergy
Underground Lines

Notes:

- This drawing is intended as a guide only. For more accurate information please contact the respective utilities and the City of Saskatoon Building Standards Branch.
- A garage, or other accessory building may not be built over the gas line or electrical service.
- The Land Branch takes no responsibility regarding the legality of the building layout on your lot.

Blocks 736 and 737 Sharma Crescent

Water and Sewer Connection Requirements

Please refer to the Design and Development Standards Manual, Section 7, Part 3

This manual can be found on the City of Saskatoon website (www.saskatoon.ca)

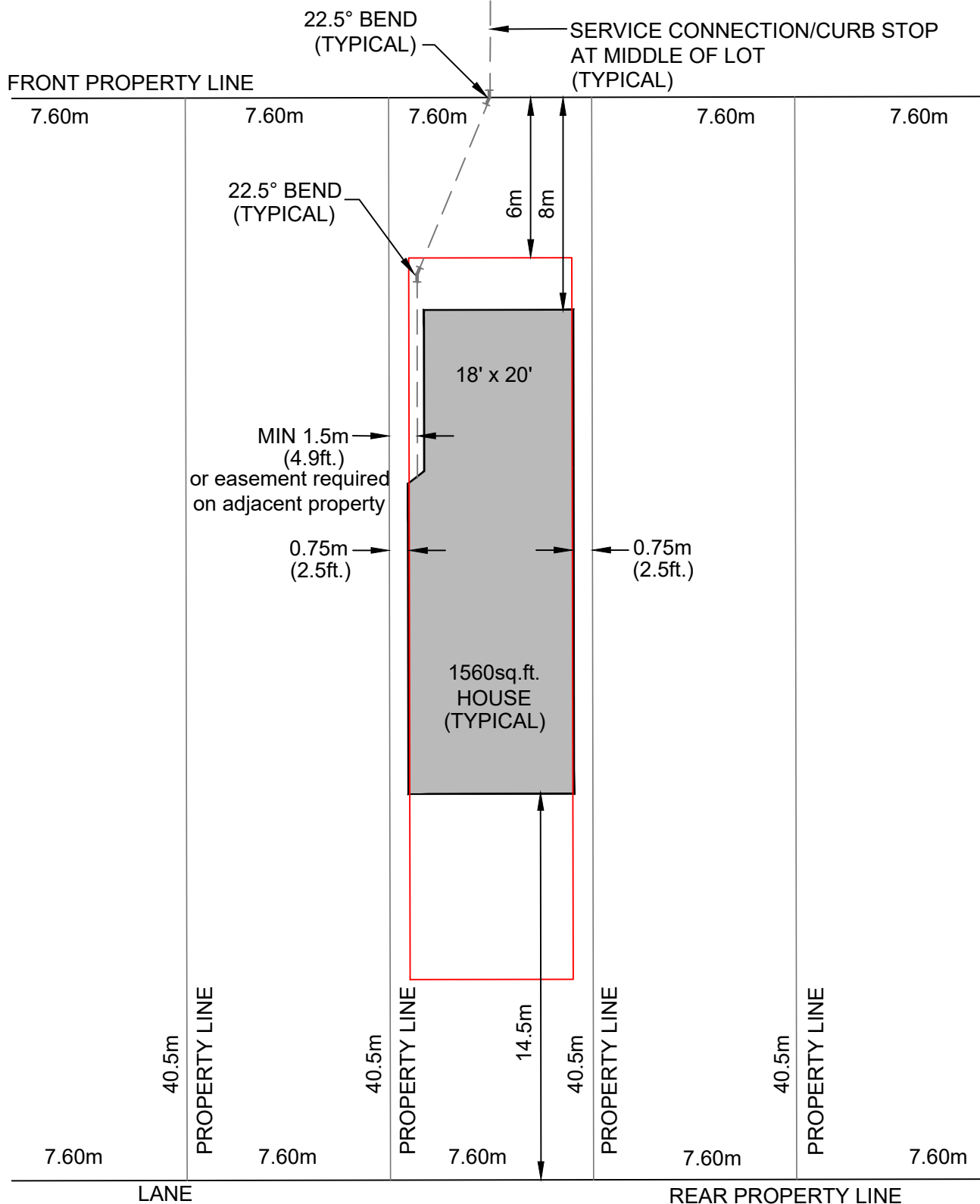
- Click on Business & Development along the top
- Under Development Regulation - Click on Specifications and Standards
- Click on Design & Development Standards Manual

If the dwelling needs to be less than 1.5 meters from the property line and the entire frontage of dwelling is garage, there are a few alternatives that could be done on future installations to meet City Specification and Standards for similar buildings/situations:

- A water and sewer connection can be installed less than 1.5 meters from the side property line if both adjacent lot owners agree to registering an easement on the adjoining property.
- The water and sewer connection could be installed under the garage, given that the following is met:
 - For the sewer connection the licensed water and sewer contractor installs the sewer up to the front of the garage only, and the sewer connection under the garage is installed by a licensed plumbing contractor to meet plumbing code requirements. As part of the sewer plumbing installation, the piping materials, connections, cleanouts, and air testing need to meet the plumbing code.
 - The water connection can be installed by the licensed water and sewer contractor under the garage up to the foundation, or the licensed water and sewer contractor can stop installing the water connection at the garage front where they stopped installing the sewer and leave enough polyethylene pipe for the plumbing contractor to complete the water installation at the same time as the sewer connection installation under the garage.

The main reason for the 1.5 meter side yard requirement is for future maintenance. When connections require maintenance or replacement in the future there needs to be enough room for the excavation without effecting the adjacent property, unless an easement on that adjoining property is in place.

SHARMA CRESCENT



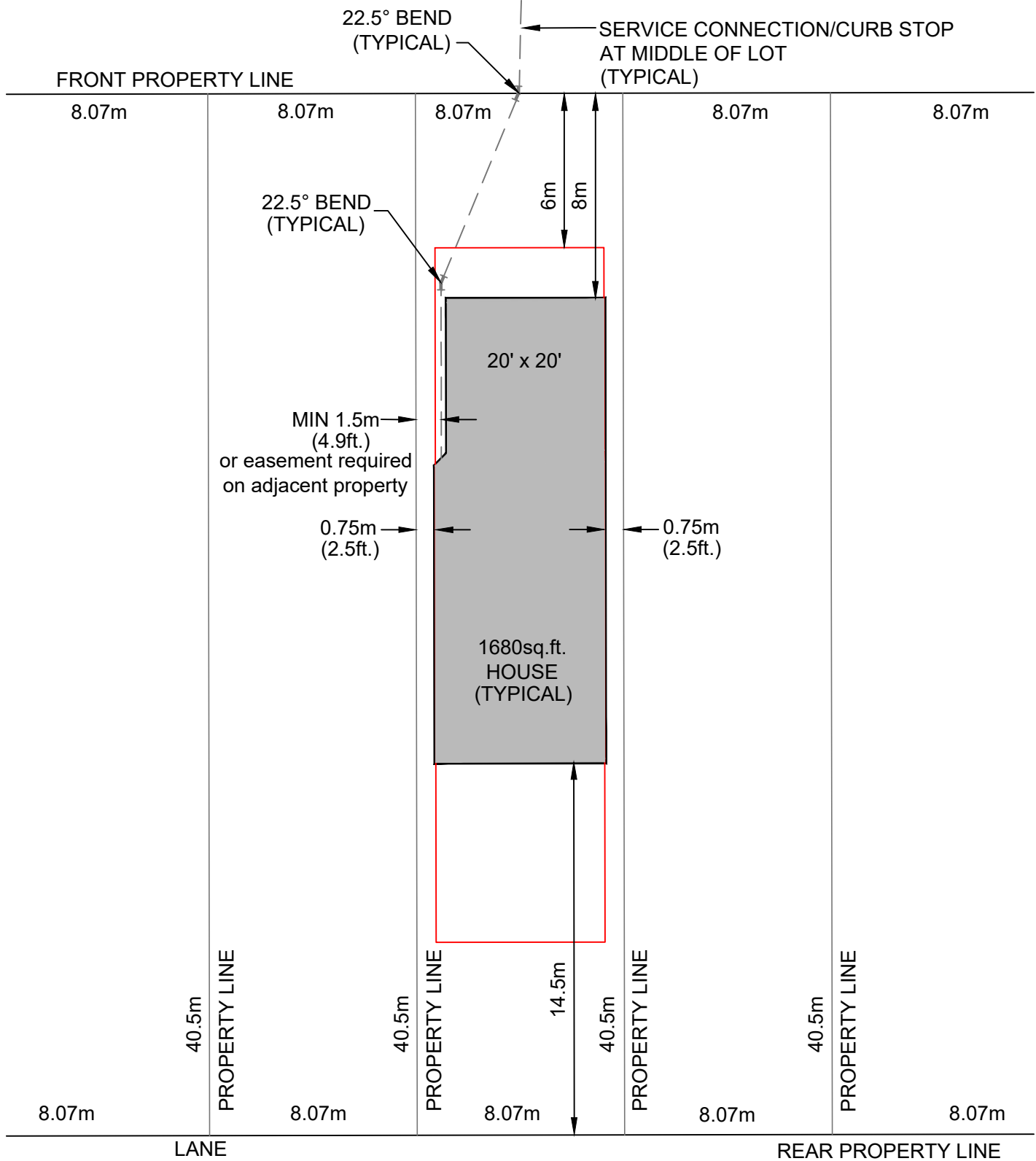
Notes:

1. Service connection is stubbed from street to front property line at middle of lot.
2. Use of two 22.5° bends will be required to ensure service connections miss the attached garage.
3. Service connections should be a minimum of 1.5m from side yard property line.
4. Any water utility access point (curb stop shut off) located within a driveway will be required to be encased in a valve casing and cap prior to pouring the driveway.
5. 45° long sweep bends may be allowed to achieve proper service connection spacing.
6. Service connections cannot be installed underneath the attached garage.
7. Any inquiries should be directed to the City of Saskatoon Connections Desk at 306-975-1475.

I acknowledge receipt of this appendix

please initial here

SHARMA CRESCENT



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