

## **ORDER OF BUSINESS**

### **REGULAR MEETING OF CITY COUNCIL**

**WEDNESDAY, AUGUST 15, 2012 AT 6:00 P.M.**

1. **Approval of Minutes** of meeting held on July 18, 2012.
  
2. **Public Acknowledgements**
  
3. **Hearings (6:00 p.m.)**
  - a) **Proposed Rezoning from FUD District and R1A District to R1B District  
302 to 358 and 303 to 351 Rosewood Boulevard West  
Rosewood Neighbourhood  
Applicant: City of Saskatoon, Land Branch  
Proposed Bylaw No. 9037  
(File No. CK. 4351-012-8)**

The purpose of this hearing is to consider proposed Bylaw No. 9037.

Attached is a copy of the following material:

- Proposed Bylaw No. 9037.
  
- Report of the General Manager, Community Services Department dated May 15, 2012, recommending that the proposed amendment to the Zoning Bylaw No. 8770 to rezone 302 to 358 and 303 to 351 Rosewood Boulevard West from FUD – Future Urban Development District, and R1A – One-Unit Residential District, to R1B – Small Lot One-Unit Residential District, be approved.
  
- Clause 1, Report No. 3-2012 of the Municipal Planning Commission, which was adopted by City Council at its meeting held on June 18, 2012;
  
- Notice that appeared in the local press on July 28, 2012.

**b) Proposed Official Community Plan Amendments  
Urban Holding to Residential; and Phase 2 to Phase 1  
Kensington Neighbourhood  
Applicant: City of Saskatoon  
Proposed Bylaw No. 9038  
(File No. CK. 4351-012-6)**

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The purpose of this hearing is to consider proposed Bylaw No. 9038.

Attached is a copy of the following material:

- Proposed Bylaw No. 9038.
- Report of the General Manager, Community Services Department dated May 15, 2012 recommending that the proposed amendments to the Official Community Plan Bylaw No. 8769 to reclassify the land use designation of W ½ 35-36-6-W3, and LSD 3, 5, and 6, on S ½ 2-37-6-W3 from Urban Holding Area to Residential within the Official Community Plan Land Use Map, and the Official Community Plan Phasing Map from Phase 2 to Phase 1, be approved.
- Clause 2, Report No. 3-2012 of the Municipal Planning Commission, which was adopted by City Council at its meeting held on June 18, 2012;
- Notice that appeared in the local press on July 28, 2012.

**c) Proposed Zoning Bylaw Amendment  
Text Amendments to DCD1 District  
Bylaw No. 9044  
(File No. CK. 4350-012-004)**

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The purpose of this hearing is to consider proposed Bylaw No. 9044.

Attached is a copy of the following:

- Proposed Bylaw No. 9044;
- Report of the General Manager, Community Services Department dated June 22, 2012, recommending that the Zoning Bylaw No. 8770 text amendment be approved.

- Clause 2, Report No. 5-2012 of the Municipal Planning Commission, which was adopted by City Council at its meeting held on July 18, 2012;
- Notice that appeared in the local press on July 28, 2012.

**d) Proposed Adult Services Licensing Bylaw Amendment  
Bylaw No. 9045  
(File No. CK. 3250-012-2)**

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The purpose of this hearing is to consider proposed Bylaw No. 9045.

Attached is a copy of the following:

- Proposed Bylaw No. 9045;
- Report of the General Manager, Community Services Department dated June 29, 2012, recommending that the Adult Services Licensing Bylaw, 2012, Bylaw No. 9011 be amended to provide for a minimum 160 metre separation distance between adult service agencies that may provide in-call services located in the IL1 – Light Industrial and IH – Heavy Industrial Districts be approved.
- Clause 1, Report No. 5-2012 of the Municipal Planning Commission, which was adopted by City Council at its meeting held on July 18, 2012;
- Notice that appeared in the local press on August 4, 2012.

**4. Matters Requiring Public Notice**

**a) Median Closure and New Median Opening on Idylwyld Drive at 25<sup>th</sup> Street  
(Files CK. 6320-5 and IS. 6280-1)**

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The following is a report of the General Manager, Infrastructure Services Department, dated August 1, 2012:

- “RECOMMENDATION:**
- 1) that the closure of the existing median opening on Idylwyld Drive at 25<sup>th</sup> Street be approved; and
  - 2) that the new median opening on Idylwyld Drive at 25<sup>th</sup> Street, as shown on Attachment 1, be approved.

### **REPORT**

At its meeting held on March 16, 2009, City Council approved the alignment of the 25<sup>th</sup> Street extension.

In June 2010, the City of Saskatoon submitted a Notice to Proceed on required railway modifications. Notices were served to all affected parties and adjacent landowners. The Holiday Inn officially objected to eliminating the northbound left-turn. As required by the *Railway Safety Act*, the City applied to the Minister of Transport Canada for permission to reject the Holiday Inn’s objection, and was successful in obtaining the Minister’s approval to proceed with the proposed railway work.

The final alignment of the 25<sup>th</sup> Street extension in relation to the Canadian Pacific (CP) Railway at-grade railway crossing was the only acceptable design to CP and Transport Canada for safety reasons, because of the complexity of the grade crossing system. However, one of the conditions to obtaining the necessary approvals was that the existing median opening at 25<sup>th</sup> Street be closed. This requirement is stipulated by Transport Canada rules entitled, “RTD 10 Technical Standards for Road/Railway Grade Crossings”, as referenced in the “Grade Crossings Regulations”, made pursuant to the *Railway Safety Act*.

The requirement to close the median opening, and thus eliminate the northbound left-turn movement, is primarily due to its proximity to CP Railway’s tracks. Attachment 1 illustrates the proposed closure.

In order to accommodate access off Idylwyld Drive onto 25<sup>th</sup> Street in the northbound direction, a new median opening on Idylwyld Drive, a short distance to the north of 25<sup>th</sup> Street (sufficient clearance from the railway tracks), as illustrated in Attachment 1, is proposed. This opening will only accommodate passenger vehicles wishing to make a northbound U-turn to access properties along the existing 25<sup>th</sup> Street, in the Caswell neighborhood.

### **ENVIRONMENTAL IMPLICATIONS**

There are no environmental implications.

### **FINANCIAL IMPLICATIONS**

All costs associated with the median closure and opening are included as part of Capital Project 200 - 25<sup>th</sup> Street Extension. The estimated costs are approximately \$100,000.

### **PUBLIC NOTICE**

Public Notice is required for consideration of this matter, pursuant to Section 3b) of Policy No. C01-021, The Public Notice Policy. The following notice was given:

- Advertised in the StarPhoenix on the weekend of August 4, 2012;
- Posted on the City Hall Notice Board on Thursday, August 4, 2012; and
- Posted on the City of Saskatoon website on Thursday, August 4, 2012.

### **ATTACHMENTS**

1. Proposed Median Closure, 25<sup>th</sup> Street and Idylwyld Drive.
2. Copy of Public Notice.”

## **5. Unfinished Business**

## **6. Reports of Administration and Committees:**

- a) Report No. 6-2012 of the Municipal Planning Commission;
- b) Administration Report No. 12-2012; and
- c) Legislative Report No. 10-2012.

It is anticipated that there will also be additional reports from the following Committees which will be distributed at the Council meeting:

- Planning and Operations Committee
- Administration and Finance Committee
- Executive Committee

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The City of Saskatoon

## **LEGISLATIVE REPORTS**

### **Section B – OFFICE OF THE CITY SOLICITOR**

**B1) Discontinuation of Prepayment Discounts**  
**(File No. CK. 1920-2)**

**RECOMMENDATION:** that City Council consider Bylaw No. 9046.

At its meeting on June 18, 2012, City Council received a report from the Corporate Services Department regarding the discontinuation of property tax prepayment discounts (ie. Administrative Report No. 10-2012, Section B - Corporate Services, June 18, 2012). Upon consideration of the report, Council resolved “that the City of Saskatoon discontinue offering prepayment discounts for the early payment of property taxes starting in the 2013 tax year”.

We are pleased to submit for Council’s consideration Bylaw No. 9046 *The Tax Discount and Penalties Amendment Bylaw, 2012*. This Bylaw removes the discount for prepayment of property taxes contained in section 2.1 of Bylaw No. 6673.

### **PUBLIC NOTICE**

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

### **ATTACHMENT**

1. Proposed Bylaw No. 9046, *The Tax Discount and Penalties Amendment Bylaw, 2012*.

**B2) Proposed Amendments to Bylaw 7200 - The Traffic Bylaw**  
**Speed Limit Changes**  
**(File No. CK. 6320-1)**

**RECOMMENDATION:** that City Council consider Bylaw No. 9047.

At its meeting held on July 18, 2012 City Council adopted the recommendations of Clause 3, Administrative Report No. 11-2012 as set out below:

“That the City Solicitor be instructed to amend Bylaw 7200 - The Traffic Bylaw and update corresponding Schedule No. 4 - Maximum Speeds as follows:

- a) lower the speed limit from 70 kph to 60 kph on Millar Avenue north of 60<sup>th</sup> Street;
- b) remove subsection 4(r): 80 kph - Central Avenue from Agra Road to the North City Limit; and
- c) lower the speed limit from 60 kph to 50 kph on Spadina Crescent West south of Schuler Street, to 30 metres south of crosswalk.”

We are pleased to enclose Bylaw No. 9047 which enacts the requested changes to *The Traffic Bylaw No. 7200*.

**PUBLIC NOTICE**

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

**ATTACHMENT**

1. Proposed Bylaw No. 9047, *The Traffic Amendment Bylaw, 2012 (No. 2)*.

Respectfully submitted,

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Theresa Dust, City Solicitor

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**BYLAW NO. 9046**

**The Tax Discount and Penalties Amendment Bylaw, 2012**

The Council of The City of Saskatoon enacts:

**Short Title**

- 1. This Bylaw may be cited as *The Tax Discount and Penalties Amendment Bylaw, 2012*.

**Purpose**

- 2. The purpose of this Bylaw is to amend Bylaw No. 6673, being "*A bylaw of The City of Saskatoon to provide for the payment of taxes and the application of discounts and penalties thereto*", to discontinue offering prepayment discounts for the early payment of property taxes.

**Bylaw No. 6673 Amended**

- 3. Bylaw No. 6673, being "*A bylaw of The City of Saskatoon to provide for the payment of taxes and the application of discounts and penalties thereto*", is amended in the manner set forth in this Bylaw.

**Section 2.1 Repealed**

- 4. Section 2.1 is repealed.

**Section 6.6 Amended**

- 5. Section 6.6 is amended by striking out "Section 2.1 and".

**Coming Into Force**

- 6. This Bylaw shall come into force on the day of its final passing.

Read a first time this	day of	, 2012.
Read a second time this	day of	, 2012.
Read a third time and passed this	day of	, 2012.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

**BYLAW NO. 9047**

**The Traffic Amendment Bylaw 2012 (No. 2)**

The Council of The City of Saskatoon enacts:

**Short Title**

1. This Bylaw may be cited as The Traffic Amendment Bylaw, 2012 (No. 2).

**Purpose**

2. The purpose of this Bylaw is to amend The Traffic Bylaw No. 7200, to change the maximum speed permitted on various roadways within the City.

**Bylaw No. 7200 Amended**

3. The Traffic Bylaw is amended in the manner set forth in this Bylaw.

**Schedule No. 4 Amended**

4. Schedule No. 4 is repealed and the schedule marked as Schedule "A" to this Bylaw is substituted.

**Coming into Force**

5. This Bylaw shall come into force on the day of its final passing.

Read a first time this \_\_\_\_\_ day of \_\_\_\_\_, 2012.

Read a second time this \_\_\_\_\_ day of \_\_\_\_\_, 2012.

Read a third time and passed this \_\_\_\_\_ day of \_\_\_\_\_, 2012.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

Schedule "A" to Bylaw No. 9047

**Schedule No. 4**  
**Maximum Speeds**

1. **10 km/h:**
  - (a) in any parking structure.
2. **20 km/h:**
  - (a) in any parking area;
  - (b) in any alley;
  - (c) in any public park.
3. **30 km/h:**
  - (a) in any posted school or playground speed zone.
4. **60 km/h on the following roads:**
  - (a) 8th Street between Moss Avenue and the East City Limit;
  - (b) College Drive between Cumberland Avenue and a point 100 metres east of Preston Avenue;
  - (c) Spadina Crescent from a point 430 metres south of Schulyer Street to the West City Limit;
  - (d) Warman Road between 33rd Street and 51st Street;
  - (e) Spadina Crescent between Windsor Street and Pinehouse Drive;
  - (f) 22nd Street between Witney Avenue and Grid No. 684 (the Dalmeny Grid);
  - (g) Lorne Avenue between Jasper Avenue and the South City Limit;
  - (h) 51st Street between Idylwyld Drive and Warman Road;
  - (i) Lenore Drive between Warman Road and Russell Road;
  - (j) Airport Drive between Circle Drive and 45th Street;

- (k) Avenue C between Idylwyld Drive and 45th Street;
- (l) Clarence Avenue from a point 150 metres north of Circle Drive to the South City Limit;
- (m) Attridge Drive between Circle Drive and McOrmond Drive;
- (n) Preston Avenue between Circle Drive North and 14th Street;
- (o) Boychuk Drive between 8<sup>th</sup> Street and Highway No. 16;
- (p) Central Avenue from Attridge Drive north to the City Limit;
- (q) Agra Road between Central Avenue and Lowe Road;
- (r) Battleford Trail from Hughes Drive northwest to the City Limit;
- (s) Wanuskewin Road between 51st Street north to Adilman Drive;
- (t) Lowe Road from a point 600 metres north of Nelson Road to a point 200 metres south of Atton Crescent;
- (u) Betts Avenue between 22<sup>nd</sup> Street West and Hart Road;
- (v) McOrmond Drive from Highway 5 north to the City Limit;
- (w) Marquis Drive between Wanuskewin Road and Idylwyld Drive;
- (x) Claypool Drive from Airport Drive west to the City Limits;
- (y) Millar Avenue from a point 200 metres north of 60<sup>th</sup> Street to the North City Limit.

5. **70 km/h on the following roadways:**

- (a) Circle Drive, from a point 200 metres west of Avenue C to a point 200 metres west of Airport Drive;
- (b) Idylwyld Drive between 8th Street and 19th Street;
- (c) Circle Drive from the west abutment of the Circle Drive Bridge to Millar Avenue;
- (d) Wanuskewin Road between Adilman Drive and the North City Limit;

- (e) Attridge Drive between Circle Drive and Rever Road;
  - (f) 71st Street between Idylwyld Drive and Wanuskewin Road.
6. **80 km/h on the following roadways:**
- (a) College Drive from a point 450 metres east of Central Avenue to a point 100 metres east of Preston Avenue;
  - (b) Idylwyld Drive North between Circle Drive North and 39th Street;
  - (c) Circle Drive from a point 200 metres north of Laurier Drive to a point 250 metres north of 11th Street;
  - (d) 11th Street between Chappell Drive and the West City Limit;
  - (e) (Repealed – Bylaw No. 8593 – March 12, 2007);
  - (f) 11th Street from Highway No. 7 west to the City Limit;
  - (g) 33rd Street from a point 300 metres west of Steeves Avenue to the City Limit;
  - (h) Range Road 3063 from the South City Limit to the North City Limit;
  - (i) Range Road 3064 from the South City Limit to the North City Limit;
  - (j) Agra Road from Lowe Road east to the City Limit;
  - (k) 22<sup>nd</sup> Street West from Grid No. 684 (the Dalmeny Grid) to a point 800 metres west of Grid No. 684 (the Dalmeny Grid);
  - (l) Betts Avenue (Highway No. 7) between Hart Road and 11<sup>th</sup> Street West;
  - (m) Lowe Road from Agra Road to the North City Limit;
  - (n) McOrmond Drive from 8<sup>th</sup> Street to College Drive/Highway 5;
  - (o) Zimmerman Road from College Drive/Highway 5 to the South City Limit;
  - (p) Blackley Road from Highway 41 to the North City Limit;
  - (q) Fleury Road from Range Road 3045 to the East City Limit;
  - (r) Millar Avenue from 71<sup>st</sup> Street to the North City Limit.

7. **90 km/h on the following roadways:**

- (a) (Repealed – Bylaw No. 8650 – December 17, 2007)
- (b) Idylwyld Drive between 8th Street and Circle Drive South;
- (c) Circle Drive South between Idylwyld Drive and the south intersection of Highways No. 11 and 16;
- (d) College Drive from a point 450 metres east of Central Avenue to a point 1,617 metres east of Central Avenue;
- (e) Idylwyld Drive North between Circle Drive North and the North City Limit;
- (f) Highway No. 11 between the south intersection of Highway Nos. 11 and 16 and the South City Limit;
- (g) Highway No. 16 between the south intersection of Highway Nos. 11 and 16 and the East City Limit;
- (h) Circle Drive from the west abutment of Circle Drive Bridge to the south intersection of Highways No. 11 and 16;
- (i) Dalmeny Grid from Highway No. 14 north to the City Limit;
- (j) Circle Drive from a point 200 metres west of Airport Drive to a point 200 metres north of Laurier Drive.

8. **100 km/h on the following roadways:**

- (a) 22<sup>nd</sup> Street West (Highway No. 14) from a point 800 metres west of Grid No. 684 (the Dalmeny Grid) to the West City Limit;
- (b) Betts Avenue (Highway No. 7) from 11<sup>th</sup> Street West to the West City Limit;
- (c) College Drive from a point 1,617 metres east of Central Avenue to the East City Limit.

9. **50 km/h:**

- (a) on all roadways of the City not previously provided for.

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The City of Saskatoon

## ADMINISTRATIVE REPORTS

### Section A – COMMUNITY SERVICES

**A1) Land-Use Applications Received by the Community Services Department  
For the Period Between July 5, 2012 and August 1, 2012  
(For Information Only)  
(Files CK. 4000-5, PL. 4355-D, PL. 4350, and PL. 4300)**

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**RECOMMENDATION:** that the information be received.

The following applications have been received and are being processed:

#### Discretionary Use

- Application No. D3/12: 424 Evergreen Boulevard  
Applicant: 101208136 Saskatchewan Ltd.  
Legal Description: Lot 25, Block 617, Plan No. 102038150  
Current Zoning: R1A  
Proposed Use: Type II Residential Care Home  
Neighbourhood: Evergreen  
Date Received: July 17/12

#### Rezoning

- Application No. Z20/12: Evergreen Boulevard/Manek Road/Rajput Way/  
Johns Road/Maningas Bend/Arscott Crescent  
City of Saskatoon, Land Branch  
Applicant:  
Legal Description: **Parcel DD** - Manek Road and Evergreen Boulevard:  
Part of NW ¼ 7-37-4-W3, and Part of Parcel A, Plan  
No. 78S34536;  
**Parcels S and T** - Rajput Way and Johns Road;  
**Parcel U** - Maningas Bend and Johns Road;  
**Parcels X and Z** - Rajput Way and Manek Road;  
**Parcel W** - Maningas Bend and Manek Road;  
**Parcel CC** - Manek Road and Arscott Crescent:  
Part of NW ¼ 7-37-4-W3, and Part SW ¼ 7-37-4-W3;  
**Parcel BB** - Evergreen Boulevard and Manek Road:  
Part of SW ¼ 7-37-4-W3;  
**Parcel AA** - Maningas Bend and Manek Road:

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The City of Saskatoon

**REPORT**  
**of the**  
**MUNICIPAL PLANNING COMMISSION**

Composition of Commission

Mr. Kurt Soucy, Chair  
Mr. Leanne DeLong, Vice Chair  
Councillor Charlie Clark  
Ms. Carole Beitel  
Mr. Laurier Langlois  
Mr. Aditya Garg  
Mr. Al Douma  
Mr. Stan Laba  
Ms. Debbie Marcoux  
Ms. Kathy Weber  
Mr. James Yachyshen  
Ms. Janice Braden  
Mr. Andy Yuen

1. **Proposed Rezoning from R1A to RM3, RMTN and R1B; and from AG to R1A  
Richardson Road, Lehrer Manor, Lehrer Crescent and 37<sup>th</sup> Street West  
Hampton Village Neighbourhood  
Applicant: City of Saskatoon, Land Branch  
(File No. CK. 4351-012-015)**
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- RECOMMENDATION:**
- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in the report of the General Manager, Community Services Department dated July 16, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
  - 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and

- 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone the land parcels located on Richardson Road, Lehrer Manor, Lehrer Crescent, and a utility parcel (see Attachment 2 to the report of the General Manager, Community Services Department dated July 16, 2012) from R1A – One-Unit Residential District to RM3 – Medium Density Multiple-Unit Dwelling District, RMTN – Townhouse Residential District, and R1B – Small Lot One-Unit Residential District; and AG – Agricultural District to R1A – One-Unit Residential District, be approved based on the reasons outlined in the report of the General Manager, Community Services Department, dated July 16, 2012.

Attached is a report of the General Manager, Community Services Department dated July 16, 2012, with respect to the above proposed rezoning.

Your Committee has reviewed the proposed rezoning with the Administration and supports the above recommendations.

**2. Proposed New Commercial Zoning District – B5B Broadway and  
Proposed New Overlay Zoning District – AC2 –  
B5B Architectural Control Overlay Zoning District  
Applicant: City of Saskatoon, Planning and Development Branch  
(File No. CK. 4350-012-005)**

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- RECOMMENDATION:**
- 1) that City Council approve the advertising respecting the proposal to amend Sections 13.1.3 of Zoning Bylaw No. 8770 as outlined in the report of the General Manager, Community Services Department dated July 17, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
  - 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and

- 4) that at the time of the public hearing, City Council consider the Commission's recommendation to amend Zoning Bylaw No. 8770 to add the proposed B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District, and to amend the parking and sign regulations contained in Zoning Bylaw No. 8770 to reflect the addition of the B5B Zoning District.

Attached is a report of the General Manager, Community Services Department dated July 17, 2012, with respect to the above proposed amendments to the Zoning Bylaw.

Your Commission has reviewed the report with the Administration and the Chair of the Broadway 360 Steering Committee, including the consultation process undertaken, and is supporting the above recommendations.

**3. Proposed Rezoning from B5 to B5B and AC2 – B5B  
Nutana Neighbourhood  
Applicant: City of Saskatoon, Planning and Development Branch  
(File No. CK. 4351-012-10)**

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- RECOMMENDATION:**
- 1) that City Council approve the advertising respecting the proposal to amend Zoning Bylaw No. 8770 as outlined in the report of the General Manager, Community Services Department dated July 30, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
  - 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770;
  - 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposal to rezone the properties indicated on Attachments 2 and 3 of the report of the General Manager, Community Services Department dated July 30, 2012, from a B5 Zoning District to a B5B Zoning District, be approved; and

- 5) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposal to apply the AC2 – B5B Architectural Control Overlay District to the properties indicated on Attachments 2 and 3 of the report of the General Manager, Community Services Department dated July 30, 2012, be approved.

Attached is a report of the General Manager, Community Services Department dated July 30, 2012, with respect to the above matter.

Your Commission has reviewed the above matter with the Administration and Broadway 360 Steering Committee Chair and supports the above recommendations.

**4. Proposed Rezoning from R1A(H) to R1A,  
FUD to R1A, and R1A(H) to R1B  
Kensington Neighbourhood  
Applicant: City of Saskatoon, Land Branch  
(File No. CK. 4351-012-11)**

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- RECOMMENDATION:**
- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in the report of the General Manager, Community Services Department dated July 19, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
  - 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
  - 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposed amendments to Zoning Bylaw No. 8770 be approved, as follows:
    - a) that the properties identified in Attachment 2 to the report of the General Manager, Community Services

Department dated July 19, 2012, be rezoned from R1A(H) - One-Unit Residential District with a Holding symbol, and FUD - Future Urban Development District to R1A – One-Unit Residential District; and

- b) Phase I of the Kensington neighbourhood development, as shown on the Plan of Proposed Subdivision (see Attachment 3 to the report of the General Manager, Community Services Department dated July 19, 2012), be rezoned from:
  - i. R1A(H) - One-Unit Residential District with a Holding symbol, to R1B – Small Lot One-Unit Residential District (Lots 1 to 26, Block 100, Lots 1 to 29, Block 101, Lots 1 to 30, Block 103); and
  - ii. R1A(H) - One-Unit Residential District with a Holding symbol to R1A – One-Unit Dwelling Residential District (Lots 1 to 15, Block 102, and Lots 71 to 76, Block 103).

Attached is a report of the General Manager, Community Services Department dated July 19, 2012, with respect to the above matter.

Your Commission has reviewed the report with the Administration and is supporting the above recommendations.

**5. Proposed Rezoning from R1A to R1B and RMTN  
Evergreen Boulevard, Kloppenburg Crescent, Bend, Link, Street, and Way  
Evergreen Neighbourhood  
Applicant: City of Saskatoon, Land Branch  
(File No. CK. 4351-012-013)**

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**RECOMMENDATION:** 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in the report of the General Manager, Community Services Department dated July 16, 2012;

- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone the properties identified in the attached Proposed Zoning Map from R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District, and RMTN – Townhouse Residential District, be approved.

Attached is a report of the General Manager, Community Services Department dated July 16, 2012, with respect to the above matter.

Your Commission has reviewed the report with the Administration and supports the above recommendations.

Respectfully submitted,

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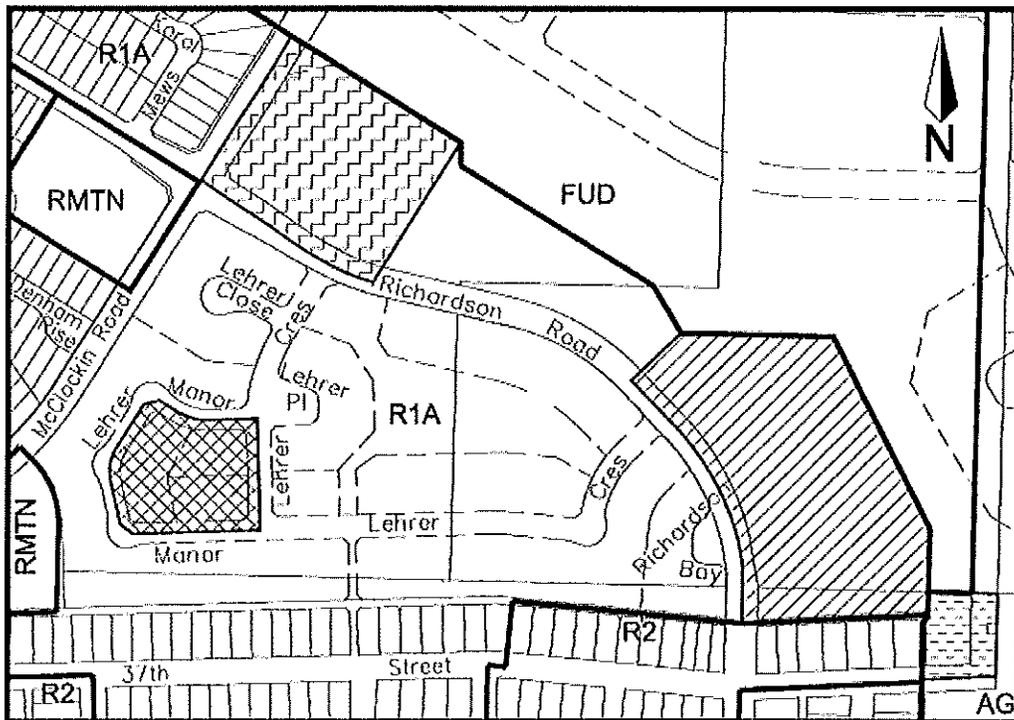
Mr. Kurt Soucy, Chair

1.

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z11/12	<b>PROPOSAL</b> Proposed Rezoning from R1A to RM3, RMTN, and R1B; and from AG to R1A	<b>EXISTING ZONING</b> R1A and AG
<b>LEGAL DESCRIPTION</b> Parcels AA, BB, CC, Lots 1 to 16, Block 24; and Parcel L, Lots 9 and 10, Block 664; and Lot L, Block 664, Plan No. 69S08033		<b>CIVIC ADDRESS</b> Richardson Road, Lehrer Manor, Lehrer Crescent, and 37 <sup>th</sup> Street West
		<b>NEIGHBOURHOOD</b> Hampton Village
<b>DATE</b> July 16, 2012	<b>APPLICANT</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7	<b>OWNER</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7

### LOCATION PLAN



**PROPOSED REZONING**

From R1A to RMTN — From AG to R1A —

From R1A to RM3 — From R1A to R1B —

**City of Saskatoon**  
Planning & Development Branch

File No. RZ11-2012

**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

That a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendment to the Zoning Bylaw No. 8770 to rezone the land parcels located on Richardson Road, Lehrer Manor, Lehrer Crescent, and a utility parcel (see Attachment 2) from R1A – One-Unit Residential District to RM3 – Medium Density Multiple-Unit Dwelling District, RMTN – Townhouse Residential District, and R1B – Small Lot One-Unit Residential District; and AG – Agricultural District to R1A – One-Unit Residential District, be approved based on the reasons outlined in this report.

**B. PROPOSAL**

The Planning and Development Branch has received an application from the City of Saskatoon, Land Branch requesting that the land parcels located on Richardson Road, Lehrer Manor, Lehrer Crescent, and a utility parcel (see Attachment 2) be rezoned as follows:

- 1) R1A – One-Unit Residential District to RM3 – Medium Density Multiple-Unit Dwelling District - this property (Parcel AA) is located on the northern corner of Richardson Road and McClocklin Road;
- 2) R1A – One-Unit Residential District to RMTN – Townhouse Residential District - these properties (Parcels BB and CC) are located adjacent to Richardson Road in the south east corner of the subject area;
- 3) R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District - these properties (Block 24) are surrounded by Lehrer Manor in the south west portion of the subject area; and
- 4) AG – Agricultural District to R1A – One-Unit Residential District - this property is located at the very outset of the south east corner of the subject area, adjacent to the

termination point of 37<sup>th</sup> Street West.

The remaining residential lots shown on the attached Location Plan of Proposed Subdivision (see Attachment 2) will retain the current R1A zoning designation.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed rezoning will permit the development of the aforementioned lands in a manner which is consistent with the Hampton Village Neighbourhood Concept Plan (Concept Plan).

**D. BACKGROUND INFORMATION**

The subject sites are currently zoned AG and R1A and are greenfield properties located within the most easterly section of Hampton Village, which is in the final phase of neighbourhood development. The Concept Plan for Hampton Village was approved by City Council in 2004 and provides a wide range of housing options, which included the RM3 and RMTN properties identified in the administrative report. In October 2011, City Council approved a minor Concept Plan Amendment that moved the R1B – Small Lot Residential area to the parcel bound by Lehrer Manor. In addition to this, there was a reconfiguration of the utility parcel (dry pond) that did not affect the overall size of that parcel. At the time leading up to the public hearing, public notice was sent to the Hampton Village Community Association and a notice was advertised in The StarPhoenix.

In order to accommodate future development, the proposed Zoning Bylaw No. 8770 amendments will change the zoning designations for the specified areas of the neighbourhood to permit residential development.

**E. JUSTIFICATION**

1. Community Services Department Comments

a) Planning and Development Branch

The proposed rezoning is in compliance with the approved Concept Plan and will accommodate a diversity of housing types in the Hampton Village neighbourhood. Future development on this site will comply with the development standards identified in the R1A, RM3, RMTN, and R1B Zoning Districts. As such, the Planning and Development Branch has no concerns regarding the proposed rezoning of the identified properties.

2. Comments by Others

a) Infrastructure Services Department

The proposed Zoning Bylaw No. 8770 amendment is acceptable to the Infrastructure Services Department.

Please note that the properties zoned RMTN and RM3 may require a Traffic Impact Study upon development of the parcels.

b) Utility Services Department, Transit Services Branch

At present, the Transit Branch's closest bus stop is approximately 450 metres from the above referenced properties, located on the east side of East Hampton Boulevard and north of McClocklin Road.

Bus service is at 30 minute intervals Monday to Saturday and at 60 minute intervals after 18:00 Monday to Friday, early Saturday mornings, Sundays, and statutory holidays.

**F. COMMUNICATION PLAN**

Once this application has been considered by the Municipal Planning Commission, it will be advertised in accordance with Public Notice Policy No. C01-021, and a date for a public hearing will be set. The property owners affected by this rezoning will be notified in writing. The Planning and Development Branch will also notify the Community Consultant and the Ward Councillor of the public hearing date by letter. A notice will be placed in The StarPhoenix two weeks prior to the public hearing. Notice boards will also be placed on the site.

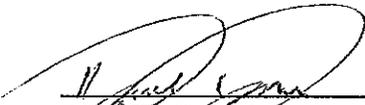
**G. ENVIRONMENTAL IMPLICATIONS**

There are no environmental and/or greenhouse gas implications.

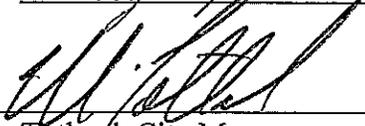
**H. ATTACHMENTS**

1. Fact Summary Sheet
2. Location Plan

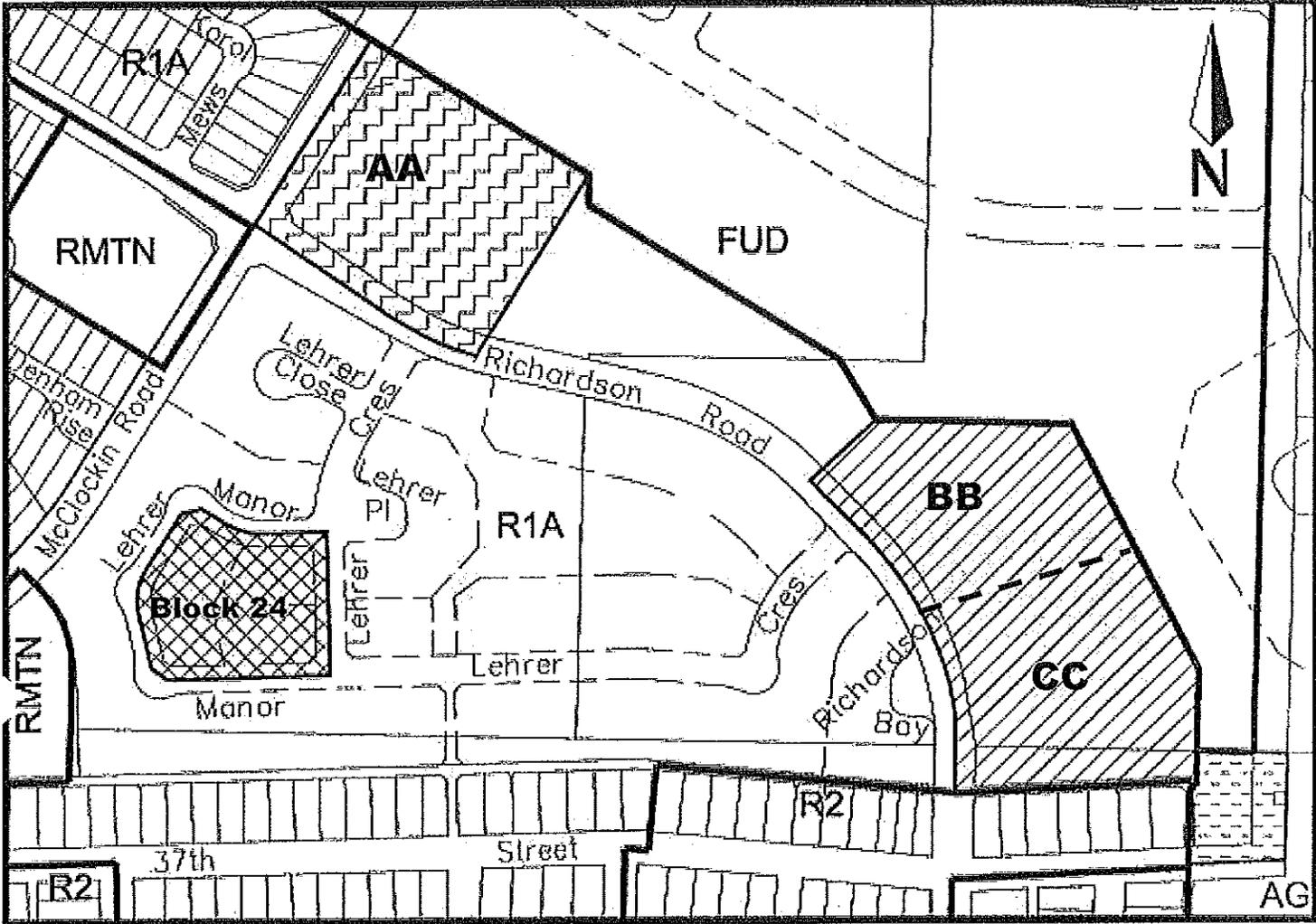
Written by: Daniel Gray, Planner 16

Reviewed by:  For  
Alan Wallace, Manager  
Planning and Development Branch

Approved by:   
Randy Grauer, General Manager  
Community Services Department  
Dated: July 31, 2012

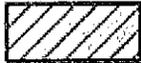
Approved by:   
Murray Totland, City Manager  
Dated: Aug 11/12

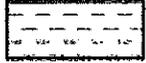
<b>FACT SUMMARY SHEET</b>	
<b>A. Location Facts</b>	
1. Municipal Address	Richardson Road, Lehrer Manor, Lehrer Crescent, and Right-of-Way Parcel
2. Legal Description	Parcels AA, BB, CC, Lots 1 to 16, Block 24; and Parcel L, Lots 9 and 10, Block 664; and Lot L, Block 664, Plan No. 69S08033
3. Neighbourhood	Hampton Village
4. Ward	4
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Residential – R1A
2. Proposed Use of Property	Residential – RM3, RMTN, and R1B
3. Adjacent Land Uses and Zoning	Residential and FUD
North – Aerogreen Neighbourhood	Future Urban Development - FUD
South – Westview Neighbourhood	Residential – R1A and R2
East – Aerogreen Neighbourhood	Future Urban Development - FUD
West – Hampton Village Neighbourhood	Residential – R1A
4. No. of Existing Off-Street Parking Spaces	N/A
5. No. of Off-Street Parking Spaces Required	N/A
6. No. of Off-Street Parking Spaces Provided	N/A
7. Site Frontage	N/A
8. Site Area	N/A
9. Street Classification	Richardson Road – Major Collector Richardson Bay – Local Street Lehrer Crescent – Local Street Lehrer Manor – Local Street
<b>C. Official Community Plan Policy</b>	
1. Existing Official Community Plan Designation	Residential
2. Proposed Official Community Plan Designation	Residential
3. Existing Zoning District	R1A
4. Proposed Zoning District	RM3, RMTN, and R1B



**PROPOSED REZONING**



From R1A to RMTN —   
 From R1A to RM3 — 

From AG to R1A —   
 From R1A to R1B — 

File No. RZ11-2012

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z14/12	<b>PROPOSAL</b> Proposed New Commercial Zoning District -- B5B Broadway Proposed New Overlay Zoning District -- AC2 -- B5B Architectural Control Overlay Zoning District	<b>EXISTING ZONING</b>
<b>LEGAL DESCRIPTION</b>	<b>CIVIC ADDRESS</b>	
	<b>NEIGHBOURHOOD</b>	
<b>DATE</b> July 17, 2012	<b>APPLICANT</b> City of Saskatoon Planning and Development Branch 222 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 0J5	<b>OWNER</b>

### LOCATION PLAN

A. **COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

that a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising respecting the proposal to amend Sections 13.1.3 of Zoning Bylaw No. 8770 as outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation to amend Zoning Bylaw No. 8770 to add the proposed B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District, and that parking and sign regulations contained in Zoning Bylaw No. 8770 are amended to reflect the addition of the B5B Zoning District.

B. **PROPOSAL**

The Planning and Development Branch is requesting that Zoning Bylaw No. 8770 be amended to add the B5B Broadway Commercial Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District. The addition of the B5B Zoning District also requires amendments to the sign and parking regulations contained in Section 6.0 and Appendix A of Zoning Bylaw No. 8770 (see Attachments 2 and 3).

C. **REASON FOR PROPOSAL (By Applicant)**

The proposed commercial B5B Zoning District is intended to be applied in the Broadway Avenue commercial area. This district will provide development standards to ensure that new development enhances the existing urban environment along this unique commercial corridor.

The proposed Architectural Control Overlay Zoning District is also intended to be applied in the Broadway Avenue commercial area. This Architectural Control Overlay Zoning District will ensure that new buildings reinforce and enhance the best qualities of the Broadway commercial area.

The B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District is intended to be applied to the Broadway area, which is currently zoned B5, as indicated on Attachment 1. The rezoning of these properties is covered under a separate report.

**D. BACKGROUND INFORMATION**

In 2007, the Broadway Business Improvement District (BID) and the Nutana Community Association proposed that a study of the Broadway commercial area be undertaken. It was noted by both groups that some of the recommendations contained in the 2001 Nutana Local Area Plan (LAP) had become outdated, other recommendations appeared to be difficult to implement due to community or property owner resistance, and there were concerns about the future character of Broadway Avenue. The City of Saskatoon (City) was invited to participate as a partner with the Broadway BID and Nutana Community Association in the development of a "Broadway Area Plan" to address outstanding recommendations in the Nutana LAP.

The Planning Partnership, a Toronto-based urban planning consultant firm, was hired to prepare what would become the Broadway 360 Development Plan (Plan). The Plan involved a thorough public consultation process that engaged residents, business and commercial property owners, area schools, churches, and those representing Saskatoon's heritage community.

The Plan explored practical urban development solutions to address land use, street character, safety, parking, and traffic issues in the Broadway area. During its September 28, 2009 meeting, City Council received the Plan.

A Steering Committee that was struck during the creation of the Plan was comprised of developers, commercial property owners, Nutana residents, the Nutana Community Association, the BID, and the Ward Councillor. Following City Council receiving the Plan, the Steering Committee met and prioritized the recommendations. The items that were identified as having the highest priority were those concerning land use. These included a new zoning district that would put greater controls on form and massing of building and implementation of an architectural control district. There was initially a lack of consensus within the Steering Committee regarding these proposed development standards.

The Administration worked very closely with the Steering Committee over several months to prepare detailed zoning requirements that would be acceptable to the various stakeholders. The Administration and the Steering Committee met 13 times over the course of 20 months to prepare these proposed Zoning Bylaw No. 8770 amendments.

During its April 16, 2012 meeting, City Council approved an implementation strategy for the Plan. This implementation plan indicated that two tasks, adoption of the recommended development standards and consideration of an architectural control district, would be undertaken in 2012.

E. JUSTIFICATION

1. Community Services Department Comments

a) Official Community Plan Bylaw No. 8769

This area is designated as “Special Area Commercial” on the Nutana Land Use map contained in Official Community Plan Bylaw No. 8769. The proposal to apply a new zoning treatment and architectural control district in the Broadway Avenue commercial area is consistent with the objectives and policies in related to Special Area Commercial Areas, as stated in Section 6.4 of the Official Community Plan:

“Historic Commercial Areas

The Special Area Commercial designation has been applied to certain commercial lands along 20<sup>th</sup> Street, 33<sup>rd</sup> Street, Central Avenue and Broadway Avenue, primarily due to their long and unique development history. In general, these areas contain a built form that is oriented to pedestrians, with limited front or side yard setbacks, and with a relatively high density of development. As a consequence, the Zoning Bylaw shall prescribe development standards for these areas which reflect their unique character, while also promoting compatibility with surrounding residential land use.

Specific local area plans or design studies may also be undertaken in these areas to further define future land use patterns and design and development standards.”

b) Planning and Development Branch Comments

i) Proposed B5B Zoning District

The purpose of the B5B district is to recognize the historic Broadway Avenue commercial area and to facilitate mixed-use developments, including a range of commercial, institutional, and medium to high density residential (see Attachment 2). The following table summarizes the differences between the existing B5 and the proposed B5B Zoning District.

B5 Zoning District (Current Regulations)	PROPOSED B5B ZONING DISTRICT
<b>PERMITTED USES</b>	
There are a wide range of uses permitted in the B5 district. This district permits commercial, institutional (office), and residential uses.	The same uses as permitted in B5, with the following vehicle-oriented uses being deleted: a) service stations b) car washes c) motor vehicle, marine, and trailer coach sales establishments d) public garages
<b>SETBACK REQUIREMENTS</b>	
<b>Front Yard Setback – Building Base:</b> a) no minimum for most uses b) 3 metres for vehicle-oriented use c) 6 metres for multiple-unit dwellings	a) no minimum b) maximum setback – A minimum of 70 percent of the aggregate width of the front building line shall be located within one metre of the front property line
<b>Front Yard Setback – Building Cap:</b> a) no current standard	a) Minimum setback of 3 metres up to 3 storeys above the building base and 0.6 metres for every storey above 3, to a maximum setback of 6 metres
<b>Side Yard Setback – Building Base:</b> a) zero, with exceptions	a) no minimum; however, where a B5B District abuts an R District without the intervention of a street or lane, an abutting side yard shall be provided of at least 1.5 metres
<b>Side Yard Setback – Building Cap:</b> a) no current standard	a) the side yard setback shall be increased by 0.3 metres for each additional storey above the building base, to a maximum of 3 metres
<b>Rear Yard Setback – Building Base:</b> a) zero, with exceptions	a) no minimum; however, where a B5B District abuts an R District, a rear yard shall be provided of at least 3.0 metres
<b>Rear Yard Setback – Building Cap:</b> a) no current standard	a) no minimum; however, where a B5B District abuts an R District, the rear yard setback shall be increased by 0.6 metres for each additional storey above the building base to a maximum of 7.5 metres
<b>BUILDING HEIGHT</b>	
<b>Building Base:</b> a) 76 metres for entire building	a) shall be a minimum of 7.5 metres and a maximum of 12 metres
<b>Building Cap:</b> a) 76 metres for entire building	a) no maximum height limit, however, other factors govern height
<b>GROSS FLOOR AREA RATIO</b>	
a) 5:1 – site width less than 15 metres b) 7:1 – site width between 15 metres and 30 metres c) 10:1 – site width greater than 30 metres	a) the gross floor area ratio shall not exceed 7:1

PARKING REQUIREMENTS FOR MULTIPLE-UNIT DWELLINGS	
a) 1.25 per dwelling unit plus 0.125 visitor spaces	a) parking for multiple-unit dwellings shall be provided at the rate of 1.0 space per dwelling unit, plus 0.125 visitor spaces per dwelling unit
b) units smaller than 50 m <sup>2</sup> – 1.0 spaces per unit	

The intent of the B5B District is to ensure that buildings have an identifiable base and cap. The base of the building would have minimal setbacks while the building cap setback would provide appropriate sunlight penetration and ensure that development has an appropriate pedestrian scale.

The proposed B5B Zoning District does not contain a maximum height requirement. The development standards contained in the B5B District utilize tools that will limit the maximum building height. Those factors that have an effect on height include: gross floor area ratio of 7:1, site size, height of base building (must be a minimum 7.5 metres to a maximum of 12 metres), setback and stepbacks, and parking (both the amount of required parking and where it is located on the site).

Amendments are also required to the signage and parking requirements contained in Zoning Bylaw No. 8770 to reflect the addition of the B5B Zoning District. The regulations for parking and signage will be the same as those currently applied in the B5 and B6 Districts.

ii) Proposed AC2 – B5B Architectural Control Overlay District

It is recommended that an Architectural Control District (ACD), including specific Architectural Control Guidelines, be adopted in conjunction with the above noted B5B District (see Attachment 3).

The ACD will provide direction to ensure high quality architectural design for new construction in the Broadway commercial area. The ACD is intended to allow for flexibility and foster creativity in building design.

It is proposed that the ACD will be applied to the construction of all new buildings and where City funding is being requested under programs, such as the Heritage Conservation Program, the Facade Renovation and Rehabilitation Program, or the Affordable Housing Program.

Property owners and developers will be encouraged to follow the guidelines in any other cases.

As noted in Appendix 3, the ACD will contain 14 design guidelines that address the following: building expression, orientation and placement, street wall, heritage contexts, corner sites, storefronts, residential street access units, roof treatment, above-grade parking, material and architectural quality, sidewalk cafes, building lighting, signage, and sustainable design.

The review process for the ACD will be administered by the Planning and Development Branch, Community Services Department, in the same manner as currently undertaken for development projects in River Landing. A development review committee consisting of design professionals (community planners, landscape architects, and other architects) will review each application. Approvals under the ACD are proposed to be delegated to the Administration.

iii) Compatibility with Adjacent Land Uses

These new development standards should reinforce the best qualities that characterize the Broadway Area and ensure an appropriate interface with existing heritage resources and the adjacent low-rise residential neighbourhood. The new B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District are intended ensure that all new development adheres to the development principles contained in the Plan.

4. Comments by Others

a) Infrastructure Services Department

1. The allowed land uses within the proposed zoning district vary widely in capacity use from a water and sewer perspective. High density/high capacity, such as hotels and multi-story residential, may significantly affect water and sewer concerns with respect to fire flows and sanitary sewer capacity. The wide variation makes it very difficult to determine if any water and sewer conditions exist. Storm sewer capacity is not a concern.
2. With varied land uses, it is possible for the first high density new land development (i.e. a multi-story residential or hotel) to effectively consume all the available sanitary sewer capacity in the district, thereby “sterilizing” the area for other high density uses.

Since zoning is the only control for regulating land use it would be prudent to either determine a method of regulating high density uses for the zoning district or determine a levy payment method so that the first user does not benefit from “free” existing capacity while future users must pay for all additional upgrades to water and sewer infrastructure.

Note: The proposed B5B Zoning District will be applied in the area currently zoned B5 in the Broadway Commercial Area. The B5B District limits the maximum development potential in this area compared to what is currently permitted in the B5 District.

The Integrated Growth Plan will be addressing issues related to the financing of infrastructure upgrades necessary to accommodate infill development.

b) Transit Services Branch, Utilities Services Department

At present, Saskatoon Transit has bus stops throughout the district. Service is at 15 minute intervals Monday to Saturday; at 30 minute intervals evenings, early Saturday mornings, Sundays, and statutory holidays.

F. ENVIRONMENTAL IMPLICATIONS

There are no environmental implications and/or greenhouse gas implications.

G. COMMUNICATION PLAN

As noted above, this Plan has been undertaken as a joint initiative of the Broadway BID, the Nutana Community Association, and the City. Extensive public consultation was undertaken throughout the Broadway 360 study process.

In addition, a public open house was held on January 12, 2012, at the Cosmopolitan Senior Citizens Centre on 10<sup>th</sup> Street in Nutana. A presentation by your Administration was followed by a question and answer period. A technical workshop was also held on January 14, 2012. Notices for these consultations were distributed throughout the area by flyer drop, in the Nutana Community Association newsletter, and by direct mail to Broadway commercial property owners and business owners.

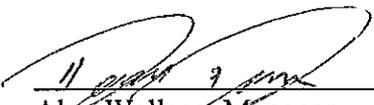
The technical workshop was targeted towards developers, commercial property owners, architects, and others in the development industry. A summary of these consultations is included as Attachment 4.

If this application is approved for advertising by City Council, a notice will be placed in The StarPhoenix for a minimum of seven days prior to the date on which the matter will be considered by City Council. Notice boards will also be placed throughout the area. The Steering Committee members, the Nutana Community Association, and Broadway BID will be notified of the hearing date when set. The property owners affected by this rezoning will also be notified, in writing, by mail.

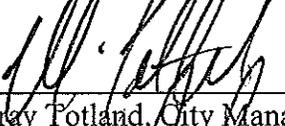
**H. ATTACHMENTS**

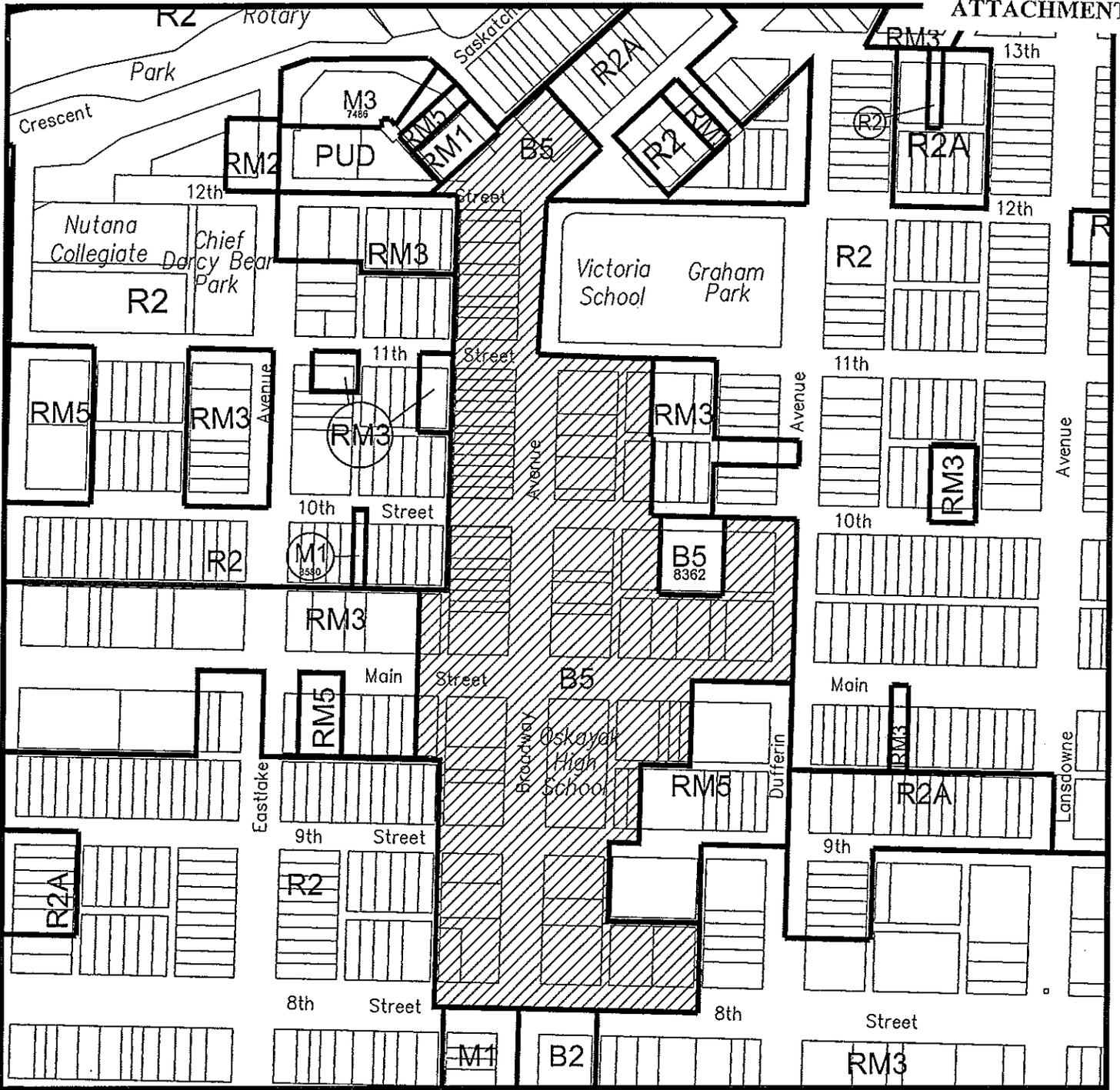
1. Map from B5 to B5B and to AC2 – B5B
2. Proposed B5B Zoning District
3. Proposed AC2 – B5B Architectural Control Overlay Zoning District
4. Feedback from January 2012 Consultation

Written by: Paula Kotasek-Toth, Heritage and Design Coordinator

Reviewed by:  *For*  
 Alan Wallace, Manager  
 Planning and Development Branch

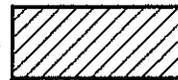
Approved by:   
 Randy Grauer, General Manager  
 Community Services Department  
 Dated: July 30, 2012

Approved by:   
 Murray Potland, City Manager  
 Dated: July 30/12



# REZONING

From B5 to B5B and B5B(ACD2)



## X.X. B5B - Broadway Commercial District

### X.X.1 Purpose

The purpose of the B5B District is to recognize the historic Broadway Commercial area and facilitate mixed use development including a range of commercial, institutional and residential uses in medium to high density form.

### X.X.2 Permitted Uses

The Permitted Uses and Minimum Development Standards in a B5B District are set out in the following chart:

B5B District	Minimum Development Standards (in Metres)						
	Site Width	Site Area (m <sup>2</sup> )	Front Yard	Side Yard	Rear Yard	Building Height (Max.)	Amenity Space Per Unit (m <sup>2</sup> )
<b>10.8.2 Permitted Uses</b>							
(1) Hotels and motels	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(2) Restaurants and lounges	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(3) Bakeries	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(4) Dry cleaners	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(5) Theatres	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(6) Commercial recreation uses	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(7) Photography studios	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(8) Retail stores	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(9) Personal service trades and health clubs	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(10) Offices and office buildings	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(11) Medical clinics	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(12) Medical, dental and optical laboratories	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(13) Financial institutions	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(14) Private schools and educational institutions	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(15) Repair services restricted to the repair of household goods and appliances	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(16) Places of worship	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(17) Public halls and community centres	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(18) Private clubs	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(19) Libraries, art galleries and museums	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(20) Funeral homes	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-

B5B District	Minimum Development Standards (in Metres)						
	Site Width	Site Area (m <sup>2</sup> )	Front Yard	Side Yard	Rear Yard	Building Height (Max.)	Amenity Space Per Unit (m <sup>2</sup> )
<b>10.8.2 Permitted Uses (continued)</b>							
(21) Radio or television studios	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(22) Motion picture or recording studios	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(23) Duplicating or copying centres	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(24) Dwelling units in conjunction with and attached to any other permitted use	-	-	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(25) Multiple-unit dwellings	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(26) Commercial parking lots	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(27) Storage garages	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(28) Banquet halls	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(29) Catering halls and catering kitchens	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(30) Neighbourhood recycling and collection depots	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(31) Parking stations	7.5	225	0 <sub>1</sub>	Refer to Section 6.0.			
(32) Shopping centres	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(33) Veterinary clinics	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(34) Commercial printers	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(35) Public elementary & high schools	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(36) Boarding apartments	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(37) Boarding houses	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(38) Custodial care facilities - Type I & II	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(39) Small animal grooming	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(40) Accessory buildings and uses	-	-	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-

### X.X. 3 Discretionary Uses

The Discretionary Uses and Minimum Development Standards in a B5B District are set out in the following chart:

B5B District	Minimum Development Standards (in Metres)						
	Site Width	Site Area (m <sup>2</sup> )	Front Yard	Side Yard	Rear Yard	Building Height (Max.)	Amenity Space Per Unit (m <sup>2</sup> )
<b>10.8.3 Discretionary Uses</b>							
(1) Special needs housing	15	560	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(2) Child care centres and pre-schools	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(3) Adult day care centres - Type I & II	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(4) Custodial care facilities - Type III	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(5) Night clubs and taverns	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-

#### **X.X. 4 Notes to Development Standards**

1. (a) *Building Base:* a minimum of 70% of the aggregate width of the front building line shall be located within 1.0 metre of the front property line.
- (b) *Building Cap:* a minimum front yard shall be provided of 3.0 metres from the front property line up to three storeys from the front property line shall be provided for every storey above the three storey building cap, however, the minimum setback of the building cap shall not exceed 6.0 metres from the front property line.
2. (a) *Building Base:* where a B5B District abuts an R District without the intervention of a street or lane, an abutting side yard shall be provided of at least 1.5 metres.
- (b) *Building Cap:* setback increased by 0.3 metres for each additional story above the building base to a maximum of 3.0 metres.
3. (a) *Building Base:* where a B5B District abuts an R District, a rear yard shall be provided of at least 3.0 metres.
- (b) *Building Cap:* where a B5B District abuts an R District, the rear setback shall be increased by 0.6 metres for each additional story above the building base to a maximum of 7.5 metres.
4. *Building Base:* shall be a minimum of 7.5 metres and a maximum of 12.0 metres. Exceptions may be made for corner sites where the architectural feature is included that may encourage massing and designs that accentuate the visual prominence of the site.

#### **X.X.5 Signs**

The regulations governing signs in a B5B District are contained in **Appendix A - Sign Regulations**.

#### **X.X.6 Parking**

The regulations governing parking and loading in a B5B District are contained in **Section 6.0**.

### **X.X.7 Gross Floor Space Ratio**

- (1) The gross floor space ratio shall not exceed 7:1.
- (2) In the B5B District, above grade parking floor areas shall not be exempt from the calculation of the gross floor space ratio.
- (3) Notwithstanding Section (2), above grade parking floor areas which are needed to provide 1.25 parking spaces per dwelling unit and parking floor areas which are needed to provide parking at the rate of one parking space for every 24 square metres of gross leasable floor area for all other uses shall be exempt from the calculation of Gross Floor Space Ratio.
- (4) Floor areas used for below grade parking shall be deducted from the exemption outlined in Section (3), at the rate outlined in said Section.

### **X.X.8 Landscaping**

- (1) On sites used for, commercial parking lots, parking stations or multiple-unit dwellings a landscaped strip of not less than 3 metres in depth throughout lying parallel to and abutting the front site line shall be provided and shall be used for no purpose except landscaping and necessary driveway access to the site, and on corner lots, in addition to the landscaping required in the front yard, a landscaped strip of not less than 1.5 metres in width throughout lying parallel to the flanking street shall be provided.

In addition, on sites used for commercial parking lots or parking stations located at grade level, screening of the site from front streets, flanking streets and public lanes shall be provided to the satisfaction of the Development Officer.

### **X.X.9 Garbage Pickup Area**

- (1) A space to be used exclusively for garbage storage and pickup, having minimum dimensions of 2.7 metres by 6.0 metres, shall be provided on each site to the satisfaction of the General Manager, Utility Services Department. The required loading and pick up spaces may be combined where considered appropriate by the Development Officer.

## Theme

# *Broadway*

# *Heritage*

# *Core*

“The Broadway area comprises the ‘heart’, ‘town centre’, and ‘main street’ of the Nutana community. Established as a temperance colony in the late 19th century, Nutana is one of Saskatoon’s most desirable neighbourhoods, due in large part to its historic ‘small town’ charm, the success of Broadway Avenue commercial area, and the proximity to the Downtown and the University of Saskatchewan.

Neighbourhoods are constantly changing and evolving, and Broadway Avenue and the Nutana Neighbourhood is no exception. At the heart of Saskatoon’s original neighbourhood, the Broadway area has been the social and commercial core for over 125 years. In more recent years, the area’s central location, and its unique history can character, and the distinct mix of restaurants and stores has attracted new residents and visitors to the neighbourhood, contributing to the revitalization that has made Broadway Avenue a region-wide destination.”

Notwithstanding the intent of this document to control aspects of architectural development in the Broadway Commercial Area, the controls or guidelines provide direction for the quality of design for a variety of built form elements. As a means for ensuring that new buildings reinforce and enhance the best qualities of the Broadway area. Therefore the projects must support the theme of “Broadway Heritage Core “.

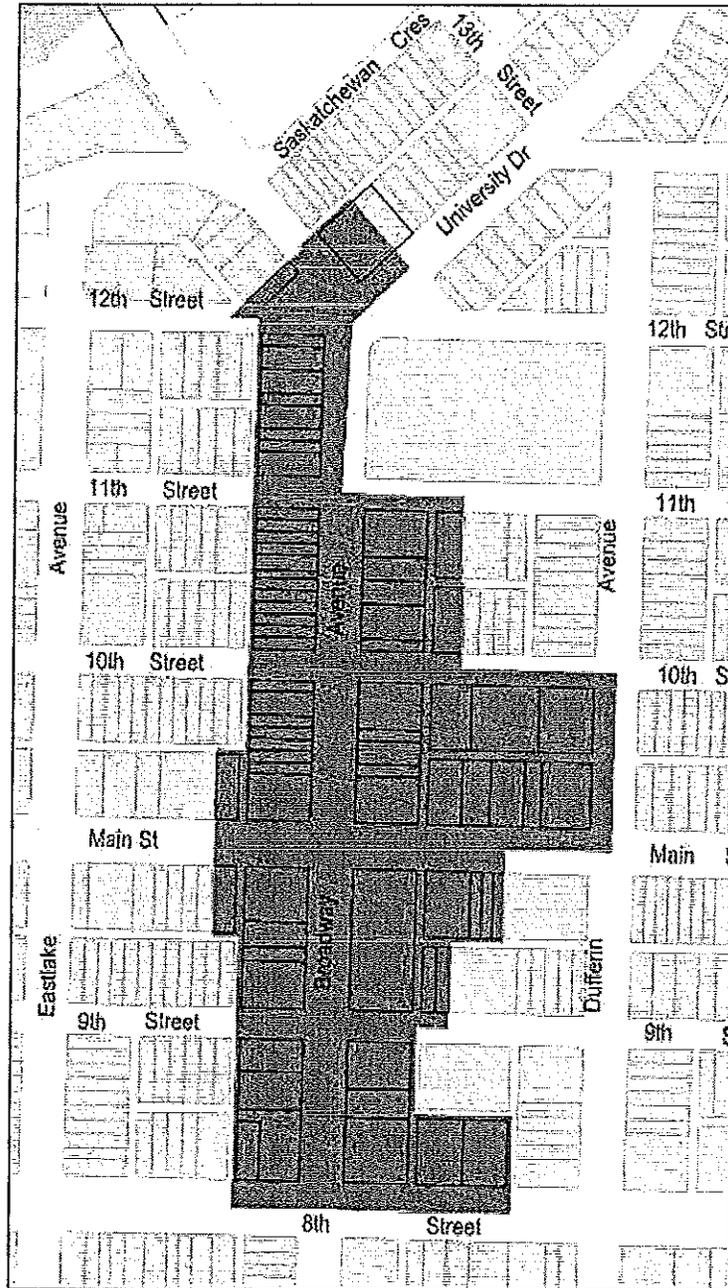
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## Development Principles

A central objective is to ensure that new buildings reinforce a coherent, harmonious and appealing urban environment, as well as contribute to the enhancement of the public realm. Informed by the consultation process and rooted in good planning and urban design practice, the key principles include:

- ❖ Protecting heritage resources and retaining their visual prominence.
- ❖ Protecting and strengthening established low-rise residential areas and ensuring compatible infill development and sympathetic developments in adjacent higher density areas.
- ❖ Ensuring base building conditions that form an appropriately scaled and designed street wall that reinforces the desired character at the street level.
- ❖ Ensuring appropriate building massing and height taking into consideration existing and permitted heights; proportional relationships to streets; and, visual and physical impacts on pedestrians and adjacent areas.
- ❖ Ensuring that new developments provide for appropriate transitions between areas of differing intensities and scales.
- ❖ Reinforcing important intersections and corners through massing and design.
- ❖ Well designed and articulated buildings that positively contribute to the quality and animation of the streetscape and the overall defining character and image of the area.

# Map of B5B Area



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# 1

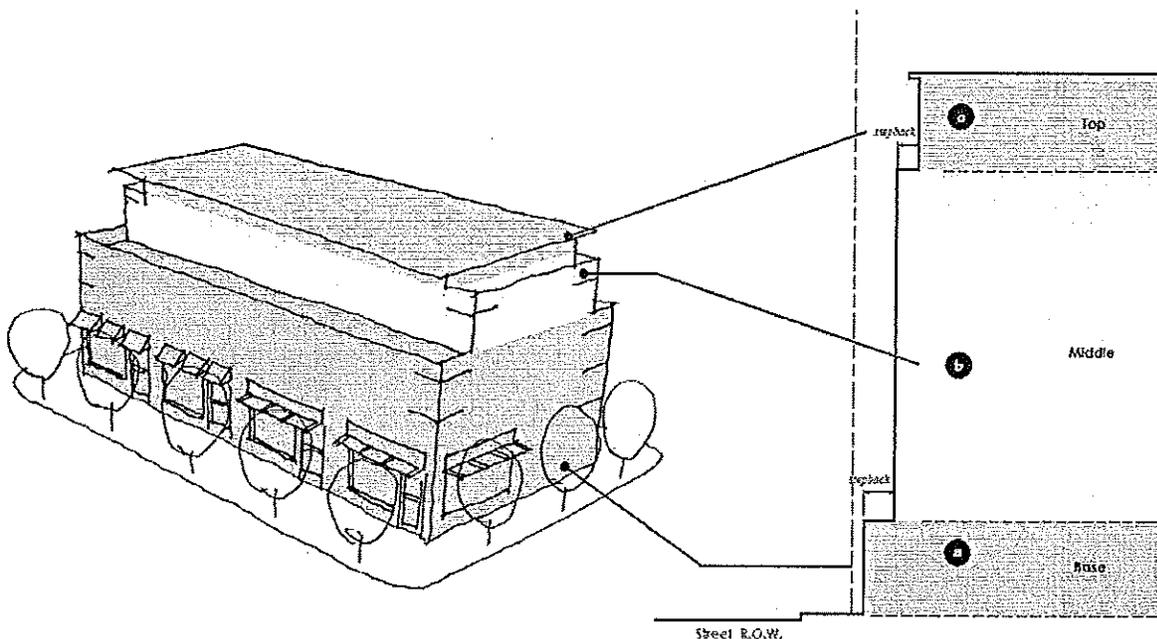
## Building Expression

Buildings should reinforce a *base, middle, and top* in their design.

**Base** – Within the first three storeys of a building, a clearly defined base will contribute to the quality of the pedestrian environment by providing animation, transparency, and articulation.

**Middle** – The body of the building should contribute to the physical and visual quality of the overall streetscape.

**Top** – The roof should be distinguished from the rest of the building and designed to contribute to the visual quality of the streetscape.



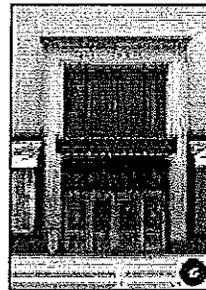
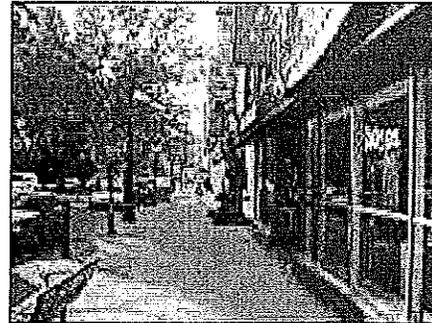
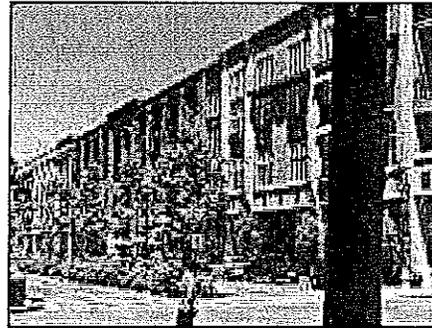
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## 2

### Orientation and Placement

Buildings can enhance the pedestrian environment by creating a sense of enclosure. This is achieved by framing the street with parallel aligned buildings and providing the appropriate levels of animation and use.

- ❖ All buildings should orient to the street with clearly defined entry points that directly access the sidewalk.
- ❖ A minimum of 70% of the front building line shall be located within 1.0 metre of the front property line.

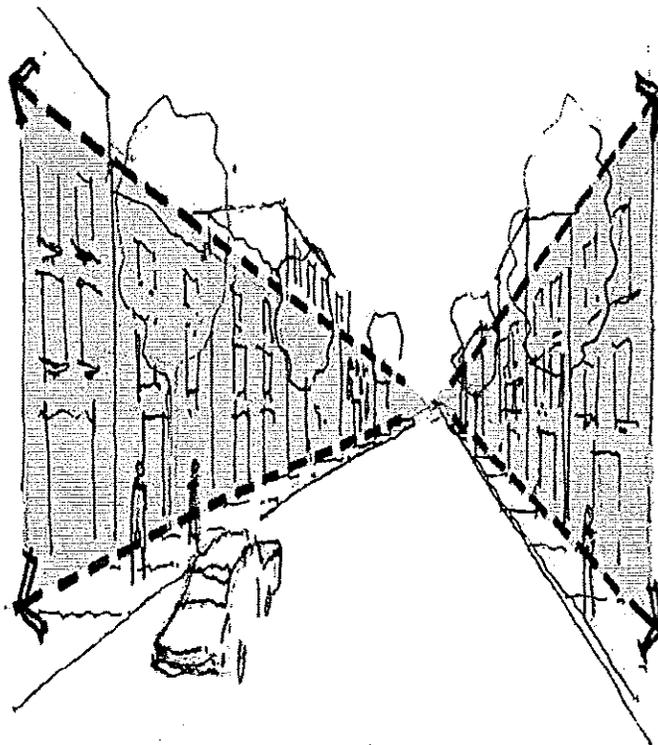


### 3

## Street Wall

The street wall has the greatest impact on the character of the street experience. The key design objectives for street walls in the Broadway Area ensure visual continuity, pedestrian scale, animation and design quality.

- ❖ A street wall of a new building should align with those of neighbouring buildings or have the same setback as the predominant buildings on the block.
- ❖ The height of the street wall should be consistent with historic heights of no greater than 3 storeys and no less than 2 storeys. Levels above the street wall should be set back to reinforce a low-rise interface with the sidewalk.
- ❖ The height of ground-level floors should be visually prominent and no less than 4.5 metres for commercial and 4.0 metres for residential uses.



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## 4

### Heritage Contexts

New buildings on Broadway Avenue should complement, rather than detract from, the character of older buildings.

#### General Guidelines

New buildings should avoid historical misrepresentation by not replicating past architectural styles.

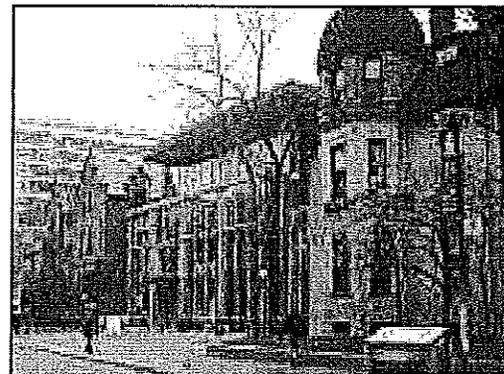
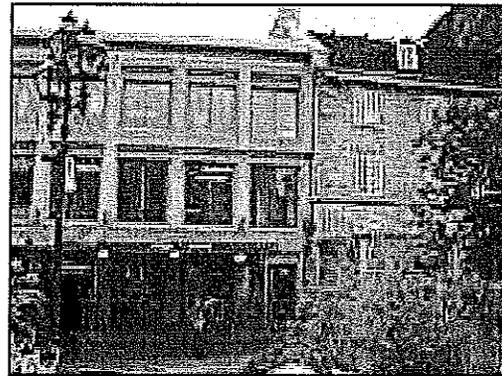
New buildings should consider and respect the scale, material and massing of adjacent heritage significant buildings.

#### Façade Articulation

New buildings should respect the pattern of façade division by ensuring the horizontal and vertical architectural orders, including windows and entries, are aligned with neighbouring heritage buildings or the established pattern on the block.

#### Façade Materials

New buildings should consider materials and colours evident in existing heritage significant properties. Building materials should be chosen for their functional and aesthetic quality. Exterior finishes should exhibit quality of workmanship, sustainability, permanence, and ease of maintenance.

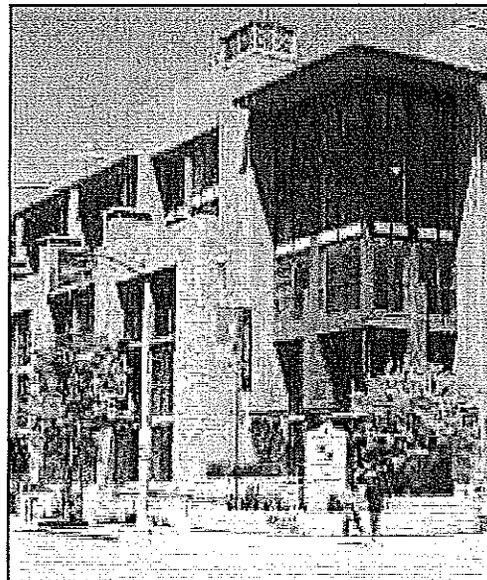


# 5

## Corner Sites

Corner buildings have a greater visual prominence given that they front onto two streets and frame intersections. To enhance the distinction of new buildings at Key Corner Sites, modest exceptions to setbacks and height restrictions should be permitted to encourage massing and designs that accentuate the visual prominence of the site.

- ❖ New developments on all corner sites should orient to both street frontages.
- ❖ Corner entrances should be encouraged wherever possible to address the two street frontages.

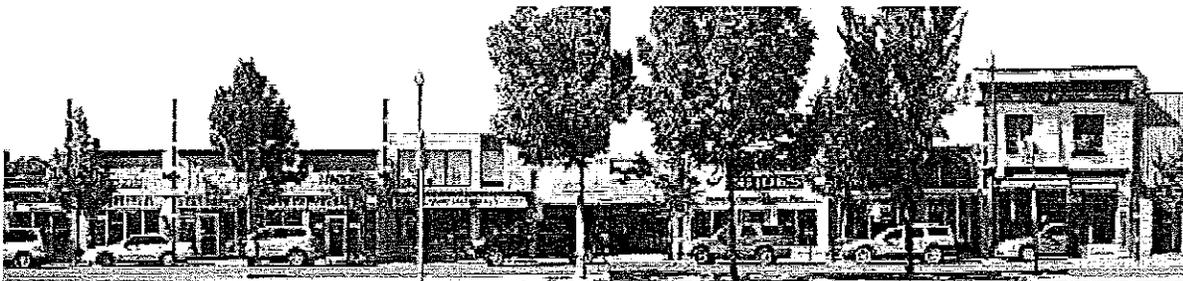


## 6

### Storefronts

Well proportioned and designed storefronts can provide animation and visual interest at the sidewalk.

- ❖ To reflect the existing character and context, storefronts should generally have a frontage in the range of 7.5 metres but not greater than 15 metres.
- ❖ Where frontages are greater than 7.5 metres, they should articulate narrow storefronts in the design of the facade.
- ❖ Storefronts should have a minimum of 75% glazing to maximize visual animation.
  - Clear glass should be used for windows and doors along the street-level façade. Dark tinted, reflective or opaque glazing should be discouraged for storefronts.
- ❖ Storefront entrances should be highly visible and clearly articulated. Entrances should be located at or near grade.
- ❖ Storefront signage should be consistent with the signage guidelines, but add diversity and interest to the street.
- ❖ Weather protection for pedestrians is encouraged through the use of awnings and canopies.



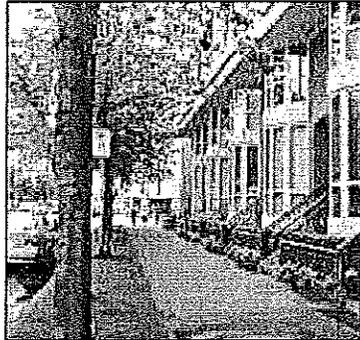
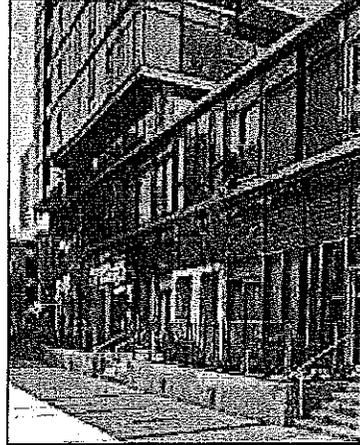
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# 7

## Residential Street Access Units

Where retail is not required, and residential uses are proposed at-grade, the following guidelines apply:

- ❖ Residential uses at-grade should include individual units accessed from the street.
- ❖ Appropriate front yard privacy measures should be considered such as setbacks, landscaping, and porches.
- ❖ Access to the individual units should be clearly visible, and the scale, rhythm and articulation of the street wall should be consistent with the residential character of adjacent neighbourhoods. Grade-level units should be designed to accommodate live-work opportunities and potential conversion into commercial or retail uses.

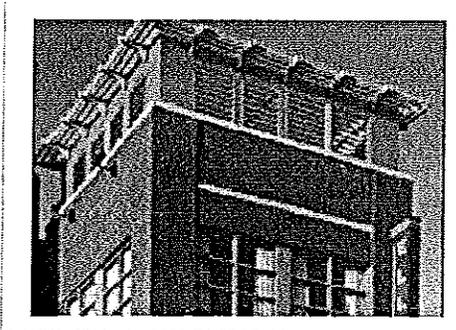
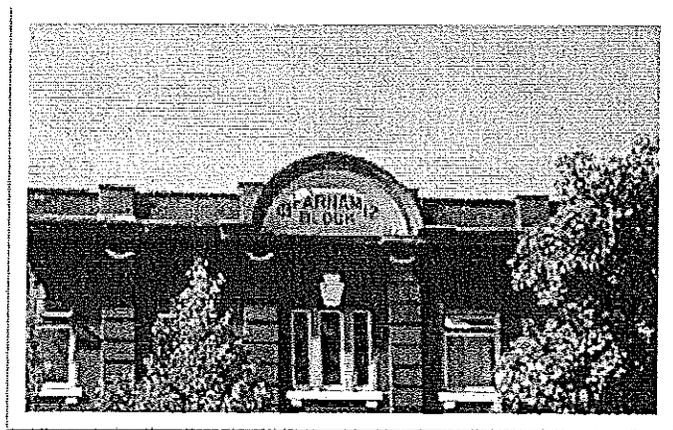
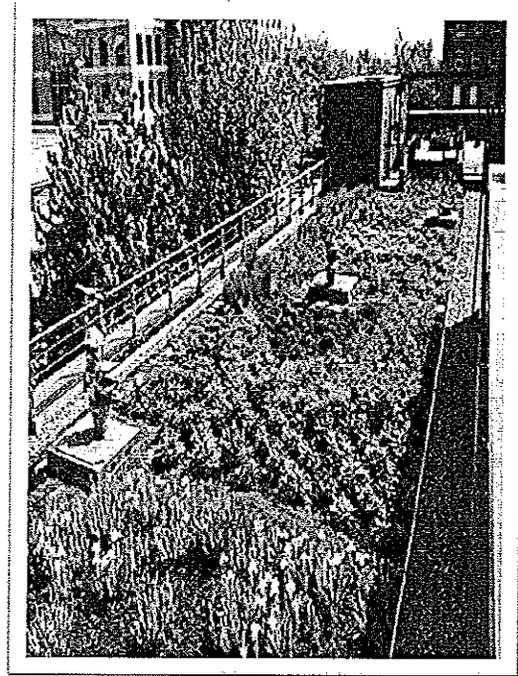


# 8

## Roof Treatment

The design of the roof can make an impact on the character of the streetscape, especially from great distances. Roofs are also seen from other buildings of equal or greater height.

- ❖ The expression of the building top and roof should be clearly distinguished from the rest of the building through treatments such as stepbacks, change in materials, cornices lines, and overhangs.
- ❖ Mechanical penthouses should be integrated with the architectural treatment of roofs and/or screened from view.
- ❖ Green roofs should be encouraged.



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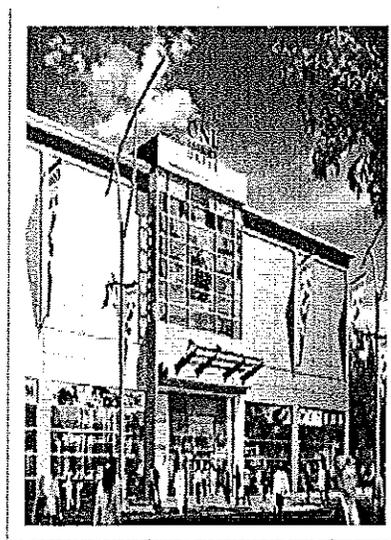
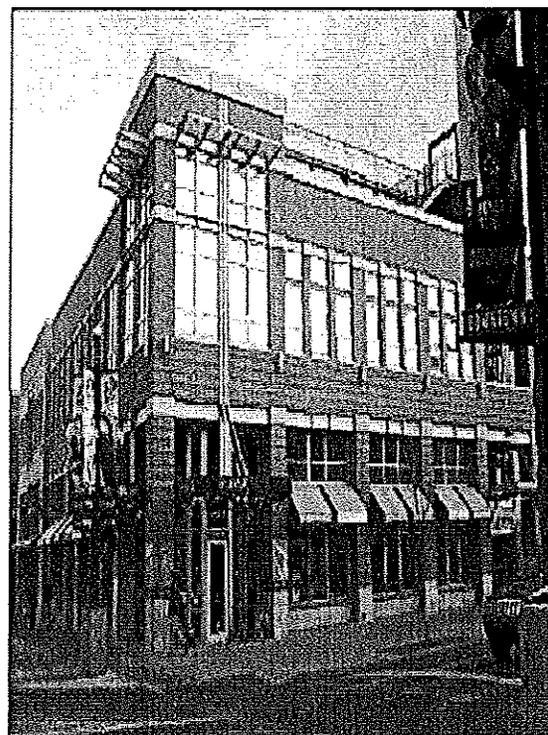
## AC2 – B5B – Architectural Control Overlay District

### 9

#### Above Grade Parking

Wherever possible, parking for new developments should be provided at the rear or below-grade and accessed off the rear lane. However, where parking is provided above-grade within the base building, the following guidelines address the design and quality of such structures.

- ❖ Direct access for parking, loading, and service areas from the street should be discouraged.
- ❖ Where an above-grade parking facility fronts on a street, the ground-level frontage should incorporate retail, public or other active uses.
- ❖ Above-grade parking structures should be designed in such a way that they reinforce the intended built character and blend into the streetscape.



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# 10

## Material & Architectural Quality

New developments should ensure excellence in architectural design and in the use of high-grade materials, particularly at street-level. A key objective of the Broadway 360° Development Plan is to achieve a balance between consistencies in design quality and street interface, while enabling individual expression in new developments. Key guidelines for architectural and material quality include:

- ❖ The Broadway area has a rich history of development that is reflected in the Prairie-style ‘main street’ buildings that are constructed in a variety of materials. New developments should seek to contribute to this mix and variety.
- ❖ Building materials should be chosen for their functional and aesthetic quality and exterior finishes should exhibit quality of workmanship, longevity, sustainability and ease of maintenance.
- ❖ Building materials recommended for new construction include brick, stone, wood, glass, in-situ concrete and pre-cast concrete.
- ❖ In general, the appearance of building materials should be true to their nature and should not mimic other materials.
- ❖ Vinyl siding, plastic, plywood, concrete block, darkly tinted and mirrored glass and metal siding utilizing exposed fasteners should be discouraged.

Recommended Materials	Not Recommended Materials
Brick	Vinyl siding
Stone	Plastic
Wood	Plywood
Glass	Darkly tinted or mirrored glass
In-situ concrete	Concrete Block
Pre-cast concrete	Metal siding with exposed fasteners



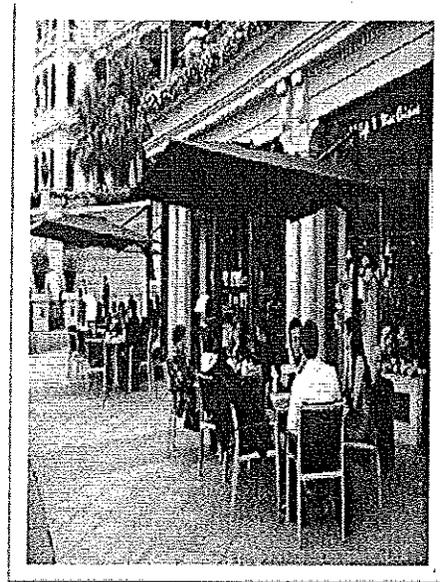
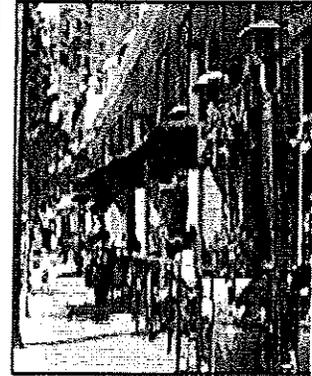
# 11

## Sidewalk Cafes

Sidewalk cafés enhance the vibrancy of street life, further enable social interaction, and are major destinations in the warmer months.

Sidewalk cafés should be encouraged throughout the Broadway Area provided there are no conflicts with adjacent land uses and they are able to be accommodated within the existing sidewalk width dimensions without encumbering pedestrians.

- ❖ Where permitting, small sidewalk cafés should be encouraged along streets with narrower sidewalks as well.
- ❖ Sidewalk cafés should be designed to contribute and integrate into the streetscape.
- ❖ Curb bump-outs should be encouraged at all corners to provide for additional sidewalk café opportunities.
- ❖ Rear yard and roof top patios should be directed to properties that are not directly adjacent to residential neighbourhood.



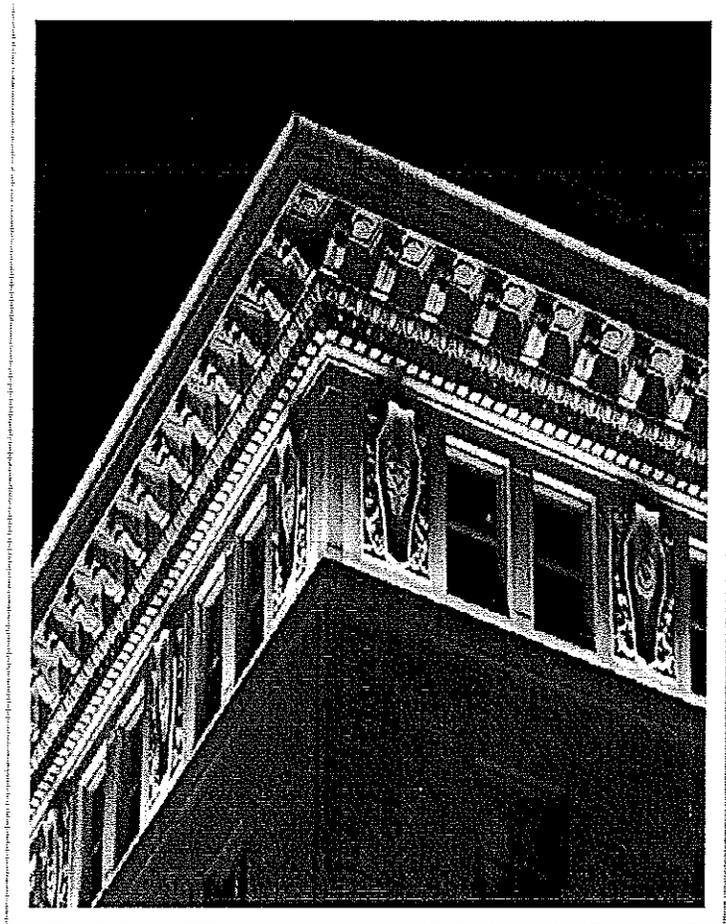
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# 12

## Building Lighting

The image and experience at night is an important aspect of any mixed-use area

- ❖ Attractive landscape and architectural features can be highlighted with spot-lighting or general lighting placement.
- ❖ Heritage and institutional buildings, as well as landmark elements such as public art, steeples or distinctive rooflines, should be illuminated.
- ❖ Subtle night-lighting of retail display windows should be encouraged.

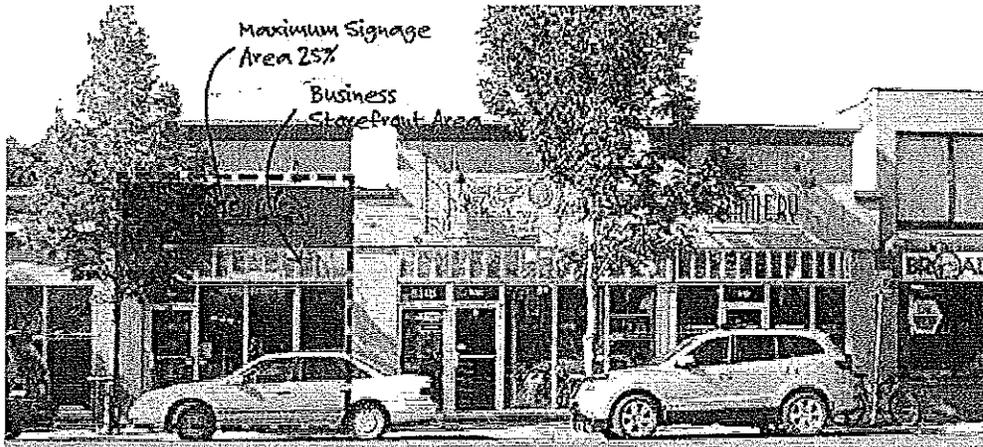
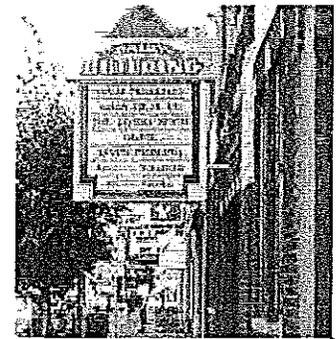


# 13

## Signage

Signage plays an important role in the overall image of any area. Signs should contribute to the quality of individual buildings and the overall streetscape. This includes compatibility with heritage buildings, where appropriate. High quality, imaginative, and innovative signs are also encouraged.

- ❖ The maximum signage area for storefront signs should be no more than 25% of the business storefront.
- ❖ Back lit illuminated rectangular sign boxes are discouraged.
- ❖ Signage should not obscure windows, cornices or other architectural elements.
- ❖ Signage should aid pedestrians and drivers in navigating the area, especially at night.
- ❖ Billboards, super boards, and roof mounted signs are not permitted.



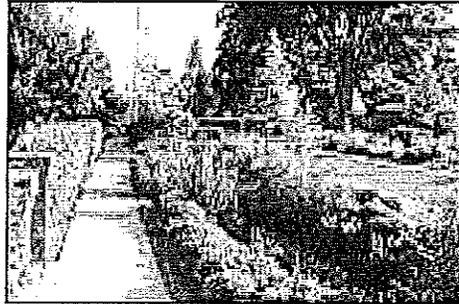
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# 14

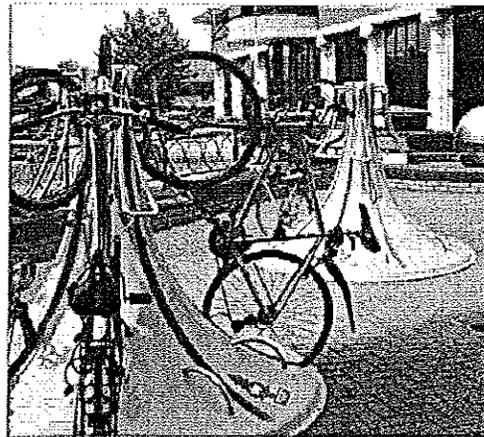
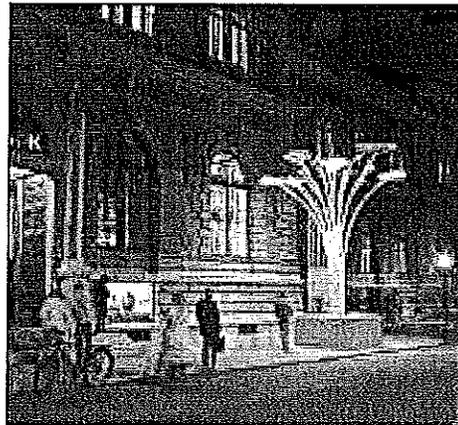
## Sustainable Design

Conservation of natural resources and systems should be a primary consideration in the planning, design, and construction process. To achieve this, all proposed projects should strive for sustainable building practices. This includes public as well as private development, and encompasses streets, parks, and buildings.

New additions, exterior alterations, or related new construction should not destroy historic materials, features, and spatial relationships that characterize the property. The new work should be differentiated from the old and should be compatible with the historic materials, features, size, scale, height, proportion and massing to protect the integrity of the property and its environment.



Good view towards from the street is captured in a natural setting.



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## List of Terms

- ❖ **Animation** concerns spaces that have an animated quality; liveliness; movement; activity.
- ❖ **Articulation** is the emphasis or accentuation of different parts of a building so that they are distinct and stand out clearly.
- ❖ **Building setback** is the distance at-grade (ground level) that the building is set back from the property line.
- ❖ **Building stepback** is the distance, above the base building, that the remaining portion of the building (building cap) is set back from the face of the base building.
- ❖ **Cornice** is a decorative molding that crowns a building.
- ❖ **Floor Area Ratio (FAR)** is the ratio of gross floor area of the building divided by the site area.
- ❖ **Glazing** is the part of a wall or window that is glass.
- ❖ **Mechanical Penthouse** is the covering or enclosure on the roof of a building that houses mechanical systems or equipment for the building.
- ❖ **Rhythm**, in architecture, is the repetitive or alternating use of visual elements to create a pattern.
- ❖ **Transparency** refers to the degree to which people can see or perceive what lies beyond the street edge, often through windows, doors, fences and landscaping.
- ❖ **Parapet** is a low wall projecting from the edge of a platform, terrace, or roof.
- ❖ **Storefront** refers to an area on the frontage of a building that is delineated by features to indicate a separate or distinctive “frontage”. Larger building frontages should be divided into narrower storefronts to create visual animation and visual interest at the sidewalk.

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**Broadway 360 Technical Workshop**  
**Cosmopolitan Senior Citizens Centre – 614 11<sup>th</sup> Street East**  
**January 24, 2012**  
**5:30 pm**

On behalf of the Broadway 360 Steering Committee, Sarah Marchildon, Executive Director of the Broadway Business Improvement District, thanked everyone for attending this technical workshop on proposed land use changes. The purpose of tonight's meeting is to review the proposed B5B Zoning District and proposed Architectural Control District for the Broadway commercial area. Tim Steuart will give a presentation with question period following. After formal part of meeting, the Steering Committee will be around for more one on one discussion.

**Broadway 360 Land Use Implementation**  
**Tim Steuart, Senior Planner, Business License & Zoning Compliance Section**

A bit of background. This came together from a unique cooperative effort between property owners, residents, and the BID to make Broadway a better place. A comprehensive study was done with the goal of recognizing that Broadway is a special area, a cool area created with pedestrian environment. Everyone involved didn't want to assume it will stay this way forever, but there was a desire try to ensure it does. The Steering Committee has come up with a very good plan. One issue was zoning and to ensure the zoning treatment is satisfactory to all.

All properties in the Broadway Commercial Area currently zoned B5 (Inner-City Commercial Corridor) are proposed to be rezoned to the new B5B (Commercial Zoning District), a zoning district designed exclusively for Broadway.

The new B5B includes changes to the permitted uses and development standards. Also, all B5B-zoned properties would be subject to an Architectural Control District.

Saskatoon is growing strongly and we need to grow up as well as out, creating the urban living room.

- The first three storeys matter most.
- Density done properly has many benefits.
- The stepback enhances the pedestrian experience.

What factors will affect the overall height of a building?

- Gross Floor Area Ratio (7:1) which means that the maximum building volume can be 7 times the site area
- Site size (bigger, taller buildings can be built on larger sites)
- Height of base building (must be a minimum 7.5 metres to a maximum of 12 metres)
- Setback and stepbacks (must meet minimum requirements, setbacks affect the overall building form and height)
- Parking
  - Amount of required parking

- Location of parking (at grade at rear of site, below grade in parking structure, or above grade in parking structure)

Do not want building setback from the street. Requirement that at least 70% of building face the street, setback for courtyard. Removed service stations and commercial parking lot from permitted uses. Parking provision is currently 1.25 parking spaces for residents and 1 visitor for every 8 dwellings. This has been reduced to 1 parking space for residents while visitor parking has remained the same. Nothing set for commercial buildings and not proposing changes as it is usually self-regulating since developers understand that providing parking helps to attract potential tenants, so a minimum parking provision does not seem necessary.

The proposed B5B Zoning District would be subject to an Architectural Control District (ACD) intended to preserve the physical character of the area.

The ACD would contain a set of design guidelines, known as the Broadway Commercial Area Design Plan that all new development in the B5B District must conform to. Establishing an Architectural Control District (ACD) allows for enforceable design controls in the Broadway Commercial Area.

Currently, the only ACD in Saskatoon is River Landing, which for the most part, is new buildings and the land was mostly owned by City. This would be a first in Saskatoon with infill or a character area, maybe even the first in Saskatchewan. This has long been standard in other provinces and the U.S.

Councillor Clark stated this process involved a collection of groups that came together with different views and have created something not only for Saskatoon, but the whole province. Everyone walked down the street and pointed out their issues and these issues helped develop the guidelines through this consultative process.

The goal now is to find out what the public thinks of this proposal, before taking it to City Council.

The process for ACD is an application for development permit which is then reviewed by a committee of design professionals from landscape, architectural, community planning backgrounds. The committee has approximately 15 members, with 3 selected to review each application. The entire process usually takes about 60 days. It is on a professional level and not just someone's pet peeves or personal preferences.

## **There are 14 design guidelines in the Broadway Commercial Area Design Plan**

### **1. Building Expressions**

- Buildings should reinforce a *base*, *middle*, and *top* in their design.
- Within the first three storeys of a building, a clearly defined *base* contributes to the quality of the pedestrian environment.
- The *middle* or body of a building should contribute to the overall quality of the streetscape.

- The *top* or roof should be distinguished from the rest of the building.

## 2. Orientation & Placement

- All buildings should orient to the street with clearly defined entry points that directly access the sidewalk.
- A minimum of 70% of the front building line shall be located within 1.0 metre of the front property line.
- Buildings can enhance the pedestrian environment by creating a sense of enclosure.

## 3. Street Wall

- A street wall of a new building should align with those of neighbouring buildings or have the same setback as the predominant buildings on the block.
- The height of the street wall should be consistent with historic heights of no greater than 3 storeys and no less than 2 storeys.
- Levels above the street wall should be set back to reinforce a low-rise interface with the sidewalk.

## 4. Heritage Context

- New buildings on Broadway Avenue should complement, rather than detract from, the character of older buildings.
- General Guidelines – New buildings should avoid historical misrepresentation by not replicating past architectural styles, and should respect the scale, material and massing of adjacent heritage buildings.
- Façade Articulation – New buildings should ensure the horizontal and vertical architectural orders including windows and entries, are aligned with neighboring heritage buildings or the established pattern on the block.

## 5. Corner Sites

- Corner buildings have a greater visual prominence given that they front onto two streets and frame intersections.
  - Designs and massing of corner buildings should accentuate the visual prominence of the site.
  - Corner buildings should orient to both street frontages and, wherever possible, have entrances that address both frontages.

## 6. Storefronts

- To provide animation and visual interest, storefronts should have:
  - A frontage in the range of 7.5 metres.
  - A minimum of 75% glazing to maximize visual animation.
  - Entrances that are highly visible and located at or near grade.
  - Signage that adds diversity and interest to the street.
- Dark tinted, reflective, or opaque glazing should be discouraged for storefront glazing

## 7. Residential Street Access Units

- In buildings where residential uses are located at-grade:
  - The individual units should be accessed from the street

- Appropriate front yard privacy measures should be taken
- Access to the units should be consistent with the residential street character in Nutana

## **8. Roof Treatment**

- Roof design should consider the following guidelines:
  - The use of stepbacks, changes in materials, cornice lines and overhangs
  - Screen mechanical penthouses from view
  - Green roofs are encouraged

## **9. Above Grade Parking**

- Where parking is provided at grade, the following guidelines address the design quality of the facility:
  - Direct access from the street is discouraged.
  - Ground level retail should be incorporated, where the parking structure fronts a street.
  - Parking structures should be designed to reinforce the built character and blend into the streetscape.
- Broadway 360° recommends that, wherever possible, parking should be provided in the rear yard or below grade, and should be accessed from the lane.

## **10. Material & Architectural Quality**

- New developments should contribute to the Prairie-style Main Street building style that exists
- High quality materials should be chosen that are both functional and aesthetically pleasing
- Materials chosen should not mimic other materials
- A key objective of Broadway 360 is to achieve a balance between consistencies in design quality & street interface, while enabling individual expression.

## **11. Sidewalk Cafes**

- Should be encouraged along all sidewalks
- Should contribute and integrate into the streetscape
- Corners with “curb bump outs” could provided additional opportunities
- Rear yard and roof-top patios should not abut residential areas
- Sidewalk cafes should be encouraged, provided they don’t cause land use conflicts or encumber pedestrian movements.

## **12. Building Lighting**

- Both landscape and architectural features can be highlighted
- Landmarks & distinctive features of buildings should be illuminated
- Subtle night lighting of retail displays should be encouraged

## **13. Signage**

- Storefront signs should be no more than 25% of the business storefront
- Should aid pedestrians & drivers, especially at night

- Backlit rectangular sign boxes should be discouraged
- Signage should not obscure building features.
- Signage Group 5, in the City of Saskatoon Zoning Bylaw is applied to Broadway Avenue

#### 14. Sustainable Design

- Projects should strive for sustainable building practices
- When adaptive reuse projects are undertaken to rehabilitate historic buildings, the old and new should be compatible in terms of historic materials, features, size, scale, height, proportion and massing to protect the integrity of the property and its environment

#### Questions:

- Could you briefly describe gross floor space exemptions for parking?

Under the B5 Zoning District currently, certain things are exempt from, like indoor parking is not counted towards the total area of the building. E.g. you have a parking floor it does not count as gross floor space. Steering committee was concerned about very large and tall buildings also providing a significant amount of above grade commercial parking. Parking above grade will count towards the ration with the exception of the parking you need for the building itself. We will look at the parking area and the parking you have to provide for dwelling units and the parking for commercial area at rate of 24 square meters plus the drive lanes and the ramps. We will credit that back to you; it will not count toward parking structure.

- Parking with mixed used development, how do you treat the artist who works and lives in place?

The live/work unit would count as a residential dwelling, so one parking space would be required.

- Are there still no parking requirements for commercial property?

No, this remains the same.

#### Comments:

- It is a huge challenge task to bring this into place. Almost to mold into a campus area. Guidelines are good to control design, but if too wide open it's difficult to administer. However, you don't want it too stringent either. Couple concerns with the first two guidelines.

- Base, middle and cap seems simple and perfect, but concern with wall aspect of a 2 or 3 storey building, not sure if good for street. Maybe need to have a restriction of number of stories as well. Need more to guide this.
- 70% frontage and street wall... How will this affect the Extra Foods or Oskayak School if they change ownership?

- Signage and indirect architectural lighting sections are good.
- Storefront is critical for this process.

Tim noted many issues will be covered off by codes as they are more stringent than guidelines can be.

- Some concern with the height allowed for building, seems allowance is too high. Buds is 5 stories and that is fine, but going 12 stories seems high.

Tim noted the ratio of the historic building height will be followed.

- Good solid principle, it allows for a bit of variety, very sound, but is there any room allowed for odd variety that still maintains the character?
- It seems like the smaller lots may not be able to follow the 7:1 ratio.

Tim noted it is important to maintain the pedestrian feel.

- Last area is sustainable design, which is about providing general encouragement and not specific guidelines.

Tim stated the design and statement of intent. It is more becoming the norm, it is good business sense. Don't really need to regulate it as it is the way of good business, much the same as the parking provision.

- **Is there a goal regarding the ratio of residential to commercial property?**

There is no specific quota, but the goal is to have mixed uses. Fact of the matter is Broadway is a very healthy area of mixed use and hope it will continue.

Tim thanked everyone for their time and reminded the group that the Steering Committee members will be around for anyone who wishes to have a more one to one conversation.

For more information please visit: [www.broadway360.ca](http://www.broadway360.ca)

**Public Open House  
Broadway 360 Land Use Implementation  
Proposed B5B and ACD  
Thursday, January 19, 2012**

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#### **Submitted via Broadway 360 website:**

14.

- I think the type of successful community that Broadway has been since it was redeveloped almost 30 years ago, is one that is people driven rather than architecture driven. Though heritage buildings provide a basic element, with new construction a sort of stylized ambience can be built, but the hollow act of simply inserting nice looking buildings won't prove fruitful for the continued success of the district in the long term.

My belief is that architecture should follow use and that design is for people to work, live and play in, and this can be satisfied through community building that essentially relies on establishing mixed use and mixed levels of affordability.

The things that make Broadway special are the people who live and work there every day. The owner-operator businesses provide the attraction to people coming into the area for the unique shops and restaurants. The BBID support every aspect of day-to-day life and year round events that Saskatoon has

become known for – unfortunately these independent businesses are fading with the end of each lease period and may soon be gone. Housing is a prime issue in Saskatoon; increased density can be achieved gracefully through multi-level-use planning.

What I suggest is that we, in whatever way it can be arranged, strive to put in place in a rule book to guarantee we can grow and preserve at once, for example: the B5B outlines the perimeters, sets heights defines set backs etc. But it's the 'inner workings' of a building that will contain and define whether or not a building is successfully integrated to support the community.

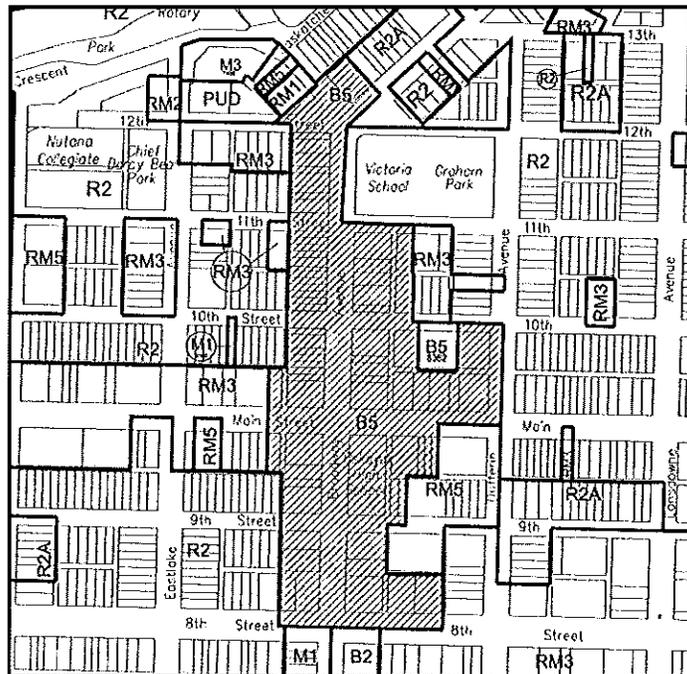
To adhere to an integration and affordability platform will ensure that each building is designed in measured percentages that include mixed use lease and resale units – a variety of sizes of each type of unit (because size determines market value, lease rate, sale price, etc), and a variety of each unit per a percentage of each of type of use; be it retail, office or residential.

That this kind of coding of multiple use, multi-layered affordability could be what in fact defines a very healthy future for Broadway. And this may be the only way we can ensure that Broadway remains a people place, that there will be something for everyone by-design regulations for sizes and uses of interior spaces. In this way the 360 plan can fulfill its mandate, be a model, impress on developers that mixed use, community affordability is incredibly important to people now and future generations of the Broadway District and for that matter, anywhere in Saskatoon.

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z19/12	<b>PROPOSAL</b> Proposed Rezoning from B5 to B5B and AC2 - B5B	<b>EXISTING ZONING</b> B5 Inner City Commercial Corridor District
<b>LEGAL DESCRIPTION</b> Various		<b>CIVIC ADDRESS</b> Various
		<b>NEIGHBOURHOOD</b> Nutana
<b>DATE</b> July 30, 2012	<b>APPLICANT</b> City of Saskatoon Planning and Development Branch 222 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 0J5	<b>OWNER</b> Various

### LOCATION PLAN



**REZONING**

From B5 to B5B and AC2-B5B

City of  
**Saskatoon**  
Planning & Development Branch

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**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATIONS:**

that a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising respecting the proposal to amend Zoning Bylaw No. 8770 as outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770;
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposal to rezone the properties indicated on Attachments 2 and 3 from a B5 Zoning District to a B5B Zoning District be approved; and
- 5) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposal to apply the AC2 – B5B Architectural Control Overlay District to the properties indicated on Attachments 2 and 3 be approved.

**B. PROPOSAL**

The Planning and Development Branch is requesting that the properties in the Broadway commercial area, as indicated on Attachments 2 and 3, be rezoned from a B5 to a B5B Zoning District and that the AC2 – B5B Architectural Control Overlay Zoning District be applied to the properties.

A companion report has been submitted to amend Zoning Bylaw No. 8770 to create the B5B Broadway Commercial Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District.

The Planning and Development Branch is requesting 72 properties, located in the Nutana neighbourhood (see Attachment 3), be rezoned from a B5 Zoning District to a B5B Zoning District. The B5B Zoning District contains development standards that will ensure that new development in the area enhances the existing urban environment in the Broadway Avenue commercial area.

The Planning and Development Branch is also requesting that the AC2 – B5B Architectural Control Overlay Zoning District be applied to these properties in the Broadway Avenue

commercial area. This district is an architectural overlay district containing 14 design guidelines that will ensure the quality of design for new construction.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed B5B Zoning District is intended to be applied in the Broadway Avenue commercial area. This district will provide development standards to ensure that new development enhances the existing urban environment along this important commercial corridor.

The proposed AC2 – B5B Architectural Control Overlay Zoning District is also intended to be applied in the Broadway Avenue commercial area; and it will impose design guidelines on all new developments in this area. The design guidelines will ensure that new buildings preserve character and enhance the best qualities of the Broadway Commercial Area. The proposed B5B Zoning District is a component of the implementation of the Broadway 360 Development Plan.

**D. BACKGROUND INFORMATION**

In 2007, the Broadway Business Improvement District (BID) and the Nutana Community Association proposed that a study of the Broadway Commercial Area be undertaken. It was noted by both groups that some of the recommendations contained in the 2001 Nutana Local Area Plan (LAP) had become outdated, or difficult to implement due to community or property owner resistance, and there were concerns about the future character of Broadway Avenue. The City of Saskatoon (City) was invited to participate as a partner with the Broadway BID and Nutana Community Association in the development of a “Broadway Area Plan” to address outstanding recommendations in the Nutana LAP.

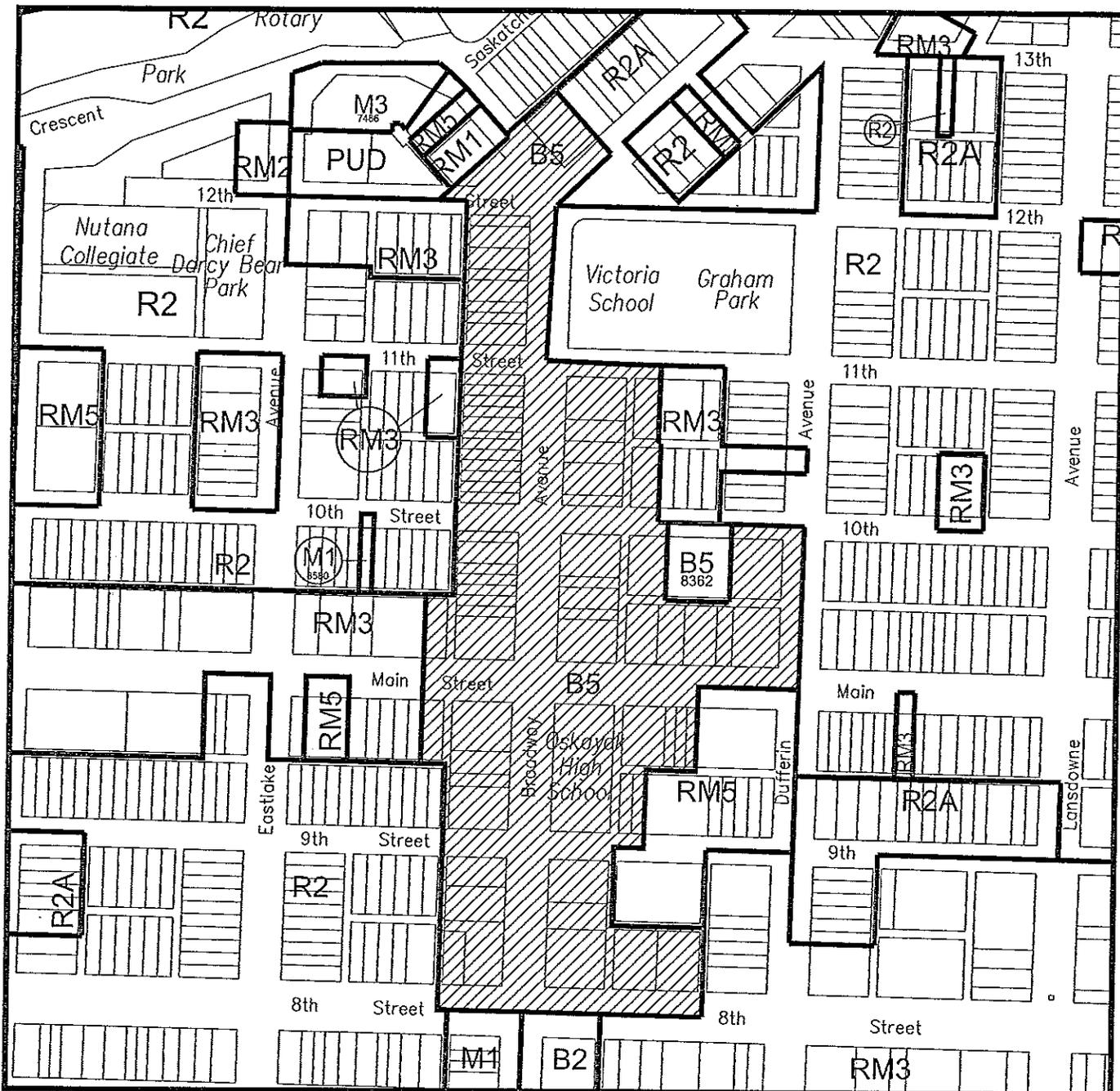
The Planning Partnership, a Toronto-based urban planning consultant firm, was hired to prepare what would become the Broadway 360 Development Plan (Plan). The Plan involved a thorough public consultation process that engaged residents, business and commercial property owners, area schools, churches, and those representing Saskatoon’s heritage community.

The Plan explored practical urban development solutions to address land use, street character, safety, parking, and traffic issues in the Broadway area.

The Steering Committee that was struck during the creation of the Plan was comprised of developers, commercial property owners, Nutana residents, the Nutana Community Association, the BID, and the Ward Councillor. The Administration worked very closely with the Steering Committee over several months to prepare detailed zoning requirements that would be acceptable to the various stakeholders. A report creating the B5B Zoning District and the AC2 – B5B Architectural Control Overlay District has been submitted separately (see Application No. Z14/12).

ATTACHMENT 1

<b>FACT SUMMARY SHEET</b>	
<b>A. Location Facts</b>	
1. Municipal Address	See Attachment 2
2. Legal Description	Various
3. Neighbourhood	Nutana
4. Ward	6
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Various
2. Proposed Use of Property	Various
3. Adjacent Land Uses and Zoning	
North	Residential – RM1, R2A, and R2
South	Institutional – M1, Commercial – B2, and Residential – RM3
East	Residential – RM5 and RM3
West	Residential – RM3 and R2
4. No. of Existing Off-Street Parking Spaces	Not applicable
5. No. of Off-Street Parking Spaces Required	Not applicable
6. No. of Off-Street Parking Spaces Provided	Not applicable
7. Site Frontage	Various
8. Site Area	Various
9. Street Classification	Arterial and Local
10. No. of Sites	72
<b>C. Development Plan Policy</b>	
1. Existing Development Plan Designation	Commercial – Special Area
2. Proposed Development Plan Designation	Commercial – Special Area
3. Existing Zoning District	B5 – Inner-City Commercial Corridor District
4. Proposed Zoning District	B5B – Broadway Commercial District AC2 – B5B Architectural Control Overlay Zoning District



# REZONING

From B5 to B5B and B5B(ACD2) —



**City of  
Saskatoon**

Planning & Development Branch

## List of Properties Proposed to be Rezoned from B5 to B5B and AC2 – B5B

615	10th	St	E	806	Broadway	Ave	
616	10th	St	E	810	Broadway	Ave	
634	10th	St	E	813	Broadway	Ave	
612	11th	St	E	814	Broadway	Ave	
535	8th	St	E	817	Broadway	Ave	
601	8th	St	E	818	Broadway	Ave	
617	8th	St	E	820	Broadway	Ave	
619	8th	St	E	821	Broadway	Ave	
611	9th	St	E	824	Broadway	Ave	
613	9th	St	E	834	Broadway	Ave	
1002	Broadway	Ave		835	Broadway	Ave	
1005	Broadway	Ave		906	Broadway	Ave	
1006	Broadway	Ave		912	Broadway	Ave	
1010	Broadway	Ave		916	Broadway	Ave	
1011	Broadway	Ave		919	Broadway	Ave	
616	Broadway	Ave		922	Broadway	Ave	
626	Broadway	Ave		526	Main	St	
630	Broadway	Ave		527	Main	St	
632	Broadway	Ave		616	Main	St	
638	Broadway	Ave		617	Main	St	
640	Broadway	Ave		619	Main	St	
642	Broadway	Ave		620	Main	St	
644	Broadway	Ave		621	Main	St	
650	Broadway	Ave		622	Main	St	
702	Broadway	Ave		626	Main	St	
704	Broadway	Ave		629	Main	St	
706	Broadway	Ave		639	Main	St	
707	Broadway	Ave		611	University	Dr	
708	Broadway	Ave		*628	10 <sup>th</sup>	St	E
712	Broadway	Ave			*AC2-B5B only		
714	Broadway	Ave					
715	Broadway	Ave					
718	Broadway	Ave					
720	Broadway	Ave					
723	Broadway	Ave					
724	Broadway	Ave					
726	Broadway	Ave					
730	Broadway	Ave					
732	Broadway	Ave					
733	Broadway	Ave					
735	Broadway	Ave					
801	Broadway	Ave					
802	Broadway	Ave					

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**Submitted via Broadway 360 website:**

14.

- I think the type of successful community that Broadway has been since it was redeveloped almost 30 years ago, is one that is people driven rather than architecture driven. Though heritage buildings provide a basic element, with new construction a sort of stylized ambience can be built, but the hollow act of simply inserting nice looking buildings won't prove fruitful for the continued success of the district in the long term.

My belief is that architecture should follow use and that design is for people to work, live and play in, and this can be satisfied through community building that essentially relies on establishing mixed use and mixed levels of affordability.

The things that make Broadway special are the people who live and work there every day. The owner-operator businesses provide the attraction to people coming into the area for the unique shops and restaurants. The BBID support every aspect of day-to-day life and year round events that Saskatoon has

become known for – unfortunately these independent businesses are fading with the end of each lease period and may soon be gone. Housing is a prime issue in Saskatoon; increased density can be achieved gracefully through multi-level-use planning.

What I suggest is that we, in whatever way it can be arranged, strive to put in place in a rule book to guarantee we can grow and preserve at once, for example: the B5B outlines the perimeters, sets heights defines set backs etc. But it's the 'inner workings' of a building that will contain and define whether or not a building is successfully integrated to support the community.

To adhere to an integration and affordability platform will ensure that each building is designed in measured percentages that include mixed use lease and resale units – a variety of sizes of each type of unit (because size determines market value, lease rate, sale price, etc), and a variety of each unit per a percentage of each of type of use; be it retail, office or residential.

That this kind of coding of multiple use, multi-layered affordability could be what in fact defines a very healthy future for Broadway. And this may be the only way we can ensure that Broadway remains a people place, that there will be something for everyone by-design regulations for sizes and uses of interior spaces. In this way the 360 plan can fulfill its mandate, be a model, impress on developers that mixed use, community affordability is incredibly important to people now and future generations of the Broadway District and for that matter, anywhere in Saskatoon.

**Broadway 360 Technical Workshop**  
**Cosmopolitan Senior Citizens Centre – 614 11<sup>th</sup> Street East**  
**January 24, 2012**  
**5:30 pm**

On behalf of the Broadway 360 Steering Committee, Sarah Marchildon, Executive Director of the Broadway Business Improvement District, thanked everyone for attending this technical workshop on proposed land use changes. The purpose of tonight's meeting is to review the proposed B5B Zoning District and proposed Architectural Control District for the Broadway commercial area. Tim Steuart will give a presentation with question period following. After formal part of meeting, the Steering Committee will be around for more one on one discussion.

**Broadway 360 Land Use Implementation**  
**Tim Steuart, Senior Planner, Business License & Zoning Compliance Section**

A bit of background. This came together from a unique cooperative effort between property owners, residents, and the BID to make Broadway a better place. A comprehensive study was done with the goal of recognizing that Broadway is a special area, a cool area created with pedestrian environment. Everyone involved didn't want to assume it will stay this way forever, but there was a desire try to ensure it does. The Steering Committee has come up with a very good plan. One issue was zoning and to ensure the zoning treatment is satisfactory to all.

All properties in the Broadway Commercial Area currently zoned B5 (Inner-City Commercial Corridor) are proposed to be rezoned to the new B5B (Commercial Zoning District), a zoning district designed exclusively for Broadway.

The new B5B includes changes to the permitted uses and development standards. Also, all B5B-zoned properties would be subject to an Architectural Control District.

Saskatoon is growing strongly and we need to grow up as well as out, creating the urban living room.

- The first three storeys matter most.
- Density done properly has many benefits.
- The stepback enhances the pedestrian experience.

What factors will affect the overall height of a building?

- Gross Floor Area Ratio (7:1) which means that the maximum building volume can be 7 times the site area
- Site size (bigger, taller buildings can be built on larger sites)
- Height of base building (must be a minimum 7.5 metres to a maximum of 12 metres)
- Setback and stepbacks (must meet minimum requirements, setbacks affect the overall building form and height)
- Parking
  - Amount of required parking

- Location of parking (at grade at rear of site, below grade in parking structure, or above grade in parking structure)

Do not want building setback from the street. Requirement that at least 70% of building face the street, setback for courtyard. Removed service stations and commercial parking lot from permitted uses. Parking provision is currently 1.25 parking spaces for residents and 1 visitor for every 8 dwellings. This has been reduced to 1 parking space for residents while visitor parking has remained the same. Nothing set for commercial buildings and not proposing changes as it is usually self-regulating since developers understand that providing parking helps to attract potential tenants, so a minimum parking provision does not seem necessary.

The proposed B5B Zoning District would be subject to an Architectural Control District (ACD) intended to preserve the physical character of the area.

The ACD would contain a set of design guidelines, known as the Broadway Commercial Area Design Plan that all new development in the B5B District must conform to. Establishing an Architectural Control District (ACD) allows for enforceable design controls in the Broadway Commercial Area.

Currently, the only ACD in Saskatoon is River Landing, which for the most part, is new buildings and the land was mostly owned by City. This would be a first in Saskatoon with infill or a character area, maybe even the first in Saskatchewan. This has long been standard in other provinces and the U.S.

Councillor Clark stated this process involved a collection of groups that came together with different views and have created something not only for Saskatoon, but the whole province. Everyone walked down the street and pointed out their issues and these issues helped develop the guidelines through this consultative process.

The goal now is to find out what the public thinks of this proposal, before taking it to City Council.

The process for ACD is an application for development permit which is then reviewed by a committee of design professionals from landscape, architectural, community planning backgrounds. The committee has approximately 15 members, with 3 selected to review each application. The entire process usually takes about 60 days. It is on a professional level and not just someone's pet peeves or personal preferences.

## **There are 14 design guidelines in the Broadway Commercial Area Design Plan**

### **1. Building Expressions**

- Buildings should reinforce a *base*, *middle*, and *top* in their design.
- Within the first three storeys of a building, a clearly defined *base* contributes to the quality of the pedestrian environment.
- The *middle* or body of a building should contribute to the overall quality of the streetscape.

- The *top* or roof should be distinguished from the rest of the building.

## 2. Orientation & Placement

- All buildings should orient to the street with clearly defined entry points that directly access the sidewalk.
- A minimum of 70% of the front building line shall be located within 1.0 metre of the front property line.
- Buildings can enhance the pedestrian environment by creating a sense of enclosure.

## 3. Street Wall

- A street wall of a new building should align with those of neighbouring buildings or have the same setback as the predominant buildings on the block.
- The height of the street wall should be consistent with historic heights of no greater than 3 storeys and no less than 2 storeys.
- Levels above the street wall should be set back to reinforce a low-rise interface with the sidewalk.

## 4. Heritage Context

- New buildings on Broadway Avenue should complement, rather than detract from, the character of older buildings.
- General Guidelines – New buildings should avoid historical misrepresentation by not replicating past architectural styles, and should respect the scale, material and massing of adjacent heritage buildings.
- Facade Articulation – New buildings should ensure the horizontal and vertical architectural orders including windows and entries, are aligned with neighboring heritage buildings or the established pattern on the block.

## 5. Corner Sites

- Corner buildings have a greater visual prominence given that they front onto two streets and frame intersections.
  - Designs and massing of corner buildings should accentuate the visual prominence of the site.
  - Corner buildings should orient to both street frontages and, wherever possible, have entrances that address both frontages.

## 6. Storefronts

- To provide animation and visual interest, storefronts should have:
  - A frontage in the range of 7.5 metres.
  - A minimum of 75% glazing to maximize visual animation.
  - Entrances that are highly visible and located at or near grade.
  - Signage that adds diversity and interest to the street.
- Dark tinted, reflective, or opaque glazing should be discouraged for storefront glazing

## 7. Residential Street Access Units

- In buildings where residential uses are located at-grade:
  - The individual units should be accessed from the street

- Appropriate front yard privacy measures should be taken
- Access to the units should be consistent with the residential street character in Nutana

#### **8. Roof Treatment**

- Roof design should consider the following guidelines:
  - The use of stepbacks, changes in materials, cornice lines and overhangs
  - Screen mechanical penthouses from view
  - Green roofs are encouraged

#### **9. Above Grade Parking**

- Where parking is provided at grade, the following guidelines address the design quality of the facility:
  - Direct access from the street is discouraged.
  - Ground level retail should be incorporated, where the parking structure fronts a street.
  - Parking structures should be designed to reinforce the built character and blend into the streetscape.
- Broadway 360° recommends that, wherever possible, parking should be provided in the rear yard or below grade, and should be accessed from the lane.

#### **10. Material & Architectural Quality**

- New developments should contribute to the Prairie-style Main Street building style that exists
- High quality materials should be chosen that are both functional and aesthetically pleasing
- Materials chosen should not mimic other materials
- A key objective of Broadway 360 is to achieve a balance between consistencies in design quality & street interface, while enabling individual expression.

#### **11. Sidewalk Cafes**

- Should be encouraged along all sidewalks
- Should contribute and integrate into the streetscape
- Corners with “curb bump outs” could provided additional opportunities
- Rear yard and roof-top patios should not abut residential areas
- Sidewalk cafes should be encouraged, provided they don’t cause land use conflicts or encumber pedestrian movements.

#### **12. Building Lighting**

- Both landscape and architectural features can be highlighted
- Landmarks & distinctive features of buildings should be illuminated
- Subtle night lighting of retail displays should be encouraged

#### **13. Signage**

- Storefront signs should be no more than 25% of the business storefront
- Should aid pedestrians & drivers, especially at night

- Backlit rectangular sign boxes should be discouraged
- Signage should not obscure building features.
- Signage Group 5, in the City of Saskatoon Zoning Bylaw is applied to Broadway Avenue

#### 14. Sustainable Design

- Projects should strive for sustainable building practices
- When adaptive reuse projects are undertaken to rehabilitate historic buildings, the old and new should be compatible in terms of historic materials, features, size, scale, height, proportion and massing to protect the integrity of the property and its environment

#### Questions:

- **Could you briefly describe gross floor space exemptions for parking?**

Under the B5 Zoning District currently, certain things are exempt from, like indoor parking is not counted towards the total area of the building. E.g. you have a parking floor it does not count as gross floor space. Steering committee was concerned about very large and tall buildings also providing a significant amount of above grade commercial parking. Parking above grade will count towards the ration with the exception of the parking you need for the building itself. We will look at the parking area and the parking you have to provide for dwelling units and the parking for commercial area at rate of 24 square meters plus the drive lanes and the ramps. We will credit that back to you; it will not count toward parking structure.

- **Parking with mixed used development, how do you treat the artist who works and lives in place?**

The live/work unit would count as a residential dwelling, so one parking space would be required.

- **Are there still no parking requirements for commercial property?**

No, this remains the same.

#### Comments:

- It is a huge challenge task to bring this into place. Almost to mold into a campus area. Guidelines are good to control design, but if too wide open it's difficult to administer. However, you don't want it too stringent either. Couple concerns with the first two guidelines.
  - Base, middle and cap seems simple and perfect, but concern with wall aspect of a 2 or 3 storey building, not sure if good for street. Maybe need to have a restriction of number of stories as well. Need more to guide this.
  - 70% frontage and street wall... How will this affect the Extra Foods or Oskayak School if they change ownership?

- Signage and indirect architectural lighting sections are good.
- Storefront is critical for this process.

Tim noted many issues will be covered off by codes as they are more stringent than guidelines can be.

- Some concern with the height allowed for building, seems allowance is too high. Buds is 5 stories and that is fine, but going 12 stories seems high.

Tim noted the ratio of the historic building height will be followed.

- Good solid principle, it allows for a bit of variety, very sound, but is there any room allowed for odd variety that still maintains the character?
- It seems like the smaller lots may not be able to follow the 7:1 ratio.

Tim noted it is important to maintain the pedestrian feel.

- Last area is sustainable design, which is about providing general encouragement and not specific guidelines.

Tim stated the design and statement of intent. It is more becoming the norm, it is good business sense. Don't really need to regulate it as it is the way of good business, much the same as the parking provision.

- **Is there a goal regarding the ratio of residential to commercial property?**

There is no specific quota, but the goal is to have mixed uses. Fact of the matter is Broadway is a very healthy area of mixed use and hope it will continue.

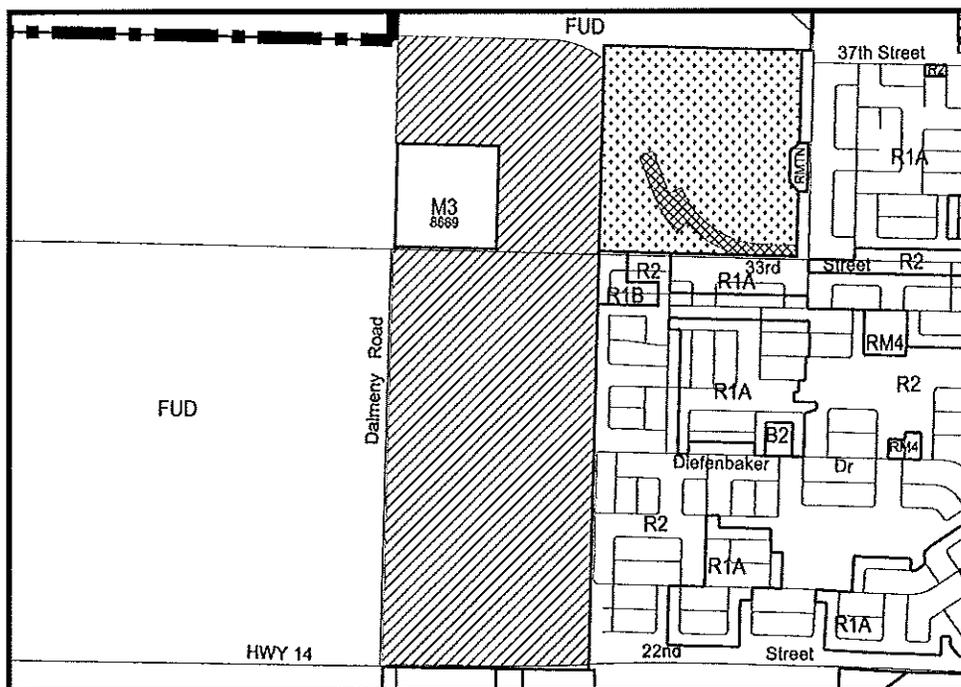
Tim thanked everyone for their time and reminded the group that the Steering Committee members will be around for anyone who wishes to have a more one to one conversation.

For more information please visit: [www.broadway360.ca](http://www.broadway360.ca)

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z15/12	<b>PROPOSAL</b> Proposed Rezoning from R1A(H) to R1A, FUD to R1A, and R1A(H) to R1B	<b>EXISTING ZONING</b> R1A (H) and FUD
<b>LEGAL DESCRIPTION</b> Various (see Attachment 1)	<b>CIVIC ADDRESS</b> N/A	
	<b>NEIGHBOURHOOD</b> Kensington	
<b>DATE</b> July 19, 2012	<b>APPLICANT</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7	<b>OWNER</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7

### LOCATION PLAN



**PROPOSED REZONING**

From FUD to R1A

From R1A(H) to R1B

From R1A(H) to R1A

File No. RZ15-2012


City of Saskatoon  
Planning & Development Branch

**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

That a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendments to Zoning Bylaw No. 8770 be approved:
  - a) that the properties identified in Attachment 2 be rezoned from R1A(H) - One-Unit Residential District with a Holding symbol, and FUD - Future Urban Development District to R1A - One-Unit Residential District; and
  - b) Phase I of the Kensington neighbourhood development, as shown on the Plan of Proposed Subdivision (see Attachment 3), be rezoned from:
    - i. R1A(H) - One-Unit Residential District with a Holding symbol, to R1B - Small Lot One-Unit Residential District (Lots 1 to 26, Block 100, Lots 1 to 29, Block 101, Lots 1 to 30, Block 103); and
    - ii. R1A(H) - One-Unit Residential District with a Holding symbol to R1A - One-Unit Dwelling Residential District (Lots 1 to 15, Block 102, and Lots 71 to 76, Block 103).

**B. PROPOSAL**

The Planning and Development Branch has received an application from the City of Saskatoon (City), Land Branch requesting that the identified properties within the Kensington Neighbourhood Concept Plan area be rezoned from R1A(H) - One-Unit Residential District with a Holding symbol, and FUD - Future Urban Development District to R1A - One-Unit Residential District; and to rezone Kensington Phase I from R1A(H) - One-Unit Residential District with a Holding symbol, to R1A - One-Unit Residential District and R1B - Small Lot One-Unit Residential District.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed rezoning of the identified properties would facilitate residential development that is consistent with the Kensington Neighbourhood Concept Plan.

**D. BACKGROUND INFORMATION**

During its April 16, 2012 meeting, City Council approved the Kensington Neighbourhood Concept Plan, which provides a wide range of housing options, as well as neighbourhood commercial services.

**E. JUSTIFICATION**

1. Community Services Department Comments

a) Planning and Development Branch

The proposed rezoning is consistent with the approved Concept Plan. Properties in the northwest part of the Kensington neighbourhood will be zoned R1B District and R1A District to accommodate development of Phase I of the neighbourhood. The remainder of the properties identified will be zoned as a R1A District. Any parcels intended for other forms of development will be subject to future rezoning when detailed survey plans are prepared.

2. Comments by Others

a) Infrastructure Services Department

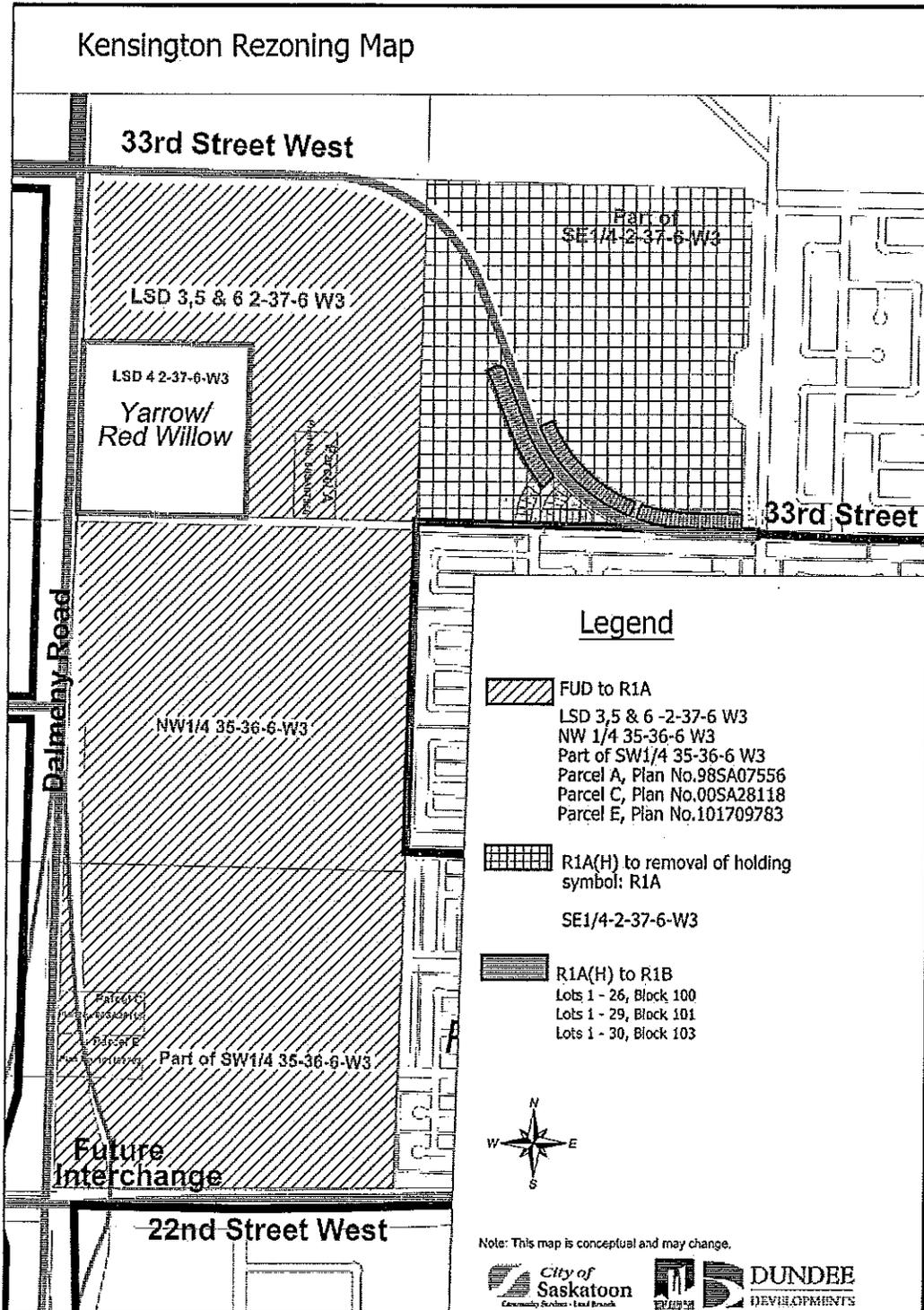
The proposed Zoning Bylaw No. 8770 amendment is acceptable to the Infrastructure Services Department.

b) Utility Services Department, Transit Services Branch

The Transit Services Branch has no easement requirements regarding the above referenced properties. At present, the Transit Services Branch has no service within 450 metres.

## FACT SUMMARY SHEET

FACT SUMMARY SHEET	
<b>A. Location Facts</b>	
1. Municipal Address	N/A
2. Legal Description	LSD 3, 5, and 6 2-37-6-W3; NW¼ 35-36-6-W3; Part of SW¼ 35-36-6-W3; Parcel A, Plan No. 98SA07556; Parcel C, Plan No. 00SA28118; Parcel E, Plan No. 101709783; and Lots 1 to 26, Block 100, Lots 1 to 29, Block 101, Lots 1 to 30, Block 103 as shown on Plan of Proposed Subdivision showing Part of SE¼ 2-37-6-W3, Plan No. 101836076
3. Neighbourhood	Kensington
4. Ward	3
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Residential with a Holding symbol - R1A(H) and Future Urban Development -FUD
2. Proposed Use of Property	Residential – R1A and R1B
3. Adjacent Land Uses and Zoning	Future Urban Development, Residential, Direct Control District, Institutional, and Commercial
North - Undeveloped	Future Urban Development – FUD
South – Blairmore Suburban Centre	Direct Control District – DCD6, Commercial – B3, and Institutional – M3
East – Confederation Park Neighbourhood Pacific Heights Neighborhood	Residential – R2, R1B, and RMTN
West - Undeveloped	Future Urban Development – FUD
4. Street Classification	22 <sup>nd</sup> Street West – Major Arterial Diefenbaker Drive – Major Arterial 33 <sup>rd</sup> Street West – Major Arterial
<b>C. Official Community Plan Policy</b>	
1. Existing Official Community Plan Designation	Residential
2. Proposed Official Community Plan Designation	Residential
3. Existing Zoning District	FUD and R1A(H)
4. Proposed Zoning District	R1A and R1B

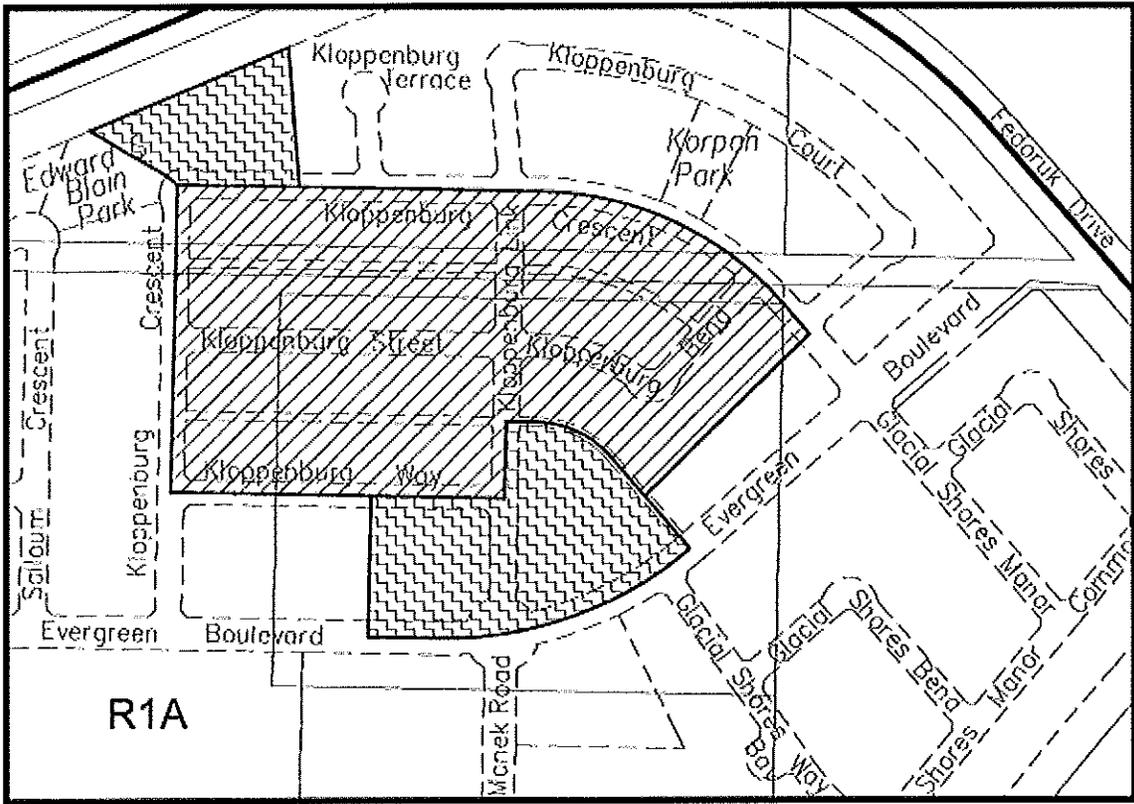




## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z17/12	<b>PROPOSAL</b> Proposed Rezoning from R1A to R1B and RMTN	<b>EXISTING ZONING</b> R1A
<b>LEGAL DESCRIPTION</b> Plan to be Approved		<b>CIVIC ADDRESS</b> Evergreen Boulevard, Kloppenburg Crescent, Bend, Link, Street, and Way
		<b>NEIGHBOURHOOD</b> Evergreen
<b>DATE</b> July 16, 2012	<b>APPLICANT</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7	<b>OWNER</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7

**LOCATION PLAN**



**PROPOSED REZONING**

From R1A to R1B ———

From R1A to RMTN ———

File No. RZ17-2012

**City of Saskatoon**  
Planning & Development Branch

**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

That a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone the properties identified in the attached Proposed Zoning Map from R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District, and RMTN – Townhouse Residential District, be approved.

**B. PROPOSAL**

The Planning and Development Branch has received an application from the City of Saskatoon, Land Branch, requesting that the properties identified in the attached Proposed Zoning Map (see Attachment 2) be rezoned as follows:

- 1) Blocks 1 to 7 be rezoned from R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District; and
- 2) Parcels EE, FF, and GG, be rezoned from R1A – One-Unit Residential District to RMTN – Townhouse Residential District.

The rezoning of these lands would accommodate small lot one-unit and townhouse residential development.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed rezoning will permit the development of the aforementioned lands in a manner which is consistent with the Evergreen Neighborhood Concept Plan (Concept Plan).

**D. BACKGROUND INFORMATION**

During its June 1, 2009 meeting, City Council approved the Concept Plan. The subject

sites are currently under the blanket zoning of a R1A District (One-Unit Residential District), which was applied to the area after the approval of the Concept Plan. The Concept Plan provides a wide range of housing options, as well as neighbourhood commercial services to serve the area. The proposed amendments will change the zoning designations for the specified areas of the neighbourhood in order to accommodate the variety of residential development that is desired within the Evergreen neighbourhood.

## **E. JUSTIFICATION**

### **1. Community Services Department Comments**

#### **a) Planning and Development Branch**

The proposed rezoning is in compliance with the approved Concept Plan and will accommodate a diversity of housing types in the Evergreen neighbourhood. Future development on this site will comply with the development standards identified in the R1B and RMTN Zoning Districts.

### **2. Comments by Others**

#### **a) Infrastructure Services Department**

The proposed amendment is acceptable to the Infrastructure Services Department.

#### **b) Utility Services Department, Transit Services Branch**

The Transit Branch has no easement requirements regarding the above referenced property. At present, the Transit Branch has no service within 450 metres, but has services within this development.

## **F. COMMUNICATION PLAN**

Once this application has been considered by the Municipal Planning Commission, it will be advertised in accordance with Public Notice Policy No. C01-021, and a date for a public hearing will be set. The property owners affected by this rezoning will be notified in writing. The Planning and Development Branch will also notify the Community Consultant and the Ward Councillor of the public hearing date by letter. A notice will be placed in The StarPhoenix two weeks prior to the public hearing. Notice boards will also be placed on the site.

**G. ENVIRONMENTAL IMPLICATIONS**

There are no environmental and/or greenhouse gas implications.

**H. ATTACHMENTS**

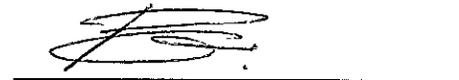
1. Fact Summary Sheet
2. Proposed Zoning Map

Written by: Daniel Gray, Planner 16

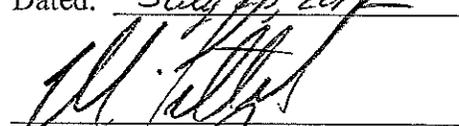
Reviewed by:

  
Alan Wallace, Manager  
Planning and Development Branch

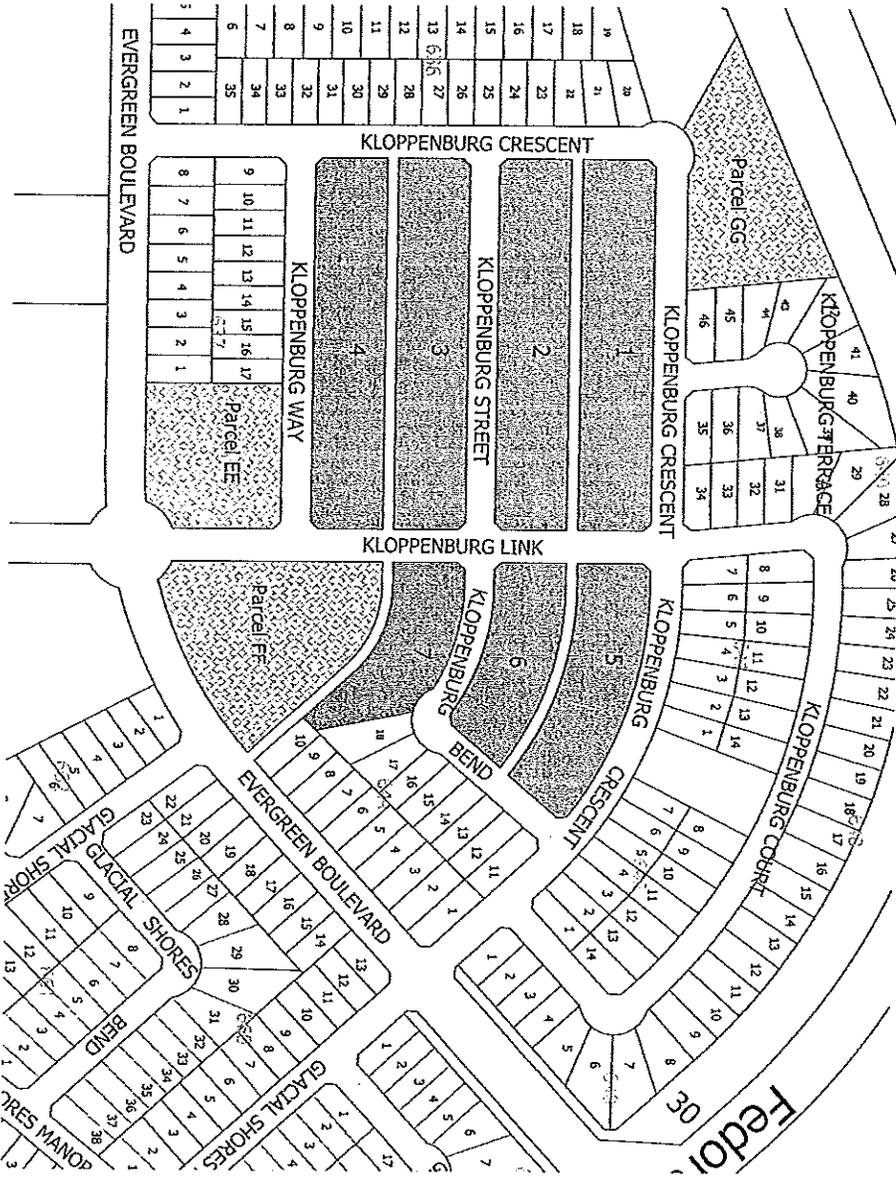
Approved by:

  
Randy Grauer, General Manager  
Community Services Department  
Dated: July 23, 2012

Approved by:

  
Murray Totland, City Manager  
Dated: July 26/12.

<b>FACT SUMMARY SHEET</b>	
<b>A. Location Facts</b>	
1. Municipal Address	Evergreen Blvd, Kloppenburg Crescent, Bend, Link, Street, and Way
2. Legal Description	Plan to be Approved
3. Neighbourhood	Evergreen
4. Ward	10
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Residential – R1A
2. Proposed Use of Property	Residential – R1B and RMTN
3. Adjacent Land Uses and Zoning	Residential
North - Undeveloped	Future Urban Development - FUD
South - Evergreen Neighbourhood	Residential – R1A
East - Evergreen Neighbourhood	Residential – R1A
West - Evergreen Neighbourhood	Residential – R1A
4. No. of Existing Off-Street Parking Spaces	N/A
5. No. of Off-Street Parking Spaces Required	N/A
6. No. of Off-Street Parking Spaces Provided	N/A
7. Site Frontage	N/A
8. Site Area	N/A
9. Street Classification	Evergreen Boulevard – Major Collector Kloppenburg Crescent, Bend, Link, Street, and Way – Local Street
<b>C. Official Community Plan Policy</b>	
1. Existing Official Community Plan Designation	Residential
2. Proposed Official Community Plan Designation	Residential
3. Existing Zoning District	R1A
4. Proposed Zoning District	R1B and RMTN



**EVERGREEN**

**Legend:**

-  R1B  
Parcel 1:  
Part of LSD 3-18-37-04-3 Ext 33 & Part of  
NW 1/4 37-4-W3M
-  R1B  
Parcels 2 - 4:  
Part of Parcel A Registered Plan  
#78534536 & Part of NW 1/4 37-4-W3M
-  R1B  
Parcel 5:  
Part of LSD 3-18-37-04-3 Ext 33 & Part of  
Registered Plan #78534536
-  R1B  
Parcel 6:  
Part of Parcel A Registered Plan  
#78534536
-  R1B  
Parcel 7:  
Part of Parcel A Registered Plan  
#78534536 & Part of NE 1/4 37-4-W3M
-  RMTN  
Parcels EE & FF:  
Part of Parcel A Registered Plan #78534536
-  R1B  
Parcel GG:  
Part of LSD 4-18-37-04-3 Ext 32 & Part of  
LSD 3-18-37-04-3 Ext 33

Current Zoning:	Part of NW ¼ 7-37-4-W3 and Part of SW ¼ 7-37-4-W3
Proposed Zoning:	R1A
Neighbourhood:	RMTN and RM3
Date Received:	Evergreen
	July 23, 2012

#### Subdivision

- Application No. 58/12: Rosewood Boulevard West  
Applicant: Webster Surveys Ltd. for Boychuk Investments Ltd.  
Legal Description: Parcel BB, Plan No. 101875394  
Current Zoning: R1A  
Neighbourhood: Rosewood  
Date Received: July 17, 2012
- Application No. 59/12: Willis Crescent  
Applicant: Webb Surveys for Northridge Developments  
Legal Description: Removal of Tie-Code for Surface Parcel No. 166089280  
Current Zoning: M2  
Neighbourhood: Stonebridge  
Date Received: July 17, 2012
- Application No. 60/12: 627 Brand Court  
Applicant: Webb Surveys for D.E. Enterprises Ltd.  
Legal Description: Part of Lot 7, Block 428, Plan No. 00SA12065  
Current Zoning: AM  
Neighbourhood: C.N. Industrial  
Date Received: July 17, 2012
- Application No. 61/12 : 1408 Main Street  
Applicant: Webb Surveys for Sheila and Phillip Reiber  
Legal Description: Lot 5, Block 190, Plan No. G779  
Current Zoning: R2  
Neighbourhood: Varsity View  
Date Received: July 24, 2012

Subdivision

- Application No. 62/12: 203 Cruise Street  
Applicant: Webb Surveys for Allan and Cecile Sawatsky  
Legal Description: Lot 13, Block 12, Plan No. G171  
Current Zoning: R2  
Neighbourhood: Forest Grove  
Date Received: July 30, 2012
  
- Application No. 63/12: 158 and 160 Carleton Drive  
Applicant: Digital Planimetrics Inc. for Peggy Susan Finn  
Legal Description: Lot 33, Block 605, Plan No. 67S10958  
Current Zoning: R2  
Neighbourhood: College Park  
Date Received: July 31, 2012

**ENVIRONMENTAL IMPLICATIONS**

There are no environmental and/or greenhouse gas implications.

**PUBLIC NOTICE**

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

**ATTACHMENTS**

1. Plan of Proposed Discretionary Use No. D3/12
2. Plan of Proposed Rezoning No. Z20/12
3. Plan of Proposed Subdivision No. 58/12
4. Plan of Proposed Subdivision No. 59/12
5. Plan of Proposed Subdivision No. 60/12
6. Plan of Proposed Subdivision No. 61/12
7. Plan of Proposed Subdivision No. 62/12
8. Plan of Proposed Subdivision No. 63/12

## **Section B – CORPORATE SERVICES**

### **B1) 2011 Annual Report and Financial Reports (Files CK. 1895-3, CK. 430-72, CS. 369-1 and CS. 1895-3)**

- RECOMMENDATION:**
- 1) that the 2011 Annual Report and Financial Reports be received as information;
  - 2) that the 2011 fuel deficit be funded from the year-end surplus rather than the Fuel Stabilization Reserve; and
  - 3) that the 2011 Snow and Ice Management Program be funded from the year-end surplus rather than the Snow and Ice Management Reserve.

### **REPORT**

Attached for City Council's information is the City of Saskatoon 2011 Annual Report which includes a summary of the progress achieved on the 2011 Business Plan, as well as the 2011 Audited Consolidated Financial Statements and Trust Fund Financial Statements (Attachment 1).

#### **2011 Business Plan**

In 2011, Saskatoon Speaks, a Community Visioning initiative was completed, bringing to a close the eight month long process where between 7,000 and 10,000 citizens were involved to talk about their values, what matters, and what they aspire to achieve as Saskatoon grows over the next 50 – 70 years. The Community Vision provided input into the City's Ten Year Strategic Plan and other City plans such as the Culture Plan and the City Centre Plan.

In the midst of rapid growth, the City of Saskatoon has taken a proactive and innovative approach to managing the City. We are investing in a number of large capital projects to maintain our competitiveness and attractiveness, and continue to have a high quality of life in Saskatoon.

A number of these projects began or continued in 2011, and are on target, including the design and construction of the new headquarters for the Saskatoon Police Service. The construction of the new South Bridge and roadway projects will move people, services, and goods more efficiently. The project is scheduled to open this fall on budget.

Additional funding was allocated for road and bridge repair. We continued to support our existing neighbourhoods through the completion of the Westmount and Varsity View Local Area Plan, while completing new Sector Plans to accommodate balanced growth on the east and west sides of our city.

In terms of community support, the City of Saskatoon supported the recruitment of volunteers from the Aboriginal community and new Canadians for our community associations; the final design of

the Remai Art Gallery of Saskatchewan was started; and work began on the implementation of the Municipal Culture Plan.

This is a very brief summary of the types of initiatives that took place in 2011, and the Annual Report provides a summary status report on all of the initiatives that were in the 2011 Business Plan.

### Financial Reports

The audited 2011 City of Saskatoon Financial Report has been prepared in accordance with the financial reporting recommendations of the Public Sector Accounting Board (PSAB) of the Institute of Chartered Accountants. The Audit Committee approved the audited financial statements at its meeting on June 1, 2012, at which time the Auditors indicated that the City's significant accounting policies are appropriate and in accordance with PSAB.

The City of Saskatoon 2011 year-end results were finalized with a surplus of \$3.073 million as previously reported to City Council.

### Stabilization Reserves

It is recommended that the 2011 year-end surplus be transferred to the City's Fiscal Stabilization Reserve. This reserve was redefined from the previous Revenue Stabilization Reserve and approved by City Council on November 7, 2011. The reserve has a targeted balance equal to 5% of the current year's tax-supported expenditures, or \$6.8 million which is to be obtained over the next five years. The reserve's current balance of \$2.7 million plus the \$3.073 million will bring the reserve to \$5.77 million.

There are two program stabilization reserves that require City Council direction regarding the funding of the related program deficits.

In 2011, a fuel stabilization reserve was created to deal with fluctuations in fuel pricing and reduce the impact to the year-end financial results from large variances. \$1 million was transferred from the Fiscal Stabilization Reserve to the Fuel Stabilization Reserve as approved by City Council when the reserve was established. A negative variance of about \$800,000 was recorded in 2011; however, in light of the total operational surplus realized of \$3.073 million, it would not be beneficial to fund the fuel variance which would deplete most of the fuel reserve. Your Administration recommends that the fuel variance not be stabilized, which would leave the surplus at the current level and keep the Fuel Stabilization Reserve balance at \$1 million for future needs.

Similarly, the Snow and Ice Program recorded a deficit of \$636,000 which included a transfer of \$548,600 to the Snow and Ice Management Reserve. Rather than reducing the program deficit by

the \$548,600, your Administration is proposing to leave the program deficit as is and keep the transferred amount in the reserve for future stabilization requirements. The impact of this would be that the operational surplus would remain at the current level, and the Snow and Ice Management Reserve balance would remain at \$548,600.

### Other Financial Reports

In addition to the 2011 Annual Report, copies of the following reports are also attached:

- 2011 City of Saskatoon Public Accounts (Attachment 2)
- 2011 Capital Status Report (Attachment 3)
- 2011 Financial Reports – Superannuation Plans (Attachment 4)
- 2011 Audited Financial Statements for the Saskatoon Public Library (Attachment 5)

The Public Accounts document is legislated by *The Cities Act* to be generated each year by municipalities and lists, among other things, remuneration over \$50,000 to civic employees including employees of its boards and commissions, as well as remuneration for all elected officials. Included in the salaries figure are all amounts paid related to employment including severances, overtime, and any adjustments.

### PUBLIC NOTICE

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

### ENVIRONMENTAL IMPLICATIONS

There are no environmental implications.

### ATTACHMENTS

1. City of Saskatoon 2011 Annual Report.
2. 2011 City of Saskatoon Public Accounts.
3. 2011 Capital Status Report.
4. 2011 Financial Reports – Superannuation Plans.
5. 2011 Audited Financial Statements Saskatoon Public Library.

Copies of the following attachments can be found on the City's website at [www.saskatoon.ca](http://www.saskatoon.ca), click on "C" for City Clerk's Office and go to Reports and Publications.

**B2) Open Data Catalogue**  
**(Files CK. 261-1 and CS. 260-1)**

**RECOMMENDATION:** that City Council approve a post-budget request to fund a project to implement an open data catalogue, at a cost of \$40,000, funded from the CIS Development Reserve.

**BACKGROUND**

Many Canadian municipalities have joined the open data movement and are actively engaged with interested parties in providing information and data files using a web-based open data catalogue. Interested parties can browse the catalogue and download information and data files in a way that allows them to easily view the information or develop a mobile application using the information and data files provided. They can then use the information and data files as they see fit. Many of the mobile application developers attribute the information and data files to the government that provided it. Your Administration has recognized the need to develop an open data catalogue and would like to outsource the development of the delivery framework and use internal resources to develop information and data files for the open data catalogue.

**What is Open Data?**

- Open data is about offering government data in a more useful and useable format to enable citizens, the private sector, and non-government organizations to leverage it in innovative and value-added ways.
- Open data is the idea that certain data should be freely available to everyone to use and republish as they wish, without restrictions from copyright, patents, or other mechanisms of control.
- Open government data (referred to as open data in the balance of the report) is any information and data produced or commissioned by the City of Saskatoon, including Boards and Commissions, that does not infringe on an individuals, groups, or an organizations privacy and confidentiality rights.

**Why is it Important?**

In the City's Strategic Plan one of the leadership commitments is "Effective Communication, Openness, and Accountability". Open data can help the City meet this commitment and also help achieve a number of the success factors identified in the Strategic Plan by providing information and data files to software developers. The software developers can create applications that may be used by our residents to view emergency response times, crime statistics, transit information, financial information, and environmental information. Basically, open data is a tremendous resource that is as of yet, largely untapped. The City collects a vast

quantity of high-quality information and data as part of its ordinary working activities. If this information and data is made open, it can have huge potential benefits.

Part of the beauty of open data is that it is impossible to predict precisely how it will be used to create value. The nature of innovation is that developments often appear in unlikely places.

Some areas, in other jurisdictions, in which open data is already having a positive impact include:

- Transparency and democratic control
- Civil participation
- Improved and new private products and services
- Innovation
- Governmental efficiency

### **How does Open Data Work?**

Civic departments, with input from Saskatoon's mobile application development community and other interested groups or individuals, create various approved information and data files and place them in the open data catalogue and make them available for download to interested parties. Some points around the uses of open data:

- Software developers create a mobile application, attribute the data source to the City, and provide the mobile application free of charge or sell for a small price – generates economic activity.
- Win-win for our residents, and tourists, as they can have access to City information on their Smartphone's and other mobile devices. Civic employees can make use of externally and internally developed mobile applications which can help develop a cultural movement to being more open within the organization.
- The City does not pay to have these applications developed, beyond the initial cost to make the data available.
- The development of a mobile application is based on supply and demand. The supply of information and data files from the City and the demand for mobile access to data from the community. Software developers will tend to develop applications that support the resident's needs and the City can help identify opportunities to provide information and data files of interest to the software development community.
- Researchers and university students can use the information and data files to help them support their findings or studies.
- Consideration of data currency and accuracy is important as people will rely on the information provided to make decisions.

The open data movement is well established with many levels of government worldwide who publish information and data files for people to consume and use as they wish. One of the main uses is in the development of mobile applications.

## **REPORT**

In late May, staff from Transit, Planning and Development, Finance, and CIS met with members of Saskatoon's software development community. The Saskatoon software development community expressed a strong interest in municipal data and were very excited about the prospect of gaining access to information and data files that they could use for their mobile applications. Interest was expressed in transit data, mapping data, garbage collection area data, assessment data, demographic, and financial data. They were particularly excited about gaining access to real-time GPS information from buses once it becomes available in late 2013 or early 2014. The local press is also interested in when the City is going to provide an open data catalogue and have written a number of articles on the subject.

While the City does provide free data to interested parties, it has been done on a limited ad-hoc basis. For example, GIS data on the City's infrastructure has been provided to a number of construction companies, mapping data has been provided to ESRI for their Open Communities Initiative, transit route data has been provided for Google Transit, and most recently ward boundary data has been provided to a developer. The developer has created a mobile application that can be used by voters in the upcoming civic election to determine which ward they reside in.

This \$40,000 budget request represents a reasonable starting point to develop an open data catalogue. However, the future budget requirements may need to be revised based on the demand for information and data files and the effort required to support more complex information and data requirements.

## **OPTIONS**

There are no options.

## **POLICY IMPLICATIONS**

There are no policy implications.

**FINANCIAL IMPLICATIONS**

As noted above, the project cost is estimated at \$40,000 and can be funded from the CIS Development Reserve. The ongoing operating costs to store the information and data files will be budgeted for in 2014 and will be based on the 2013 experience.

**ENVIRONMENTAL IMPLICATIONS**

There are no environmental implications.

**PUBLIC NOTICE**

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

## **Section E – INFRASTRUCTURE SERVICES**

### **E1) Proposed Limited Residential Parking Program J.J. Thiessen Crescent, Terrace and Way and O'Brien Crescent, Court and Terrace (Files CK. 6120-4-2 and IS. 6120-6)**

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- RECOMMENDATION:**
- 1) that the 200 Block of O'Brien Court and the 300 Block of O'Brien Terrace be removed from the Limited Residential Parking Permit Program; and
  - 2) that the City Solicitor be requested to prepare the necessary amendments to Bylaw 7862, The Residential Parking Program Bylaw, 1999, for approval by City Council.

### **BACKGROUND**

City Council, at its meeting held on November 8, 2010, considered a report of the General Manager, Infrastructure Services, dated October 18, 2010, regarding the establishment of a Limited Residential Parking Program on J.J. Thiessen Crescent, Terrace and Way and O'Brien Crescent, Court and Terrace. Council resolved, in part:

“that the Administration report back to the Planning and Operations Committee in six months time with respect to implementation of the Limited Residential Parking Permit Program under the Residential Parking Permit Policy.”

### **REPORT**

The Limited Residential Parking Permit (LRPP) Program was implemented on September 1, 2011, and has been in effect for almost one year. Forty-three residential permits have been sold to date, generating \$580 in revenue. A supply of permits for the upcoming year is available for distribution commencing August 1, 2012.

A recent visit to the area showed that 32 vehicles were parked on the street, of which 26 were displaying a permit.

As outlined in Policy A07-014 - Administration of Residential Parking Permits, enforcement in a LRPP occurs in response to complaints. During the course of the year, Parking Enforcement responded to 12 complaints of vehicles being parked in violation, with 14 citations being issued. Since its implementation, petitions have been received from the residents on the 200 Block of O'Brien Court and the 300 Block of O'Brien Crescent, requesting to be removed from the program. Both blocks have residential frontage. In order to determine the impact of removal of these blocks, the signs were covered in November 2011. To date, no complaints have been received regarding transient parking on these two blocks. The Administration is, therefore,

recommending the formal removal of the 200 Block of O'Brien Court and the 300 Block of O'Brien Crescent from the LRPP in this area, as outlined in the attached plan.

### **ENVIRONMENTAL IMPLICATIONS**

There are no environmental implications.

### **COMMUNICATIONS PLAN**

The residents included within the boundaries of the program will be advised of the changes to the parking restrictions.

### **POLICY IMPLICATIONS**

Removal of the blocks from the LRPP in this area, as outlined in this report, are consistent with Policy A07-014 - Administration of Residential Parking Permits and Policy C07-014 - Residential Parking Permit Program.

### **PUBLIC NOTICE**

Public Notice pursuant to Section 3 of Policy C01-021, Public Notice Policy, is not required.

### **ATTACHMENT**

1. Plan 260-0001-001r002 – J.J. Thiessen and O'Brien Limited Residential Parking Permit Program.

**E2) Enquiry – Councillor M. Loewen (February 27, 2012)**  
**Traffic Calming – Galloway Road**  
**(File No. CK. 6320-1)**

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**RECOMMENDATION:** that the information be received.

### **BACKGROUND**

The following enquiry was made by Councillor M. Loewen at the meeting of City Council held on February 27, 2012:

“Will the Administration please report back with recommendations for traffic calming measures along Galloway Road, in particular those areas close to Robert H. Freeland Park and Alexander MacGillivray Young Park.”

## **REPORT**

Traffic calming devices are often used to reduce vehicular speeding; to enhance the safety and the perception of safety for pedestrians; and to reduce short cutting through residential neighbourhoods.

Galloway Road is classified as a major collector roadway, with a posted speed limit of 50 kph, and is designed to carry up to 5,000 vehicles per day (vpd). Attachment 1 includes a roadmap of Galloway Road, which shows that it runs from the roundabout at Gordon Road to Stonebridge Common, where it has right-of-way over all adjacent streets, including a stop control at Stonebridge Common. Galloway Road curves in both the westbound and eastbound direction, creating sightline difficulties for pedestrians at the crosswalk at Robert H. Freeland Park, which is located on the south side of Galloway Road, between Borlase Cove/Bolton Crescent and Hettle Cove to McIntosh Street. Galloway Road comes to a T-intersection at Stonebridge Common, by Alexander MacGillivray Young Park. This results in a 90-degree turn, which creates natural traffic calming in the eastbound and westbound directions.

Due to its recent construction, collision history along Galloway Road is not yet available from Saskatchewan Government Insurance.

A speed study and traffic volume count was conducted along Galloway Road at two different locations, from June 23 to June 30, 2012. Location 1 was between LeValley Cove and Langlois Way; and Location 2 was between Borlas Cove and Hettle Cove. Speed studies measure the 85<sup>th</sup> percentile speed (the speed at which 85 percent of traffic is travelling at or below) and the traffic volume measures the average daily traffic (ADT). As outlined in the City’s guidelines for neighbourhood traffic calming, it is generally acceptable for traffic to be travelling within five kilometres of the posted speed limit.

The 85<sup>th</sup> percentile speed at Location 1, which is close to Alexander MacGillivray Young Park, was 57 kph, and the traffic count showed that the average daily traffic was 2,393 vpd.

The 85<sup>th</sup> percentile speed at Location 2, close to Robert H. Freeland Park, was 64 kph, and the traffic count showed that the average daily traffic was 2,423 vpd.

The study indicates that the traffic volume is acceptable for this roadway in both locations, and that the traffic speed for Location 1 is within acceptable levels. Therefore, traffic calming will

not be installed at this location. The results of the speed study will be forwarded to the Saskatoon Police Services for enforcement.

With regards to the second location, near Robert H. Freeland Park, the 85<sup>th</sup> percentile speed indicates that vehicles are travelling in excess of five kph over the posted speed limit. Therefore, temporary curb extensions will be installed at the intersection of Galloway Road and McIntosh Street (directly adjacent to Robert H. Freeland Park), as shown on Attachment 2. A curb extension is a horizontal intrusion of the curb into the roadway, resulting in a narrower section of roadway. Curb extensions improve visibility for both motorists and pedestrians through increased signage, and allow pedestrians to approach the roadway to see oncoming vehicles without having to actually step into the vehicle travel lane. In addition, they assist in reducing vehicle speeds.

A future study will be completed in 2013 to determine the impact of the temporary curb extensions. If proven effective, they will be installed permanently.

#### **POLICY IMPLICATIONS**

There are no policy implications.

#### **COMMUNICATIONS PLAN**

A communications plan is not required.

#### **ENVIRONMENTAL IMPLICATIONS**

There are no environmental implications.

#### **FINANCIAL IMPACT**

The estimated cost to place the temporary curb extensions is \$1,500. There is sufficient funding within Capital Project 1512 – Neighbourhood Traffic Management.

#### **PUBLIC NOTICE**

Public Notice pursuant to Section 3 of Policy C01-021, Public Notice Policy, is not required.

## **ATTACHMENTS**

1. Galloway Road Map.
2. Temporary Traffic Calming.

**E3) Request for Sole Source  
Capital Project 1493 – East Riverbank Slope Stabilization  
Award of Engineering Services  
200 Block of 11<sup>th</sup> Street East  
(Files CK. 4205-5 and IS. 7821-4)**

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- RECOMMENDATION:**
- 1) that the sole source proposal submitted by Golder Associates Ltd., for engineering services for the geotechnical work in response to the slope movement at the 200 Block of 11<sup>th</sup> Street East, on a time and expense basis, at an estimated total cost of \$138,700 (plus G.S.T.) be accepted; and
  - 2) that the City Solicitor be instructed to prepare the necessary Engineering Services Agreement for execution by His Worship the Mayor and the City Clerk, under the corporate seal.

## **REPORT**

Capital Project 1493 – East Riverbank Stabilization has approved funding which is designated for the rehabilitation of the east riverbank slope failures and slope stability monitoring instrumentation.

On June 20, 2012, there was a slope movement on the 200 Block of 11<sup>th</sup> Street East, which required immediate observation and installation of slope monitoring instrumentation. Due to the emergency nature of the work, Golder Associates Ltd. submitted a work plan for geotechnical engineering services and commenced work. A sole source proposal was also received, which was priced on a time and expense basis, at an estimated cost of \$138,700 (plus G.S.T.).

The proposal covers the frequent monitoring of the movement for a two-month period and the completion of an engineering report.

**FINANCIAL IMPACT**

The estimated net cost to the City for the proposal from Golder Associates Ltd. is as follows:

Estimated Cost	\$138,700
G.S.T.	<u>\$ 6,935</u>
Total Fees	\$145,635
Less G.S.T. Rebate	<u>(\$6,935)</u>
<b>Total</b>	<b><u>\$138,700</u></b>

There is sufficient funding available within Capital Project 1493 – East Riverbank Stabilization.

**ENVIRONMENTAL IMPLICATIONS**

There are no environmental implications.

**COMMUNICATIONS PLAN**

A communications plan is not required.

**PUBLIC NOTICE**

Public Notice pursuant to Section 3 of Policy C01-021, Public Notice Policy, is not required.

**E4) Request for Change Order**  
**Contract 12-0024 - 2012 Rosewood B2 Roadways**  
**(Files CK. 292-012-15 and IS. 4111-48-5)**

**RECOMMENDATION:** that a Change Order, in the amount of \$872,715.24, for Contract 12-0024 - 2012 Rosewood B2 Roadways, be approved.

**REPORT**

Contract 12-0024 - Rosewood B2 Roadways was awarded to Central Paving and Asphalt in the amount of \$3,149,861.85. This contract covers the construction of 1,400 metres of local roads within the Rosewood subdivision, and 550 metres of collector roadways along Rosewood Gate North and Rosewood Boulevard West.

The Land Branch has requested that an additional 300 metres of collector road on Rosewood Boulevard West, and 100 metres of local road on Hastings Crescent be extended and constructed to accommodate their development plans for the Rosewood neighbourhood.

The primary benefit of adding this work is that it will provide a completed collector road (Rosewood Boulevard West) which will route throughout the Rosewood Subdivision from Boychuk Drive to Taylor Street. A portion of the collector road cost will be shared with private development. The cost to the developer is estimated to be \$252,500 of the total additional cost to Contract 12-0024 of \$872,715.24.

The increase of \$872,715.24 brings the total of Contract 12-0024 to \$4,022,577.09. Since this increase is greater than 25% of the original contract amount, Council approval is required for the Change Order.

#### **FINANCIAL IMPACT**

There is sufficient funding within Capital Project 1403-06 – Infrastructure Services, Land Development – Rosewood.

#### **POLICY IMPLICATIONS**

The requested approval of the Change Order is in accordance with Policy A02-027 – Corporate Purchasing Policy.

#### **ENVIRONMENTAL IMPLICATIONS**

There are no environmental implications.

#### **COMMUNICATIONS PLAN**

A communications plan is not required.

#### **PUBLIC NOTICE**

Public Notice pursuant to Section 3 of Policy C01-021, Public Notice Policy, is not required.

## **Section G – CITY MANAGER**

### **G1) Second Quarter Results – City of Saskatoon’s 2012 Budget and Business Plan (Files CK. 1700-1, x 430-72 and CC. 100-14)**

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**RECOMMENDATION:** that the information be received.

#### **BACKGROUND**

In 2011, the City of Saskatoon implemented major changes to its planning and budgeting process, to create a framework which would allow the City to be more adaptive and responsive to the changing dynamics in our city, our province, and our country. Approved in December 2011, The City of Saskatoon’s 2012 Corporate Business Plan and Budget – *Investing in What Matters* builds on that new foundation.

The new approach to business planning is intended to:

- improve transparency and decision-making by providing City Council and citizens more information about where City funds are used, linking service costs to service levels and outcomes, and better connecting long-term goals to short-term spending decisions;
- increase the City’s accountability in delivering services to citizens effectively and efficiently, while maintaining the focus on a sustainable future; and
- help the City transform its organization by providing for more regular, ongoing, and thorough examination of City services to ensure that services are relevant to citizens’ needs and priorities.

#### **REPORT**

In an effort to be more accountable, transparent, and efficient in the management of the City’s key priorities and financial resources, this report presents the second quarter results for the City of Saskatoon’s 2012 Business Plan and Budget – *Investing in What Matters*, as at June 30, 2012.

Quarterly status updates on the City’s Budget and Business Plan will help to ensure that City Council and the Administration are aware of emerging budget and business plan issues and challenges, so they can react accordingly. By providing these quarterly reports, this will help the City to mitigate any financial or corporate risks that may result from unplanned events, or new challenges that may emerge throughout 2012.

The initiatives that are approved in the Business Plan have been sorted according to the Strategic Goals that were approved as part of the City of Saskatoon Strategic Plan on February 6, 2012. The budget allocations are reported according to Business Lines.

#### **Business Plan Highlights**

Attachment 1 provides a summary of the status, as of June 30, 2012, of the key projects outlined in the 2012 Business Plan. Below are highlights of these projects:

### Continuous Improvement

- A comprehensive review of the Disability Assistance Program is underway.
- The workforce strategy has been completed for the City of Saskatoon, and is the blueprint for achieving our strategic priority of creating a culture of continuous improvement.

### Asset and Financial Responsibility

#### Streets and Sidewalks

- The state of the infrastructure continues to be a priority, as roads and sidewalks emerge as a significant issue in the annual Civic Services Survey. Reports are being prepared for City Council to consider on the state of the infrastructure, including sidewalks, roads, bridges, lanes, and boundary roads, which will include recommended service and funding levels for each. These reports will be submitted to City Council in late 2012.

#### Expansion of Water Reservoirs

- The design is underway for the expansion at the 42<sup>nd</sup> Street West reservoir, and the project will be tendered for construction in October. The Avenue H Reservoir expansion is well underway with the reservoir and pump house under construction. Both projects are on target.

#### Reassessment

- The 2013 property revaluation is on target. All preliminary analysis of properties is complete, and updated values have been determined for the majority of properties.

### Quality of Life

- In order to increase positive race relations among our young adult community, the Youth Action Network, under the leadership of Youth Launch, helped facilitate the Mayor's Youth Forum held in May 2012.
- The new headquarters for the Saskatoon Police Service is 33% complete, on time and on budget, with substantial completion expected on October 31, 2013. A site webcam has been installed, and can be viewed on the Saskatoon Police Service website.
- The Master Plan is complete for the Kinsmen Park and Area redevelopment.

### Environmental Leadership

- City Council, agreed in principle, with the Single-Family Residential Curbside Recycling proposal submitted by Loraas Recycle. This program will be implemented in early 2013.
- In addition, the City is partnering with Cosmopolitan Industries to be the service provider for recyclables generated from multi-family units.

### Sustainable Growth

- The Kensington Neighborhood Concept Plan was approved.
- Lots continue to be serviced in Evergreen, Rosewood, Hampton Village, and the Marquis Industrial Area.

### Moving Around

- To improve Transit service between downtown and the University of Saskatchewan, traffic signal priority hardware was installed on Transit DART buses, and College Drive intersections.
- The five-year capital plan for the expansion of cycling pathways, bike lanes and sharrows is complete. The Blairmore Bikeway, Broadway Bridge Access, and Spadina Cycling projects are underway. The “Learn to Ride Safe” school-based program has been developed.

### Economic Diversity and Prosperity

Of the \$603M of permitted value of construction in the first half of 2012, only four projects exceeded \$10M, and 83 projects had a value between \$1M and \$10M. Many of the permits issued in the first half were for new one-and-two unit dwellings and alterations to these buildings, and detached residential garages. In the first half of 2012, 1,659 residential units were created compared to 1,398 residential units in the same time period of 2011. Of the 361 non-residential permits issued in the first half of 2012, 283 or 78% were for improvements to existing buildings (see Attachment 2 for Building Permit Statistics for YTD June 30, 2012 vs. YTD June 30, 2011).

### Year End Budget Projections

While variances within specific business lines have been identified, overall a small surplus is currently being projected. Attachment 3 is the 2012 Year End Projection for period ending June 30, 2012.

**OPTIONS**

There are no options.

**POLICY IMPLICATIONS**

There are no policy implications.

**FINANCIAL IMPLICATIONS**

There are no financial implications.

**ENVIRONMENTAL IMPLICATIONS**

There are no environmental implications.

**PUBLIC NOTICE**

Public Notice pursuant to Section 3 of Policy No. C01-021, Public Notice Policy, is not required.

**ATTACHMENTS**

1. 2012 Corporate Business Plan 2<sup>nd</sup> Quarter Report (April 1, 2012 to June 30, 2012).
2. Building Permit Statistics for YTD June 30, 2012 vs. YTD June 30, 2011.
3. City of Saskatoon 2012 Year End Projection for Period Ending June 30, 2012.

Respectfully submitted,

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Randy Grauer, General Manager  
Community Services Department

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Marlys Bilanski, General Manager  
Corporate Services Department

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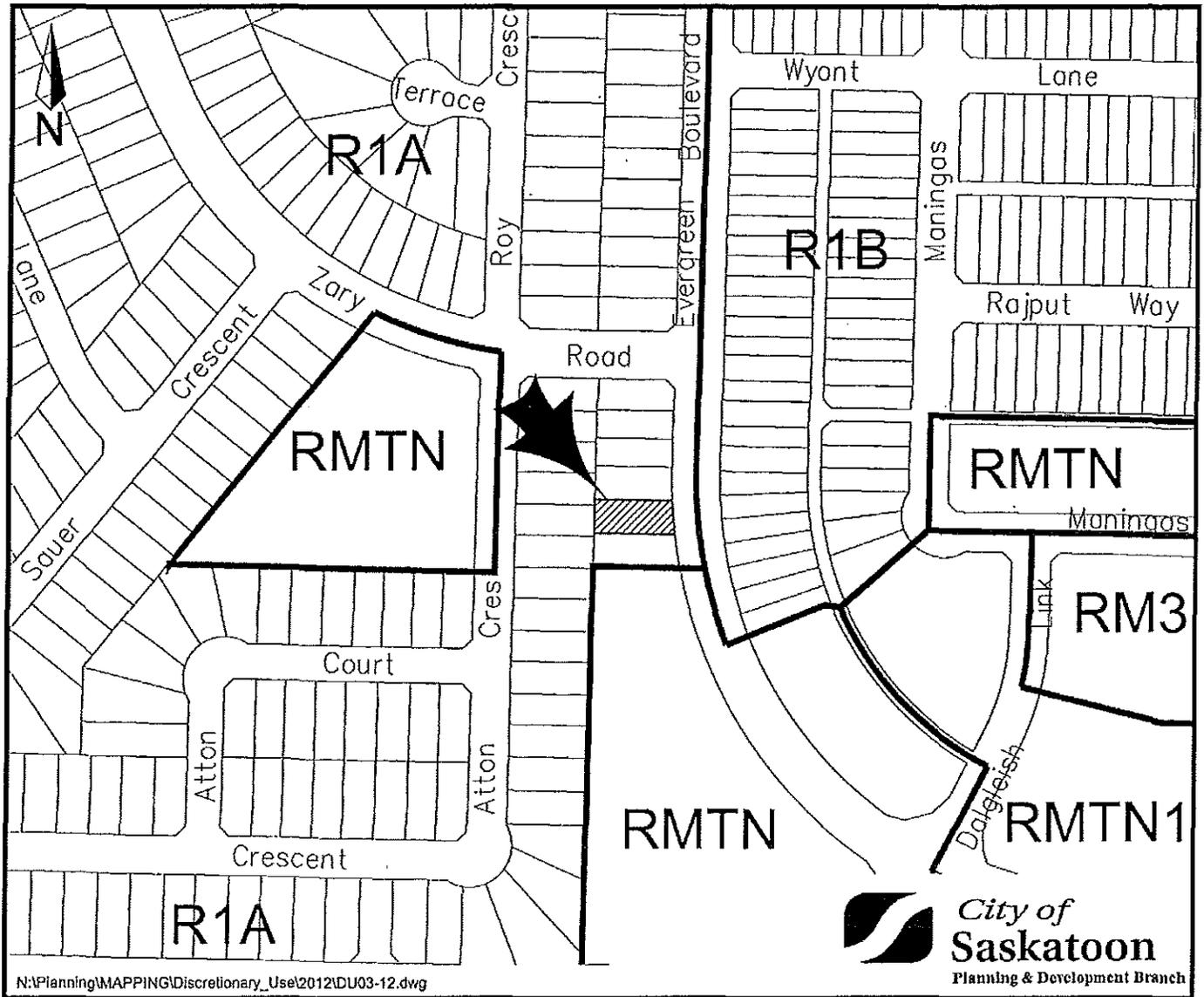
Mike Gutek, General Manager  
Infrastructure Services Department

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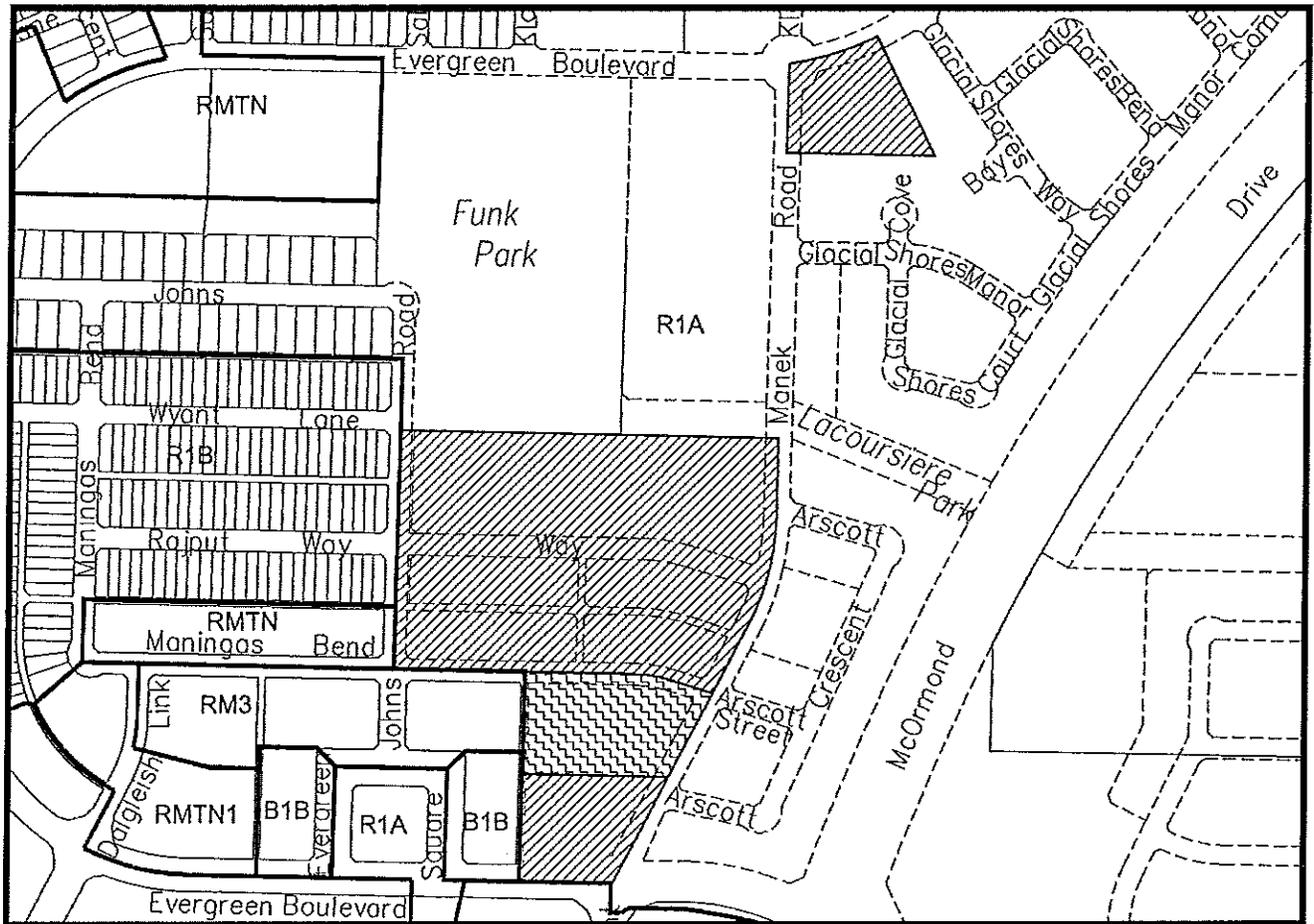
Murray Totland  
City Manager

A1

Plan of Proposed Discretionary Use D3/12

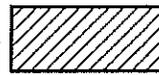


Proposed Rezoning No. Z20/12

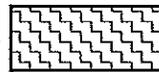


PROPOSED REZONING

From R1A to RMTN

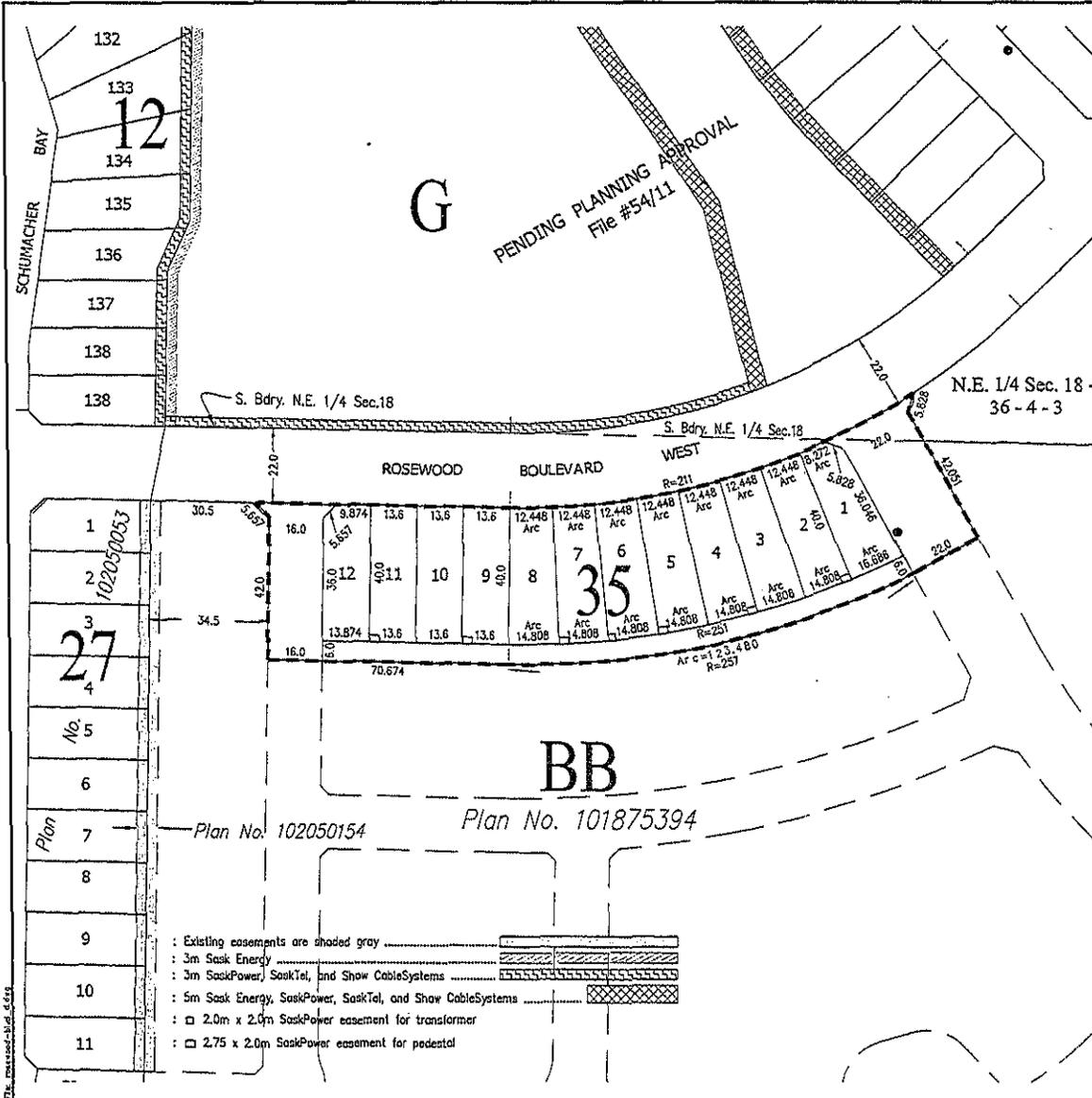


From R1A to RM3



City of  
Saskatoon

Planning & Development Branch



# ROSEWOOD

PLAN  
 SHOWING PROPOSED SUBDIVISION OF PART OF  
 PARCEL BB, PLAN NO. 101875394 IN  
 S.E. 1/4 SEC. 18  
 AND PART OF  
 N.E. 1/4 SEC. 18 ALL IN  
 TWP. 36 - RGE. 4 - W. 3rd MER.  
 SASKATOON, SASKATCHEWAN  
 BY : R.A. WEBSTER, S.L.S.  
 SCALE 1 : 1,000

*[Signature]*  
 Saskatchewan Land Surveyor May 24th, A.D. 2012

LEGEND :  
 -Distance dimensions shown are in metres and decimals thereof.  
 -Distances are approximate and may vary by ±1.0 metres.  
 -Portion of this plan proposed for subdivision is outlined with a heavy broken line, and contains 0.9406 hectares (2.32 acres).  
 : ● Indicates Mail Box location.

EXAMINED : OWNER : BOYCHUK INVESTMENTS LTD.  
*[Signature]*  
*[Signature]*



EXAMINED : CITY OF SASKATOON  
*[Signature]*  
 SENIOR CLERK  
 LAND BRANCH OF THE COMMUNITY SERVICES DEPARTMENT  
 Date : June 4, A.D. 2012

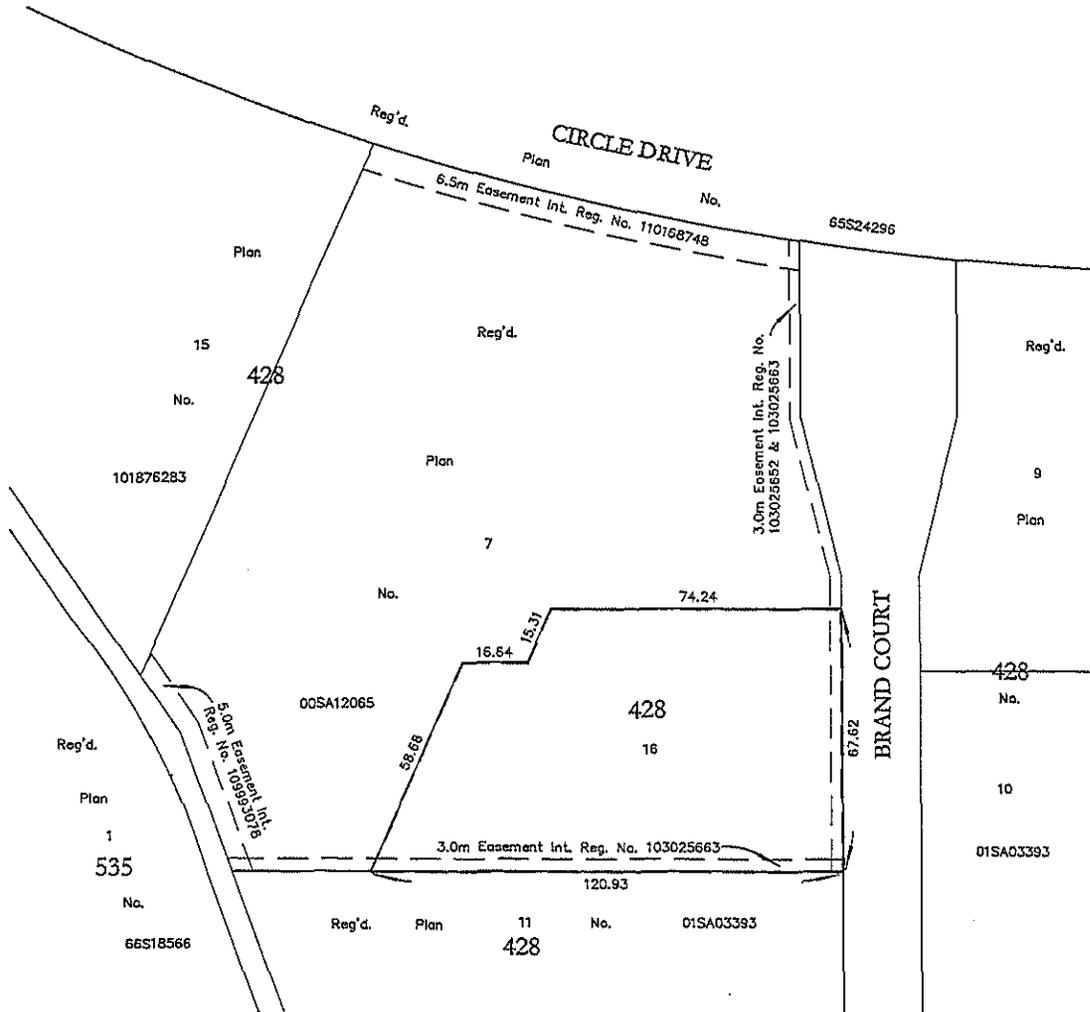
EXAMINED : CITY OF SASKATOON  
 : Approved under the provisions of Bylaw No. 6537 of the City of Saskatoon.

: General Manager of the Community Services Department  
 Date: \_\_\_\_\_, A.D. 2012.



File processed by 4-4-12





PLAN OF PROPOSED  
 SUBDIVISION OF PART OF  
 LOT 7, BLOCK 428  
 REG'D PLAN NO. 00SA12065  
 S.E. 1/4 SEC. 16  
 TWP. 36, RGE. 5, W. 3RD MER.  
 627 BRAND COURT  
 SASKATOON, SASKATCHEWAN  
 BY T.R. WEBB, S.L.S.  
 SCALE 1:1500

Dimensions shown are in metres and decimals thereof.

Portion of this plan to be approved is outlined in red with a bold, dashed line and contains 0.69± ha (1.71± ac.).

Distances shown are approximate and may vary from the final plan of survey by ± 0.5 m

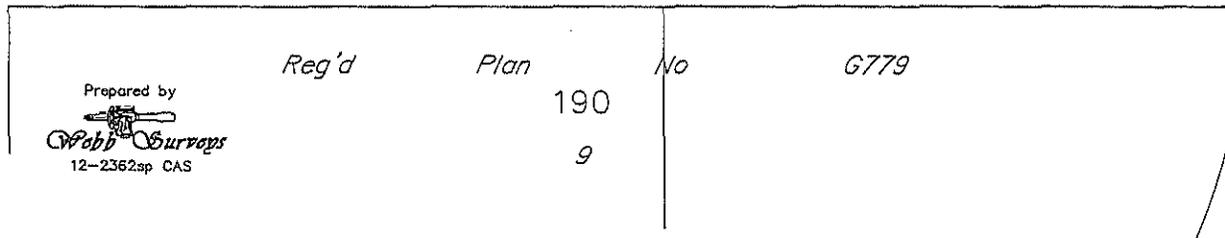
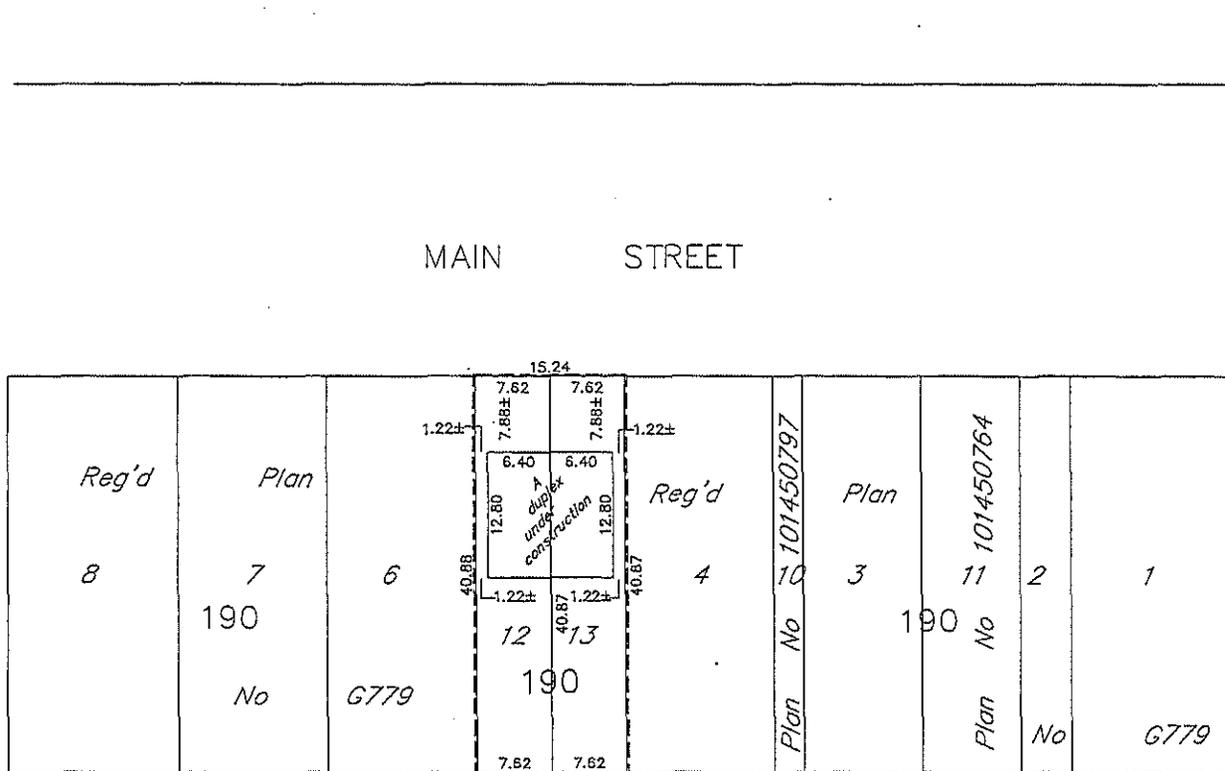
T.R. Webb July 17, 2012  
 Saskatchewan Land Surveyor

Seal

Approved under the provisions of  
 Bylaw No. 6537 of the  
 City of Saskatoon

Date \_\_\_\_\_  
 Community Services Department

Prepared by  
  
 © 2012  
 11-2266sk NLD



PLAN OF PROPOSED  
SUBDIVISION OF  
LOT 5, BLOCK 190  
Reg'd Plan No. G779  
SW 1/4 SEC 27-TWP 36-  
RGE 5-W 3RD MER  
1408 MAIN STREET  
SASKATOON, SASK.  
SCALE 1: 500

I.R. Webb July 22, 2012  
Saskatchewan Land Surveyor

Seal

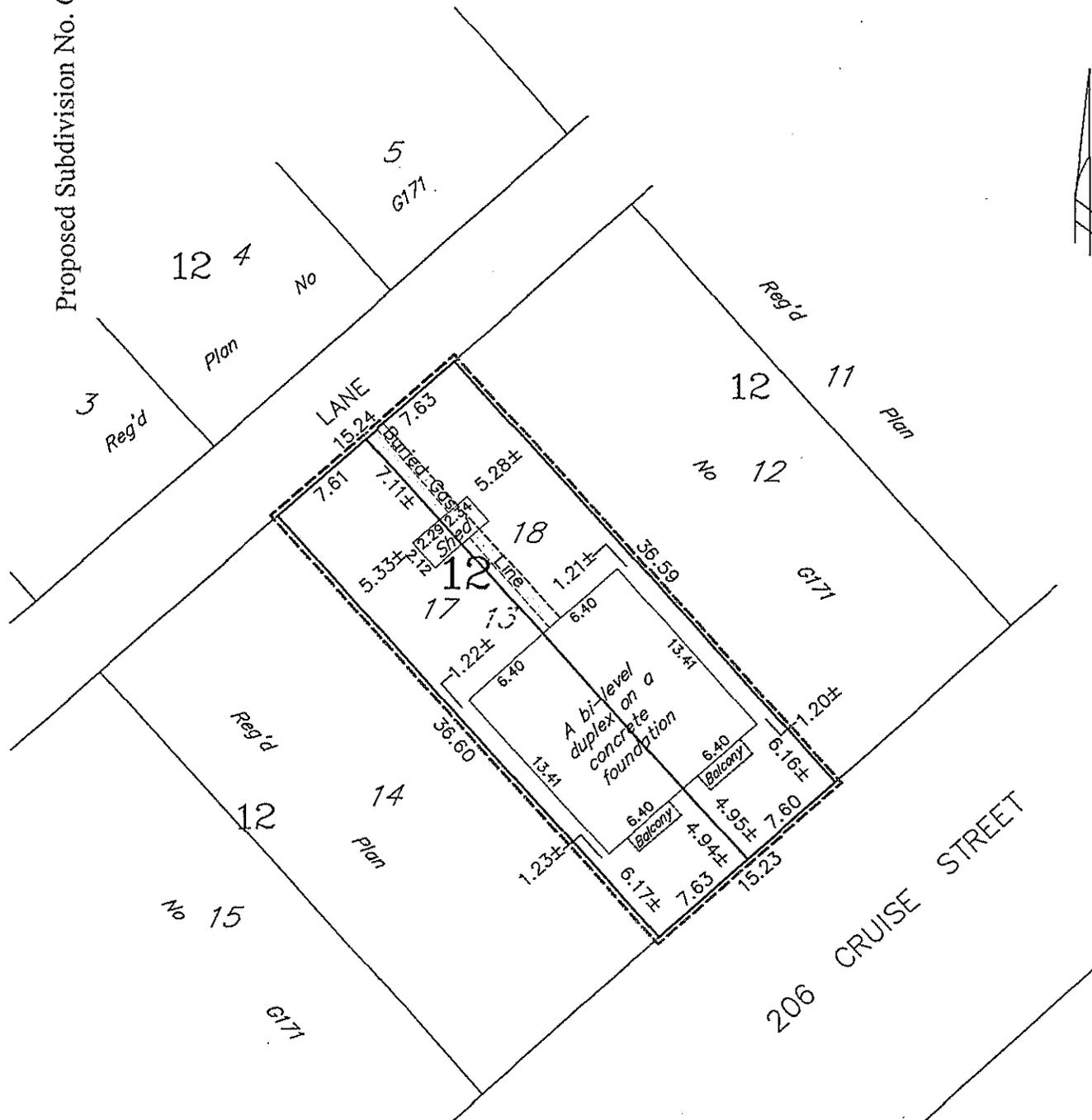
Dimensions shown are in metres and decimals thereof.  
Portion of this plan to be approved is outlined in red with a bold, dashed line and contains 0.06± ha (0.15± ac.).

Approved under the provisions of  
Bylaw No. 6537 of the  
City of Saskatoon

Date \_\_\_\_\_

Community Services Department

Proposed Subdivision No. 62/12



PLAN OF PROPOSED  
 SUBDIVISION OF  
 LOT 13, BLOCK 12  
 REG'D PLAN NO G171  
 NW 1/4 SEC 36-36-5-3  
 206 CRUISE STREET  
 SASKATOON, SASK.  
 SCALE 1: 400

T.R. Webb July 27, 2012  
 Saskatchewan Land Surveyor

Seal

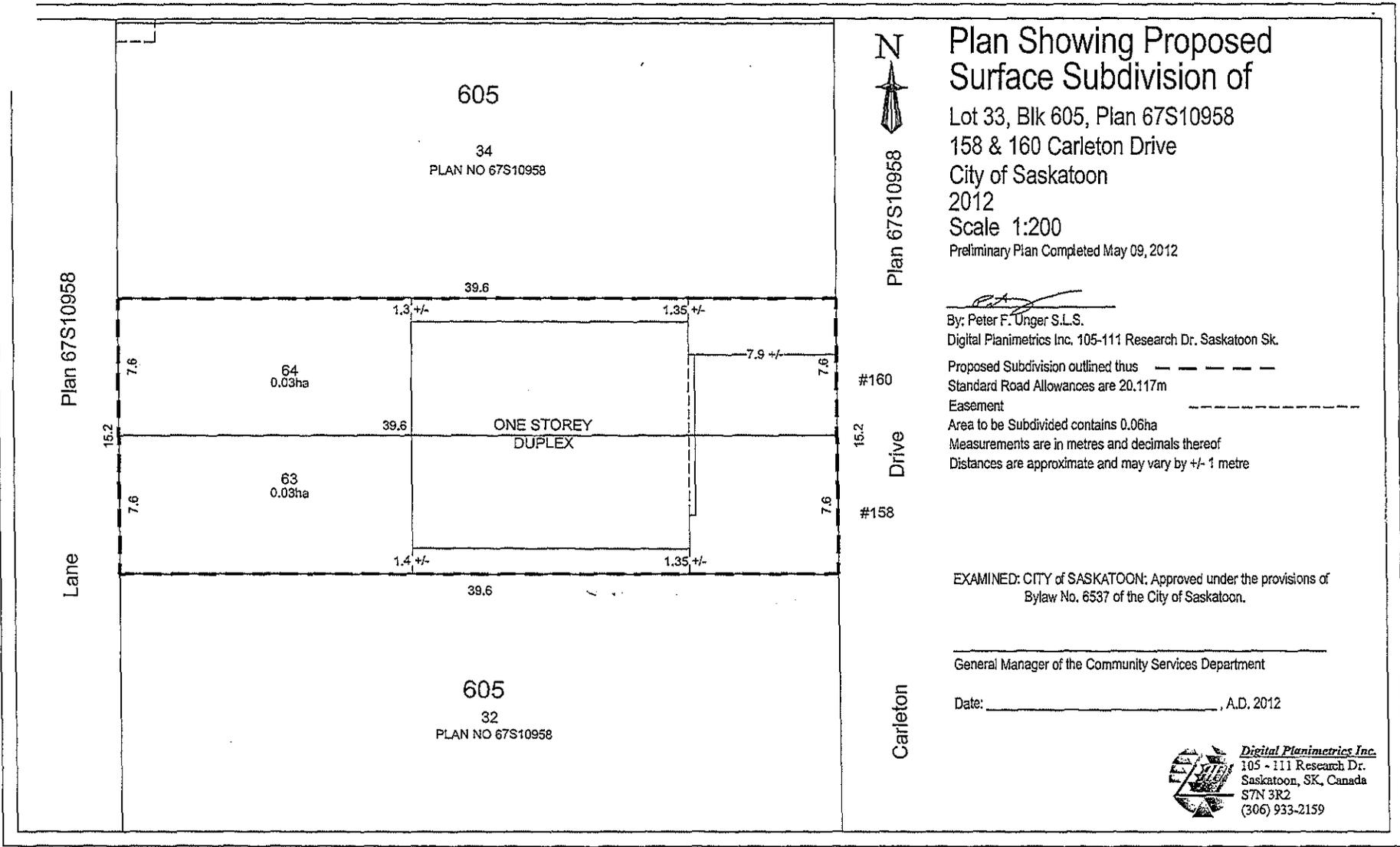
Dimensions shown are in metres and decimals thereof.  
 Portion of this plan to be approved is outlined in red with a bold, dashed line and contains 0.06± ha (0.14± ac.).  
 Dimensions shown are approximate and may differ from the final plan of survey by 0.5± metres.  
 Proposed 1.5 m SaskEnergy easement

Approved under the provisions of  
 Bylaw No. 6537 of the  
 City of Saskatoon

Date  
 Community Services Department

Prepared by

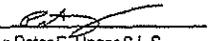
Webb Surveys  
 12-2303ss CAS



Plan 67S10958

### Plan Showing Proposed Surface Subdivision of

Lot 33, Blk 605, Plan 67S10958  
158 & 160 Carleton Drive  
City of Saskatoon  
2012  
Scale 1:200  
Preliminary Plan Completed May 09, 2012

By:  Peter F. Unger S.L.S.

Digital Planimetrics Inc. 105-111 Research Dr. Saskatoon Sk.

Proposed Subdivision outlined thus   
Standard Road Allowances are 20.117m  
Easement 

Area to be Subdivided contains 0.06ha  
Measurements are in metres and decimals thereof  
Distances are approximate and may vary by +/- 1 metre

EXAMINED: CITY OF SASKATOON: Approved under the provisions of  
Bylaw No. 6537 of the City of Saskatoon.

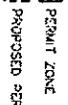
General Manager of the Community Services Department

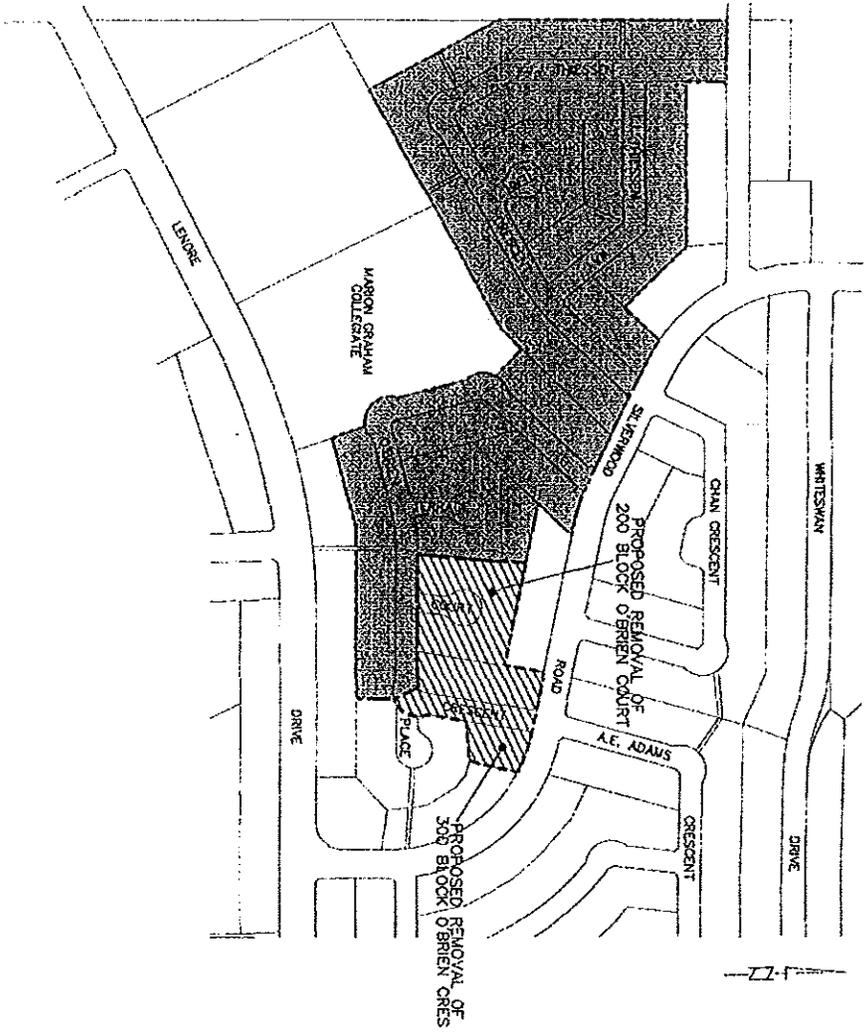
Date: \_\_\_\_\_, A.D. 2012



Digital Planimetrics Inc.  
105 - 111 Research Dr.  
Saskatoon, SK, Canada  
S7N 3R2  
(306) 933-2159

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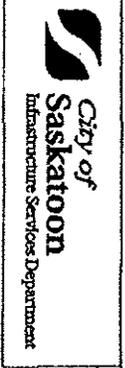
 PERMIT ZONE  
 PROPOSED PERMIT ZONE ADDITION/REMOVAL



NO.	DESCRIPTION	DATE
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APPROVED BY: *[Signature]*  
 DATE: *[Date]*

APPROVED BY: *[Signature]*  
 DATE: *[Date]*



ESSENTIAL PARKING PERMIT ZONE  
 J.L. THIESSEN

250-0001-2011022



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**2012 Corporate Business Plan**

**2nd Quarter Report**

April 1, 2012 to June 30, 2012

**Goal: Continuous Improvement**

**Business Line: Corporate Governance and Finance**

Action	Comments	Status
Introduce a competency model to develop leadership competencies. Align the different functions such as employment and performance to this model.	Senior Management approved commencement of work on the competency model in June. Currently in process of completing RFI to identify technological tools that would meet our requirements.	In Progress
Introduce electronic agenda management system which will streamline process, increase staff efficiencies, and result in photocopy savings.	Research on the project is continuing with plans to issue an RFP in the third or fourth quarter of 2012. A staged implementation of the modules is planned for commence at the end of 2012 or the beginning of 2013. Implementation and training is expected to be completed by the end of 2013.	In Progress
Develop a comprehensive workforce strategy to respond to aging workforce and competitive job market.	The workforce futures strategy is complete with an expected date of September 2012 for implementation.	In Progress
Integrated Health and Safety Management System for Civic Employees.	Implementation of Health and Safety Management System continues for each branch. As part of the HSMS, a comprehensive review and update of the Disability Assistance Program began in April and is expected to be complete in September. Target for completion of DAP review and updating is December 2012 with implementation in January 2013.	Completed

**Goal: Asset and Financial Sustainability**

**Business Line: Corporate Asset Management**

Action	Comments	Status
Review Civic Buildings Comprehensive Maintenance (CBCM) Reserve Model.	In April 2012, Senior Management determined that the current Civic Buildings Comprehensive Maintenance (CBCM) Reserve Model was sound, and should continue to be used.	Completed
Implement new enterprise asset management and energy management system.	Remote implementation meetings have been ongoing with the vendor, Enterprise Asset Management, for the past three months. In addition, interdepartmental meetings have been held to finalize application integration. Data conversion is underway and will be completed in approximately six months. The Enterprise Asset Management application interface with electronic time sheet entry and the corporate accounting system will begin when the data conversion is completed.	In Progress
Add remote trunked radio system site.	Motorola Canada began work on construction of the site located at the Saskatoon Light and Power facility in June 2012. Infrastructure equipment installation has been completed; however, wireless connectivity has not been completed. It is expected that the project will be completed in September 2012.	In Progress
Develop a plan to relocate the bus barns and City Yards to a multi-use site to a new Civic Operations Centre as outlined in the Corporate Asset Management business line.	In 2010, AECOM submitted their Transit building program, which outlines the functional needs of the new Transit facility. The Administration has a Council-approved high-level funding strategy in place, and is currently working with P3 Canada on a funding application for the Transit facility and the Public Works facility. A decision on the funding application from P3 Canada is expected in September 2012.	In Progress
Develop facility site infrastructure maintenance reserve.	A report will be submitted to City Council during the 2013 Business Plan and Budget review.	In Progress
Develop an alternate business model to address fleet replacement.	The Administration continues to utilize two specific alternate vehicle replacement mechanisms: lease to purchase; and purchase of "newly used" equipment. These two business models have proven to be the most effective alternative, and now form part of the Vehicle and Equipment replacement strategy.	Completed

**Goal: Asset and Financial Sustainability - continued**

**Business Line: Corporate Governance and Finance**

Action	Comments	Status
Revalue all properties in preparation for 2013 reassessment.	The 2013 property revaluation is on target. All preliminary analysis has been completed and updated values are now available internally for most properties. A residential and condominium preview program was initiated in early June 2012. As a result, some properties have been identified for further review. Commercial Sector group meetings have begun where market trending and initial analysis is reported to commercial property stakeholders for review and feedback.	In Progress

**Business Line: Transportation**

Action	Comments	Status
Endeavour to increase funding for basic infrastructure to define acceptable service levels and address the maintenance backlog of roads, lanes, bridges, sidewalks, curbs, and overpasses.	The Administration is preparing state of the infrastructure reports for each capital asset, including sidewalks, roads, bridges, lanes and boundary roads, which will include recommended service and funding levels for each. The reports will be presented in the fall of 2012.	In Progress
Develop a short and long-term bus refurbishment and replacement plan.	Transit's bus replacement strategy has been amended to include the purchase of "used and in good operating condition" buses to replace old fleet. Transit recently purchased 12 used buses for a capital cost savings of approximately \$6.7M. A report will be tabled with Council in the fall/winter of 2012 outlining a future funding plan for longer term bus refurbishment and replacement.	In Progress

**Goal: Asset and Financial Sustainability - continued**

**Business Line: Utilities**

Action	Comments	Status
<p>A study is currently underway to review the condition of the City's electrical distribution system against industry standards. The study will identify capital improvements that will be necessary over the next several years. This will also provide a basis for developing long-term rehabilitation strategies in the future. Once the study has been completed, projections will be presented to Council indicating the impact that this infrastructure investment will have on the Utility.</p>	<p>Navigant Consulting was hired to undertake the Planning Study in 2011. They presented the study on June 19, 2012. Administration will prepare a report to Council with recommendations regarding implementation and funding.</p>	<p>In Progress</p>
<p>Energy Recovery &amp; Electrical Redundancy for the Wastewater Utility.</p>	<p>This co-generation project appears to be strongly viable. An update report will be considered by the Administration and Finance Committee in summer 2012, and Administration will be proceeding with detailed design of the project.</p>	<p>In Progress</p>
<p>Odour Abatement for the Wastewater Utility.</p>	<p>An Odour Abatement Study is nearing completion. The Design and Construction of Odour Mitigation Technology is dependent on the results of the Water/Wastewater Treatment Plant Long-Term Capital Development Plan.</p>	<p>In Progress</p>
<p>Operations/Maintenance Building Upgrades for the Wastewater Utility.</p>	<p>The detailed building design is 90% complete with projected completion in July. Tender documents will also be completed in July. Construction services are to be secured by September. Expected completion date of the project is December 2013.</p>	<p>In Progress</p>
<p>A Long-Term Capital Development and Expansion Plan will be Completed in 2012 at the Wastewater Treatment Plant.</p>	<p>A draft copy of Long-Term Capital Plan has been received and is currently under review by Water/Wastewater Treatment Plant staff.</p>	<p>In Progress</p>
<p>At a cost of \$22 million, the reservoir located at 42nd Street West will be expanded and combined with a new pumping facility to service the industrial and northeast residential areas.</p>	<p>The pre-design of the reservoir is complete. Consultants are working on the pumphouse design. The tender for the project plan has been changed so that both the pumphouse and reservoir are tendered together. This will simplify the construction process and lower the cost. The tender is expected to be released in October 2012. Total project is on target.</p>	<p>In Progress</p>
<p>The Avenue H Reservoir Expansion and Enhanced Disinfection High Lift Pumping Station are the major initiatives for the 2012 – 2014 periods in the Water Utility. The projects combined will expand the overall reservoir capacity and reconfigure the distribution system piping while installing an ultraviolet disinfection system and integrated chlorine contact chamber.</p>	<p>The tender closed on January 16, 2012. The construction contract was awarded to Graham Construction. Site preparation, including clearing and traffic detour arrangements were completed in March. The reservoir and pumphouse construction is well underway but currently two weeks behind schedule due to wet weather conditions; however, there is no effect on the final completion day. Project is on target.</p>	<p>In Progress</p>

**Goal: Quality of Life**

**Business Line: Community Support**

Action	Comments	Status
Implement Corporate Accessibility Plan, a collaborative, long-range, ongoing effort to address accessibility of civic facilities, programs, services, and infrastructure including the installation of audible pedestrian signals and curb ramps.	The tender for curb ramps has been awarded and numerous curb ramps are being installed around the city in priority areas. Audible pedestrian signals, with latest technology, have been purchased and will be installed this summer at five key intersections in the downtown core.	In Progress
Additional accessible playground units.	A public open house was hosted May 29 and the contract for the project was awarded in May. In June 2012, a project initiation meeting was held and installation of the accessible playground began.	In Progress
Complete a review of best practices and develop a cost effective plan for the ongoing delivery of neighbourhood based programs as the City continues to grow.	Completed a review of the operations and program offerings by two Community Associations to see if there are opportunities for efficiencies. Developed a questionnaire and contracted Fast Consulting to undertake research of other municipalities across Canada and their practices for the delivery of neighbourhood based-programs.	In Progress
Enhance programming for the Urban Aboriginal Leadership Program.	All three camps are confirmed, with two camps for urban Aboriginal youth and one camp for rural Aboriginal youth. In collaboration with the Saskatoon Tribal Council and Gabriel Dumont Institute, are actively recruiting youth for the camp. First camp to be hosted July 9 to 20.	In Progress
Enhance activities for the Youth Action Network to increase positive race relations among our young adult community.	Youth Action Network, under the leadership of Youth Launch, helped facilitate the Mayor's Youth Forum on May 9. The Administration is working with Youth Launch to prepare a summary report for City Council on the feedback received during the February and May Mayor's Youth Forum. Youth Action Network also planning for a July Community Awareness event on diversity.	In Progress
Complete an assessment of municipal immigration initiatives.	Joe Garcea and Associates continue to work on the immigration initiatives. Formalized the framework for the assessment and have hosted focus group discussions with the Regional Intersectoral Committee, key City staff, and the staff in the Community Development Branch.	In Progress
Through our involvement in the Saskatoon Collaborative Funding Partnership, develop and implement capacity building tools and initiatives for community based organizations.	In June, 2012, wrapped up a review of the pilot projects hosted in 2011 and met with partners to discuss potential capacity projects for 2012	In Progress
Develop race relations monitoring tools and public consultations.	Facilitated strategic planning session as follow up to annual retreat with Cultural Diversity and Race Relations (CDRR) committee on February 9th to identify current and future focus in relation to sources of information to measure progress in achieving outcome statements of the CD&RR policy.	In Progress

**Goal: Quality of Life - continued**

**Business Line: Corporate Asset and Management**

Action	Comments	Status
Upgrade trunked radio systems.	The Trunked Radio Infrastructure Upgrade project, which began in May 2012, has progressed as planned. All acceptance testing has been completed and project completion has been signed off, with the exception of agreed upon deficiencies, which will be rectified over the next several weeks.	In Progress

**Business Line: Corporate Governance and Finance**

Action	Comments	Status
Civic Election in 2012.	Progress to date includes establishment of mail-in ballot option; special/mobile/hospital polls established; ballot tender awarded, ten AutoMark units delivered; candidate's booklets completed and nomination forms available. Pursuing temporary election office space; recruiting workers, and developing training materials.	In Progress

**Business Line: Fire and Protective Services**

Action	Comments	Status
Development of policy for special event emergency planning.	Policy drafted and ready for presentation to Senior Management.	Completed
Construction of Fire Hall #10 Hampton Village.	This project is on hold, pending funding.	In Progress
New software programs required for fire inspection, asset management and training tracking.	Purchase of the Inspection Module is in process with installation slated for June.	In Progress
Creation of vacant properties/buildings bylaw.	Working with City Solicitor's Office on implementation of a bylaw to maintain vacant buildings/properties upkeep and physical appearance from a property maintenance standpoint. Also, buildings are regularly inspected and maintained from a fire and life safety standpoint.	In Progress
Development of neighbourhood evacuation planning strategies for both civic and community resource deployment (Montgomery and Silverwood) to be applied across the City.	Police evacuation plan has been revised and updated. Public notification plan still pending.	In Progress
Required overall upgrade of self-contained breathing apparatus (SCBA) for fire fighters.	In process of evaluating proper replacement of current SCBAs. No determination will be made until January 2013.	In Progress

**Business Line: Police**

Action	Comments	Status
Construction of new headquarters for Saskatoon Police Services.	Project is 33% complete, on budget, and on time with substantial completion expected on October 31, 2013. Site webcam has been installed from atop the John Deere building. Viewing is available on the Saskatoon Police Service website. The immediate task over the next several months is to enclose the building for interior work to be completed during the winter. Steel installation is almost complete and exterior masonry installation is to commence in late July.	In Progress

**Goal: Quality of Life - continued**

**Business Line: Recreation and Culture**

Action	Comments	Status
Design and construction of the Remai Art Gallery of Saskatchewan at River Landing.	The 50% tender drawing package was received. Review of the 50% package, including costing, was completed in early May. Detailed design continued throughout the quarter, including vault layouts, signage, reception area, cafe and shop. The 75% tender drawing package was issued in June, and coordination with River Landing operations and Persephone Theatre continues.	In Progress
Development of new off-leash dog/recreation areas.	A survey of a random selection of dog owners within the city was conducted to better understand the dog owners' awareness, usage, and opinion of off-leash recreation areas. The survey results provided the basis for developing a program plan and development standards for off-leash recreation areas in the city. Locations southwest of Montgomery Place and another within the Caswell Hill neighbourhood are currently being pursued.	In Progress
Completion of construction and reopening of the new Mayfair outdoor swimming pool.	Final commissioning and substantial completion is expected by the end of July 2012, which will enable Community Services to begin programming. Construction deficiencies will be rectified over the next several weeks.	In Progress
\$12.5 million in park upgrades and new development.	The following projects, which commenced in 2011, will be completed in 2012: John Brockelbank Park; George Dyck Park; Prebble Park; Jill Postlethwaite Park; Rendall Park; and Parc Canada. WJL Harvey Park and the Evergreen neighbourhood buffers and traffic islands projects are now complete.	In Progress
Conduct Future Sport and Recreation Facility Needs Assessment Study.	Administration will present the Future Sport and Recreation Facility Development Model to City Council after the consultant has populated the model with the information gathered from the city-wide Leisure Survey completed in December 2011. Project completion on target for December 31, 2012.	In Progress
Complete Kinsmen Park and Area Master Plan.	The Master Plan is complete and the project is in the park design and development phase.	Completed
Preparation of capital and operating financial plan for the proposed \$9.8 million upgrade to Gordon Howe Bowl. The proposal includes an artificial turf field, new service building, score clock and sound system, and outdoor field lighting.	Administration is working with partners to establish a Gordon Howe Sports Foundation for the purpose of raising capital funds and sponsorship for upgrades at Gordon Howe Bowl.	In Progress
Construction of an outdoor pavilion at the Forestry Farm Park and Zoo to host special events and act as a public weather shelter.	Construction is underway with foundation cement piles and electrical service line in place. Project is on schedule for completion in June, 2012.	In Progress

**Goal: Quality of Life - continued**

**Business Line: Recreation and Culture- continued**

Action	Comments	Status
Implement modifications to the paddling pool program and recreation units.	Worked with Occupational Health and Safety staff to complete revisions to procedures with respect to staff safety; incorporated new procedures into training for the 2012 summer staff; worked with Facilities Branch staff in recreation building upgrades; worked with CIS to develop iMap application to promote the summer program which launched July 3.	In Progress
Develop an alternative or supplementary funding model to increase contributions to the Holiday Park Golf Course Redevelopment Reserve.	A report has been prepared and will be received by the Executive Committee by December, 2012. The report is proposing a supplementary funding source to increase annual contribution to the reserve.	In Progress
Begin implementation of the Culture Plan with the recommendations as outlined in the Plan for year 1 (2012), year 2 (2013), and year 3 (2014).	In the 2nd quarter of 2012, staff continued to work on implementation of the Culture Plan. Received approval from City Council for matching funding for a grant from Sask Culture to hire a temporary Culture Consultant. Continued work on a discussion paper to guide the development of a public art strategy and program for Saskatoon. In partnership with the U of S, began research work comparing per capita arts and culture spending in mid-sized Canadian cities and then measuring the economic impact of that spending. Temporary Culture Consultant was hired for 6 months and began work on June 25.	In Progress

**Business Line: Urban Planning and Development**

Action	Comments	Status
Continue to pursue the target of 500 affordable housing units per year.	The Housing Business Plan has set a target of 500 new affordable housing units per year between 2008 and 2012. A variety of new incentive programs, land predesignation, and partnerships have yielded successful results. The 2011 annual report projects that 2,239 new affordable, rental and entry-level housing units will be supported to the end of 2012, or 448 units per year. Policy and Incentive Programs are under review to encourage affordable housing in more neighbourhoods and avoid concentration. Funding from the Province will allow the City to set targets and continue programs to the end of 2013, extending the Housing Business Plan from five years to six.	In Progress
Develop a secure funding source for affordable housing programs after 2013.	The strategy calls for the allocation of \$1.5 million to be provided annually to the affordable housing reserve. Beginning in 2011, this will be funded by a \$250,000 allocation from the operating budget each year until \$1.5 million is reached in 2016. The draw on the Neighbourhood Land Development Fund will be reduced by \$250,000 per year until all \$1.5 million is provided through the mil rate. The first year of this strategy was implemented (2011). However, due to new Provincial Funding available to the City, this strategy will be deferred for at least two years. The new funding commitment from the Provincial Government will be used as the source of funding for existing programs.	Deferred

**Goal: Quality of Life - continued**

**Business Line: Urban Planning and Development - continued**

Action	Comments	Status
<p>Enhance relationships with First Nations that have land development interests in the Saskatoon region, which will include working with partners to develop educational materials and deliver workshops.</p>	<p>City staff, in partnership with SREDA and a number of First Nations with Reserves or Treaty Land Entitlement (TLE) holdings in and near Saskatoon, hosted tours for the CAMA and FCM conferences in May and June. In June, the City and SREDA partnered on an Aboriginal Business Development luncheon with Roberta Jamieson (IndSpire) as the keynote speaker. The City distributed a new brochure on TLE and the Reserve creation process to the 260 attendees.</p>	<p>In Progress</p>

**Goal: Environmental Leadership**

**Business Line: Corporate Asset Management**

Action	Comments	Status
Implement the use of bio-diesel in the vehicle and equipment fleet.	The Environmental Services Branch will be assisting the Facilities Branch with a bio-diesel implementation plan. Preliminary plan development and implementation meetings are scheduled to begin in August 2012.	In Progress

**Business Line: Environmental Health**

Action	Comments	Status
Prepare and implement the Energy and Greenhouse Gas Reduction Business Plan to establish priorities and a sustainable effort toward emissions reduction.	A Business Plan identifying funding and initiatives to bridge the gap between inventory and Plan targets for emissions reductions is near completion. A strategy for 2012 activities has been completed. Work on the creation of an Environmental Sustainability Reserve is underway.	In Progress
An \$11 million gas collection system was initiated at the Saskatoon Landfill in 2011. This infrastructure has the potential to reduce annual greenhouse gas emissions by over 90,000 tonnes per year and provide electricity for up to 2,600 homes. This facility is expected to be operational by early 2013.	Construction of the wellfield began in May 2012 and drilling is nearing completion. Construction of the remaining infrastructure and facilities will continue throughout 2012 and the first half of 2013 with the facility operational by the fall of 2013.	In Progress
Participate in the Saskatoon-Lloydminster Airshed Advisory Committee to guide air protection efforts within the region.	The Administration has an active representative on the Board. Parameters and locations to be studied are under development.	In Progress
Expand recycling under the Waste and Recycling Plan to enhance cost effectiveness and efficiency in the diversion of materials suitable for resource capture from the waste stream.	The Preferred Proponent has been selected. Negotiations are beginning for an anticipated program launch of January 2013. The Program options for multi-unit dwellings are under development.	In Progress
Implement Landfill Optimization to extend Landfill life, improve environmental performance, and increase the level of service provided; and begin expansion of the landfill through construction of a new cell.	Landfill optimization design (master plan) has been approved by Council. Equipment upgrades and staff training are progressing. Daily cover practices and size of active filling area (working face) has improved. Traffic flow and storm water management plans are completed. The filling of inefficiently filled areas has begun. Construction of a new cell on existing site has been tendered.	In Progress
Expand the organic waste program under the Waste and Recycling Plan to divert organic material from the landfill.	Program options and preliminary costing is complete. An organics strategy report is being developed. The existing programs (compost depots and Leaves and Grass Subscription Program) are ongoing.	In Progress
Improve the sophistication of waste handling to enhance cost effectiveness and efficiency.	Optimized routing for 2012 has been implemented. Fleet asset planning is underway. A Waste Utility concept report has been approved by Council.	In Progress

**Goal: Environmental Leadership - continued**

**Business Line: Environmental Health - continued**

Action	Comments	Status
Construct Recovery Park, a facility to divert residential construction and demolition waste from the landfill for re-use in other construction projects.	A detailed "Recovery Park Development Strategy" report has been prepared. Consultation has taken place with similar facilities in other centres to determine size requirements and suitable materials for acceptance. An Expression of Interest in design-build for a facility has been issued. Phase I development is expected to be completed later this year.	In Progress
Continue participation in the South Saskatchewan River Watershed Stewards as a leader in watershed protection and improvements.	A City Councillor and Administrative representative are active members of the Board. A water quality benchmarking study is underway. Future demands on the River are being considered.	In Progress
Establish soil protection and remediation policies and procedures for City-owned properties.	Preliminary criteria thresholds for City-owned land have been developed. A Terms of Reference for Soil and Water Management Plan at the Saskatoon Regional Waste Management Facility (Landfill) and 3 to 5 major construction project sites is being prepared. A procedural review of soil handling protocols with all project staff is ready to deliver this year.	In Progress
Continue the tree planting and maintenance program to ensure a healthy urban forest.	Contracted tree maintenance in Eastview, Avalon and King George are now complete. All remaining in-house tree maintenance from 2011 in Silverwood Heights has been completed. In addition, all miscellaneous elm tree pruning has been completed. Tree Planting and Watering contracts have been awarded for 2012. In-house tree planting and watering commenced at the end of May.	In Progress

**Business Line: Urban Planning and Development**

Action	Comments	Status
Complete a Wetland Policy and Implementation Plan which will conserve ecologically significant wetlands in new growth areas of the city and assist in managing storm water and prevent flooding.	The City hosted a Wetlands Workshop in June 2012 with 50 attendees including staff, developers and environmental groups. The feedback from the Workshop is being used to draft the Wetland Policy and the Implementation Plan.	In Progress

**Goal: *Environmental Leadership - continued***

**Business Line: *Utilities***

Action	Comments	Status
<p>A new \$4.5 million Turbo Expander Power Generation Facility located at Sask Energy's Natural Gas Regulating Station, west of the Saskatoon Landfill, and will produce electricity for up to 600 homes with zero emissions by recovering pressure energy and heat energy. This facility was initiated in 2011 and is expected to be operational by early 2013.</p>	<p>Detailed design of the facility is complete. The project is expected to be tendered in the third quarter, 2012 and operational by the third quarter, 2013.</p>	<p>In Progress</p>
<p>The flood control strategy will be expanded with the installation of additional "superpipes" in Confederation Park, Westview, Early Drive, Erindale, Lakeview, and Dundonald.</p>	<p>Weather has created landscaping delays at the Westview tank. The current schedule provides for completion at the end of July 2012. The Sutherland Park tank has been awarded to Acadia Construction, for \$1.05 million. Construction will begin in late August 2012. Preliminary Design is ongoing for the eastside tank facilities. Dundonald tank's service and capacity will be expanded in the spring of 2013, to coincide with the Dundonald Park upgrade.</p>	<p>In Progress</p>

**Goal: Sustainable Growth**

**Business Line: Land Development**

Action	Comments	Status
The final phases of the Hampton Village neighbourhood, developed in partnership with Dundee Developments, will result in 153 city lots and 143 Dundee lots being serviced in 2012.	Tender for roadways/curb in Phase 8 (Hargreaves Manor) awarded, completion scheduled for August 31, 2012. Lot draw process was completed first week of June. Remaining lots put for sale over the counter; lots will be sold with a delayed possession.	In Progress
A concept plan for the development of the Kensington neighbourhood is being finalized for approval in 2012. Servicing will commence in 2012, with 103 lots scheduled for sale in 2012 and 180 lots in 2013.	The finalized Kensington Neighbourhood Concept Plan was approved by City Council on April 16, 2012.	Completed
New park development in Stonebridge, Rosewood, Evergreen, Lakewood Suburban, and Hampton Village neighbourhoods.	Tenders for the following projects have been awarded for work to be completed in 2012: Pleasant Hill Park, Girgulis Park, Adams Park, Cannan Park, Klombies Park, Hyde Park, Mark Thompson Park and Patricia Roe Park. The following projects are still to be tendered: Funk Park (Phase 1) and the 22nd Street Cut Entrance. Rotary Park and Dundonald Park will be designed in 2012.	In Progress
Review new neighbourhood design standards from a long-term Transit perspective. If changes are desired, the City will then work with developers to update these standards.	Transit, Transportation Branch, and Planning and Development Branch have been working collaboratively to implement the outcomes described in the City's Integrated Approach to Growth strategy. New neighbourhoods are being designed to inherently support Transit. This is achieved by concentrating density along nodes and corridors that will be long-term Transit routes, and ensuring there is connectivity between neighbourhoods along these corridors. Continued joint work between these three Branches will be key to ensuring all new neighbourhoods continue to be Transit-oriented developments. New neighbourhood design standards will be researched and reviewed for the duration of 2012 and brought forward for consideration in early 2013.	In Progress
A total of 28 acres of multi-family land will be serviced in 2012, with a further 22 acres in 2013, and 26 acres in 2014.	<p>Evergreen - 15.01 acres (7 parcels) multi-family land. Water/Sewer and Roadway Tenders issued for the 2012 construction season.</p> <p>Rosewood- 9.95 acres (2 parcels) Water /Sewer and Roadway Tenders issued for the 2012 construction season.</p> <p>Hampton Village - 7.36 acres (2 parcels) Servicing of these parcels is being coordinated by Dundee Developments on behalf of the Land Branch. Grading, water/sewer and roadway work will commence in the spring with an estimated completion date of fall 2012.</p>	In Progress

**Goal: Sustainable Growth - continued**

**Business Line: Land Development - continued**

Action	Comments	Status
In the Marquis Industrial Area, 72 acres of land will be serviced in 2012, with a further 43 acres in 2013, and 48 acres in 2014.	Phase 6 (72 acres) Water and sewer servicing is complete. The remaining road work is ongoing and should be complete fall 2012. Public Tendering of this land will take place in fall 2012. Servicing plans for 2013 are being expanded from 43 acres to 94 acres.	In Progress
In Evergreen, 342 lots are expected to be serviced and made available for sale in 2012. In 2013 and 2014 work will commence on the Evergreen Village Square and the planned green pedestrian bridge over McOrmond Drive.	Tenders for servicing 453 lots in 2012 are in place. Grading and water/sewer work is in progress. Roadway commenced in late April, 2012, Lot draw for 203 lots on Mahabir and Salloum started in June 2012 and will be complete by July 2012 with projected sell out. Further lot draws will take place in fall 2012, dependent on servicing completion. Water/ Sewer Tenders for lots scheduled to be complete in 2013 have been awarded.	In Progress
In Rosewood, 135 lots are scheduled for sale in 2012 and approximately 20 remaining lots within the City's ownership in 2013.	14 lots remaining in Phase 1 inventory. Roadway Tender for Phase 2 (135 lots) awarded to Central Asphalt. Construction of roadway and shallow buried utilities began in spring 2012. Lot draw planned for Phase 2 (135 lots) in fall 2012.	In Progress
New arterial road construction of Marquis Drive from Millar to Siemens Avenue.	Detailed design and construction drawings are complete. The tender was awarded to ASL Paving Inc. at the City Council meeting held on July 18, 2012. Construction of the four-lane arterial road is on hold until storm water issues in the area have been resolved. Completion of this project is scheduled for 2013.	In Progress
New trunk sewer extensions in the following locations: Storm - McOrmond Drive (Attridge to Highway 5), Hampton Village - Richardson Road (McClocklin to Westview), and Kensington (sanitary/storm trunks)	<p>McOrmond Drive Storm Sewer Trunk - Michael's Canada was awarded the project at the May 28, 2012 City Council meeting. Design is scheduled to be completed by October 2012; construction is scheduled to commence in November 2012; and completion is scheduled for November 2013. An Open House is scheduled to be held in September 2012 to communicate the project to the public.</p> <p>Hampton Village Storm Trunk - Preliminary design of the storm trunk is complete. The detailed design and tender are to be completed by the end of August. Construction is scheduled to be completed by October 31, 2012.</p> <p>Kensington Storm Trunks - Design and construction drawings for the trunk sewers servicing the north half of the neighborhood are complete. Contractors have been retained and construction of these trunks is in progress, with completion scheduled for October 31, 2012. Design and construction of the trunk sewers required to service the south half of the neighborhood has been accelerated to accommodate Dundee Development's request to be in a position to service lots in the south portion of Kensington in 2014. Detailed design of these trunk sewers is complete, and the City is awaiting execution of a servicing agreement with Dundee before proceeding further.</p>	In Progress
Extension of the primary watermain along McOrmond Drive from Evergreen Boulevard to Fedoruk Drive.	The tender was awarded to Hy-Way Construction Inc. at the May 14, 2012 City Council meeting. Construction has begun and is scheduled to be completed by October 31, 2012.	In Progress

**Goal: Sustainable Growth - continued**

**Business Line: Transportation**

Action	Comments	Status
Implement a Transportation Demand Management Strategy to make more efficient use of transportation resources.	A survey to establish baseline travel information has been reviewed by the Communications Branch and will be conducted internally, as a pilot project, in September 2012. A report on the implementation status and priorities of the strategic plan has been reviewed by the Transportation Demand Management Steering Committee. Revisions are being made and the information will be included in the report on the Integrated Growth Plan.	In Progress
Undertake a Transit Demand Study to understand current travel needs throughout the city.	The terms of reference for a Household Travel Survey is being prepared, which will be conducted following the opening of the Circle Drive South River Crossing Project.	Not Started

**Business Line: Urban Planning and Development**

Action	Action	Action
Submit East Sector Plan to City Council for review.	The East Sector Plan was approved by City Council on April 16, 2012. City Council approved the name "Holmwood" for the Sector.	Completed
Begin a North Sector Plan.	Background information on the North Sector is being collected and a Natural Areas Screening Study is underway.	In Progress
Begin a Concept Plan for the South West study area in the Confederation Sector.	Background information on the South West study area is being collected and a Natural Areas Screening Study is underway.	In Progress
Outside City limits, in partnership with the RM of Corman Park, begin a South West concept Plan for the Planning District.	RM of Corman Park Council approved funding for the South West Concept Plan and an RFP is being drafted.	In Progress
Development of the Blairmore Neighbourhood #2 Concept Plan.	Background work to undertake design of the Neighbourhood Concept Plan is continuing and is on schedule.	In Progress
Development of the University Heights Neighbourhood #2 Concept Plan.	Background work to undertake design of the Neighbourhood Concept Plan is continuing and is on schedule.	In Progress
Submit College Quarter Concept Plan to City Council for review.	Approved by City Council February 6, 2012. Project is complete.	Completed
Complete Central Avenue Streetscape work from 109th to 110th Streets.	Prepared construction documents for tender; project tendered in March 2012. Expect construction to begin by July; estimate construction to conclude in fall 2012.	In Progress
Begin new Local Area Plan in Mayfair/Kelsey Woodlawn.	New Local Area Plan in Mayfair/Kelsey Woodlawn will begin in fall 2012.	Not Started
Provide a leadership role in the North Downtown Master Plan to create a comprehensive vision for the vacant city-owned land resulting from the extension of 25th Street to Idylwyld and the relocation of the City Yards from the city's central business district.	The quarter was spent developing and defining the terms of reference for the project. Project objectives and a schedule and the scope of work were taken to the Senior Management Team on June 8. The Steering Committee convened in mid-June to discuss the terms of reference and the consultant call preparation. An article on the project was published in The Star Phoenix on June 2. The Environmental Site Report is on-going, and is expected to be issued for review by the end of the third quarter.	In Progress

**Goal: Sustainable Growth - continued**

**Business Line: Urban Planning and Development - continued**

<p>Complete City Centre Plan Phases 3 and 4: The Overall Plan for the City Centre and the Civic Plaza Master Plan.</p>	<p>Adjustments to project timing were made to allow the City Centre Plan phase to occur before the new Civic Plaza Master Plan is developed. Phase 3 is underway and a Request for Proposals has been issued for consulting services to develop the new Plan. Phase 4 (Civic Plaza Master Plan) will follow in 2014 using information contained in the new City Centre Plan. All phases of the City Centre Plan are fully funded.</p>	<p>In Progress</p>
<p>Outside City limits, in partnership with the RM of Corman Park, begin a North Concept Plan for the Planning District.</p>	<p>RM of Corman Park Council approved funding for the North Concept Plan, and an RFP is being drafted.</p>	<p>In Progress</p>

**Goal: Moving Around**

**Business Line: Corporate Governance and Finance**

Action	Comments	Status
Implement City Council recommended revisions to the Taxi Bylaw.	Two of the three taxi companies have new management, and we have met with all managers to continue with the incorporation of data collection and analysis. The analysis of content for drive training has begun.	In Progress

**Business Line: Transportation**

Action	Comments	Status
National attention on successful Transit services during the 2012 Brier and 2012 Canadian Country Music Week and Awards Show.	Transit delivered a highly successful charter service for the 2012 Brier. Additional information will be provided subject to final review and debriefing with event coordinators and Transit employees.	Completed
Continued focus on ECO bus pass partnerships with businesses, volunteer groups and agencies.	Saskatoon Transit has formalized 13 Eco Pass Agreements resulting in 354 employer-supported Eco Passes. Transit has had discussions with seven new agencies and one existing participating institution which has potential to result in approximately 800 new passes in 2012.  U Pass Agreement with SIIT was implemented on January 1, 2012.  Transit and Public School Board meeting was held March 1st to discuss pilot high school bus pass program for implementation in fall 2012. Due to funding constraints, the Public School Board is unable to pursue a pilot project unless other funding options are identified.	In Progress
Design and construct a bus terminal and route for buses for easier entrance and exit from Credit Union Centre during special events.	Plans are to proceed with traffic signals, pathway construction and on-site transit safety improvements in 2012. Confirmation of cost estimate prior to tendering and construction are underway. Further discussions are required with respect to long-term improvements.  IS plans to have signalling constructed and operating by end of 2012. Transit terminal fencing (permanent) currently being confirmed between participating parties (i.e. CUC, Transit and Transportation) and construction intended to be tendered and completed in 2012.	In Progress

**Goal: Moving Around - continued**

**Business Line: Transportation - continued**

<p>Pilot signal-priority system at five intersections to improve Transit service between downtown and the University of Saskatchewan.</p>	<p>Hardware installed on Transit DART buses and College Drive intersections. Progress of project will be monitored in 2012 and reports generated to determine impact to Transit service between downtown and the U of S.</p>	<p>In Progress</p>
<p>Implement technological innovations to increase Transit ridership such as real-time passenger information through cellular/SMS alerts, smart phone and website applications such as "live transit" and both audio and visual bus stop announcements.</p>	<p>Transit is testing its technology on a portion of the Intelligent Transit System fleet in 2012 and is targeting the latter part of 2013 to implement live transit for public consumption pending budget availability. The first stage of testing technology on a portion of its fleet is complete and the manufacturer will install hardware in 29 additional buses for 2<sup>nd</sup> stage of testing beginning April 23. All 30 buses now have hardware installed and Transit is continuing to monitor and make necessary adjustments in preparation to install hardware on entire fleet. Transit is working alongside CIS and the Trapeze Group to ensure this product works to the maximum capability.</p>	<p>In Progress</p>
<p>Continue a discounted bus pass program to ensure Transit is an affordable mode of transportation for citizens with low income.</p>	<p>20% increase in the number of Low Income passes sold for the same time period in 2011 (January to June).</p>	<p>In Progress</p>
<p>Preliminary investigation of Rapid Transit options for the city.</p>	<p>This project is part of the Integrated Growth Plan. A Rapid Transit Study Task Team has been established to identify and assess potential rapid transit technologies and corridor options. The project is on target, to be completed in 2014.</p>	<p>Not Started</p>
<p>Opening of Circle Drive South, the new South Bridge, related interchanges, and overpasses (2012).</p>	<ul style="list-style-type: none"> <li>• River Crossing - girders completed on east side. Bridge deck construction started and to be completed by July. Completion of bridge in summer 2012.</li> <li>• Preston Interchange - girder installation complete. Deck to be constructed after girder installation.</li> <li>• Northwest - roadwork in this area started. Sound walls along 11th Street Bypass started; Dudley Street extension to be tendered. Rail to be completed in summer 2012.</li> <li>• Idylwyld Overpass - sound walls are completed in the area. Miscellaneous concrete finishing taking place on structures. Finishing work on the drainage in the area.</li> <li>• Lorne Ave Overpass - roadwork under construction. Retaining walls under construction. All ramps and adjacent roadwork to be completed in 2012.</li> <li>• Valley Road Overpasses - miscellaneous concrete finishing taking place on structures. Retaining walls are being completed. Final drainage hookups are underway. Roadwork under construction.</li> </ul>	<p>In Progress</p>

**Goal: Moving Around - continued**

**Business Line: Transportation - continued**

Action	Comments	Status
Construction of Phase I of Highway 7 and Highway 14 interchange.	Central Asphalt was awarded the tender for construction of the roadway, which commenced on April 16, 2012, and is expected to be completed on November 15, 2012.	In Progress
Implement new technologies for replacement of parking meter infrastructure, which may include the reintroduction of the City Card payment and pay by phone payment for parking.	Two license plate recognition (LPR) systems have been implemented for parking enforcement, with positive results. Evaluation of technologies for parking meter upgrades is in progress. Options for proceeding with a Request for Proposals will be presented to City Council in late 2012.	In Progress
Develop a long-range transportation master plan integrating all transportation modes including transit, walking, cycling, and driving.	The Future Growth Delivery Team is working to develop an integrated overarching plan for the coordination of transportation and transit efforts. A report will be submitted during Capital Budget deliberations requesting funding for 2013 through 2015.  The high level plan was presented to Council in June 2012. A refined version of the plan is scheduled for completion in early 2013.	In Progress
Present a five-year capital plan for the expansion of cycling infrastructure (pathways, bike lanes, sharrows) and expand educational and awareness initiatives.	The five-year capital plan and program direction for 2012 have been approved by City Council. The "Learn to Ride Safe" school based program is complete. The Blairmore Bikeway, Broadway Bridge Access and Spadina Cycling projects are underway.	In Progress
Develop a Pedestrian Master Plan.	A report will be submitted to City Council during the 2013 Business Plan and Budget deliberations requesting funding for work to begin in 2013 through 2015. No further work will be completed until funding has been approved.	Not Started

**Business Line: Urban Planning and Development**

Action	Comments	Status
Construction, extension, and streetscaping of 25th Street from Idylwyld Drive to 1st Avenue.	Water and Sewer construction has been completed. Construction of the roadway commenced on April 16, 2012, and is expected to be completed on October 31, 2012.	In Progress
Continue to work with the Ministry of Highways and Infrastructure to finalize designs for future Perimeter Highway.	Continuing discussions with the Saskatoon Business Community and the provincial government.	In Progress
Continue execution of the replacement of the Traffic Bridge.	The consultant, Stantec, is currently working on Request for Proposal documents for the design build project, which is scheduled to be completed in fall 2012, so that the project will be ready to proceed when complete funding to replace the Traffic Bridge is available. Stantec will be proceeding with pier testing in September 2012, to rate the current capacity of the piers. The overland span (Span 1 - South Side) will be demolished in September/October 2012. The tender will be awarded in August.	In Progress

**Goal: *Moving Around - continued***

**Business Line: *Urban Planning and Development - continued***

Evaluate the suitability of and formalize the level of service for snow and ice removal on priority travel routes, areas near schools, bus stops, and accessibility of roadways in new neighbourhoods, and budget implications of changes or improvements.

Public Works Branch will be conducting an extensive review of the winter program in order to evaluate levels of service with respect to budget implications and the expansion of the city (i.e. new neighborhoods and the opening of Circle Drive South).

In Progress

**Goal: *Economic Diversity and Prosperity***

**Business Line: *Urban Planning and Development***

Action	Comments	Status
Online applications for business licenses to be implemented in 2012.	Conceptual web layout and web broker components completed. Consultant expected to resume development of website application for online applications for new business licenses in August 2012. Testing and implementation will occur in late fall of 2012. Website application for renewal of existing business licenses online, to be developed by the Administration, will follow.	In Progress

2012 2nd Quarter Report to Council.doc/deb  
August 8, 2012

## Building Permit Statistics for YTD Jun 30, 2012 vs. YTD Jun 30, 2011

Category	Number of Permits			Construction Value (\$million)		
	YTD Jun 30/12	YTD Jun 30/11	Change %	YTD Jun 30/12	YTD Jun 30/11	Change %
Residential	2090	1669	25.2%	\$266.7	\$198.6	34.3%
Apartments & Housing Projects	158	157	0.6%	\$85.2	\$103.2	-17.4%
Commercial	199	156	27.6%	\$158.4	\$39.1	305.1%
Industrial	97	99	-2.0%	\$55.7	\$36.1	54.3%
Institutional & Assembly	59	38	55.3%	\$25.4	\$13.9	82.7%
Other (includes demolition permits)	113	107	5.6%	\$12.1	\$3.4	255.9%
<b>TOTAL</b>	<b>2716</b>	<b>2226</b>	<b>22.0%</b>	<b>\$603.5</b>	<b>\$394.3</b>	<b>53.1%</b>

### Summary of major projects over \$10M approved to June 30, 2012

New Office Building - 220 Wall Street	\$10.0M	(Included in "Commercial" above)
New Apartment Condominium - 1010 Ruth Street	\$12.9M	(Included in "Residential" above)
New Apartment Condominium - 1035 Moss Avenue	\$12.0M	(Included in "Residential" above)
New Police Station (Final Phase) - 76-25th St. East	\$74.7M	(Included in "Commercial" above)

**City of Saskatoon**  
**2012 Year End Projection**  
**For Period Ending June 30, 2012**

**ATTACHMENT 3**

The following table summarizes the 2012 budget projected to year end. While a number of variances have been identified, overall, a small surplus is currently projected.

<b>Corporate Summary (in 000's)</b>	<b>2011 Actual Results</b>	<b>2011 Total Budget</b>	<b>2012 Total Budget</b>	<b>2012 Projection</b>	<b>2012 Variance</b>	<b>% Variance</b>
<b>Corp Asset Management</b>	\$6,186.7	\$7,213.3	\$7,654.5	\$7,836.4	\$181.9	2.38%
<b>Community Support</b>	10,664.8	10,497.8	11,294.8	11,409.9	115.1	1.02%
<b>Corp Governance &amp; Finance</b>	(55,966.0)	(51,910.1)	(52,714.3)	(53,652.2)	(937.9)	-1.78%
<b>Environmental Health</b>	10,585.6	9,598.4	9,782.4	10,840.3	1,059.9	12.72%
<b>Fire &amp; Protective Services</b>	38,766.7	38,920.8	40,118.4	40,118.4	0.0	0.00%
<b>Policing</b>	64,522.4	64,406.3	68,343.4	68,319.2	(24.2)	-0.04%
<b>Recreation &amp; Culture</b>	26,601.5	26,693.3	28,181.7	28,091.9	(89.8)	-0.32%
<b>Transportation</b>	58,691.3	56,306.2	58,882.2	59,070.3	188.1	0.96%
<b>Urban Planning &amp; Development</b>	5,296.4	5,547.2	5,472.9	5,218.7	(254.2)	-2.53%
<b>Utilities</b>	0.0	0.0	0.0	0.0	0.0	0.00%
<b>Taxation</b>	(168,322.4)	(167,273.2)	(177,016.0)	(177,335.5)	(319.5)	-0.16%
<b>Total (Surplus) Deficit</b>	<b>(\$3,073.0)</b>	<b>\$0.0</b>	<b>\$0.0</b>	<b>(\$80.6)</b>	<b>(\$80.6)</b>	<b>-0.02%</b>

The variances noted above include the following:

- Continued city growth exceeds the initial budget assumptions. The actual 2012 tax roll will result in \$541,000 more in tax revenue than budgeted. Supplementary property assessment revenue is also projected to result in additional tax revenue of \$640,000.
- Actual results to June 30, 2012, for parking and traffic violations are higher than budgeted by \$620,000.
- The landfill tipping fee revenue is projected to be approximately \$200,000 less than budget due to an initial overstatement of budgeted revenues resulting from an increased proportion of small, subsidized loads, as well as changing waste patterns due to the new fee structure. In addition, expenditures on salaries are larger to accommodate construction, and the complexities of filling in historically inefficiently filled areas to achieve optimization of the facility.
- The revenue budget from the recycling depot program includes a projection of proceeds from the revenue-sharing agreement with Cosmopolitan Industries based on the five-year average. These revenues are unlikely to be achieved in 2012.
- The initial budget for grants-in-lieu of taxes to the City assumed increased electrical rates. This assumption has been revised, resulting in an estimated \$835,000 shortfall in revenue.
- The water and wastewater utilities are projecting a \$1.1 million shortfall in metered revenue due to lower consumption. It is anticipated that this will be funded by the utility's stabilization reserve and a reduction to capital reserve contributions.
- The Saskatoon Light and Power budget assumed a SaskPower rate increase for 2012 that will not materialize. In addition, the warmer than normal winter has resulted in a projected \$1.5 million deficit within the utility. It is anticipated that this will be funded by the utility's revenue stabilization reserve and through a reduction to capital reserve contributions.

- 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone the land parcels located on Richardson Road, Lehrer Manor, Lehrer Crescent, and a utility parcel (see Attachment 2 to the report of the General Manager, Community Services Department dated July 16, 2012) from R1A – One-Unit Residential District to RM3 – Medium Density Multiple-Unit Dwelling District, RMTN – Townhouse Residential District, and R1B – Small Lot One-Unit Residential District; and AG – Agricultural District to R1A – One-Unit Residential District, be approved based on the reasons outlined in the report of the General Manager, Community Services Department, dated July 16, 2012.

Attached is a report of the General Manager, Community Services Department dated July 16, 2012, with respect to the above proposed rezoning.

Your Committee has reviewed the proposed rezoning with the Administration and supports the above recommendations.

**2. Proposed New Commercial Zoning District – B5B Broadway and  
Proposed New Overlay Zoning District – AC2 –  
B5B Architectural Control Overlay Zoning District  
Applicant: City of Saskatoon, Planning and Development Branch  
(File No. CK. 4350-012-005)**

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- RECOMMENDATION:**
- 1) that City Council approve the advertising respecting the proposal to amend Sections 13.1.3 of Zoning Bylaw No. 8770 as outlined in the report of the General Manager, Community Services Department dated July 17, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
  - 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and

- 4) that at the time of the public hearing, City Council consider the Commission's recommendation to amend Zoning Bylaw No. 8770 to add the proposed B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District, and to amend the parking and sign regulations contained in Zoning Bylaw No. 8770 to reflect the addition of the B5B Zoning District.

Attached is a report of the General Manager, Community Services Department dated July 17, 2012, with respect to the above proposed amendments to the Zoning Bylaw.

Your Commission has reviewed the report with the Administration and the Chair of the Broadway 360 Steering Committee, including the consultation process undertaken, and is supporting the above recommendations.

**3. Proposed Rezoning from B5 to B5B and AC2 – B5B  
Nutana Neighbourhood  
Applicant: City of Saskatoon, Planning and Development Branch  
(File No. CK. 4351-012-10)**

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- RECOMMENDATION:**
- 1) that City Council approve the advertising respecting the proposal to amend Zoning Bylaw No. 8770 as outlined in the report of the General Manager, Community Services Department dated July 30, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
  - 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770;
  - 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposal to rezone the properties indicated on Attachments 2 and 3 of the report of the General Manager, Community Services Department dated July 30, 2012, from a B5 Zoning District to a B5B Zoning District, be approved; and

- 5) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposal to apply the AC2 – B5B Architectural Control Overlay District to the properties indicated on Attachments 2 and 3 of the report of the General Manager, Community Services Department dated July 30, 2012, be approved.

Attached is a report of the General Manager, Community Services Department dated July 30, 2012, with respect to the above matter.

Your Commission has reviewed the above matter with the Administration and Broadway 360 Steering Committee Chair and supports the above recommendations.

**4. Proposed Rezoning from R1A(H) to R1A,  
FUD to R1A, and R1A(H) to R1B  
Kensington Neighbourhood  
Applicant: City of Saskatoon, Land Branch  
(File No. CK. 4351-012-11)**

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- RECOMMENDATION:**
- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in the report of the General Manager, Community Services Department dated July 19, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
  - 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
  - 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposed amendments to Zoning Bylaw No. 8770 be approved, as follows:
    - a) that the properties identified in Attachment 2 to the report of the General Manager, Community Services

Department dated July 19, 2012, be rezoned from R1A(H) - One-Unit Residential District with a Holding symbol, and FUD - Future Urban Development District to R1A – One-Unit Residential District; and

- b) Phase I of the Kensington neighbourhood development, as shown on the Plan of Proposed Subdivision (see Attachment 3 to the report of the General Manager, Community Services Department dated July 19, 2012), be rezoned from:
  - i. R1A(H) - One-Unit Residential District with a Holding symbol, to R1B – Small Lot One-Unit Residential District (Lots 1 to 26, Block 100, Lots 1 to 29, Block 101, Lots 1 to 30, Block 103); and
  - ii. R1A(H) - One-Unit Residential District with a Holding symbol to R1A – One-Unit Dwelling Residential District (Lots 1 to 15, Block 102, and Lots 71 to 76, Block 103).

Attached is a report of the General Manager, Community Services Department dated July 19, 2012, with respect to the above matter.

Your Commission has reviewed the report with the Administration and is supporting the above recommendations.

**5. Proposed Rezoning from R1A to R1B and RMTN  
Evergreen Boulevard, Kloppenburg Crescent, Bend, Link, Street, and Way  
Evergreen Neighbourhood  
Applicant: City of Saskatoon, Land Branch  
(File No. CK. 4351-012-013)**

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**RECOMMENDATION:** 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in the report of the General Manager, Community Services Department dated July 16, 2012;

- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone the properties identified in the attached Proposed Zoning Map from R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District, and RMTN – Townhouse Residential District, be approved.

Attached is a report of the General Manager, Community Services Department dated July 16, 2012, with respect to the above matter.

Your Commission has reviewed the report with the Administration and supports the above recommendations.

Respectfully submitted,

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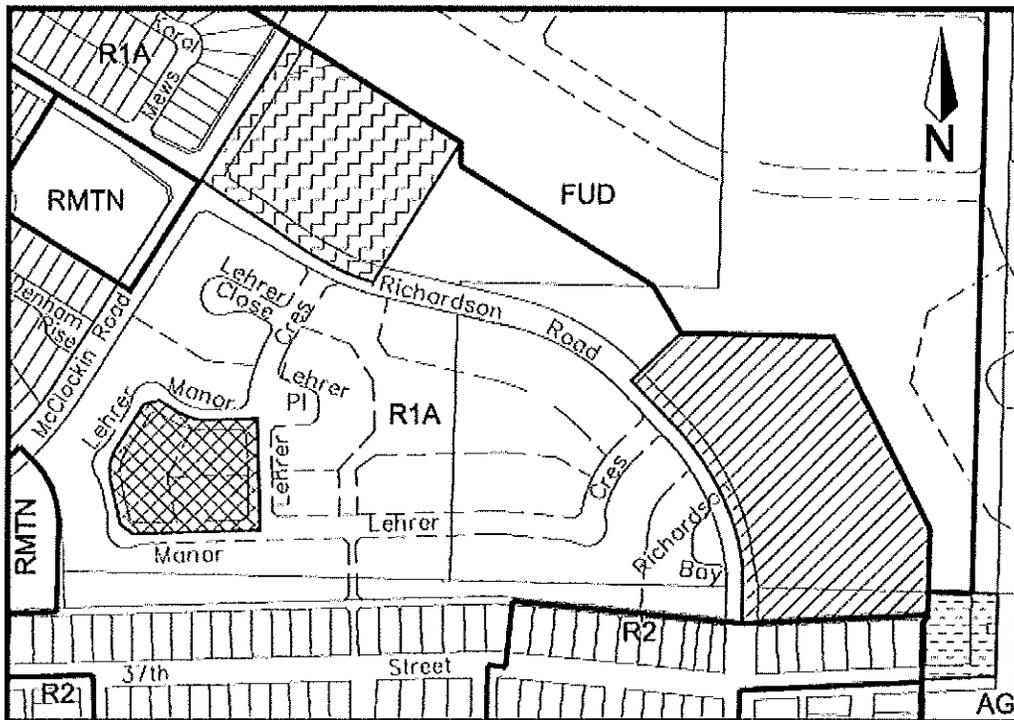
Mr. Kurt Soucy, Chair

1.

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z11/12	<b>PROPOSAL</b> Proposed Rezoning from R1A to RM3, RMTN, and R1B; and from AG to R1A	<b>EXISTING ZONING</b> R1A and AG
<b>LEGAL DESCRIPTION</b> Parcels AA, BB, CC, Lots 1 to 16, Block 24; and Parcel L, Lots 9 and 10, Block 664; and Lot L, Block 664, Plan No. 69S08033		<b>CIVIC ADDRESS</b> Richardson Road, Lehrer Manor, Lehrer Crescent, and 37 <sup>th</sup> Street West
		<b>NEIGHBOURHOOD</b> Hampton Village
<b>DATE</b> July 16, 2012	<b>APPLICANT</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7	<b>OWNER</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7

### LOCATION PLAN



**PROPOSED REZONING**

From R1A to RMTN —

From R1A to RM3 —

From AG to R1A —

From R1A to R1B —

File No. RZ11-2012

**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

That a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendment to the Zoning Bylaw No. 8770 to rezone the land parcels located on Richardson Road, Lehrer Manor, Lehrer Crescent, and a utility parcel (see Attachment 2) from R1A – One-Unit Residential District to RM3 – Medium Density Multiple-Unit Dwelling District, RMTN – Townhouse Residential District, and R1B – Small Lot One-Unit Residential District; and AG – Agricultural District to R1A – One-Unit Residential District, be approved based on the reasons outlined in this report.

**B. PROPOSAL**

The Planning and Development Branch has received an application from the City of Saskatoon, Land Branch requesting that the land parcels located on Richardson Road, Lehrer Manor, Lehrer Crescent, and a utility parcel (see Attachment 2) be rezoned as follows:

- 1) R1A – One-Unit Residential District to RM3 – Medium Density Multiple-Unit Dwelling District - this property (Parcel AA) is located on the northern corner of Richardson Road and McClocklin Road;
- 2) R1A – One-Unit Residential District to RMTN – Townhouse Residential District - these properties (Parcels BB and CC) are located adjacent to Richardson Road in the south east corner of the subject area;
- 3) R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District - these properties (Block 24) are surrounded by Lehrer Manor in the south west portion of the subject area; and
- 4) AG – Agricultural District to R1A – One-Unit Residential District - this property is located at the very outset of the south east corner of the subject area, adjacent to the

termination point of 37<sup>th</sup> Street West.

The remaining residential lots shown on the attached Location Plan of Proposed Subdivision (see Attachment 2) will retain the current R1A zoning designation.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed rezoning will permit the development of the aforementioned lands in a manner which is consistent with the Hampton Village Neighbourhood Concept Plan (Concept Plan).

**D. BACKGROUND INFORMATION**

The subject sites are currently zoned AG and R1A and are greenfield properties located within the most easterly section of Hampton Village, which is in the final phase of neighbourhood development. The Concept Plan for Hampton Village was approved by City Council in 2004 and provides a wide range of housing options, which included the RM3 and RMTN properties identified in the administrative report. In October 2011, City Council approved a minor Concept Plan Amendment that moved the R1B – Small Lot Residential area to the parcel bound by Lehrer Manor. In addition to this, there was a reconfiguration of the utility parcel (dry pond) that did not affect the overall size of that parcel. At the time leading up to the public hearing, public notice was sent to the Hampton Village Community Association and a notice was advertised in The StarPhoenix.

In order to accommodate future development, the proposed Zoning Bylaw No. 8770 amendments will change the zoning designations for the specified areas of the neighbourhood to permit residential development.

**E. JUSTIFICATION**

1. Community Services Department Comments

a) Planning and Development Branch

The proposed rezoning is in compliance with the approved Concept Plan and will accommodate a diversity of housing types in the Hampton Village neighbourhood. Future development on this site will comply with the development standards identified in the R1A, RM3, RMTN, and R1B Zoning Districts. As such, the Planning and Development Branch has no concerns regarding the proposed rezoning of the identified properties.

2. Comments by Others

a) Infrastructure Services Department

The proposed Zoning Bylaw No. 8770 amendment is acceptable to the Infrastructure Services Department.

Please note that the properties zoned RMTN and RM3 may require a Traffic Impact Study upon development of the parcels.

b) Utility Services Department, Transit Services Branch

At present, the Transit Branch's closest bus stop is approximately 450 metres from the above referenced properties, located on the east side of East Hampton Boulevard and north of McClocklin Road.

Bus service is at 30 minute intervals Monday to Saturday and at 60 minute intervals after 18:00 Monday to Friday, early Saturday mornings, Sundays, and statutory holidays.

**F. COMMUNICATION PLAN**

Once this application has been considered by the Municipal Planning Commission, it will be advertised in accordance with Public Notice Policy No. C01-021, and a date for a public hearing will be set. The property owners affected by this rezoning will be notified in writing. The Planning and Development Branch will also notify the Community Consultant and the Ward Councillor of the public hearing date by letter. A notice will be placed in The StarPhoenix two weeks prior to the public hearing. Notice boards will also be placed on the site.

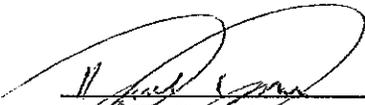
**G. ENVIRONMENTAL IMPLICATIONS**

There are no environmental and/or greenhouse gas implications.

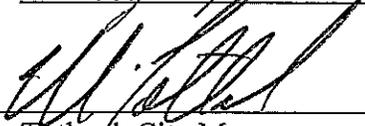
**H. ATTACHMENTS**

1. Fact Summary Sheet
2. Location Plan

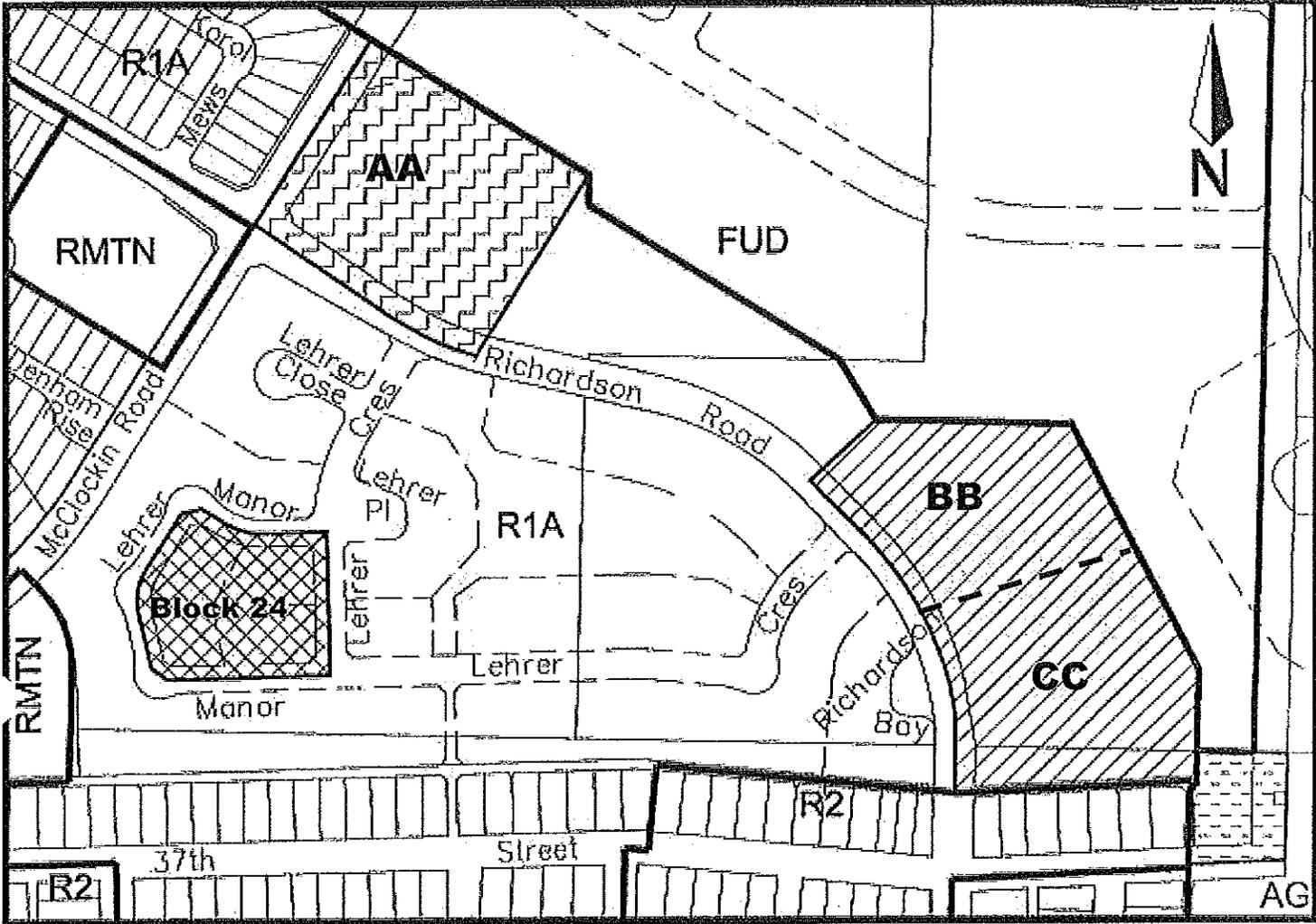
Written by: Daniel Gray, Planner 16

Reviewed by:  For  
Alan Wallace, Manager  
Planning and Development Branch

Approved by:   
Randy Grauer, General Manager  
Community Services Department  
Dated: July 31, 2012

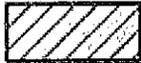
Approved by:   
Murray Totland, City Manager  
Dated: Aug 11/12

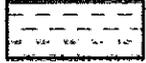
<b>FACT SUMMARY SHEET</b>	
<b>A. Location Facts</b>	
1. Municipal Address	Richardson Road, Lehrer Manor, Lehrer Crescent, and Right-of-Way Parcel
2. Legal Description	Parcels AA, BB, CC, Lots 1 to 16, Block 24; and Parcel L, Lots 9 and 10, Block 664; and Lot L, Block 664, Plan No. 69S08033
3. Neighbourhood	Hampton Village
4. Ward	4
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Residential – R1A
2. Proposed Use of Property	Residential – RM3, RMTN, and R1B
3. Adjacent Land Uses and Zoning	Residential and FUD
North – Aerogreen Neighbourhood	Future Urban Development - FUD
South – Westview Neighbourhood	Residential – R1A and R2
East – Aerogreen Neighbourhood	Future Urban Development - FUD
West – Hampton Village Neighbourhood	Residential – R1A
4. No. of Existing Off-Street Parking Spaces	N/A
5. No. of Off-Street Parking Spaces Required	N/A
6. No. of Off-Street Parking Spaces Provided	N/A
7. Site Frontage	N/A
8. Site Area	N/A
9. Street Classification	Richardson Road – Major Collector Richardson Bay – Local Street Lehrer Crescent – Local Street Lehrer Manor – Local Street
<b>C. Official Community Plan Policy</b>	
1. Existing Official Community Plan Designation	Residential
2. Proposed Official Community Plan Designation	Residential
3. Existing Zoning District	R1A
4. Proposed Zoning District	RM3, RMTN, and R1B



**PROPOSED REZONING**



From R1A to RMTN —   
 From R1A to RM3 — 

From AG to R1A —   
 From R1A to R1B — 

File No. RZ11-2012

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z14/12	<b>PROPOSAL</b> Proposed New Commercial Zoning District – B5B Broadway Proposed New Overlay Zoning District – AC2 – B5B Architectural Control Overlay Zoning District	<b>EXISTING ZONING</b>
<b>LEGAL DESCRIPTION</b>	<b>CIVIC ADDRESS</b>	
	<b>NEIGHBOURHOOD</b>	
<b>DATE</b> July 17, 2012	<b>APPLICANT</b> City of Saskatoon Planning and Development Branch 222 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 0J5	<b>OWNER</b>

### LOCATION PLAN

A. **COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

that a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising respecting the proposal to amend Sections 13.1.3 of Zoning Bylaw No. 8770 as outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation to amend Zoning Bylaw No. 8770 to add the proposed B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District, and that parking and sign regulations contained in Zoning Bylaw No. 8770 are amended to reflect the addition of the B5B Zoning District.

B. **PROPOSAL**

The Planning and Development Branch is requesting that Zoning Bylaw No. 8770 be amended to add the B5B Broadway Commercial Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District. The addition of the B5B Zoning District also requires amendments to the sign and parking regulations contained in Section 6.0 and Appendix A of Zoning Bylaw No. 8770 (see Attachments 2 and 3).

C. **REASON FOR PROPOSAL (By Applicant)**

The proposed commercial B5B Zoning District is intended to be applied in the Broadway Avenue commercial area. This district will provide development standards to ensure that new development enhances the existing urban environment along this unique commercial corridor.

The proposed Architectural Control Overlay Zoning District is also intended to be applied in the Broadway Avenue commercial area. This Architectural Control Overlay Zoning District will ensure that new buildings reinforce and enhance the best qualities of the Broadway commercial area.

The B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District is intended to be applied to the Broadway area, which is currently zoned B5, as indicated on Attachment 1. The rezoning of these properties is covered under a separate report.

**D. BACKGROUND INFORMATION**

In 2007, the Broadway Business Improvement District (BID) and the Nutana Community Association proposed that a study of the Broadway commercial area be undertaken. It was noted by both groups that some of the recommendations contained in the 2001 Nutana Local Area Plan (LAP) had become outdated, other recommendations appeared to be difficult to implement due to community or property owner resistance, and there were concerns about the future character of Broadway Avenue. The City of Saskatoon (City) was invited to participate as a partner with the Broadway BID and Nutana Community Association in the development of a "Broadway Area Plan" to address outstanding recommendations in the Nutana LAP.

The Planning Partnership, a Toronto-based urban planning consultant firm, was hired to prepare what would become the Broadway 360 Development Plan (Plan). The Plan involved a thorough public consultation process that engaged residents, business and commercial property owners, area schools, churches, and those representing Saskatoon's heritage community.

The Plan explored practical urban development solutions to address land use, street character, safety, parking, and traffic issues in the Broadway area. During its September 28, 2009 meeting, City Council received the Plan.

A Steering Committee that was struck during the creation of the Plan was comprised of developers, commercial property owners, Nutana residents, the Nutana Community Association, the BID, and the Ward Councillor. Following City Council receiving the Plan, the Steering Committee met and prioritized the recommendations. The items that were identified as having the highest priority were those concerning land use. These included a new zoning district that would put greater controls on form and massing of building and implementation of an architectural control district. There was initially a lack of consensus within the Steering Committee regarding these proposed development standards.

The Administration worked very closely with the Steering Committee over several months to prepare detailed zoning requirements that would be acceptable to the various stakeholders. The Administration and the Steering Committee met 13 times over the course of 20 months to prepare these proposed Zoning Bylaw No. 8770 amendments.

During its April 16, 2012 meeting, City Council approved an implementation strategy for the Plan. This implementation plan indicated that two tasks, adoption of the recommended development standards and consideration of an architectural control district, would be undertaken in 2012.

E. JUSTIFICATION

1. Community Services Department Comments

a) Official Community Plan Bylaw No. 8769

This area is designated as “Special Area Commercial” on the Nutana Land Use map contained in Official Community Plan Bylaw No. 8769. The proposal to apply a new zoning treatment and architectural control district in the Broadway Avenue commercial area is consistent with the objectives and policies in related to Special Area Commercial Areas, as stated in Section 6.4 of the Official Community Plan:

“Historic Commercial Areas

The Special Area Commercial designation has been applied to certain commercial lands along 20<sup>th</sup> Street, 33<sup>rd</sup> Street, Central Avenue and Broadway Avenue, primarily due to their long and unique development history. In general, these areas contain a built form that is oriented to pedestrians, with limited front or side yard setbacks, and with a relatively high density of development. As a consequence, the Zoning Bylaw shall prescribe development standards for these areas which reflect their unique character, while also promoting compatibility with surrounding residential land use.

Specific local area plans or design studies may also be undertaken in these areas to further define future land use patterns and design and development standards.”

b) Planning and Development Branch Comments

i) Proposed B5B Zoning District

The purpose of the B5B district is to recognize the historic Broadway Avenue commercial area and to facilitate mixed-use developments, including a range of commercial, institutional, and medium to high density residential (see Attachment 2). The following table summarizes the differences between the existing B5 and the proposed B5B Zoning District.

B5 Zoning District (Current Regulations)	PROPOSED B5B ZONING DISTRICT
<b>PERMITTED USES</b>	
There are a wide range of uses permitted in the B5 district. This district permits commercial, institutional (office), and residential uses.	The same uses as permitted in B5, with the following vehicle-oriented uses being deleted: a) service stations b) car washes c) motor vehicle, marine, and trailer coach sales establishments d) public garages
<b>SETBACK REQUIREMENTS</b>	
<b>Front Yard Setback – Building Base:</b> a) no minimum for most uses b) 3 metres for vehicle-oriented use c) 6 metres for multiple-unit dwellings	a) no minimum b) maximum setback – A minimum of 70 percent of the aggregate width of the front building line shall be located within one metre of the front property line
<b>Front Yard Setback – Building Cap:</b> a) no current standard	a) Minimum setback of 3 metres up to 3 storeys above the building base and 0.6 metres for every storey above 3, to a maximum setback of 6 metres
<b>Side Yard Setback – Building Base:</b> a) zero, with exceptions	a) no minimum; however, where a B5B District abuts an R District without the intervention of a street or lane, an abutting side yard shall be provided of at least 1.5 metres
<b>Side Yard Setback – Building Cap:</b> a) no current standard	a) the side yard setback shall be increased by 0.3 metres for each additional storey above the building base, to a maximum of 3 metres
<b>Rear Yard Setback – Building Base:</b> a) zero, with exceptions	a) no minimum; however, where a B5B District abuts an R District, a rear yard shall be provided of at least 3.0 metres
<b>Rear Yard Setback – Building Cap:</b> a) no current standard	a) no minimum; however, where a B5B District abuts an R District, the rear yard setback shall be increased by 0.6 metres for each additional storey above the building base to a maximum of 7.5 metres
<b>BUILDING HEIGHT</b>	
<b>Building Base:</b> a) 76 metres for entire building	a) shall be a minimum of 7.5 metres and a maximum of 12 metres
<b>Building Cap:</b> a) 76 metres for entire building	a) no maximum height limit, however, other factors govern height
<b>GROSS FLOOR AREA RATIO</b>	
a) 5:1 – site width less than 15 metres b) 7:1 – site width between 15 metres and 30 metres c) 10:1 – site width greater than 30 metres	a) the gross floor area ratio shall not exceed 7:1

PARKING REQUIREMENTS FOR MULTIPLE-UNIT DWELLINGS	
a) 1.25 per dwelling unit plus 0.125 visitor spaces	a) parking for multiple-unit dwellings shall be provided at the rate of 1.0 space per dwelling unit, plus 0.125 visitor spaces per dwelling unit
b) units smaller than 50 m <sup>2</sup> – 1.0 spaces per unit	

The intent of the B5B District is to ensure that buildings have an identifiable base and cap. The base of the building would have minimal setbacks while the building cap setback would provide appropriate sunlight penetration and ensure that development has an appropriate pedestrian scale.

The proposed B5B Zoning District does not contain a maximum height requirement. The development standards contained in the B5B District utilize tools that will limit the maximum building height. Those factors that have an effect on height include: gross floor area ratio of 7:1, site size, height of base building (must be a minimum 7.5 metres to a maximum of 12 metres), setback and stepbacks, and parking (both the amount of required parking and where it is located on the site).

Amendments are also required to the signage and parking requirements contained in Zoning Bylaw No. 8770 to reflect the addition of the B5B Zoning District. The regulations for parking and signage will be the same as those currently applied in the B5 and B6 Districts.

ii) Proposed AC2 – B5B Architectural Control Overlay District

It is recommended that an Architectural Control District (ACD), including specific Architectural Control Guidelines, be adopted in conjunction with the above noted B5B District (see Attachment 3).

The ACD will provide direction to ensure high quality architectural design for new construction in the Broadway commercial area. The ACD is intended to allow for flexibility and foster creativity in building design.

It is proposed that the ACD will be applied to the construction of all new buildings and where City funding is being requested under programs, such as the Heritage Conservation Program, the Facade Renovation and Rehabilitation Program, or the Affordable Housing Program.

Property owners and developers will be encouraged to follow the guidelines in any other cases.

As noted in Appendix 3, the ACD will contain 14 design guidelines that address the following: building expression, orientation and placement, street wall, heritage contexts, corner sites, storefronts, residential street access units, roof treatment, above-grade parking, material and architectural quality, sidewalk cafes, building lighting, signage, and sustainable design.

The review process for the ACD will be administered by the Planning and Development Branch, Community Services Department, in the same manner as currently undertaken for development projects in River Landing. A development review committee consisting of design professionals (community planners, landscape architects, and other architects) will review each application. Approvals under the ACD are proposed to be delegated to the Administration.

iii) Compatibility with Adjacent Land Uses

These new development standards should reinforce the best qualities that characterize the Broadway Area and ensure an appropriate interface with existing heritage resources and the adjacent low-rise residential neighbourhood. The new B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District are intended ensure that all new development adheres to the development principles contained in the Plan.

4. Comments by Others

a) Infrastructure Services Department

1. The allowed land uses within the proposed zoning district vary widely in capacity use from a water and sewer perspective. High density/high capacity, such as hotels and multi-story residential, may significantly affect water and sewer concerns with respect to fire flows and sanitary sewer capacity. The wide variation makes it very difficult to determine if any water and sewer conditions exist. Storm sewer capacity is not a concern.
2. With varied land uses, it is possible for the first high density new land development (i.e. a multi-story residential or hotel) to effectively consume all the available sanitary sewer capacity in the district, thereby “sterilizing” the area for other high density uses.

Since zoning is the only control for regulating land use it would be prudent to either determine a method of regulating high density uses for the zoning district or determine a levy payment method so that the first user does not benefit from “free” existing capacity while future users must pay for all additional upgrades to water and sewer infrastructure.

Note: The proposed B5B Zoning District will be applied in the area currently zoned B5 in the Broadway Commercial Area. The B5B District limits the maximum development potential in this area compared to what is currently permitted in the B5 District.

The Integrated Growth Plan will be addressing issues related to the financing of infrastructure upgrades necessary to accommodate infill development.

b) Transit Services Branch, Utilities Services Department

At present, Saskatoon Transit has bus stops throughout the district. Service is at 15 minute intervals Monday to Saturday; at 30 minute intervals evenings, early Saturday mornings, Sundays, and statutory holidays.

F. ENVIRONMENTAL IMPLICATIONS

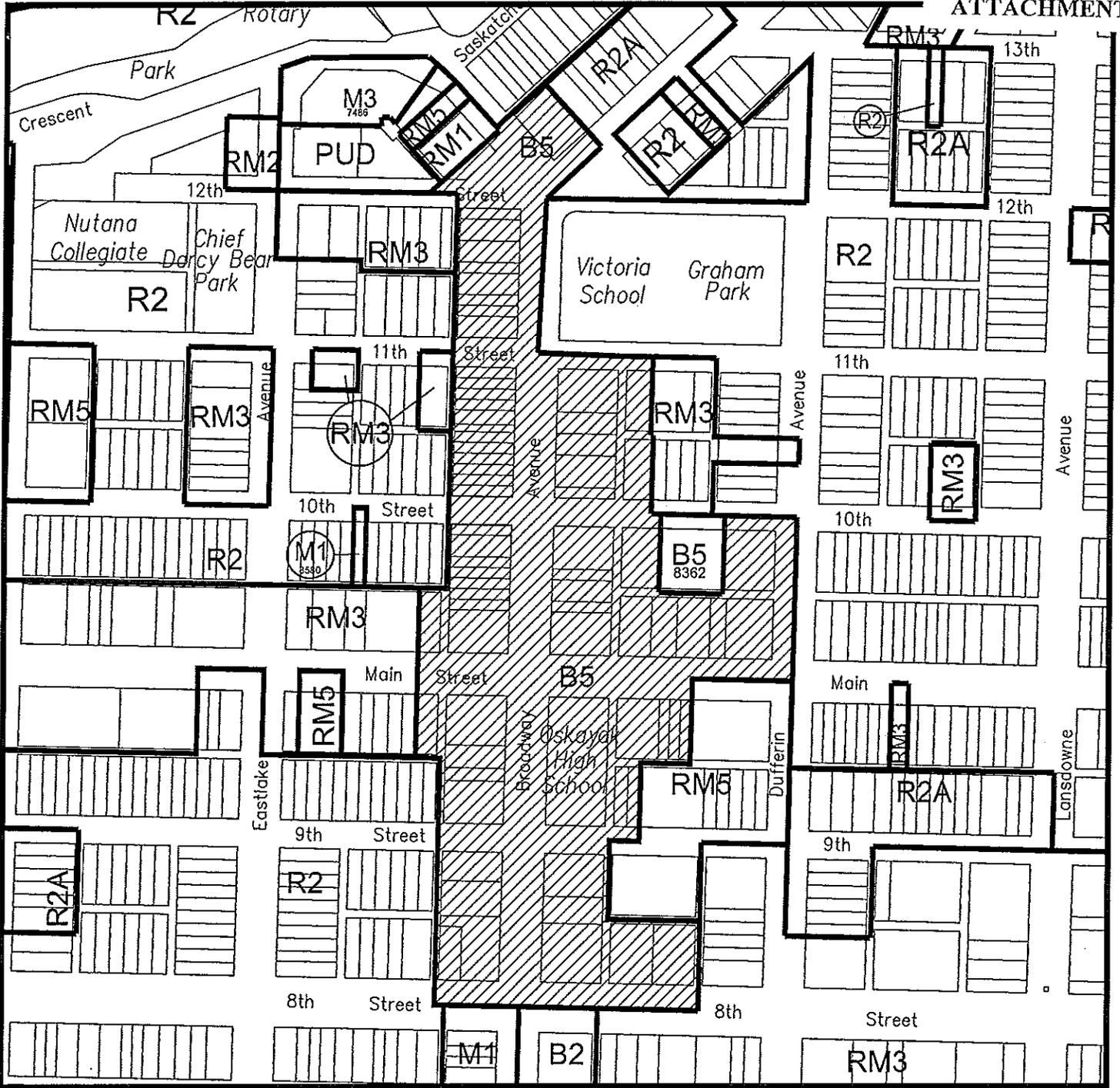
There are no environmental implications and/or greenhouse gas implications.

G. COMMUNICATION PLAN

As noted above, this Plan has been undertaken as a joint initiative of the Broadway BID, the Nutana Community Association, and the City. Extensive public consultation was undertaken throughout the Broadway 360 study process.

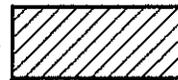
In addition, a public open house was held on January 12, 2012, at the Cosmopolitan Senior Citizens Centre on 10<sup>th</sup> Street in Nutana. A presentation by your Administration was followed by a question and answer period. A technical workshop was also held on January 14, 2012. Notices for these consultations were distributed throughout the area by flyer drop, in the Nutana Community Association newsletter, and by direct mail to Broadway commercial property owners and business owners.





# REZONING

From B5 to B5B and B5B(ACD2)



## X.X. B5B - Broadway Commercial District

### X.X.1 Purpose

The purpose of the B5B District is to recognize the historic Broadway Commercial area and facilitate mixed use development including a range of commercial, institutional and residential uses in medium to high density form.

### X.X.2 Permitted Uses

The Permitted Uses and Minimum Development Standards in a B5B District are set out in the following chart:

B5B District	Minimum Development Standards (in Metres)						
	Site Width	Site Area (m <sup>2</sup> )	Front Yard	Side Yard	Rear Yard	Building Height (Max.)	Amenity Space Per Unit (m <sup>2</sup> )
<b>10.8.2 Permitted Uses</b>							
(1) Hotels and motels	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(2) Restaurants and lounges	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(3) Bakeries	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(4) Dry cleaners	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(5) Theatres	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(6) Commercial recreation uses	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(7) Photography studios	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(8) Retail stores	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(9) Personal service trades and health clubs	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(10) Offices and office buildings	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(11) Medical clinics	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(12) Medical, dental and optical laboratories	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(13) Financial institutions	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(14) Private schools and educational institutions	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(15) Repair services restricted to the repair of household goods and appliances	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(16) Places of worship	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(17) Public halls and community centres	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(18) Private clubs	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(19) Libraries, art galleries and museums	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(20) Funeral homes	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-

B5B District	Minimum Development Standards (in Metres)						
	Site Width	Site Area (m <sup>2</sup> )	Front Yard	Side Yard	Rear Yard	Building Height (Max.)	Amenity Space Per Unit (m <sup>2</sup> )
<b>10.8.2 Permitted Uses (continued)</b>							
(21) Radio or television studios	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(22) Motion picture or recording studios	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(23) Duplicating or copying centres	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(24) Dwelling units in conjunction with and attached to any other permitted use	-	-	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(25) Multiple-unit dwellings	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(26) Commercial parking lots	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(27) Storage garages	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(28) Banquet halls	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(29) Catering halls and catering kitchens	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(30) Neighbourhood recycling and collection depots	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(31) Parking stations	7.5	225	0 <sub>1</sub>	Refer to Section 6.0.			
(32) Shopping centres	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(33) Veterinary clinics	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(34) Commercial printers	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(35) Public elementary & high schools	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(36) Boarding apartments	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(37) Boarding houses	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(38) Custodial care facilities - Type I & II	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(39) Small animal grooming	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(40) Accessory buildings and uses	-	-	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-

### X.X. 3 Discretionary Uses

The Discretionary Uses and Minimum Development Standards in a B5B District are set out in the following chart:

B5B District	Minimum Development Standards (in Metres)						
	Site Width	Site Area (m <sup>2</sup> )	Front Yard	Side Yard	Rear Yard	Building Height (Max.)	Amenity Space Per Unit (m <sup>2</sup> )
<b>10.8.3 Discretionary Uses</b>							
(1) Special needs housing	15	560	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(2) Child care centres and pre-schools	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(3) Adult day care centres - Type I & II	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(4) Custodial care facilities - Type III	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(5) Night clubs and taverns	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-

#### **X.X. 4 Notes to Development Standards**

1. (a) *Building Base:* a minimum of 70% of the aggregate width of the front building line shall be located within 1.0 metre of the front property line.
- (b) *Building Cap:* a minimum front yard shall be provided of 3.0 metres from the front property line up to three storeys from the front property line shall be provided for every storey above the three storey building cap, however, the minimum setback of the building cap shall not exceed 6.0 metres from the front property line.
2. (a) *Building Base:* where a B5B District abuts an R District without the intervention of a street or lane, an abutting side yard shall be provided of at least 1.5 metres.
- (b) *Building Cap:* setback increased by 0.3 metres for each additional story above the building base to a maximum of 3.0 metres.
3. (a) *Building Base:* where a B5B District abuts an R District, a rear yard shall be provided of at least 3.0 metres.
- (b) *Building Cap:* where a B5B District abuts an R District, the rear setback shall be increased by 0.6 metres for each additional story above the building base to a maximum of 7.5 metres.
4. *Building Base:* shall be a minimum of 7.5 metres and a maximum of 12.0 metres. Exceptions may be made for corner sites where the architectural feature is included that may encourage massing and designs that accentuate the visual prominence of the site.

#### **X.X.5 Signs**

The regulations governing signs in a B5B District are contained in **Appendix A - Sign Regulations**.

#### **X.X.6 Parking**

The regulations governing parking and loading in a B5B District are contained in **Section 6.0**.

### **X.X.7 Gross Floor Space Ratio**

- (1) The gross floor space ratio shall not exceed 7:1.
- (2) In the B5B District, above grade parking floor areas shall not be exempt from the calculation of the gross floor space ratio.
- (3) Notwithstanding Section (2), above grade parking floor areas which are needed to provide 1.25 parking spaces per dwelling unit and parking floor areas which are needed to provide parking at the rate of one parking space for every 24 square metres of gross leasable floor area for all other uses shall be exempt from the calculation of Gross Floor Space Ratio.
- (4) Floor areas used for below grade parking shall be deducted from the exemption outlined in Section (3), at the rate outlined in said Section.

### **X.X.8 Landscaping**

- (1) On sites used for, commercial parking lots, parking stations or multiple-unit dwellings a landscaped strip of not less than 3 metres in depth throughout lying parallel to and abutting the front site line shall be provided and shall be used for no purpose except landscaping and necessary driveway access to the site, and on corner lots, in addition to the landscaping required in the front yard, a landscaped strip of not less than 1.5 metres in width throughout lying parallel to the flanking street shall be provided.

In addition, on sites used for commercial parking lots or parking stations located at grade level, screening of the site from front streets, flanking streets and public lanes shall be provided to the satisfaction of the Development Officer.

### **X.X.9 Garbage Pickup Area**

- (1) A space to be used exclusively for garbage storage and pickup, having minimum dimensions of 2.7 metres by 6.0 metres, shall be provided on each site to the satisfaction of the General Manager, Utility Services Department. The required loading and pick up spaces may be combined where considered appropriate by the Development Officer.

## Theme

# *Broadway*

# *Heritage*

# *Core*

“The Broadway area comprises the ‘heart’, ‘town centre’, and ‘main street’ of the Nutana community. Established as a temperance colony in the late 19th century, Nutana is one of Saskatoon’s most desirable neighbourhoods, due in large part to its historic ‘small town’ charm, the success of Broadway Avenue commercial area, and the proximity to the Downtown and the University of Saskatchewan.

Neighbourhoods are constantly changing and evolving, and Broadway Avenue and the Nutana Neighbourhood is no exception. At the heart of Saskatoon’s original neighbourhood, the Broadway area has been the social and commercial core for over 125 years. In more recent years, the area’s central location, and its unique history can character, and the distinct mix of restaurants and stores has attracted new residents and visitors to the neighbourhood, contributing to the revitalization that has made Broadway Avenue a region-wide destination.”

Notwithstanding the intent of this document to control aspects of architectural development in the Broadway Commercial Area, the controls or guidelines provide direction for the quality of design for a variety of built form elements. As a means for ensuring that new buildings reinforce and enhance the best qualities of the Broadway area. Therefore the projects must support the theme of “Broadway Heritage Core “.

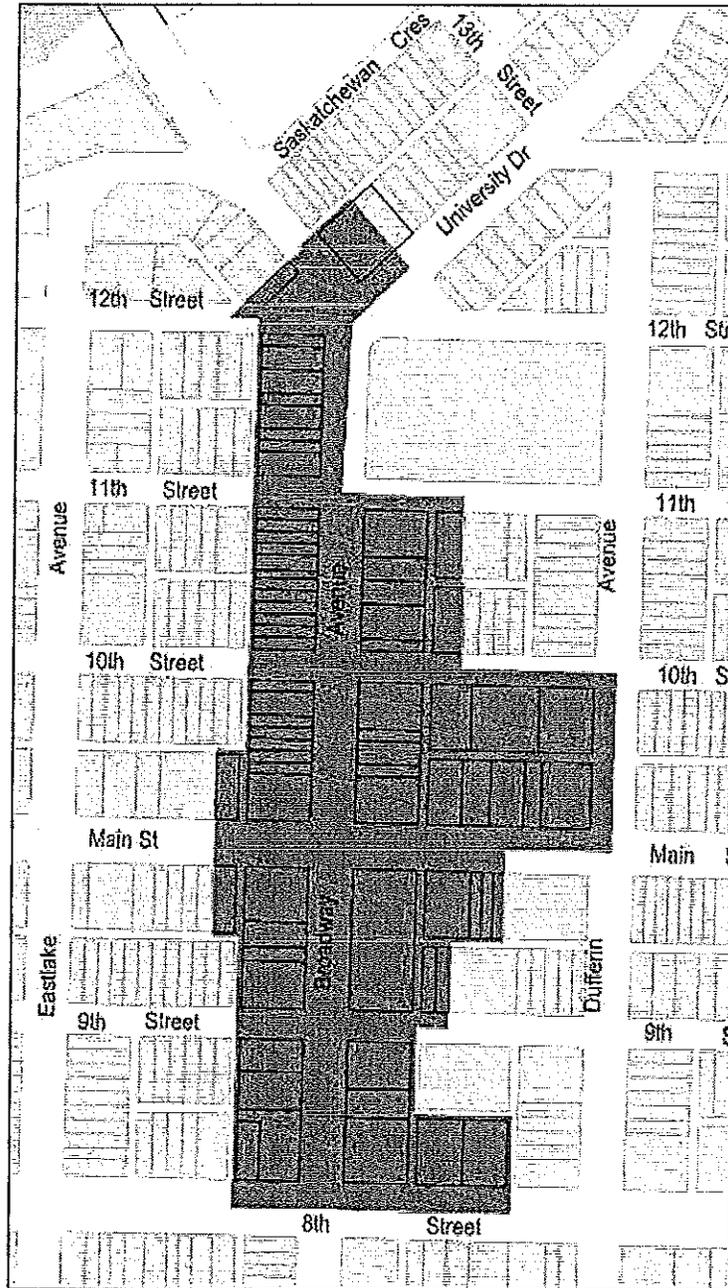
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## Development Principles

A central objective is to ensure that new buildings reinforce a coherent, harmonious and appealing urban environment, as well as contribute to the enhancement of the public realm. Informed by the consultation process and rooted in good planning and urban design practice, the key principles include:

- ❖ Protecting heritage resources and retaining their visual prominence.
- ❖ Protecting and strengthening established low-rise residential areas and ensuring compatible infill development and sympathetic developments in adjacent higher density areas.
- ❖ Ensuring base building conditions that form an appropriately scaled and designed street wall that reinforces the desired character at the street level.
- ❖ Ensuring appropriate building massing and height taking into consideration existing and permitted heights; proportional relationships to streets; and, visual and physical impacts on pedestrians and adjacent areas.
- ❖ Ensuring that new developments provide for appropriate transitions between areas of differing intensities and scales.
- ❖ Reinforcing important intersections and corners through massing and design.
- ❖ Well designed and articulated buildings that positively contribute to the quality and animation of the streetscape and the overall defining character and image of the area.

# Map of B5B Area



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# 1

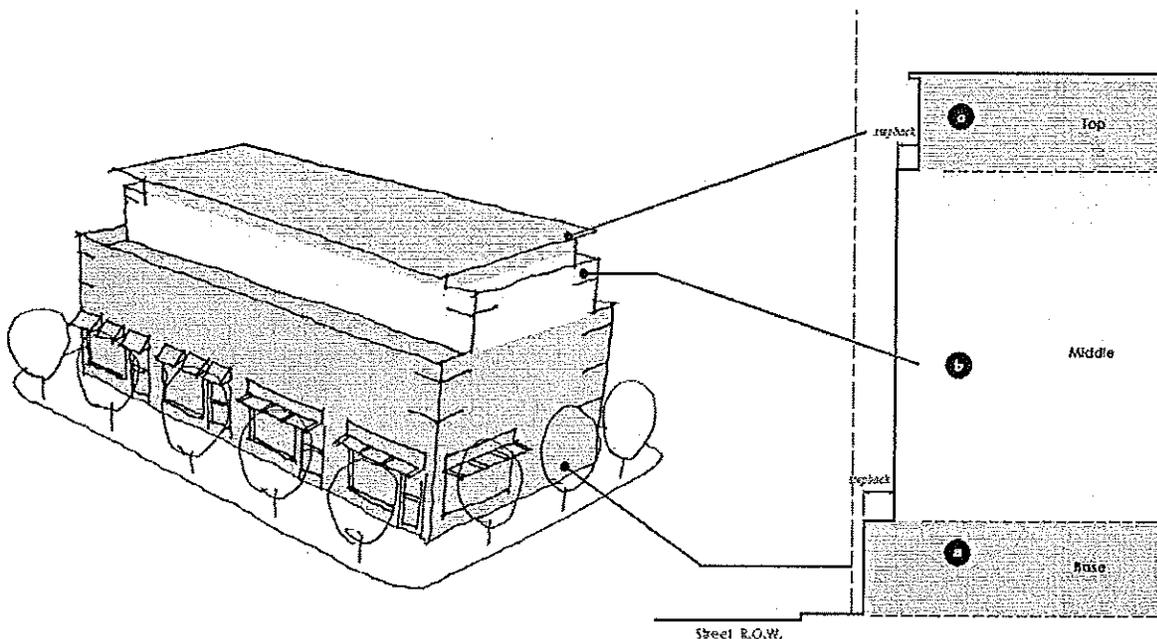
## Building Expression

Buildings should reinforce a *base, middle, and top* in their design.

**Base** – Within the first three storeys of a building, a clearly defined base will contribute to the quality of the pedestrian environment by providing animation, transparency, and articulation.

**Middle** – The body of the building should contribute to the physical and visual quality of the overall streetscape.

**Top** – The roof should be distinguished from the rest of the building and designed to contribute to the visual quality of the streetscape.



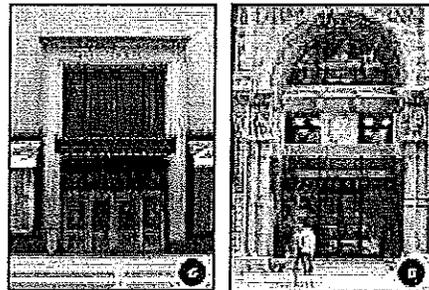
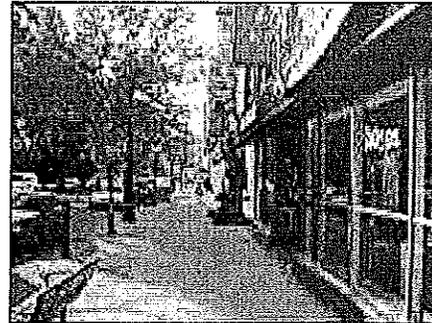
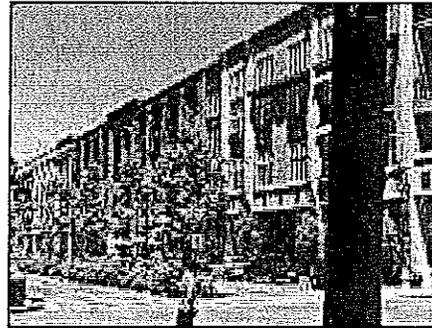
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## 2

### Orientation and Placement

Buildings can enhance the pedestrian environment by creating a sense of enclosure. This is achieved by framing the street with parallel aligned buildings and providing the appropriate levels of animation and use.

- ❖ All buildings should orient to the street with clearly defined entry points that directly access the sidewalk.
- ❖ A minimum of 70% of the front building line shall be located within 1.0 metre of the front property line.



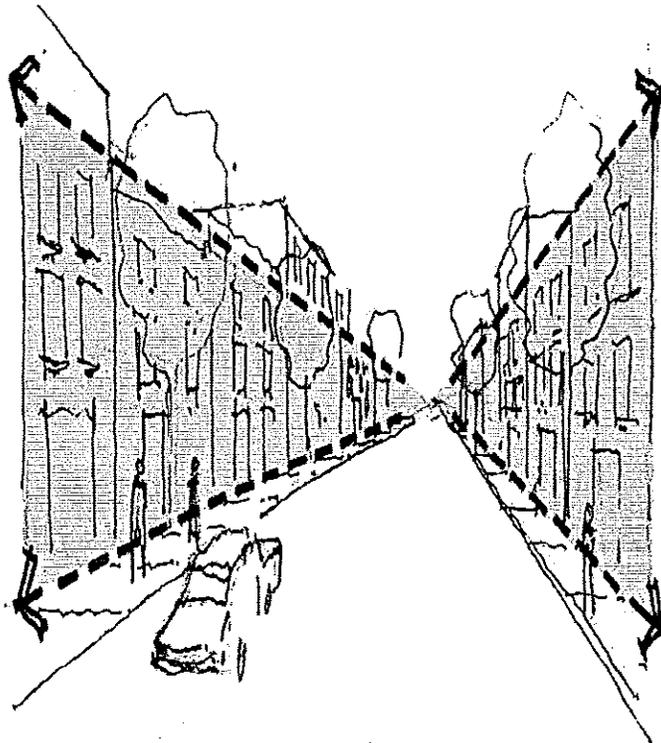
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### 3

## Street Wall

The street wall has the greatest impact on the character of the street experience. The key design objectives for street walls in the Broadway Area ensure visual continuity, pedestrian scale, animation and design quality.

- ❖ A street wall of a new building should align with those of neighbouring buildings or have the same setback as the predominant buildings on the block.
- ❖ The height of the street wall should be consistent with historic heights of no greater than 3 storeys and no less than 2 storeys. Levels above the street wall should be set back to reinforce a low-rise interface with the sidewalk.
- ❖ The height of ground-level floors should be visually prominent and no less than 4.5 metres for commercial and 4.0 metres for residential uses.



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## 4

### Heritage Contexts

New buildings on Broadway Avenue should complement, rather than detract from, the character of older buildings.

#### General Guidelines

New buildings should avoid historical misrepresentation by not replicating past architectural styles.

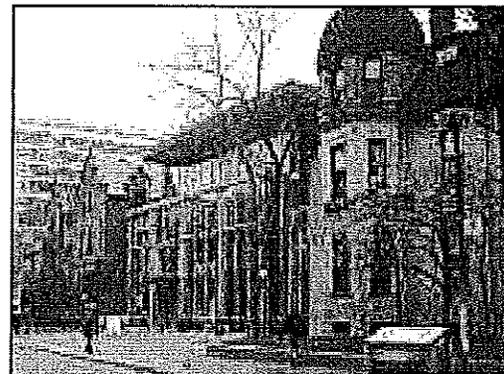
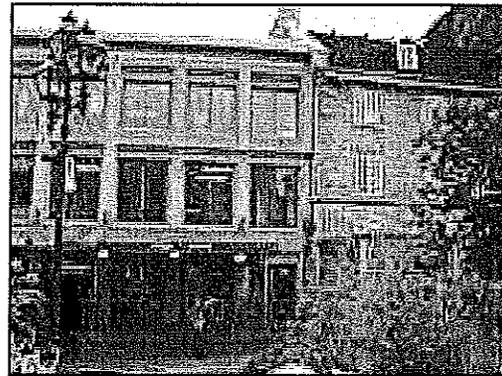
New buildings should consider and respect the scale, material and massing of adjacent heritage significant buildings.

#### Façade Articulation

New buildings should respect the pattern of façade division by ensuring the horizontal and vertical architectural orders, including windows and entries, are aligned with neighbouring heritage buildings or the established pattern on the block.

#### Façade Materials

New buildings should consider materials and colours evident in existing heritage significant properties. Building materials should be chosen for their functional and aesthetic quality. Exterior finishes should exhibit quality of workmanship, sustainability, permanence, and ease of maintenance.

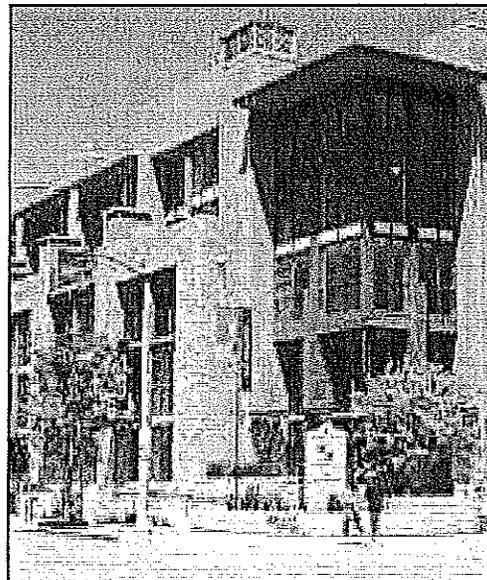


# 5

## Corner Sites

Corner buildings have a greater visual prominence given that they front onto two streets and frame intersections. To enhance the distinction of new buildings at Key Corner Sites, modest exceptions to setbacks and height restrictions should be permitted to encourage massing and designs that accentuate the visual prominence of the site.

- ❖ New developments on all corner sites should orient to both street frontages.
- ❖ Corner entrances should be encouraged wherever possible to address the two street frontages.



## 6

### Storefronts

Well proportioned and designed storefronts can provide animation and visual interest at the sidewalk.

- ❖ To reflect the existing character and context, storefronts should generally have a frontage in the range of 7.5 metres but not greater than 15 metres.
- ❖ Where frontages are greater than 7.5 metres, they should articulate narrow storefronts in the design of the facade.
- ❖ Storefronts should have a minimum of 75% glazing to maximize visual animation.
  - Clear glass should be used for windows and doors along the street-level façade. Dark tinted, reflective or opaque glazing should be discouraged for storefronts.
- ❖ Storefront entrances should be highly visible and clearly articulated. Entrances should be located at or near grade.
- ❖ Storefront signage should be consistent with the signage guidelines, but add diversity and interest to the street.
- ❖ Weather protection for pedestrians is encouraged through the use of awnings and canopies.



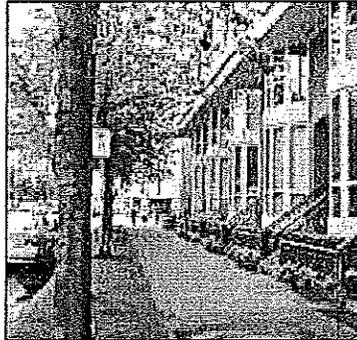
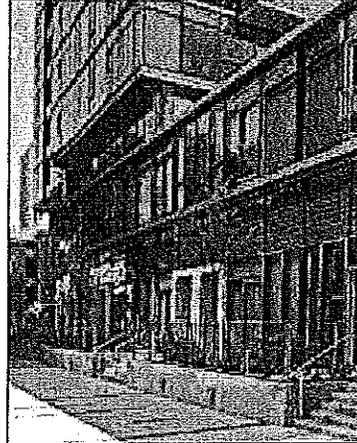
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# 7

## Residential Street Access Units

Where retail is not required, and residential uses are proposed at-grade, the following guidelines apply:

- ❖ Residential uses at-grade should include individual units accessed from the street.
- ❖ Appropriate front yard privacy measures should be considered such as setbacks, landscaping, and porches.
- ❖ Access to the individual units should be clearly visible, and the scale, rhythm and articulation of the street wall should be consistent with the residential character of adjacent neighbourhoods. Grade-level units should be designed to accommodate live-work opportunities and potential conversion into commercial or retail uses.

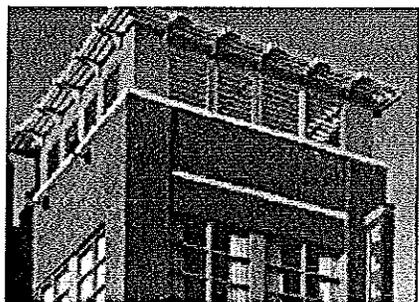
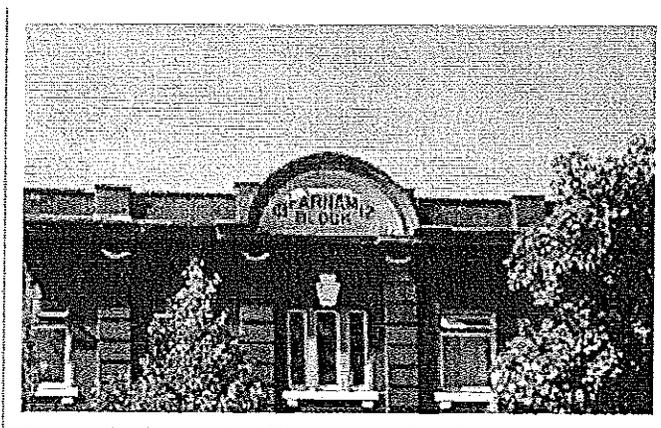
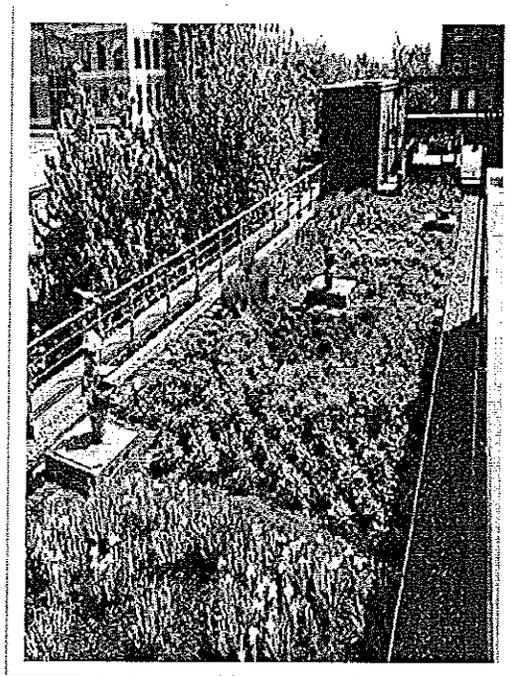


# 8

## Roof Treatment

The design of the roof can make an impact on the character of the streetscape, especially from great distances. Roofs are also seen from other buildings of equal or greater height.

- ❖ The expression of the building top and roof should be clearly distinguished from the rest of the building through treatments such as stepbacks, change in materials, cornices lines, and overhangs.
- ❖ Mechanical penthouses should be integrated with the architectural treatment of roofs and/or screened from view.
- ❖ Green roofs should be encouraged.



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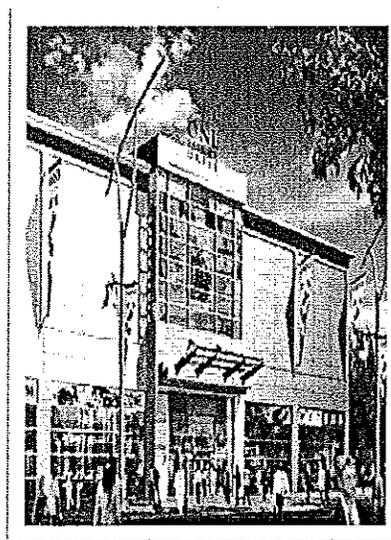
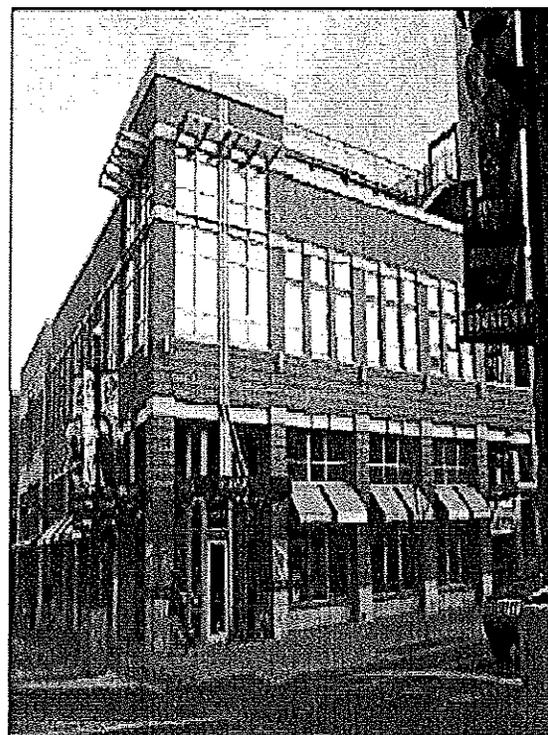
## AC2 – B5B – Architectural Control Overlay District

### 9

#### Above Grade Parking

Wherever possible, parking for new developments should be provided at the rear or below-grade and accessed off the rear lane. However, where parking is provided above-grade within the base building, the following guidelines address the design and quality of such structures.

- ❖ Direct access for parking, loading, and service areas from the street should be discouraged.
- ❖ Where an above-grade parking facility fronts on a street, the ground-level frontage should incorporate retail, public or other active uses.
- ❖ Above-grade parking structures should be designed in such a way that they reinforce the intended built character and blend into the streetscape.



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# 10

## Material & Architectural Quality

New developments should ensure excellence in architectural design and in the use of high-grade materials, particularly at street-level. A key objective of the Broadway 360° Development Plan is to achieve a balance between consistencies in design quality and street interface, while enabling individual expression in new developments. Key guidelines for architectural and material quality include:

- ❖ The Broadway area has a rich history of development that is reflected in the Prairie-style ‘main street’ buildings that are constructed in a variety of materials. New developments should seek to contribute to this mix and variety.
- ❖ Building materials should be chosen for their functional and aesthetic quality and exterior finishes should exhibit quality of workmanship, longevity, sustainability and ease of maintenance.
- ❖ Building materials recommended for new construction include brick, stone, wood, glass, in-situ concrete and pre-cast concrete.
- ❖ In general, the appearance of building materials should be true to their nature and should not mimic other materials.
- ❖ Vinyl siding, plastic, plywood, concrete block, darkly tinted and mirrored glass and metal siding utilizing exposed fasteners should be discouraged.

Recommended Materials	Not Recommended Materials
Brick	Vinyl siding
Stone	Plastic
Wood	Plywood
Glass	Darkly tinted or mirrored glass
In-situ concrete	Concrete Block
Pre-cast concrete	Metal siding with exposed fasteners



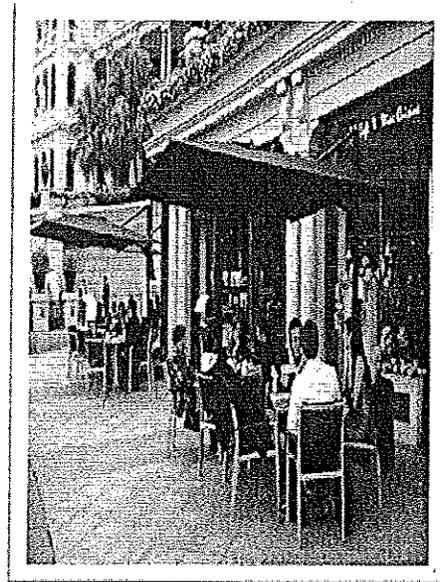
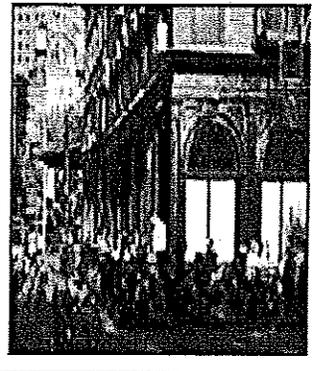
# 11

## Sidewalk Cafes

Sidewalk cafés enhance the vibrancy of street life, further enable social interaction, and are major destinations in the warmer months.

Sidewalk cafés should be encouraged throughout the Broadway Area provided there are no conflicts with adjacent land uses and they are able to be accommodated within the existing sidewalk width dimensions without encumbering pedestrians.

- ❖ Where permitting, small sidewalk cafés should be encouraged along streets with narrower sidewalks as well.
- ❖ Sidewalk cafés should be designed to contribute and integrate into the streetscape.
- ❖ Curb bump-outs should be encouraged at all corners to provide for additional sidewalk café opportunities.
- ❖ Rear yard and roof top patios should be directed to properties that are not directly adjacent to residential neighbourhood.



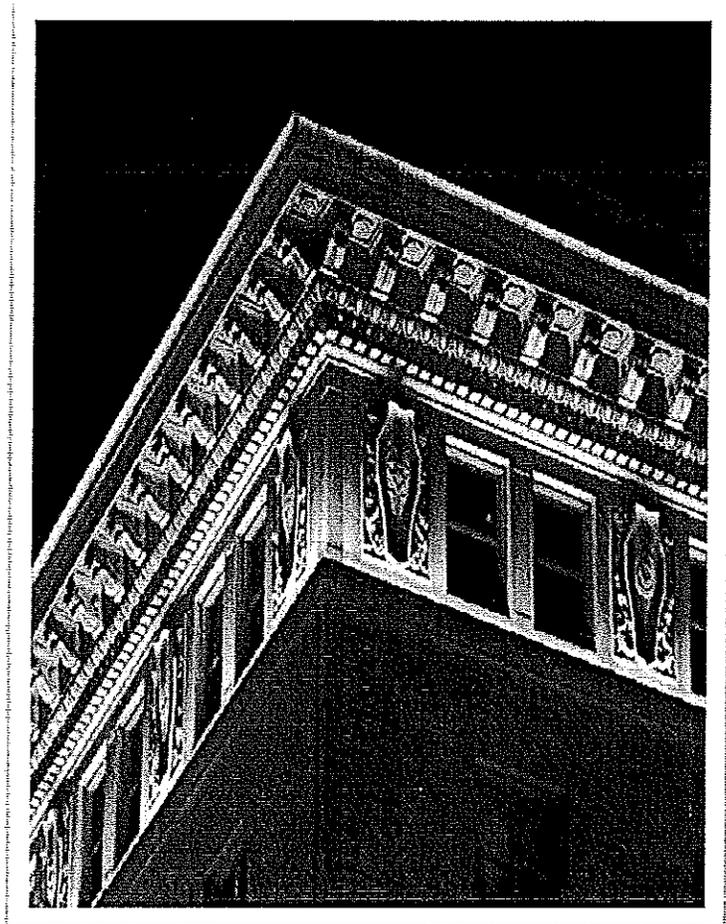
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# 12

## Building Lighting

The image and experience at night is an important aspect of any mixed-use area

- ❖ Attractive landscape and architectural features can be highlighted with spot-lighting or general lighting placement.
- ❖ Heritage and institutional buildings, as well as landmark elements such as public art, steeples or distinctive rooflines, should be illuminated.
- ❖ Subtle night-lighting of retail display windows should be encouraged.

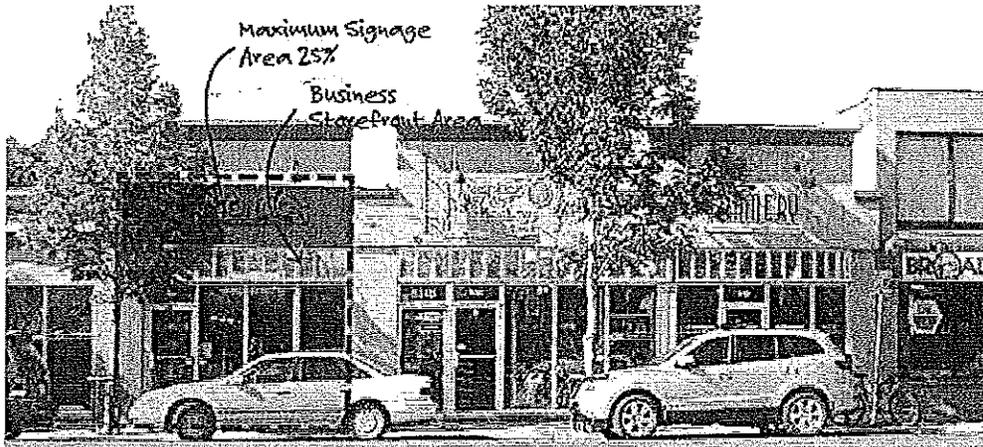
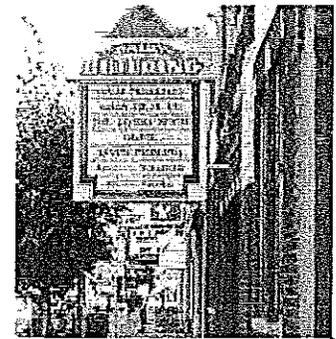


# 13

## Signage

Signage plays an important role in the overall image of any area. Signs should contribute to the quality of individual buildings and the overall streetscape. This includes compatibility with heritage buildings, where appropriate. High quality, imaginative, and innovative signs are also encouraged.

- ❖ The maximum signage area for storefront signs should be no more than 25% of the business storefront.
- ❖ Back lit illuminated rectangular sign boxes are discouraged.
- ❖ Signage should not obscure windows, cornices or other architectural elements.
- ❖ Signage should aid pedestrians and drivers in navigating the area, especially at night.
- ❖ Billboards, super boards, and roof mounted signs are not permitted.



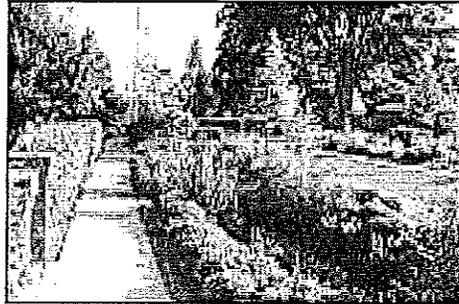
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# 14

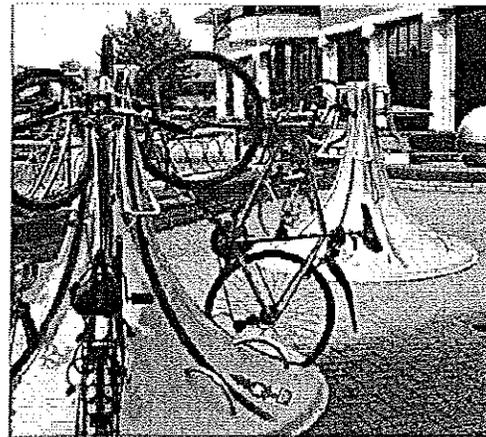
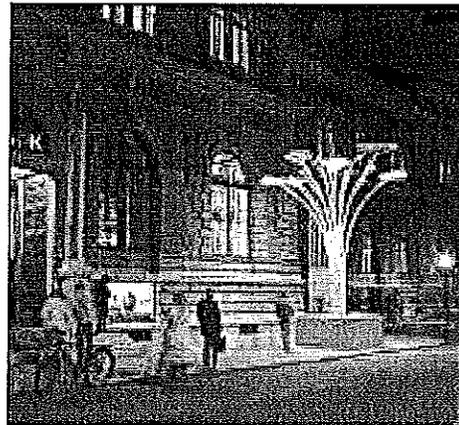
## Sustainable Design

Conservation of natural resources and systems should be a primary consideration in the planning, design, and construction process. To achieve this, all proposed projects should strive for sustainable building practices. This includes public as well as private development, and encompasses streets, parks, and buildings.

New additions, exterior alterations, or related new construction should not destroy historic materials, features, and spatial relationships that characterize the property. The new work should be differentiated from the old and should be compatible with the historic materials, features, size, scale, height, proportion and massing to protect the integrity of the property and its environment.



Good use made from the street is captured in a water fountain & sculpture



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## List of Terms

- ❖ **Animation** concerns spaces that have an animated quality; liveliness; movement; activity.
- ❖ **Articulation** is the emphasis or accentuation of different parts of a building so that they are distinct and stand out clearly.
- ❖ **Building setback** is the distance at-grade (ground level) that the building is set back from the property line.
- ❖ **Building setback** is the distance, above the base building, that the remaining portion of the building (building cap) is set back from the face of the base building.
- ❖ **Cornice** is a decorative molding that crowns a building.
- ❖ **Floor Area Ratio (FAR)** is the ratio of gross floor area of the building divided by the site area.
- ❖ **Glazing** is the part of a wall or window that is glass.
- ❖ **Mechanical Penthouse** is the covering or enclosure on the roof of a building that houses mechanical systems or equipment for the building.
- ❖ **Rhythm**, in architecture, is the repetitive or alternating use of visual elements to create a pattern.
- ❖ **Transparency** refers to the degree to which people can see or perceive what lies beyond the street edge, often through windows, doors, fences and landscaping.
- ❖ **Parapet** is a low wall projecting from the edge of a platform, terrace, or roof.
- ❖ **Storefront** refers to an area on the frontage of a building that is delineated by features to indicate a separate or distinctive “frontage”. Larger building frontages should be divided into narrower storefronts to create visual animation and visual interest at the sidewalk.

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**Broadway 360 Technical Workshop  
Cosmopolitan Senior Citizens Centre – 614 11<sup>th</sup> Street East  
January 24, 2012  
5:30 pm**

On behalf of the Broadway 360 Steering Committee, Sarah Marchildon, Executive Director of the Broadway Business Improvement District, thanked everyone for attending this technical workshop on proposed land use changes. The purpose of tonight's meeting is to review the proposed B5B Zoning District and proposed Architectural Control District for the Broadway commercial area. Tim Steuart will give a presentation with question period following. After formal part of meeting, the Steering Committee will be around for more one on one discussion.

**Broadway 360 Land Use Implementation  
Tim Steuart, Senior Planner, Business License & Zoning Compliance Section**

A bit of background. This came together from a unique cooperative effort between property owners, residents, and the BID to make Broadway a better place. A comprehensive study was done with the goal of recognizing that Broadway is a special area, a cool area created with pedestrian environment. Everyone involved didn't want to assume it will stay this way forever, but there was a desire try to ensure it does. The Steering Committee has come up with a very good plan. One issue was zoning and to ensure the zoning treatment is satisfactory to all.

All properties in the Broadway Commercial Area currently zoned B5 (Inner-City Commercial Corridor) are proposed to be rezoned to the new B5B (Commercial Zoning District), a zoning district designed exclusively for Broadway.

The new B5B includes changes to the permitted uses and development standards. Also, all B5B-zoned properties would be subject to an Architectural Control District.

Saskatoon is growing strongly and we need to grow up as well as out, creating the urban living room.

- The first three storeys matter most.
- Density done properly has many benefits.
- The stepback enhances the pedestrian experience.

What factors will affect the overall height of a building?

- Gross Floor Area Ratio (7:1) which means that the maximum building volume can be 7 times the site area
- Site size (bigger, taller buildings can be built on larger sites)
- Height of base building (must be a minimum 7.5 metres to a maximum of 12 metres)
- Setback and stepbacks (must meet minimum requirements, setbacks affect the overall building form and height)
- Parking
  - Amount of required parking

- Location of parking (at grade at rear of site, below grade in parking structure, or above grade in parking structure)

Do not want building setback from the street. Requirement that at least 70% of building face the street, setback for courtyard. Removed service stations and commercial parking lot from permitted uses. Parking provision is currently 1.25 parking spaces for residents and 1 visitor for every 8 dwellings. This has been reduced to 1 parking space for residents while visitor parking has remained the same. Nothing set for commercial buildings and not proposing changes as it is usually self-regulating since developers understand that providing parking helps to attract potential tenants, so a minimum parking provision does not seem necessary.

The proposed B5B Zoning District would be subject to an Architectural Control District (ACD) intended to preserve the physical character of the area.

The ACD would contain a set of design guidelines, known as the Broadway Commercial Area Design Plan that all new development in the B5B District must conform to. Establishing an Architectural Control District (ACD) allows for enforceable design controls in the Broadway Commercial Area.

Currently, the only ACD in Saskatoon is River Landing, which for the most part, is new buildings and the land was mostly owned by City. This would be a first in Saskatoon with infill or a character area, maybe even the first in Saskatchewan. This has long been standard in other provinces and the U.S.

Councillor Clark stated this process involved a collection of groups that came together with different views and have created something not only for Saskatoon, but the whole province. Everyone walked down the street and pointed out their issues and these issues helped develop the guidelines through this consultative process.

The goal now is to find out what the public thinks of this proposal, before taking it to City Council.

The process for ACD is an application for development permit which is then reviewed by a committee of design professionals from landscape, architectural, community planning backgrounds. The committee has approximately 15 members, with 3 selected to review each application. The entire process usually takes about 60 days. It is on a professional level and not just someone's pet peeves or personal preferences.

## **There are 14 design guidelines in the Broadway Commercial Area Design Plan**

### **1. Building Expressions**

- Buildings should reinforce a *base*, *middle*, and *top* in their design.
- Within the first three storeys of a building, a clearly defined *base* contributes to the quality of the pedestrian environment.
- The *middle* or body of a building should contribute to the overall quality of the streetscape.

- The *top* or roof should be distinguished from the rest of the building.

## 2. Orientation & Placement

- All buildings should orient to the street with clearly defined entry points that directly access the sidewalk.
- A minimum of 70% of the front building line shall be located within 1.0 metre of the front property line.
- Buildings can enhance the pedestrian environment by creating a sense of enclosure.

## 3. Street Wall

- A street wall of a new building should align with those of neighbouring buildings or have the same setback as the predominant buildings on the block.
- The height of the street wall should be consistent with historic heights of no greater than 3 storeys and no less than 2 storeys.
- Levels above the street wall should be set back to reinforce a low-rise interface with the sidewalk.

## 4. Heritage Context

- New buildings on Broadway Avenue should complement, rather than detract from, the character of older buildings.
- General Guidelines – New buildings should avoid historical misrepresentation by not replicating past architectural styles, and should respect the scale, material and massing of adjacent heritage buildings.
- Facade Articulation – New buildings should ensure the horizontal and vertical architectural orders including windows and entries, are aligned with neighboring heritage buildings or the established pattern on the block.

## 5. Corner Sites

- Corner buildings have a greater visual prominence given that they front onto two streets and frame intersections.
  - Designs and massing of corner buildings should accentuate the visual prominence of the site.
  - Corner buildings should orient to both street frontages and, wherever possible, have entrances that address both frontages.

## 6. Storefronts

- To provide animation and visual interest, storefronts should have:
  - A frontage in the range of 7.5 metres.
  - A minimum of 75% glazing to maximize visual animation.
  - Entrances that are highly visible and located at or near grade.
  - Signage that adds diversity and interest to the street.
- Dark tinted, reflective, or opaque glazing should be discouraged for storefront glazing

## 7. Residential Street Access Units

- In buildings where residential uses are located at-grade:
  - The individual units should be accessed from the street

- Appropriate front yard privacy measures should be taken
- Access to the units should be consistent with the residential street character in Nutana

## **8. Roof Treatment**

- Roof design should consider the following guidelines:
  - The use of stepbacks, changes in materials, cornice lines and overhangs
  - Screen mechanical penthouses from view
  - Green roofs are encouraged

## **9. Above Grade Parking**

- Where parking is provided at grade, the following guidelines address the design quality of the facility:
  - Direct access from the street is discouraged.
  - Ground level retail should be incorporated, where the parking structure fronts a street.
  - Parking structures should be designed to reinforce the built character and blend into the streetscape.
- Broadway 360° recommends that, wherever possible, parking should be provided in the rear yard or below grade, and should be accessed from the lane.

## **10. Material & Architectural Quality**

- New developments should contribute to the Prairie-style Main Street building style that exists
- High quality materials should be chosen that are both functional and aesthetically pleasing
- Materials chosen should not mimic other materials
- A key objective of Broadway 360 is to achieve a balance between consistencies in design quality & street interface, while enabling individual expression.

## **11. Sidewalk Cafes**

- Should be encouraged along all sidewalks
- Should contribute and integrate into the streetscape
- Corners with “curb bump outs” could provided additional opportunities
- Rear yard and roof-top patios should not abut residential areas
- Sidewalk cafes should be encouraged, provided they don’t cause land use conflicts or encumber pedestrian movements.

## **12. Building Lighting**

- Both landscape and architectural features can be highlighted
- Landmarks & distinctive features of buildings should be illuminated
- Subtle night lighting of retail displays should be encouraged

## **13. Signage**

- Storefront signs should be no more than 25% of the business storefront
- Should aid pedestrians & drivers, especially at night

- Backlit rectangular sign boxes should be discouraged
- Signage should not obscure building features.
- Signage Group 5, in the City of Saskatoon Zoning Bylaw is applied to Broadway Avenue

#### 14. Sustainable Design

- Projects should strive for sustainable building practices
- When adaptive reuse projects are undertaken to rehabilitate historic buildings, the old and new should be compatible in terms of historic materials, features, size, scale, height, proportion and massing to protect the integrity of the property and its environment

#### Questions:

- Could you briefly describe gross floor space exemptions for parking?

Under the B5 Zoning District currently, certain things are exempt from, like indoor parking is not counted towards the total area of the building. E.g. you have a parking floor it does not count as gross floor space. Steering committee was concerned about very large and tall buildings also providing a significant amount of above grade commercial parking. Parking above grade will count towards the ration with the exception of the parking you need for the building itself. We will look at the parking area and the parking you have to provide for dwelling units and the parking for commercial area at rate of 24 square meters plus the drive lanes and the ramps. We will credit that back to you; it will not count toward parking structure.

- Parking with mixed used development, how do you treat the artist who works and lives in place?

The live/work unit would count as a residential dwelling, so one parking space would be required.

- Are there still no parking requirements for commercial property?

No, this remains the same.

#### Comments:

- It is a huge challenge task to bring this into place. Almost to mold into a campus area. Guidelines are good to control design, but if too wide open it's difficult to administer. However, you don't want it too stringent either. Couple concerns with the first two guidelines.

- Base, middle and cap seems simple and perfect, but concern with wall aspect of a 2 or 3 storey building, not sure if good for street. Maybe need to have a restriction of number of stories as well. Need more to guide this.
- 70% frontage and street wall... How will this affect the Extra Foods or Oskayak School if they change ownership?

- Signage and indirect architectural lighting sections are good.
- Storefront is critical for this process.

Tim noted many issues will be covered off by codes as they are more stringent than guidelines can be.

- Some concern with the height allowed for building, seems allowance is too high. Buds is 5 stories and that is fine, but going 12 stories seems high.

Tim noted the ratio of the historic building height will be followed.

- Good solid principle, it allows for a bit of variety, very sound, but is there any room allowed for odd variety that still maintains the character?
- It seems like the smaller lots may not be able to follow the 7:1 ratio.

Tim noted it is important to maintain the pedestrian feel.

- Last area is sustainable design, which is about providing general encouragement and not specific guidelines.

Tim stated the design and statement of intent. It is more becoming the norm, it is good business sense. Don't really need to regulate it as it is the way of good business, much the same as the parking provision.

- **Is there a goal regarding the ratio of residential to commercial property?**

There is no specific quota, but the goal is to have mixed uses. Fact of the matter is Broadway is a very healthy area of mixed use and hope it will continue.

Tim thanked everyone for their time and reminded the group that the Steering Committee members will be around for anyone who wishes to have a more one to one conversation.

For more information please visit: [www.broadway360.ca](http://www.broadway360.ca)

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Broadway 360 Land Use Implementation  
Proposed B5B and ACD  
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My belief is that architecture should follow use and that design is for people to work, live and play in, and this can be satisfied through community building that essentially relies on establishing mixed use and mixed levels of affordability.

The things that make Broadway special are the people who live and work there every day. The owner-operator businesses provide the attraction to people coming into the area for the unique shops and restaurants. The BBID support every aspect of day-to-day life and year round events that Saskatoon has

become known for – unfortunately these independent businesses are fading with the end of each lease period and may soon be gone. Housing is a prime issue in Saskatoon; increased density can be achieved gracefully through multi-level-use planning.

What I suggest is that we, in whatever way it can be arranged, strive to put in place in a rule book to guarantee we can grow and preserve at once, for example: the B5B outlines the perimeters, sets heights defines set backs etc. But it's the 'inner workings' of a building that will contain and define whether or not a building is successfully integrated to support the community.

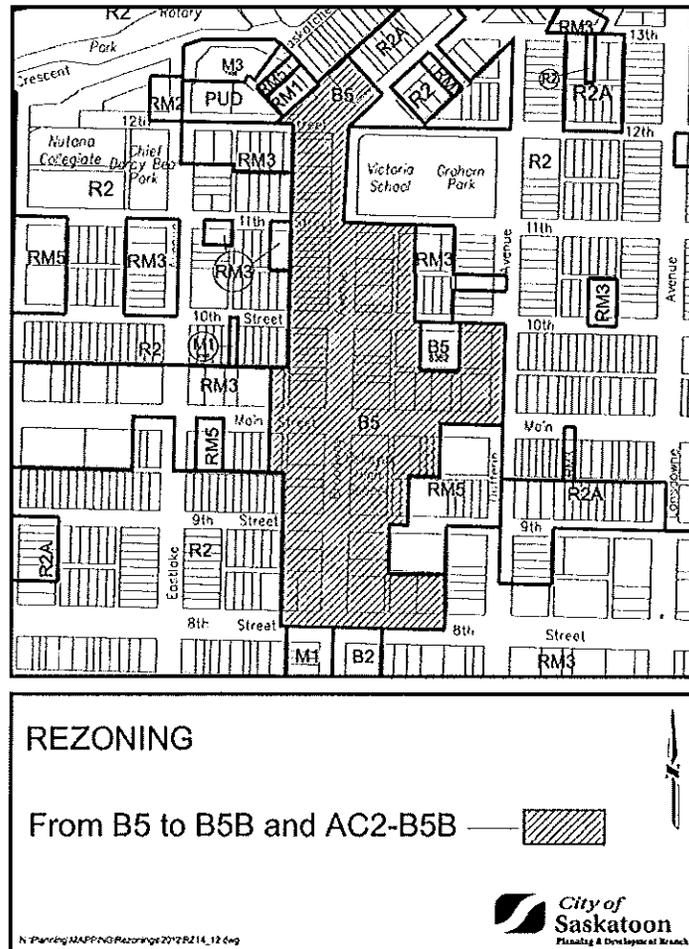
To adhere to an integration and affordability platform will ensure that each building is designed in measured percentages that include mixed use lease and resale units – a variety of sizes of each type of unit (because size determines market value, lease rate, sale price, etc), and a variety of each unit per a percentage of each of type of use; be it retail, office or residential.

That this kind of coding of multiple use, multi-layered affordability could be what in fact defines a very healthy future for Broadway. And this may be the only way we can ensure that Broadway remains a people place, that there will be something for everyone by-design regulations for sizes and uses of interior spaces. In this way the 360 plan can fulfill its mandate, be a model, impress on developers that mixed use, community affordability is incredibly important to people now and future generations of the Broadway District and for that matter, anywhere in Saskatoon.

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z19/12	<b>PROPOSAL</b> Proposed Rezoning from B5 to B5B and AC2 - B5B	<b>EXISTING ZONING</b> B5 Inner City Commercial Corridor District
<b>LEGAL DESCRIPTION</b> Various		<b>CIVIC ADDRESS</b> Various
		<b>NEIGHBOURHOOD</b> Nutana
<b>DATE</b> July 30, 2012	<b>APPLICANT</b> City of Saskatoon Planning and Development Branch 222 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 0J5	<b>OWNER</b> Various

### LOCATION PLAN



**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATIONS:**

that a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising respecting the proposal to amend Zoning Bylaw No. 8770 as outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770;
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposal to rezone the properties indicated on Attachments 2 and 3 from a B5 Zoning District to a B5B Zoning District be approved; and
- 5) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposal to apply the AC2 – B5B Architectural Control Overlay District to the properties indicated on Attachments 2 and 3 be approved.

**B. PROPOSAL**

The Planning and Development Branch is requesting that the properties in the Broadway commercial area, as indicated on Attachments 2 and 3, be rezoned from a B5 to a B5B Zoning District and that the AC2 – B5B Architectural Control Overlay Zoning District be applied to the properties.

A companion report has been submitted to amend Zoning Bylaw No. 8770 to create the B5B Broadway Commercial Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District.

The Planning and Development Branch is requesting 72 properties, located in the Nutana neighbourhood (see Attachment 3), be rezoned from a B5 Zoning District to a B5B Zoning District. The B5B Zoning District contains development standards that will ensure that new development in the area enhances the existing urban environment in the Broadway Avenue commercial area.

The Planning and Development Branch is also requesting that the AC2 – B5B Architectural Control Overlay Zoning District be applied to these properties in the Broadway Avenue

commercial area. This district is an architectural overlay district containing 14 design guidelines that will ensure the quality of design for new construction.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed B5B Zoning District is intended to be applied in the Broadway Avenue commercial area. This district will provide development standards to ensure that new development enhances the existing urban environment along this important commercial corridor.

The proposed AC2 – B5B Architectural Control Overlay Zoning District is also intended to be applied in the Broadway Avenue commercial area; and it will impose design guidelines on all new developments in this area. The design guidelines will ensure that new buildings preserve character and enhance the best qualities of the Broadway Commercial Area. The proposed B5B Zoning District is a component of the implementation of the Broadway 360 Development Plan.

**D. BACKGROUND INFORMATION**

In 2007, the Broadway Business Improvement District (BID) and the Nutana Community Association proposed that a study of the Broadway Commercial Area be undertaken. It was noted by both groups that some of the recommendations contained in the 2001 Nutana Local Area Plan (LAP) had become outdated, or difficult to implement due to community or property owner resistance, and there were concerns about the future character of Broadway Avenue. The City of Saskatoon (City) was invited to participate as a partner with the Broadway BID and Nutana Community Association in the development of a “Broadway Area Plan” to address outstanding recommendations in the Nutana LAP.

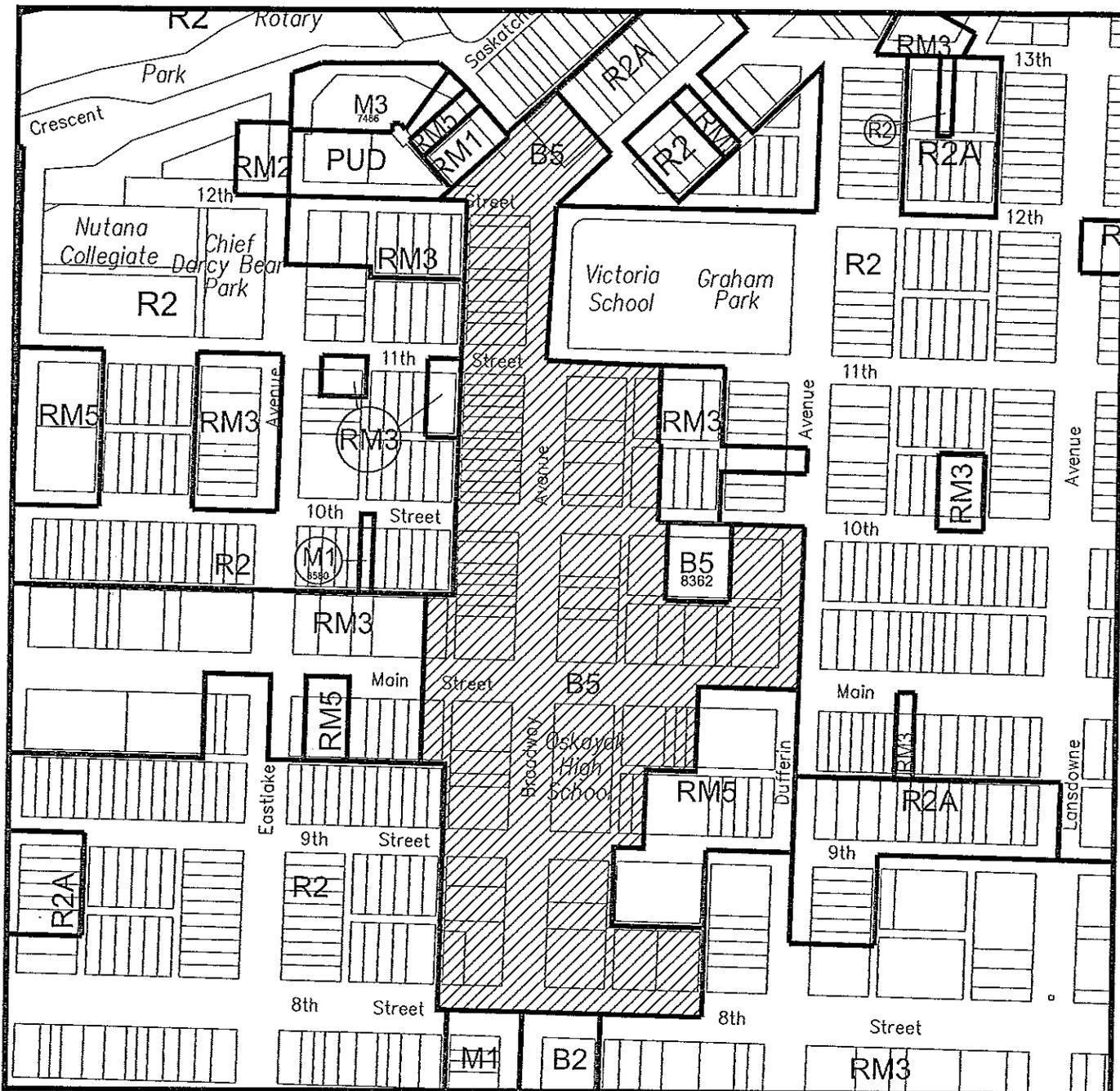
The Planning Partnership, a Toronto-based urban planning consultant firm, was hired to prepare what would become the Broadway 360 Development Plan (Plan). The Plan involved a thorough public consultation process that engaged residents, business and commercial property owners, area schools, churches, and those representing Saskatoon’s heritage community.

The Plan explored practical urban development solutions to address land use, street character, safety, parking, and traffic issues in the Broadway area.

The Steering Committee that was struck during the creation of the Plan was comprised of developers, commercial property owners, Nutana residents, the Nutana Community Association, the BID, and the Ward Councillor. The Administration worked very closely with the Steering Committee over several months to prepare detailed zoning requirements that would be acceptable to the various stakeholders. A report creating the B5B Zoning District and the AC2 – B5B Architectural Control Overlay District has been submitted separately (see Application No. Z14/12).

ATTACHMENT 1

<b>FACT SUMMARY SHEET</b>	
<b>A. Location Facts</b>	
1. Municipal Address	See Attachment 2
2. Legal Description	Various
3. Neighbourhood	Nutana
4. Ward	6
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Various
2. Proposed Use of Property	Various
3. Adjacent Land Uses and Zoning	
North	Residential – RM1, R2A, and R2
South	Institutional – M1, Commercial – B2, and Residential – RM3
East	Residential – RM5 and RM3
West	Residential – RM3 and R2
4. No. of Existing Off-Street Parking Spaces	Not applicable
5. No. of Off-Street Parking Spaces Required	Not applicable
6. No. of Off-Street Parking Spaces Provided	Not applicable
7. Site Frontage	Various
8. Site Area	Various
9. Street Classification	Arterial and Local
10. No. of Sites	72
<b>C. Development Plan Policy</b>	
1. Existing Development Plan Designation	Commercial – Special Area
2. Proposed Development Plan Designation	Commercial – Special Area
3. Existing Zoning District	B5 – Inner-City Commercial Corridor District
4. Proposed Zoning District	B5B – Broadway Commercial District AC2 – B5B Architectural Control Overlay Zoning District



# REZONING

From B5 to B5B and B5B(ACD2) — 



## List of Properties Proposed to be Rezoned from B5 to B5B and AC2 – B5B

615	10th	St	E	806	Broadway	Ave	
616	10th	St	E	810	Broadway	Ave	
634	10th	St	E	813	Broadway	Ave	
612	11th	St	E	814	Broadway	Ave	
535	8th	St	E	817	Broadway	Ave	
601	8th	St	E	818	Broadway	Ave	
617	8th	St	E	820	Broadway	Ave	
619	8th	St	E	821	Broadway	Ave	
611	9th	St	E	824	Broadway	Ave	
613	9th	St	E	834	Broadway	Ave	
1002	Broadway	Ave		835	Broadway	Ave	
1005	Broadway	Ave		906	Broadway	Ave	
1006	Broadway	Ave		912	Broadway	Ave	
1010	Broadway	Ave		916	Broadway	Ave	
1011	Broadway	Ave		919	Broadway	Ave	
616	Broadway	Ave		922	Broadway	Ave	
626	Broadway	Ave		526	Main	St	
630	Broadway	Ave		527	Main	St	
632	Broadway	Ave		616	Main	St	
638	Broadway	Ave		617	Main	St	
640	Broadway	Ave		619	Main	St	
642	Broadway	Ave		620	Main	St	
644	Broadway	Ave		621	Main	St	
650	Broadway	Ave		622	Main	St	
702	Broadway	Ave		626	Main	St	
704	Broadway	Ave		629	Main	St	
706	Broadway	Ave		639	Main	St	
707	Broadway	Ave		611	University	Dr	
708	Broadway	Ave		*628	10 <sup>th</sup>	St	E
712	Broadway	Ave			*AC2-B5B only		
714	Broadway	Ave					
715	Broadway	Ave					
718	Broadway	Ave					
720	Broadway	Ave					
723	Broadway	Ave					
724	Broadway	Ave					
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733	Broadway	Ave					
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The things that make Broadway special are the people who live and work there every day. The owner-operator businesses provide the attraction to people coming into the area for the unique shops and restaurants. The BBID support every aspect of day-to-day life and year round events that Saskatoon has

become known for – unfortunately these independent businesses are fading with the end of each lease period and may soon be gone. Housing is a prime issue in Saskatoon; increased density can be achieved gracefully through multi-level-use planning.

What I suggest is that we, in whatever way it can be arranged, strive to put in place in a rule book to guarantee we can grow and preserve at once, for example: the B5B outlines the perimeters, sets heights defines set backs etc. But it's the 'inner workings' of a building that will contain and define whether or not a building is successfully integrated to support the community.

To adhere to an integration and affordability platform will ensure that each building is designed in measured percentages that include mixed use lease and resale units – a variety of sizes of each type of unit (because size determines market value, lease rate, sale price, etc), and a variety of each unit per a percentage of each of type of use; be it retail, office or residential.

That this kind of coding of multiple use, multi-layered affordability could be what in fact defines a very healthy future for Broadway. And this may be the only way we can ensure that Broadway remains a people place, that there will be something for everyone by-design regulations for sizes and uses of interior spaces. In this way the 360 plan can fulfill its mandate, be a model, impress on developers that mixed use, community affordability is incredibly important to people now and future generations of the Broadway District and for that matter, anywhere in Saskatoon.

**Broadway 360 Technical Workshop**  
**Cosmopolitan Senior Citizens Centre – 614 11<sup>th</sup> Street East**  
**January 24, 2012**  
**5:30 pm**

On behalf of the Broadway 360 Steering Committee, Sarah Marchildon, Executive Director of the Broadway Business Improvement District, thanked everyone for attending this technical workshop on proposed land use changes. The purpose of tonight's meeting is to review the proposed B5B Zoning District and proposed Architectural Control District for the Broadway commercial area. Tim Steuart will give a presentation with question period following. After formal part of meeting, the Steering Committee will be around for more one on one discussion.

**Broadway 360 Land Use Implementation**  
**Tim Steuart, Senior Planner, Business License & Zoning Compliance Section**

A bit of background. This came together from a unique cooperative effort between property owners, residents, and the BID to make Broadway a better place. A comprehensive study was done with the goal of recognizing that Broadway is a special area, a cool area created with pedestrian environment. Everyone involved didn't want to assume it will stay this way forever, but there was a desire try to ensure it does. The Steering Committee has come up with a very good plan. One issue was zoning and to ensure the zoning treatment is satisfactory to all.

All properties in the Broadway Commercial Area currently zoned B5 (Inner-City Commercial Corridor) are proposed to be rezoned to the new B5B (Commercial Zoning District), a zoning district designed exclusively for Broadway.

The new B5B includes changes to the permitted uses and development standards. Also, all B5B-zoned properties would be subject to an Architectural Control District.

Saskatoon is growing strongly and we need to grow up as well as out, creating the urban living room.

- The first three storeys matter most.
- Density done properly has many benefits.
- The stepback enhances the pedestrian experience.

What factors will affect the overall height of a building?

- Gross Floor Area Ratio (7:1) which means that the maximum building volume can be 7 times the site area
- Site size (bigger, taller buildings can be built on larger sites)
- Height of base building (must be a minimum 7.5 metres to a maximum of 12 metres)
- Setback and stepbacks (must meet minimum requirements, setbacks affect the overall building form and height)
- Parking
  - Amount of required parking

- Location of parking (at grade at rear of site, below grade in parking structure, or above grade in parking structure)

Do not want building setback from the street. Requirement that at least 70% of building face the street, setback for courtyard. Removed service stations and commercial parking lot from permitted uses. Parking provision is currently 1.25 parking spaces for residents and 1 visitor for every 8 dwellings. This has been reduced to 1 parking space for residents while visitor parking has remained the same. Nothing set for commercial buildings and not proposing changes as it is usually self-regulating since developers understand that providing parking helps to attract potential tenants, so a minimum parking provision does not seem necessary.

The proposed B5B Zoning District would be subject to an Architectural Control District (ACD) intended to preserve the physical character of the area.

The ACD would contain a set of design guidelines, known as the Broadway Commercial Area Design Plan that all new development in the B5B District must conform to. Establishing an Architectural Control District (ACD) allows for enforceable design controls in the Broadway Commercial Area.

Currently, the only ACD in Saskatoon is River Landing, which for the most part, is new buildings and the land was mostly owned by City. This would be a first in Saskatoon with infill or a character area, maybe even the first in Saskatchewan. This has long been standard in other provinces and the U.S.

Councillor Clark stated this process involved a collection of groups that came together with different views and have created something not only for Saskatoon, but the whole province. Everyone walked down the street and pointed out their issues and these issues helped develop the guidelines through this consultative process.

The goal now is to find out what the public thinks of this proposal, before taking it to City Council.

The process for ACD is an application for development permit which is then reviewed by a committee of design professionals from landscape, architectural, community planning backgrounds. The committee has approximately 15 members, with 3 selected to review each application. The entire process usually takes about 60 days. It is on a professional level and not just someone's pet peeves or personal preferences.

## **There are 14 design guidelines in the Broadway Commercial Area Design Plan**

### **1. Building Expressions**

- Buildings should reinforce a *base*, *middle*, and *top* in their design.
- Within the first three storeys of a building, a clearly defined *base* contributes to the quality of the pedestrian environment.
- The *middle* or body of a building should contribute to the overall quality of the streetscape.

- The *top* or roof should be distinguished from the rest of the building.

## 2. Orientation & Placement

- All buildings should orient to the street with clearly defined entry points that directly access the sidewalk.
- A minimum of 70% of the front building line shall be located within 1.0 metre of the front property line.
- Buildings can enhance the pedestrian environment by creating a sense of enclosure.

## 3. Street Wall

- A street wall of a new building should align with those of neighbouring buildings or have the same setback as the predominant buildings on the block.
- The height of the street wall should be consistent with historic heights of no greater than 3 storeys and no less than 2 storeys.
- Levels above the street wall should be set back to reinforce a low-rise interface with the sidewalk.

## 4. Heritage Context

- New buildings on Broadway Avenue should complement, rather than detract from, the character of older buildings.
- General Guidelines – New buildings should avoid historical misrepresentation by not replicating past architectural styles, and should respect the scale, material and massing of adjacent heritage buildings.
- Facade Articulation – New buildings should ensure the horizontal and vertical architectural orders including windows and entries, are aligned with neighboring heritage buildings or the established pattern on the block.

## 5. Corner Sites

- Corner buildings have a greater visual prominence given that they front onto two streets and frame intersections.
  - Designs and massing of corner buildings should accentuate the visual prominence of the site.
  - Corner buildings should orient to both street frontages and, wherever possible, have entrances that address both frontages.

## 6. Storefronts

- To provide animation and visual interest, storefronts should have:
  - A frontage in the range of 7.5 metres.
  - A minimum of 75% glazing to maximize visual animation.
  - Entrances that are highly visible and located at or near grade.
  - Signage that adds diversity and interest to the street.
- Dark tinted, reflective, or opaque glazing should be discouraged for storefront glazing

## 7. Residential Street Access Units

- In buildings where residential uses are located at-grade:
  - The individual units should be accessed from the street

- Appropriate front yard privacy measures should be taken
- Access to the units should be consistent with the residential street character in Nutana

#### **8. Roof Treatment**

- Roof design should consider the following guidelines:
  - The use of stepbacks, changes in materials, cornice lines and overhangs
  - Screen mechanical penthouses from view
  - Green roofs are encouraged

#### **9. Above Grade Parking**

- Where parking is provided at grade, the following guidelines address the design quality of the facility:
  - Direct access from the street is discouraged.
  - Ground level retail should be incorporated, where the parking structure fronts a street.
  - Parking structures should be designed to reinforce the built character and blend into the streetscape.
- Broadway 360° recommends that, wherever possible, parking should be provided in the rear yard or below grade, and should be accessed from the lane.

#### **10. Material & Architectural Quality**

- New developments should contribute to the Prairie-style Main Street building style that exists
- High quality materials should be chosen that are both functional and aesthetically pleasing
- Materials chosen should not mimic other materials
- A key objective of Broadway 360 is to achieve a balance between consistencies in design quality & street interface, while enabling individual expression.

#### **11. Sidewalk Cafes**

- Should be encouraged along all sidewalks
- Should contribute and integrate into the streetscape
- Corners with “curb bump outs” could provided additional opportunities
- Rear yard and roof-top patios should not abut residential areas
- Sidewalk cafes should be encouraged, provided they don’t cause land use conflicts or encumber pedestrian movements.

#### **12. Building Lighting**

- Both landscape and architectural features can be highlighted
- Landmarks & distinctive features of buildings should be illuminated
- Subtle night lighting of retail displays should be encouraged

#### **13. Signage**

- Storefront signs should be no more than 25% of the business storefront
- Should aid pedestrians & drivers, especially at night

- Backlit rectangular sign boxes should be discouraged
- Signage should not obscure building features.
- Signage Group 5, in the City of Saskatoon Zoning Bylaw is applied to Broadway Avenue

#### 14. Sustainable Design

- Projects should strive for sustainable building practices
- When adaptive reuse projects are undertaken to rehabilitate historic buildings, the old and new should be compatible in terms of historic materials, features, size, scale, height, proportion and massing to protect the integrity of the property and its environment

#### Questions:

- **Could you briefly describe gross floor space exemptions for parking?**

Under the B5 Zoning District currently, certain things are exempt from, like indoor parking is not counted towards the total area of the building. E.g. you have a parking floor it does not count as gross floor space. Steering committee was concerned about very large and tall buildings also providing a significant amount of above grade commercial parking. Parking above grade will count towards the ration with the exception of the parking you need for the building itself. We will look at the parking area and the parking you have to provide for dwelling units and the parking for commercial area at rate of 24 square meters plus the drive lanes and the ramps. We will credit that back to you; it will not count toward parking structure.

- **Parking with mixed used development, how do you treat the artist who works and lives in place?**

The live/work unit would count as a residential dwelling, so one parking space would be required.

- **Are there still no parking requirements for commercial property?**

No, this remains the same.

#### Comments:

- It is a huge challenge task to bring this into place. Almost to mold into a campus area. Guidelines are good to control design, but if too wide open it's difficult to administer. However, you don't want it too stringent either. Couple concerns with the first two guidelines.
  - Base, middle and cap seems simple and perfect, but concern with wall aspect of a 2 or 3 storey building, not sure if good for street. Maybe need to have a restriction of number of stories as well. Need more to guide this.
  - 70% frontage and street wall... How will this affect the Extra Foods or Oskayak School if they change ownership?

- Signage and indirect architectural lighting sections are good.
- Storefront is critical for this process.

Tim noted many issues will be covered off by codes as they are more stringent than guidelines can be.

- Some concern with the height allowed for building, seems allowance is too high. Buds is 5 stories and that is fine, but going 12 stories seems high.

Tim noted the ratio of the historic building height will be followed.

- Good solid principle, it allows for a bit of variety, very sound, but is there any room allowed for odd variety that still maintains the character?
- It seems like the smaller lots may not be able to follow the 7:1 ratio.

Tim noted it is important to maintain the pedestrian feel.

- Last area is sustainable design, which is about providing general encouragement and not specific guidelines.

Tim stated the design and statement of intent. It is more becoming the norm, it is good business sense. Don't really need to regulate it as it is the way of good business, much the same as the parking provision.

- **Is there a goal regarding the ratio of residential to commercial property?**

There is no specific quota, but the goal is to have mixed uses. Fact of the matter is Broadway is a very healthy area of mixed use and hope it will continue.

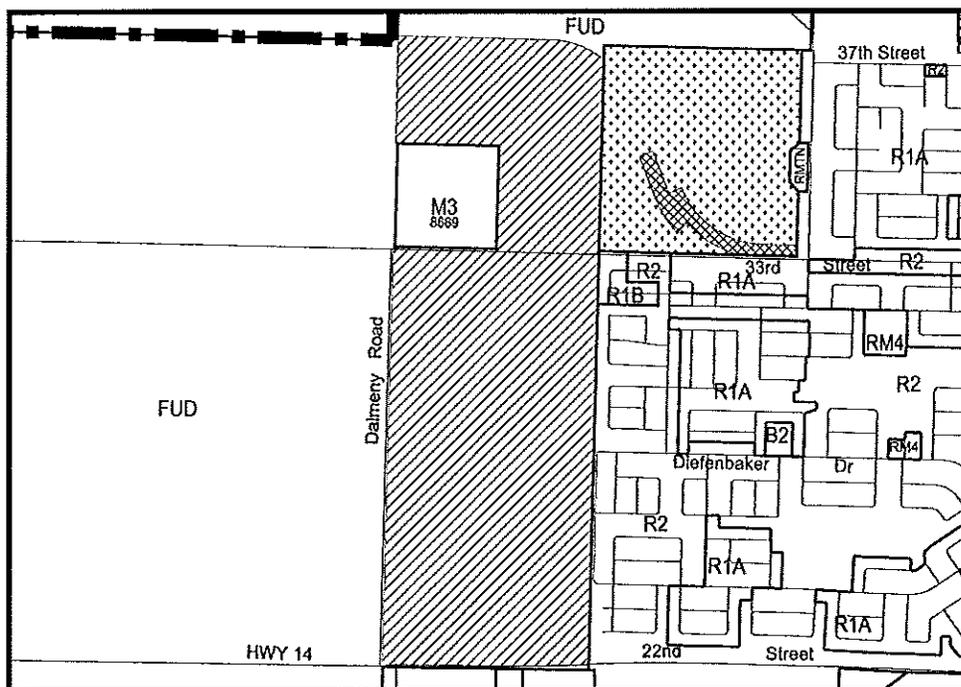
Tim thanked everyone for their time and reminded the group that the Steering Committee members will be around for anyone who wishes to have a more one to one conversation.

For more information please visit: [www.broadway360.ca](http://www.broadway360.ca)

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z15/12	<b>PROPOSAL</b> Proposed Rezoning from R1A(H) to R1A, FUD to R1A, and R1A(H) to R1B	<b>EXISTING ZONING</b> R1A (H) and FUD
<b>LEGAL DESCRIPTION</b> Various (see Attachment 1)	<b>CIVIC ADDRESS</b> N/A	
	<b>NEIGHBOURHOOD</b> Kensington	
<b>DATE</b> July 19, 2012	<b>APPLICANT</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7	<b>OWNER</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7

### LOCATION PLAN



**PROPOSED REZONING**

From FUD to R1A

From R1A(H) to R1B

From R1A(H) to R1A

File No. RZ15-2012

**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

That a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendments to Zoning Bylaw No. 8770 be approved:
  - a) that the properties identified in Attachment 2 be rezoned from R1A(H) - One-Unit Residential District with a Holding symbol, and FUD - Future Urban Development District to R1A - One-Unit Residential District; and
  - b) Phase I of the Kensington neighbourhood development, as shown on the Plan of Proposed Subdivision (see Attachment 3), be rezoned from:
    - i. R1A(H) - One-Unit Residential District with a Holding symbol, to R1B - Small Lot One-Unit Residential District (Lots 1 to 26, Block 100, Lots 1 to 29, Block 101, Lots 1 to 30, Block 103); and
    - ii. R1A(H) - One-Unit Residential District with a Holding symbol to R1A - One-Unit Dwelling Residential District (Lots 1 to 15, Block 102, and Lots 71 to 76, Block 103).

**B. PROPOSAL**

The Planning and Development Branch has received an application from the City of Saskatoon (City), Land Branch requesting that the identified properties within the Kensington Neighbourhood Concept Plan area be rezoned from R1A(H) - One-Unit Residential District with a Holding symbol, and FUD - Future Urban Development District to R1A - One-Unit Residential District; and to rezone Kensington Phase I from R1A(H) - One-Unit Residential District with a Holding symbol, to R1A - One-Unit Residential District and R1B - Small Lot One-Unit Residential District.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed rezoning of the identified properties would facilitate residential development that is consistent with the Kensington Neighbourhood Concept Plan.

**D. BACKGROUND INFORMATION**

During its April 16, 2012 meeting, City Council approved the Kensington Neighbourhood Concept Plan, which provides a wide range of housing options, as well as neighbourhood commercial services.

**E. JUSTIFICATION**

1. Community Services Department Comments

a) Planning and Development Branch

The proposed rezoning is consistent with the approved Concept Plan. Properties in the northwest part of the Kensington neighbourhood will be zoned R1B District and R1A District to accommodate development of Phase I of the neighbourhood. The remainder of the properties identified will be zoned as a R1A District. Any parcels intended for other forms of development will be subject to future rezoning when detailed survey plans are prepared.

2. Comments by Others

a) Infrastructure Services Department

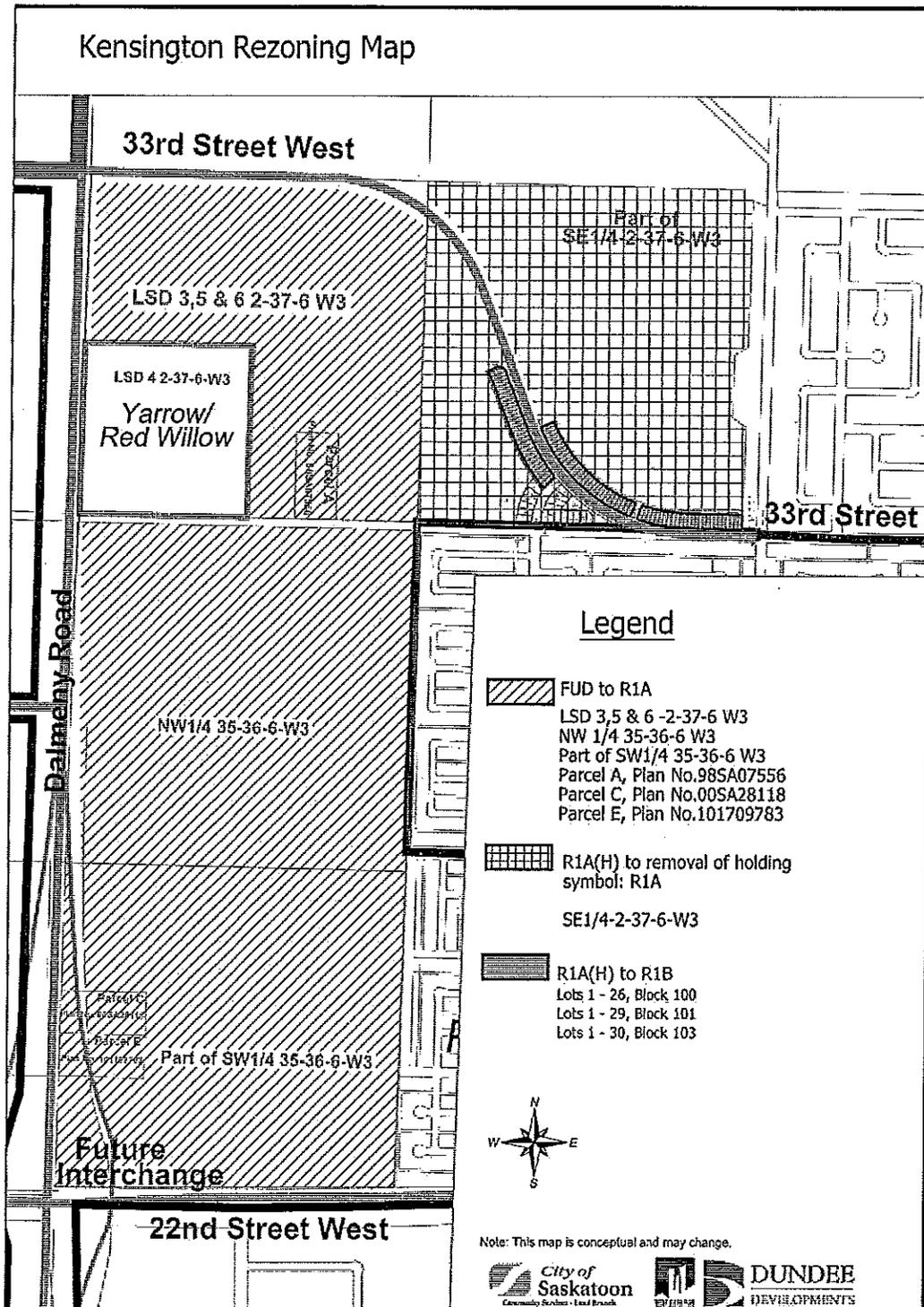
The proposed Zoning Bylaw No. 8770 amendment is acceptable to the Infrastructure Services Department.

b) Utility Services Department, Transit Services Branch

The Transit Services Branch has no easement requirements regarding the above referenced properties. At present, the Transit Services Branch has no service within 450 metres.

## FACT SUMMARY SHEET

FACT SUMMARY SHEET	
<b>A. Location Facts</b>	
1. Municipal Address	N/A
2. Legal Description	LSD 3, 5, and 6 2-37-6-W3; NW¼ 35-36-6-W3; Part of SW¼ 35-36-6-W3; Parcel A, Plan No. 98SA07556; Parcel C, Plan No. 00SA28118; Parcel E, Plan No. 101709783; and Lots 1 to 26, Block 100, Lots 1 to 29, Block 101, Lots 1 to 30, Block 103 as shown on Plan of Proposed Subdivision showing Part of SE¼ 2-37-6-W3, Plan No. 101836076
3. Neighbourhood	Kensington
4. Ward	3
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Residential with a Holding symbol - R1A(H) and Future Urban Development -FUD
2. Proposed Use of Property	Residential – R1A and R1B
3. Adjacent Land Uses and Zoning	Future Urban Development, Residential, Direct Control District, Institutional, and Commercial
North - Undeveloped	Future Urban Development – FUD
South – Blairmore Suburban Centre	Direct Control District – DCD6, Commercial – B3, and Institutional – M3
East – Confederation Park Neighbourhood Pacific Heights Neighborhood	Residential – R2, R1B, and RMTN
West - Undeveloped	Future Urban Development – FUD
4. Street Classification	22 <sup>nd</sup> Street West – Major Arterial Diefenbaker Drive – Major Arterial 33 <sup>rd</sup> Street West – Major Arterial
<b>C. Official Community Plan Policy</b>	
1. Existing Official Community Plan Designation	Residential
2. Proposed Official Community Plan Designation	Residential
3. Existing Zoning District	FUD and R1A(H)
4. Proposed Zoning District	R1A and R1B

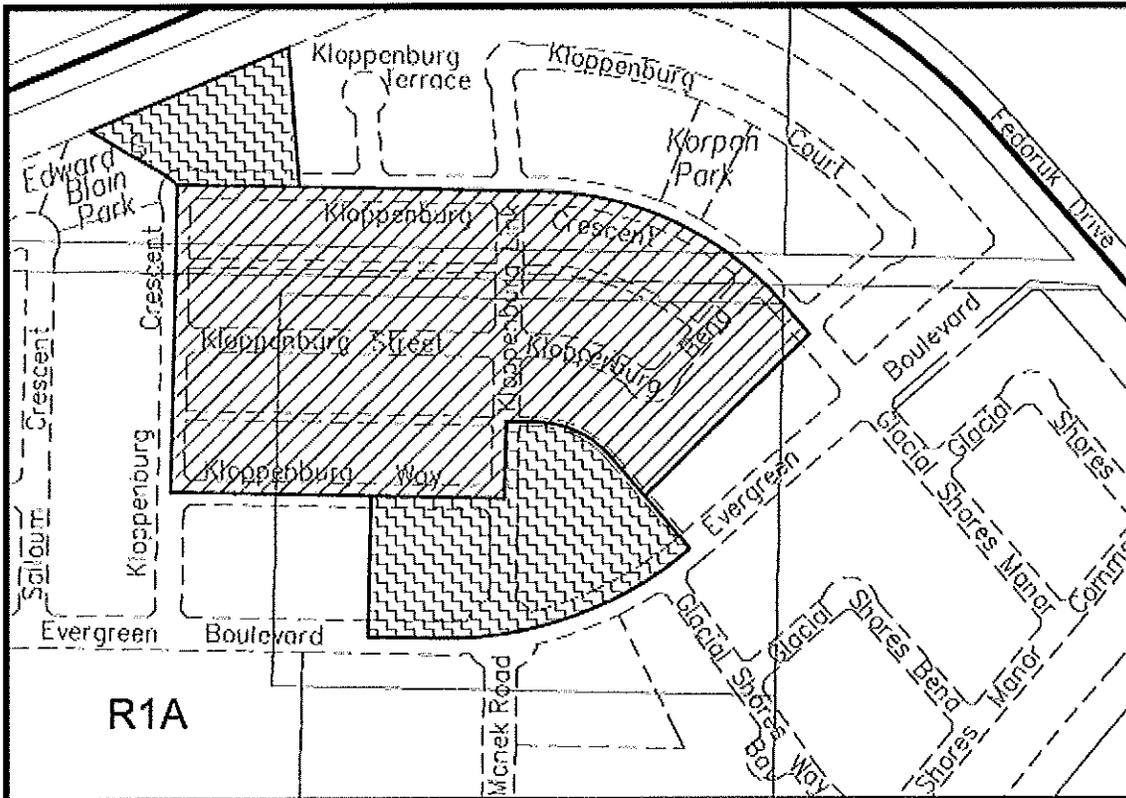




## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z17/12	<b>PROPOSAL</b> Proposed Rezoning from R1A to R1B and RMTN	<b>EXISTING ZONING</b> R1A
<b>LEGAL DESCRIPTION</b> Plan to be Approved		<b>CIVIC ADDRESS</b> Evergreen Boulevard, Kloppenburg Crescent, Bend, Link, Street, and Way
		<b>NEIGHBOURHOOD</b> Evergreen
<b>DATE</b> July 16, 2012	<b>APPLICANT</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7	<b>OWNER</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7

**LOCATION PLAN**



**PROPOSED REZONING**

From R1A to R1B

From R1A to RMTN

File No. RZ17-2012

**City of Saskatoon**  
Planning & Development Branch

**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

That a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone the properties identified in the attached Proposed Zoning Map from R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District, and RMTN – Townhouse Residential District, be approved.

**B. PROPOSAL**

The Planning and Development Branch has received an application from the City of Saskatoon, Land Branch, requesting that the properties identified in the attached Proposed Zoning Map (see Attachment 2) be rezoned as follows:

- 1) Blocks 1 to 7 be rezoned from R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District; and
- 2) Parcels EE, FF, and GG, be rezoned from R1A – One-Unit Residential District to RMTN – Townhouse Residential District.

The rezoning of these lands would accommodate small lot one-unit and townhouse residential development.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed rezoning will permit the development of the aforementioned lands in a manner which is consistent with the Evergreen Neighborhood Concept Plan (Concept Plan).

**D. BACKGROUND INFORMATION**

During its June 1, 2009 meeting, City Council approved the Concept Plan. The subject

sites are currently under the blanket zoning of a R1A District (One-Unit Residential District), which was applied to the area after the approval of the Concept Plan. The Concept Plan provides a wide range of housing options, as well as neighbourhood commercial services to serve the area. The proposed amendments will change the zoning designations for the specified areas of the neighbourhood in order to accommodate the variety of residential development that is desired within the Evergreen neighbourhood.

## **E. JUSTIFICATION**

### **1. Community Services Department Comments**

#### **a) Planning and Development Branch**

The proposed rezoning is in compliance with the approved Concept Plan and will accommodate a diversity of housing types in the Evergreen neighbourhood. Future development on this site will comply with the development standards identified in the R1B and RMTN Zoning Districts.

### **2. Comments by Others**

#### **a) Infrastructure Services Department**

The proposed amendment is acceptable to the Infrastructure Services Department.

#### **b) Utility Services Department, Transit Services Branch**

The Transit Branch has no easement requirements regarding the above referenced property. At present, the Transit Branch has no service within 450 metres, but has services within this development.

## **F. COMMUNICATION PLAN**

Once this application has been considered by the Municipal Planning Commission, it will be advertised in accordance with Public Notice Policy No. C01-021, and a date for a public hearing will be set. The property owners affected by this rezoning will be notified in writing. The Planning and Development Branch will also notify the Community Consultant and the Ward Councillor of the public hearing date by letter. A notice will be placed in The StarPhoenix two weeks prior to the public hearing. Notice boards will also be placed on the site.

**G. ENVIRONMENTAL IMPLICATIONS**

There are no environmental and/or greenhouse gas implications.

**H. ATTACHMENTS**

1. Fact Summary Sheet
2. Proposed Zoning Map

Written by: Daniel Gray, Planner 16

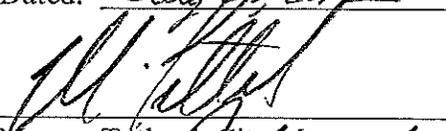
Reviewed by:

  
Alan Wallace, Manager  
Planning and Development Branch

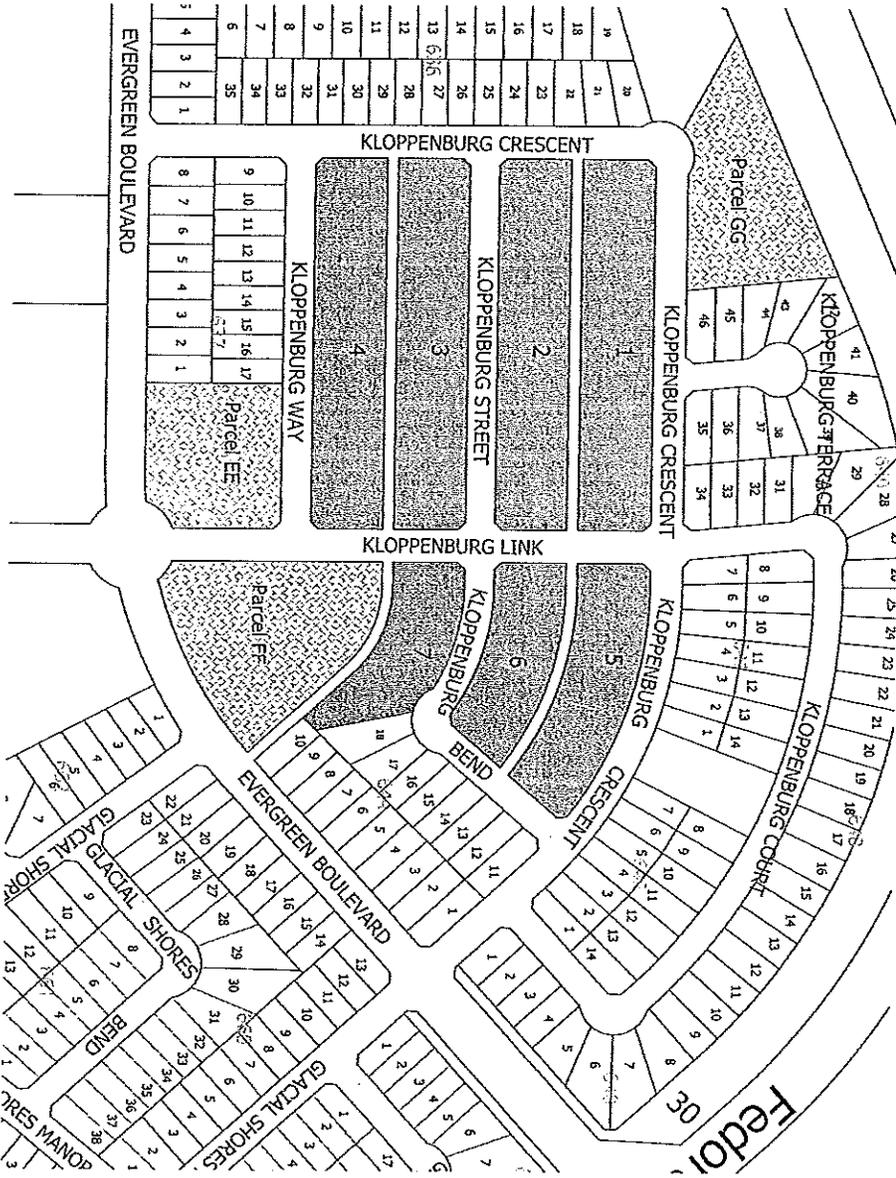
Approved by:

  
Randy Grauer, General Manager  
Community Services Department  
Dated: July 23, 2012

Approved by:

  
Murray Totland, City Manager  
Dated: July 26/12.

<b>FACT SUMMARY SHEET</b>	
<b>A. Location Facts</b>	
1. Municipal Address	Evergreen Blvd, Kloppenburg Crescent, Bend, Link, Street, and Way
2. Legal Description	Plan to be Approved
3. Neighbourhood	Evergreen
4. Ward	10
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Residential – R1A
2. Proposed Use of Property	Residential – R1B and RMTN
3. Adjacent Land Uses and Zoning	Residential
North - Undeveloped	Future Urban Development - FUD
South - Evergreen Neighbourhood	Residential – R1A
East - Evergreen Neighbourhood	Residential – R1A
West - Evergreen Neighbourhood	Residential – R1A
4. No. of Existing Off-Street Parking Spaces	N/A
5. No. of Off-Street Parking Spaces Required	N/A
6. No. of Off-Street Parking Spaces Provided	N/A
7. Site Frontage	N/A
8. Site Area	N/A
9. Street Classification	Evergreen Boulevard – Major Collector Kloppenburg Crescent, Bend, Link, Street, and Way – Local Street
<b>C. Official Community Plan Policy</b>	
1. Existing Official Community Plan Designation	Residential
2. Proposed Official Community Plan Designation	Residential
3. Existing Zoning District	R1A
4. Proposed Zoning District	R1B and RMTN



**EVERGREEN**

**Legend:**

-  R1B  
Parcel 1:  
Part of LSD 3-18-37-04-3 Ext 33 & Part of  
NW 1/4 37-4-W3M
-  R1B  
Parcels 2 - 4:  
Part of Parcel A Registered Plan  
#78534536 & Part of NW 1/4 37-4-W3M
-  R1B  
Parcel 5:  
Part of LSD 3-18-37-04-3 Ext 33 & Part of  
Registered Plan #78534536
-  R1B  
Parcel 6:  
Part of Parcel A Registered Plan  
#78534536
-  R1B  
Parcel 7:  
Part of Parcel A Registered Plan  
#78534536 & Part of NE 1/4 37-4-W3M
-  RMTN  
Parcels EE & FF:  
Part of Parcel A Registered Plan #78534536
-  R1B  
Parcel GG:  
Part of LSD 4-18-37-04-3 Ext 32 & Part of  
LSD 3-18-37-04-3 Ext 33

His Worship the Mayor and City Council  
The City of Saskatoon

**REPORT**  
**of the**  
**MUNICIPAL PLANNING COMMISSION**

Composition of Commission

Mr. Kurt Soucy, Chair  
Mr. Leanne DeLong, Vice Chair  
Councillor Charlie Clark  
Ms. Carole Beitel  
Mr. Laurier Langlois  
Mr. Aditya Garg  
Mr. Al Douma  
Mr. Stan Laba  
Ms. Debbie Marcoux  
Ms. Kathy Weber  
Mr. James Yachyshen  
Ms. Janice Braden  
Mr. Andy Yuen

1. **Proposed Rezoning from R1A to RM3, RMTN and R1B; and from AG to R1A  
Richardson Road, Lehrer Manor, Lehrer Crescent and 37<sup>th</sup> Street West  
Hampton Village Neighbourhood  
Applicant: City of Saskatoon, Land Branch  
(File No. CK. 4351-012-015)**
- 

- RECOMMENDATION:**
- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in the report of the General Manager, Community Services Department dated July 16, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
  - 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and

- 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone the land parcels located on Richardson Road, Lehrer Manor, Lehrer Crescent, and a utility parcel (see Attachment 2 to the report of the General Manager, Community Services Department dated July 16, 2012) from R1A – One-Unit Residential District to RM3 – Medium Density Multiple-Unit Dwelling District, RMTN – Townhouse Residential District, and R1B – Small Lot One-Unit Residential District; and AG – Agricultural District to R1A – One-Unit Residential District, be approved based on the reasons outlined in the report of the General Manager, Community Services Department, dated July 16, 2012.

Attached is a report of the General Manager, Community Services Department dated July 16, 2012, with respect to the above proposed rezoning.

Your Committee has reviewed the proposed rezoning with the Administration and supports the above recommendations.

**2. Proposed New Commercial Zoning District – B5B Broadway and  
Proposed New Overlay Zoning District – AC2 –  
B5B Architectural Control Overlay Zoning District  
Applicant: City of Saskatoon, Planning and Development Branch  
(File No. CK. 4350-012-005)**

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- RECOMMENDATION:**
- 1) that City Council approve the advertising respecting the proposal to amend Sections 13.1.3 of Zoning Bylaw No. 8770 as outlined in the report of the General Manager, Community Services Department dated July 17, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
  - 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and

- 4) that at the time of the public hearing, City Council consider the Commission's recommendation to amend Zoning Bylaw No. 8770 to add the proposed B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District, and to amend the parking and sign regulations contained in Zoning Bylaw No. 8770 to reflect the addition of the B5B Zoning District.

Attached is a report of the General Manager, Community Services Department dated July 17, 2012, with respect to the above proposed amendments to the Zoning Bylaw.

Your Commission has reviewed the report with the Administration and the Chair of the Broadway 360 Steering Committee, including the consultation process undertaken, and is supporting the above recommendations.

**3. Proposed Rezoning from B5 to B5B and AC2 – B5B  
Nutana Neighbourhood  
Applicant: City of Saskatoon, Planning and Development Branch  
(File No. CK. 4351-012-10)**

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- RECOMMENDATION:**
- 1) that City Council approve the advertising respecting the proposal to amend Zoning Bylaw No. 8770 as outlined in the report of the General Manager, Community Services Department dated July 30, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
  - 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770;
  - 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposal to rezone the properties indicated on Attachments 2 and 3 of the report of the General Manager, Community Services Department dated July 30, 2012, from a B5 Zoning District to a B5B Zoning District, be approved; and

- 5) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposal to apply the AC2 – B5B Architectural Control Overlay District to the properties indicated on Attachments 2 and 3 of the report of the General Manager, Community Services Department dated July 30, 2012, be approved.

Attached is a report of the General Manager, Community Services Department dated July 30, 2012, with respect to the above matter.

Your Commission has reviewed the above matter with the Administration and Broadway 360 Steering Committee Chair and supports the above recommendations.

**4. Proposed Rezoning from R1A(H) to R1A,  
FUD to R1A, and R1A(H) to R1B  
Kensington Neighbourhood  
Applicant: City of Saskatoon, Land Branch  
(File No. CK. 4351-012-11)**

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- RECOMMENDATION:**
- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in the report of the General Manager, Community Services Department dated July 19, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
  - 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
  - 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposed amendments to Zoning Bylaw No. 8770 be approved, as follows:
    - a) that the properties identified in Attachment 2 to the report of the General Manager, Community Services

Department dated July 19, 2012, be rezoned from R1A(H) - One-Unit Residential District with a Holding symbol, and FUD - Future Urban Development District to R1A – One-Unit Residential District; and

- b) Phase I of the Kensington neighbourhood development, as shown on the Plan of Proposed Subdivision (see Attachment 3 to the report of the General Manager, Community Services Department dated July 19, 2012), be rezoned from:
  - i. R1A(H) - One-Unit Residential District with a Holding symbol, to R1B – Small Lot One-Unit Residential District (Lots 1 to 26, Block 100, Lots 1 to 29, Block 101, Lots 1 to 30, Block 103); and
  - ii. R1A(H) - One-Unit Residential District with a Holding symbol to R1A – One-Unit Dwelling Residential District (Lots 1 to 15, Block 102, and Lots 71 to 76, Block 103).

Attached is a report of the General Manager, Community Services Department dated July 19, 2012, with respect to the above matter.

Your Commission has reviewed the report with the Administration and is supporting the above recommendations.

**5. Proposed Rezoning from R1A to R1B and RMTN  
Evergreen Boulevard, Kloppenburg Crescent, Bend, Link, Street, and Way  
Evergreen Neighbourhood  
Applicant: City of Saskatoon, Land Branch  
(File No. CK. 4351-012-013)**

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**RECOMMENDATION:** 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in the report of the General Manager, Community Services Department dated July 16, 2012;

- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Commission's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone the properties identified in the attached Proposed Zoning Map from R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District, and RMTN – Townhouse Residential District, be approved.

Attached is a report of the General Manager, Community Services Department dated July 16, 2012, with respect to the above matter.

Your Commission has reviewed the report with the Administration and supports the above recommendations.

Respectfully submitted,

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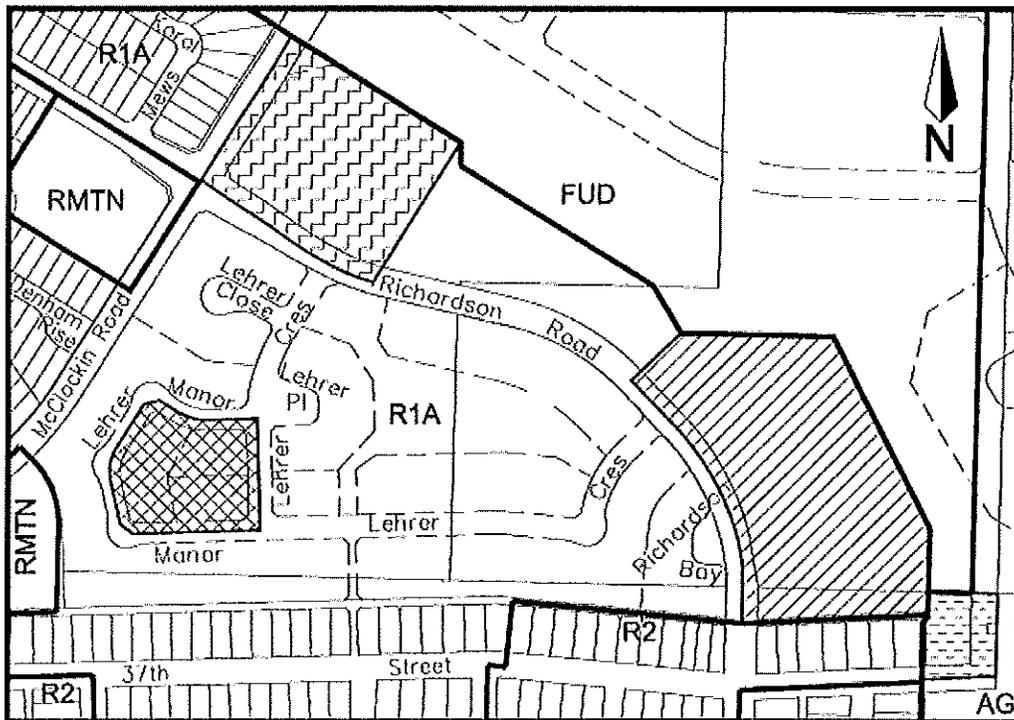
Mr. Kurt Soucy, Chair

1.

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z11/12	<b>PROPOSAL</b> Proposed Rezoning from R1A to RM3, RMTN, and R1B; and from AG to R1A	<b>EXISTING ZONING</b> R1A and AG
<b>LEGAL DESCRIPTION</b> Parcels AA, BB, CC, Lots 1 to 16, Block 24; and Parcel L, Lots 9 and 10, Block 664; and Lot L, Block 664, Plan No. 69S08033		<b>CIVIC ADDRESS</b> Richardson Road, Lehrer Manor, Lehrer Crescent, and 37 <sup>th</sup> Street West
		<b>NEIGHBOURHOOD</b> Hampton Village
<b>DATE</b> July 16, 2012	<b>APPLICANT</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7	<b>OWNER</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7

### LOCATION PLAN



**PROPOSED REZONING**

From R1A to RMTN — From AG to R1A —

From R1A to RM3 — From R1A to R1B —

**City of Saskatoon**  
Planning & Development Branch

File No. RZ11-2012

**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

That a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendment to the Zoning Bylaw No. 8770 to rezone the land parcels located on Richardson Road, Lehrer Manor, Lehrer Crescent, and a utility parcel (see Attachment 2) from R1A – One-Unit Residential District to RM3 – Medium Density Multiple-Unit Dwelling District, RMTN – Townhouse Residential District, and R1B – Small Lot One-Unit Residential District; and AG – Agricultural District to R1A – One-Unit Residential District, be approved based on the reasons outlined in this report.

**B. PROPOSAL**

The Planning and Development Branch has received an application from the City of Saskatoon, Land Branch requesting that the land parcels located on Richardson Road, Lehrer Manor, Lehrer Crescent, and a utility parcel (see Attachment 2) be rezoned as follows:

- 1) R1A – One-Unit Residential District to RM3 – Medium Density Multiple-Unit Dwelling District - this property (Parcel AA) is located on the northern corner of Richardson Road and McClocklin Road;
- 2) R1A – One-Unit Residential District to RMTN – Townhouse Residential District - these properties (Parcels BB and CC) are located adjacent to Richardson Road in the south east corner of the subject area;
- 3) R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District - these properties (Block 24) are surrounded by Lehrer Manor in the south west portion of the subject area; and
- 4) AG – Agricultural District to R1A – One-Unit Residential District - this property is located at the very outset of the south east corner of the subject area, adjacent to the

termination point of 37<sup>th</sup> Street West.

The remaining residential lots shown on the attached Location Plan of Proposed Subdivision (see Attachment 2) will retain the current R1A zoning designation.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed rezoning will permit the development of the aforementioned lands in a manner which is consistent with the Hampton Village Neighbourhood Concept Plan (Concept Plan).

**D. BACKGROUND INFORMATION**

The subject sites are currently zoned AG and R1A and are greenfield properties located within the most easterly section of Hampton Village, which is in the final phase of neighbourhood development. The Concept Plan for Hampton Village was approved by City Council in 2004 and provides a wide range of housing options, which included the RM3 and RMTN properties identified in the administrative report. In October 2011, City Council approved a minor Concept Plan Amendment that moved the R1B – Small Lot Residential area to the parcel bound by Lehrer Manor. In addition to this, there was a reconfiguration of the utility parcel (dry pond) that did not affect the overall size of that parcel. At the time leading up to the public hearing, public notice was sent to the Hampton Village Community Association and a notice was advertised in The StarPhoenix.

In order to accommodate future development, the proposed Zoning Bylaw No. 8770 amendments will change the zoning designations for the specified areas of the neighbourhood to permit residential development.

**E. JUSTIFICATION**

1. Community Services Department Comments

a) Planning and Development Branch

The proposed rezoning is in compliance with the approved Concept Plan and will accommodate a diversity of housing types in the Hampton Village neighbourhood. Future development on this site will comply with the development standards identified in the R1A, RM3, RMTN, and R1B Zoning Districts. As such, the Planning and Development Branch has no concerns regarding the proposed rezoning of the identified properties.

2. Comments by Others

a) Infrastructure Services Department

The proposed Zoning Bylaw No. 8770 amendment is acceptable to the Infrastructure Services Department.

Please note that the properties zoned RMTN and RM3 may require a Traffic Impact Study upon development of the parcels.

b) Utility Services Department, Transit Services Branch

At present, the Transit Branch's closest bus stop is approximately 450 metres from the above referenced properties, located on the east side of East Hampton Boulevard and north of McClocklin Road.

Bus service is at 30 minute intervals Monday to Saturday and at 60 minute intervals after 18:00 Monday to Friday, early Saturday mornings, Sundays, and statutory holidays.

**F. COMMUNICATION PLAN**

Once this application has been considered by the Municipal Planning Commission, it will be advertised in accordance with Public Notice Policy No. C01-021, and a date for a public hearing will be set. The property owners affected by this rezoning will be notified in writing. The Planning and Development Branch will also notify the Community Consultant and the Ward Councillor of the public hearing date by letter. A notice will be placed in The StarPhoenix two weeks prior to the public hearing. Notice boards will also be placed on the site.

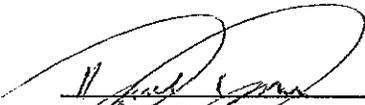
**G. ENVIRONMENTAL IMPLICATIONS**

There are no environmental and/or greenhouse gas implications.

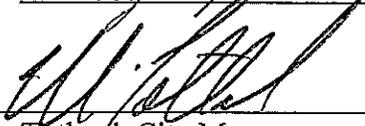
**H. ATTACHMENTS**

1. Fact Summary Sheet
2. Location Plan

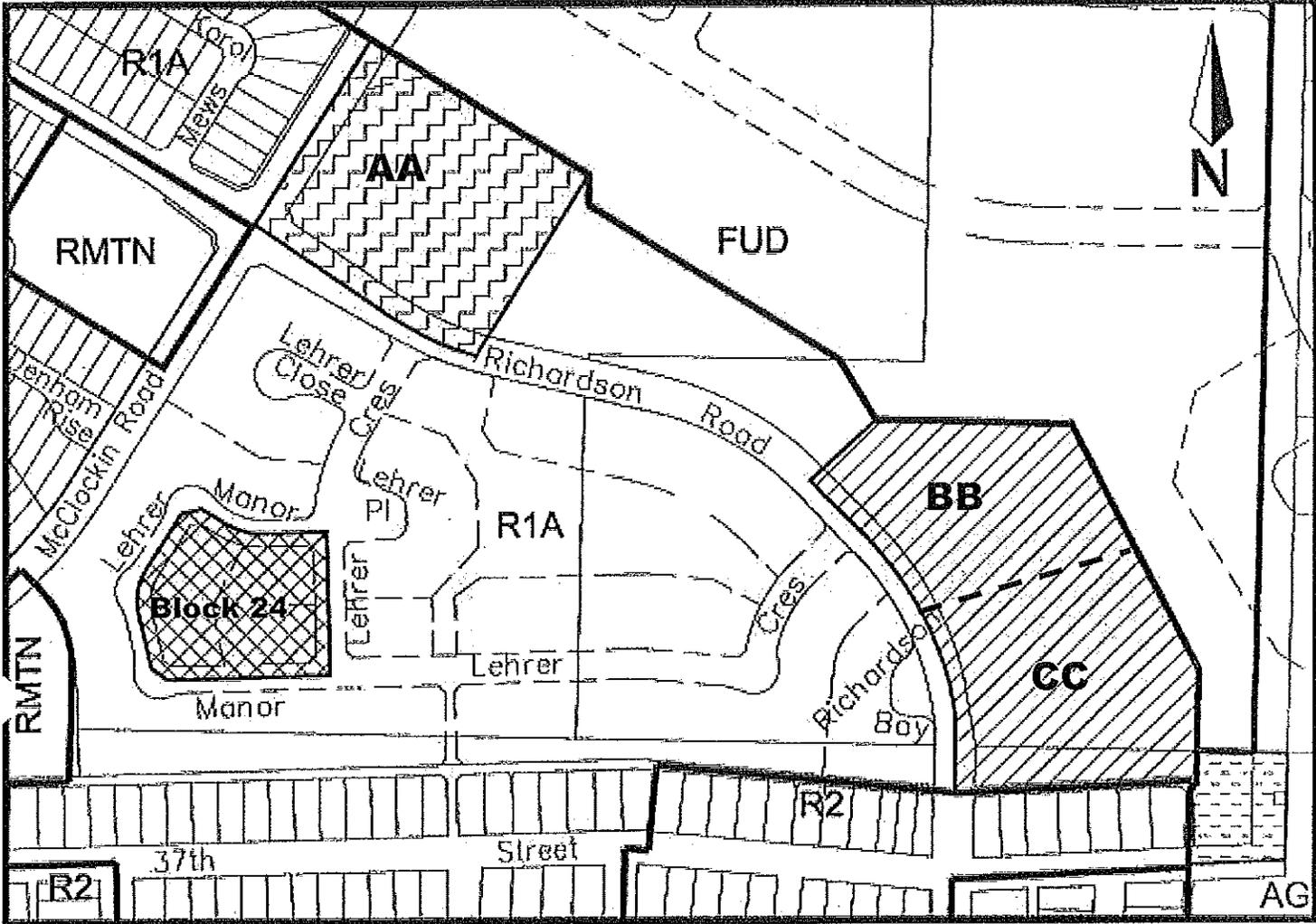
Written by: Daniel Gray, Planner 16

Reviewed by:  For  
Alan Wallace, Manager  
Planning and Development Branch

Approved by:   
Randy Grauer, General Manager  
Community Services Department  
Dated: July 31, 2012

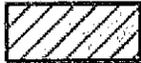
Approved by:   
Murray Totland, City Manager  
Dated: Aug 11/12

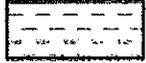
<b>FACT SUMMARY SHEET</b>	
<b>A. Location Facts</b>	
1. Municipal Address	Richardson Road, Lehrer Manor, Lehrer Crescent, and Right-of-Way Parcel
2. Legal Description	Parcels AA, BB, CC, Lots 1 to 16, Block 24; and Parcel L, Lots 9 and 10, Block 664; and Lot L, Block 664, Plan No. 69S08033
3. Neighbourhood	Hampton Village
4. Ward	4
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Residential – R1A
2. Proposed Use of Property	Residential – RM3, RMTN, and R1B
3. Adjacent Land Uses and Zoning	Residential and FUD
North – Aerogreen Neighbourhood	Future Urban Development - FUD
South – Westview Neighbourhood	Residential – R1A and R2
East – Aerogreen Neighbourhood	Future Urban Development - FUD
West – Hampton Village Neighbourhood	Residential – R1A
4. No. of Existing Off-Street Parking Spaces	N/A
5. No. of Off-Street Parking Spaces Required	N/A
6. No. of Off-Street Parking Spaces Provided	N/A
7. Site Frontage	N/A
8. Site Area	N/A
9. Street Classification	Richardson Road – Major Collector Richardson Bay – Local Street Lehrer Crescent – Local Street Lehrer Manor – Local Street
<b>C. Official Community Plan Policy</b>	
1. Existing Official Community Plan Designation	Residential
2. Proposed Official Community Plan Designation	Residential
3. Existing Zoning District	R1A
4. Proposed Zoning District	RM3, RMTN, and R1B



**PROPOSED REZONING**



From R1A to RMTN —   
 From R1A to RM3 — 

From AG to R1A —   
 From R1A to R1B — 

File No. RZ11-2012

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z14/12	<b>PROPOSAL</b> Proposed New Commercial Zoning District – B5B Broadway Proposed New Overlay Zoning District – AC2 – B5B Architectural Control Overlay Zoning District	<b>EXISTING ZONING</b>
<b>LEGAL DESCRIPTION</b>	<b>CIVIC ADDRESS</b>	
	<b>NEIGHBOURHOOD</b>	
<b>DATE</b> July 17, 2012	<b>APPLICANT</b> City of Saskatoon Planning and Development Branch 222 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 0J5	<b>OWNER</b>

### LOCATION PLAN

A. **COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

that a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising respecting the proposal to amend Sections 13.1.3 of Zoning Bylaw No. 8770 as outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation to amend Zoning Bylaw No. 8770 to add the proposed B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District, and that parking and sign regulations contained in Zoning Bylaw No. 8770 are amended to reflect the addition of the B5B Zoning District.

B. **PROPOSAL**

The Planning and Development Branch is requesting that Zoning Bylaw No. 8770 be amended to add the B5B Broadway Commercial Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District. The addition of the B5B Zoning District also requires amendments to the sign and parking regulations contained in Section 6.0 and Appendix A of Zoning Bylaw No. 8770 (see Attachments 2 and 3).

C. **REASON FOR PROPOSAL (By Applicant)**

The proposed commercial B5B Zoning District is intended to be applied in the Broadway Avenue commercial area. This district will provide development standards to ensure that new development enhances the existing urban environment along this unique commercial corridor.

The proposed Architectural Control Overlay Zoning District is also intended to be applied in the Broadway Avenue commercial area. This Architectural Control Overlay Zoning District will ensure that new buildings reinforce and enhance the best qualities of the Broadway commercial area.

The B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District is intended to be applied to the Broadway area, which is currently zoned B5, as indicated on Attachment 1. The rezoning of these properties is covered under a separate report.

**D. BACKGROUND INFORMATION**

In 2007, the Broadway Business Improvement District (BID) and the Nutana Community Association proposed that a study of the Broadway commercial area be undertaken. It was noted by both groups that some of the recommendations contained in the 2001 Nutana Local Area Plan (LAP) had become outdated, other recommendations appeared to be difficult to implement due to community or property owner resistance, and there were concerns about the future character of Broadway Avenue. The City of Saskatoon (City) was invited to participate as a partner with the Broadway BID and Nutana Community Association in the development of a "Broadway Area Plan" to address outstanding recommendations in the Nutana LAP.

The Planning Partnership, a Toronto-based urban planning consultant firm, was hired to prepare what would become the Broadway 360 Development Plan (Plan). The Plan involved a thorough public consultation process that engaged residents, business and commercial property owners, area schools, churches, and those representing Saskatoon's heritage community.

The Plan explored practical urban development solutions to address land use, street character, safety, parking, and traffic issues in the Broadway area. During its September 28, 2009 meeting, City Council received the Plan.

A Steering Committee that was struck during the creation of the Plan was comprised of developers, commercial property owners, Nutana residents, the Nutana Community Association, the BID, and the Ward Councillor. Following City Council receiving the Plan, the Steering Committee met and prioritized the recommendations. The items that were identified as having the highest priority were those concerning land use. These included a new zoning district that would put greater controls on form and massing of building and implementation of an architectural control district. There was initially a lack of consensus within the Steering Committee regarding these proposed development standards.

The Administration worked very closely with the Steering Committee over several months to prepare detailed zoning requirements that would be acceptable to the various stakeholders. The Administration and the Steering Committee met 13 times over the course of 20 months to prepare these proposed Zoning Bylaw No. 8770 amendments.

During its April 16, 2012 meeting, City Council approved an implementation strategy for the Plan. This implementation plan indicated that two tasks, adoption of the recommended development standards and consideration of an architectural control district, would be undertaken in 2012.

E. JUSTIFICATION

1. Community Services Department Comments

a) Official Community Plan Bylaw No. 8769

This area is designated as “Special Area Commercial” on the Nutana Land Use map contained in Official Community Plan Bylaw No. 8769. The proposal to apply a new zoning treatment and architectural control district in the Broadway Avenue commercial area is consistent with the objectives and policies in related to Special Area Commercial Areas, as stated in Section 6.4 of the Official Community Plan:

“Historic Commercial Areas

The Special Area Commercial designation has been applied to certain commercial lands along 20<sup>th</sup> Street, 33<sup>rd</sup> Street, Central Avenue and Broadway Avenue, primarily due to their long and unique development history. In general, these areas contain a built form that is oriented to pedestrians, with limited front or side yard setbacks, and with a relatively high density of development. As a consequence, the Zoning Bylaw shall prescribe development standards for these areas which reflect their unique character, while also promoting compatibility with surrounding residential land use.

Specific local area plans or design studies may also be undertaken in these areas to further define future land use patterns and design and development standards.”

b) Planning and Development Branch Comments

i) Proposed B5B Zoning District

The purpose of the B5B district is to recognize the historic Broadway Avenue commercial area and to facilitate mixed-use developments, including a range of commercial, institutional, and medium to high density residential (see Attachment 2). The following table summarizes the differences between the existing B5 and the proposed B5B Zoning District.

B5 Zoning District (Current Regulations)	PROPOSED B5B ZONING DISTRICT
<b>PERMITTED USES</b>	
There are a wide range of uses permitted in the B5 district. This district permits commercial, institutional (office), and residential uses.	The same uses as permitted in B5, with the following vehicle-oriented uses being deleted: a) service stations b) car washes c) motor vehicle, marine, and trailer coach sales establishments d) public garages
<b>SETBACK REQUIREMENTS</b>	
<b>Front Yard Setback – Building Base:</b> a) no minimum for most uses b) 3 metres for vehicle-oriented use c) 6 metres for multiple-unit dwellings	a) no minimum b) maximum setback – A minimum of 70 percent of the aggregate width of the front building line shall be located within one metre of the front property line
<b>Front Yard Setback – Building Cap:</b> a) no current standard	a) Minimum setback of 3 metres up to 3 storeys above the building base and 0.6 metres for every storey above 3, to a maximum setback of 6 metres
<b>Side Yard Setback – Building Base:</b> a) zero, with exceptions	a) no minimum; however, where a B5B District abuts an R District without the intervention of a street or lane, an abutting side yard shall be provided of at least 1.5 metres
<b>Side Yard Setback – Building Cap:</b> a) no current standard	a) the side yard setback shall be increased by 0.3 metres for each additional storey above the building base, to a maximum of 3 metres
<b>Rear Yard Setback – Building Base:</b> a) zero, with exceptions	a) no minimum; however, where a B5B District abuts an R District, a rear yard shall be provided of at least 3.0 metres
<b>Rear Yard Setback – Building Cap:</b> a) no current standard	a) no minimum; however, where a B5B District abuts an R District, the rear yard setback shall be increased by 0.6 metres for each additional storey above the building base to a maximum of 7.5 metres
<b>BUILDING HEIGHT</b>	
<b>Building Base:</b> a) 76 metres for entire building	a) shall be a minimum of 7.5 metres and a maximum of 12 metres
<b>Building Cap:</b> a) 76 metres for entire building	a) no maximum height limit, however, other factors govern height
<b>GROSS FLOOR AREA RATIO</b>	
a) 5:1 – site width less than 15 metres b) 7:1 – site width between 15 metres and 30 metres c) 10:1 – site width greater than 30 metres	a) the gross floor area ratio shall not exceed 7:1

PARKING REQUIREMENTS FOR MULTIPLE-UNIT DWELLINGS	
a) 1.25 per dwelling unit plus 0.125 visitor spaces	a) parking for multiple-unit dwellings shall be provided at the rate of 1.0 space per dwelling unit, plus 0.125 visitor spaces per dwelling unit
b) units smaller than 50 m <sup>2</sup> – 1.0 spaces per unit	

The intent of the B5B District is to ensure that buildings have an identifiable base and cap. The base of the building would have minimal setbacks while the building cap setback would provide appropriate sunlight penetration and ensure that development has an appropriate pedestrian scale.

The proposed B5B Zoning District does not contain a maximum height requirement. The development standards contained in the B5B District utilize tools that will limit the maximum building height. Those factors that have an effect on height include: gross floor area ratio of 7:1, site size, height of base building (must be a minimum 7.5 metres to a maximum of 12 metres), setback and stepbacks, and parking (both the amount of required parking and where it is located on the site).

Amendments are also required to the signage and parking requirements contained in Zoning Bylaw No. 8770 to reflect the addition of the B5B Zoning District. The regulations for parking and signage will be the same as those currently applied in the B5 and B6 Districts.

ii) Proposed AC2 – B5B Architectural Control Overlay District

It is recommended that an Architectural Control District (ACD), including specific Architectural Control Guidelines, be adopted in conjunction with the above noted B5B District (see Attachment 3).

The ACD will provide direction to ensure high quality architectural design for new construction in the Broadway commercial area. The ACD is intended to allow for flexibility and foster creativity in building design.

It is proposed that the ACD will be applied to the construction of all new buildings and where City funding is being requested under programs, such as the Heritage Conservation Program, the Facade Renovation and Rehabilitation Program, or the Affordable Housing Program.

Property owners and developers will be encouraged to follow the guidelines in any other cases.

As noted in Appendix 3, the ACD will contain 14 design guidelines that address the following: building expression, orientation and placement, street wall, heritage contexts, corner sites, storefronts, residential street access units, roof treatment, above-grade parking, material and architectural quality, sidewalk cafes, building lighting, signage, and sustainable design.

The review process for the ACD will be administered by the Planning and Development Branch, Community Services Department, in the same manner as currently undertaken for development projects in River Landing. A development review committee consisting of design professionals (community planners, landscape architects, and other architects) will review each application. Approvals under the ACD are proposed to be delegated to the Administration.

iii) Compatibility with Adjacent Land Uses

These new development standards should reinforce the best qualities that characterize the Broadway Area and ensure an appropriate interface with existing heritage resources and the adjacent low-rise residential neighbourhood. The new B5B Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District are intended ensure that all new development adheres to the development principles contained in the Plan.

4. Comments by Others

a) Infrastructure Services Department

1. The allowed land uses within the proposed zoning district vary widely in capacity use from a water and sewer perspective. High density/high capacity, such as hotels and multi-story residential, may significantly affect water and sewer concerns with respect to fire flows and sanitary sewer capacity. The wide variation makes it very difficult to determine if any water and sewer conditions exist. Storm sewer capacity is not a concern.
2. With varied land uses, it is possible for the first high density new land development (i.e. a multi-story residential or hotel) to effectively consume all the available sanitary sewer capacity in the district, thereby “sterilizing” the area for other high density uses.

Since zoning is the only control for regulating land use it would be prudent to either determine a method of regulating high density uses for the zoning district or determine a levy payment method so that the first user does not benefit from “free” existing capacity while future users must pay for all additional upgrades to water and sewer infrastructure.

Note: The proposed B5B Zoning District will be applied in the area currently zoned B5 in the Broadway Commercial Area. The B5B District limits the maximum development potential in this area compared to what is currently permitted in the B5 District.

The Integrated Growth Plan will be addressing issues related to the financing of infrastructure upgrades necessary to accommodate infill development.

b) Transit Services Branch, Utilities Services Department

At present, Saskatoon Transit has bus stops throughout the district. Service is at 15 minute intervals Monday to Saturday; at 30 minute intervals evenings, early Saturday mornings, Sundays, and statutory holidays.

F. ENVIRONMENTAL IMPLICATIONS

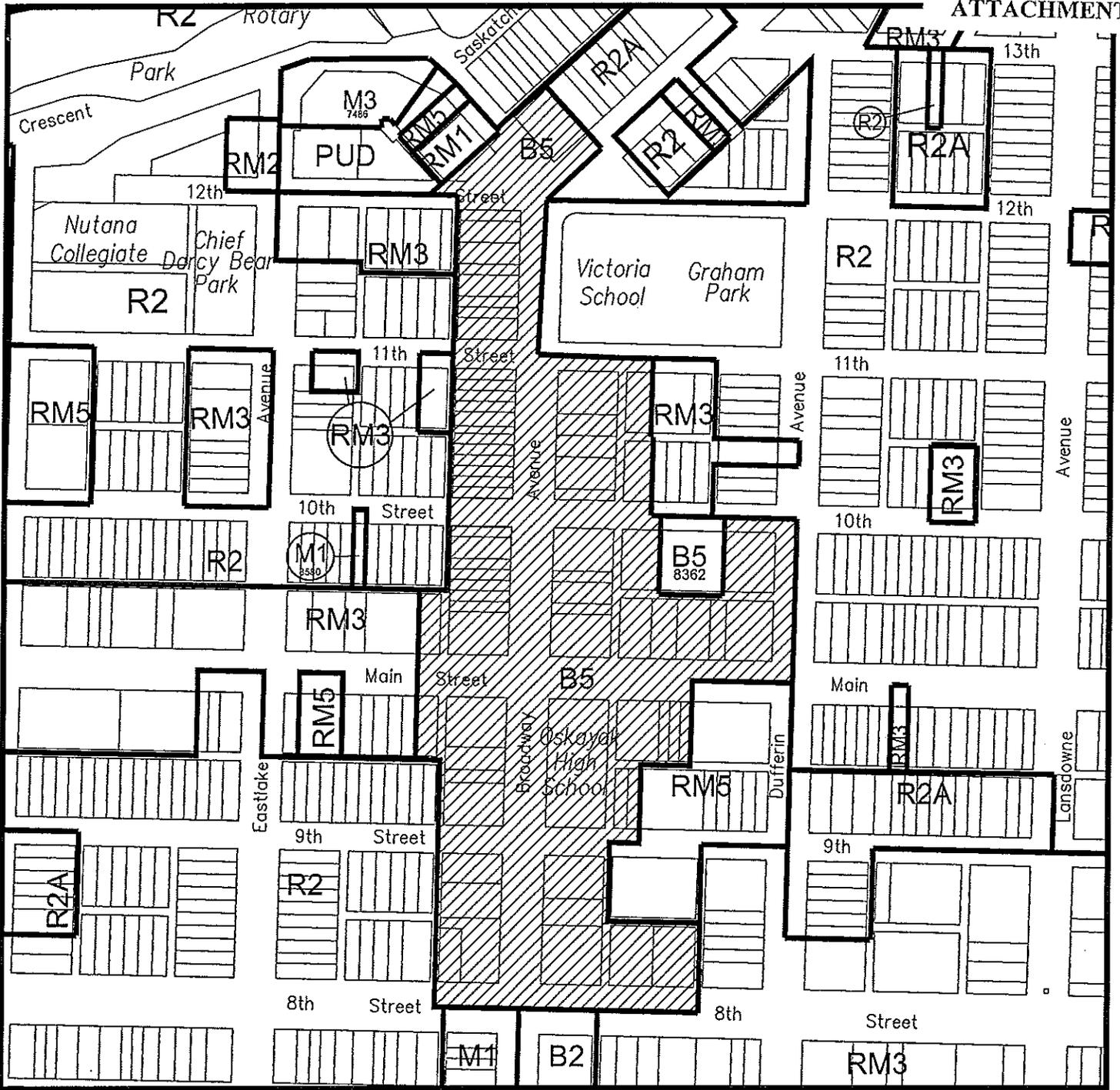
There are no environmental implications and/or greenhouse gas implications.

G. COMMUNICATION PLAN

As noted above, this Plan has been undertaken as a joint initiative of the Broadway BID, the Nutana Community Association, and the City. Extensive public consultation was undertaken throughout the Broadway 360 study process.

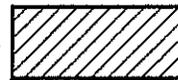
In addition, a public open house was held on January 12, 2012, at the Cosmopolitan Senior Citizens Centre on 10<sup>th</sup> Street in Nutana. A presentation by your Administration was followed by a question and answer period. A technical workshop was also held on January 14, 2012. Notices for these consultations were distributed throughout the area by flyer drop, in the Nutana Community Association newsletter, and by direct mail to Broadway commercial property owners and business owners.





# REZONING

From B5 to B5B and B5B(ACD2)



## X.X. B5B - Broadway Commercial District

### X.X.1 Purpose

The purpose of the B5B District is to recognize the historic Broadway Commercial area and facilitate mixed use development including a range of commercial, institutional and residential uses in medium to high density form.

### X.X.2 Permitted Uses

The Permitted Uses and Minimum Development Standards in a B5B District are set out in the following chart:

B5B District	Minimum Development Standards (in Metres)						
	Site Width	Site Area (m <sup>2</sup> )	Front Yard	Side Yard	Rear Yard	Building Height (Max.)	Amenity Space Per Unit (m <sup>2</sup> )
<b>10.8.2 Permitted Uses</b>							
(1) Hotels and motels	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(2) Restaurants and lounges	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(3) Bakeries	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(4) Dry cleaners	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(5) Theatres	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(6) Commercial recreation uses	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(7) Photography studios	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(8) Retail stores	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(9) Personal service trades and health clubs	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(10) Offices and office buildings	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(11) Medical clinics	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(12) Medical, dental and optical laboratories	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(13) Financial institutions	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(14) Private schools and educational institutions	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(15) Repair services restricted to the repair of household goods and appliances	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(16) Places of worship	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(17) Public halls and community centres	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(18) Private clubs	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(19) Libraries, art galleries and museums	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(20) Funeral homes	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-

B5B District	Minimum Development Standards (in Metres)						
	Site Width	Site Area (m <sup>2</sup> )	Front Yard	Side Yard	Rear Yard	Building Height (Max.)	Amenity Space Per Unit (m <sup>2</sup> )
<b>10.8.2 Permitted Uses (continued)</b>							
(21) Radio or television studios	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(22) Motion picture or recording studios	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(23) Duplicating or copying centres	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(24) Dwelling units in conjunction with and attached to any other permitted use	-	-	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(25) Multiple-unit dwellings	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(26) Commercial parking lots	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(27) Storage garages	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(28) Banquet halls	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(29) Catering halls and catering kitchens	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(30) Neighbourhood recycling and collection depots	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(31) Parking stations	7.5	225	0 <sub>1</sub>	Refer to Section 6.0.			
(32) Shopping centres	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(33) Veterinary clinics	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(34) Commercial printers	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(35) Public elementary & high schools	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(36) Boarding apartments	15	450	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(37) Boarding houses	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(38) Custodial care facilities - Type I & II	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(39) Small animal grooming	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(40) Accessory buildings and uses	-	-	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-

### X.X. 3 Discretionary Uses

The Discretionary Uses and Minimum Development Standards in a B5B District are set out in the following chart:

B5B District	Minimum Development Standards (in Metres)						
	Site Width	Site Area (m <sup>2</sup> )	Front Yard	Side Yard	Rear Yard	Building Height (Max.)	Amenity Space Per Unit (m <sup>2</sup> )
<b>10.8.3 Discretionary Uses</b>							
(1) Special needs housing	15	560	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	5
(2) Child care centres and pre-schools	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(3) Adult day care centres - Type I & II	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(4) Custodial care facilities - Type III	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-
(5) Night clubs and taverns	7.5	225	0 <sub>1</sub>	0 <sub>2</sub>	0 <sub>3</sub>	-4	-

#### **X.X. 4 Notes to Development Standards**

1. (a) *Building Base:* a minimum of 70% of the aggregate width of the front building line shall be located within 1.0 metre of the front property line.
- (b) *Building Cap:* a minimum front yard shall be provided of 3.0 metres from the front property line up to three storeys from the front property line shall be provided for every storey above the three storey building cap, however, the minimum setback of the building cap shall not exceed 6.0 metres from the front property line.
2. (a) *Building Base:* where a B5B District abuts an R District without the intervention of a street or lane, an abutting side yard shall be provided of at least 1.5 metres.
- (b) *Building Cap:* setback increased by 0.3 metres for each additional story above the building base to a maximum of 3.0 metres.
3. (a) *Building Base:* where a B5B District abuts an R District, a rear yard shall be provided of at least 3.0 metres.
- (b) *Building Cap:* where a B5B District abuts an R District, the rear setback shall be increased by 0.6 metres for each additional story above the building base to a maximum of 7.5 metres.
4. *Building Base:* shall be a minimum of 7.5 metres and a maximum of 12.0 metres. Exceptions may be made for corner sites where the architectural feature is included that may encourage massing and designs that accentuate the visual prominence of the site.

#### **X.X.5 Signs**

The regulations governing signs in a B5B District are contained in **Appendix A - Sign Regulations**.

#### **X.X.6 Parking**

The regulations governing parking and loading in a B5B District are contained in **Section 6.0**.

#### **X.X.7 Gross Floor Space Ratio**

- (1) The gross floor space ratio shall not exceed 7:1.
- (2) In the B5B District, above grade parking floor areas shall not be exempt from the calculation of the gross floor space ratio.
- (3) Notwithstanding Section (2), above grade parking floor areas which are needed to provide 1.25 parking spaces per dwelling unit and parking floor areas which are needed to provide parking at the rate of one parking space for every 24 square metres of gross leasable floor area for all other uses shall be exempt from the calculation of Gross Floor Space Ratio.
- (4) Floor areas used for below grade parking shall be deducted from the exemption outlined in Section (3), at the rate outlined in said Section.

#### **X.X.8 Landscaping**

- (1) On sites used for, commercial parking lots, parking stations or multiple-unit dwellings a landscaped strip of not less than 3 metres in depth throughout lying parallel to and abutting the front site line shall be provided and shall be used for no purpose except landscaping and necessary driveway access to the site, and on corner lots, in addition to the landscaping required in the front yard, a landscaped strip of not less than 1.5 metres in width throughout lying parallel to the flanking street shall be provided.

In addition, on sites used for commercial parking lots or parking stations located at grade level, screening of the site from front streets, flanking streets and public lanes shall be provided to the satisfaction of the Development Officer.

#### **X.X.9 Garbage Pickup Area**

- (1) A space to be used exclusively for garbage storage and pickup, having minimum dimensions of 2.7 metres by 6.0 metres, shall be provided on each site to the satisfaction of the General Manager, Utility Services Department. The required loading and pick up spaces may be combined where considered appropriate by the Development Officer.

## Theme

# *Broadway*

# *Heritage*

# *Core*

“The Broadway area comprises the ‘heart’, ‘town centre’, and ‘main street’ of the Nutana community. Established as a temperance colony in the late 19th century, Nutana is one of Saskatoon’s most desirable neighbourhoods, due in large part to its historic ‘small town’ charm, the success of Broadway Avenue commercial area, and the proximity to the Downtown and the University of Saskatchewan.

Neighbourhoods are constantly changing and evolving, and Broadway Avenue and the Nutana Neighbourhood is no exception. At the heart of Saskatoon’s original neighbourhood, the Broadway area has been the social and commercial core for over 125 years. In more recent years, the area’s central location, and its unique history can character, and the distinct mix of restaurants and stores has attracted new residents and visitors to the neighbourhood, contributing to the revitalization that has made Broadway Avenue a region-wide destination.”

Notwithstanding the intent of this document to control aspects of architectural development in the Broadway Commercial Area, the controls or guidelines provide direction for the quality of design for a variety of built form elements. As a means for ensuring that new buildings reinforce and enhance the best qualities of the Broadway area. Therefore the projects must support the theme of “Broadway Heritage Core “.

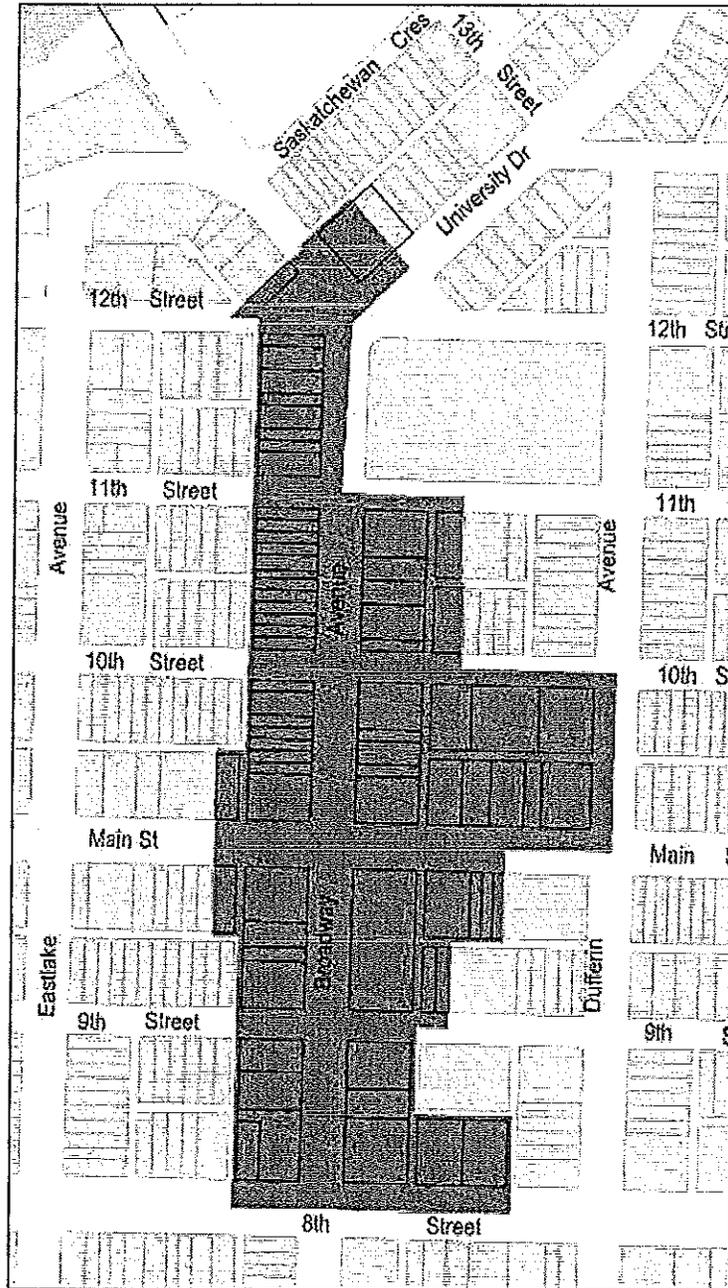
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## Development Principles

A central objective is to ensure that new buildings reinforce a coherent, harmonious and appealing urban environment, as well as contribute to the enhancement of the public realm. Informed by the consultation process and rooted in good planning and urban design practice, the key principles include:

- ❖ Protecting heritage resources and retaining their visual prominence.
- ❖ Protecting and strengthening established low-rise residential areas and ensuring compatible infill development and sympathetic developments in adjacent higher density areas.
- ❖ Ensuring base building conditions that form an appropriately scaled and designed street wall that reinforces the desired character at the street level.
- ❖ Ensuring appropriate building massing and height taking into consideration existing and permitted heights; proportional relationships to streets; and, visual and physical impacts on pedestrians and adjacent areas.
- ❖ Ensuring that new developments provide for appropriate transitions between areas of differing intensities and scales.
- ❖ Reinforcing important intersections and corners through massing and design.
- ❖ Well designed and articulated buildings that positively contribute to the quality and animation of the streetscape and the overall defining character and image of the area.

# Map of B5B Area



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# 1

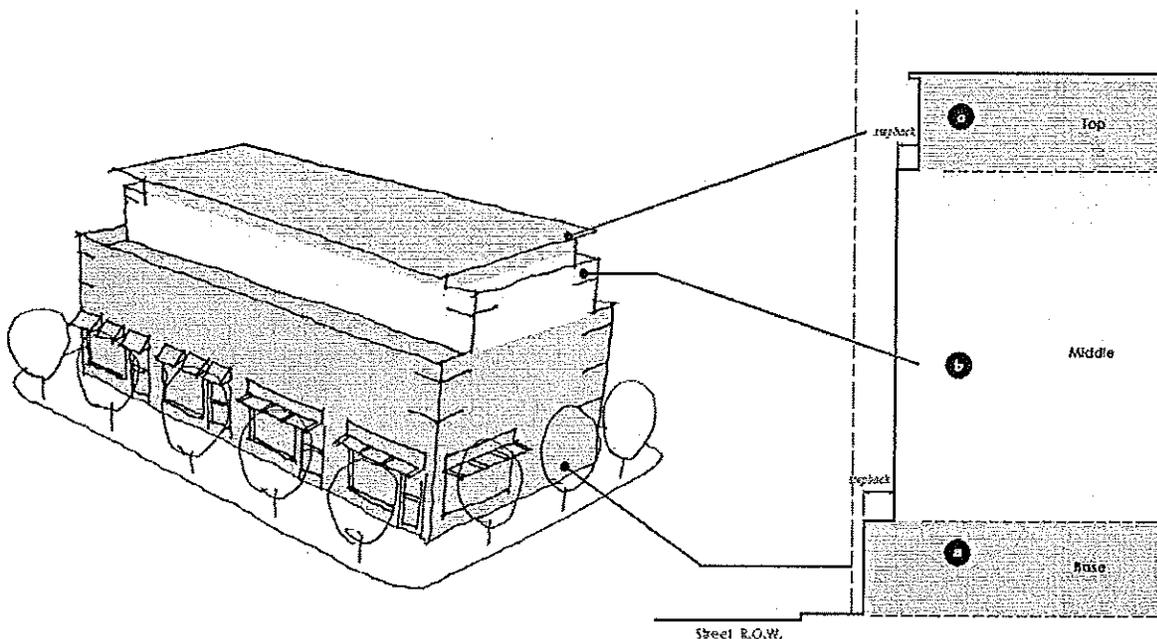
## Building Expression

Buildings should reinforce a *base, middle, and top* in their design.

**Base** – Within the first three storeys of a building, a clearly defined base will contribute to the quality of the pedestrian environment by providing animation, transparency, and articulation.

**Middle** – The body of the building should contribute to the physical and visual quality of the overall streetscape.

**Top** – The roof should be distinguished from the rest of the building and designed to contribute to the visual quality of the streetscape.



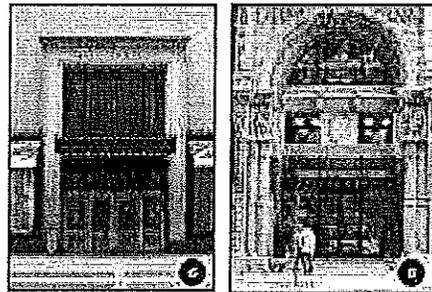
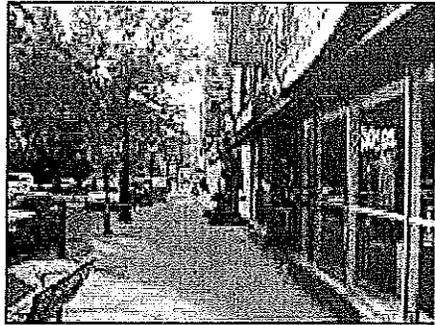
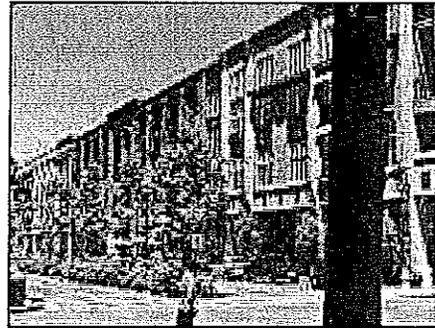
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## 2

### Orientation and Placement

Buildings can enhance the pedestrian environment by creating a sense of enclosure. This is achieved by framing the street with parallel aligned buildings and providing the appropriate levels of animation and use.

- ❖ All buildings should orient to the street with clearly defined entry points that directly access the sidewalk.
- ❖ A minimum of 70% of the front building line shall be located within 1.0 metre of the front property line.

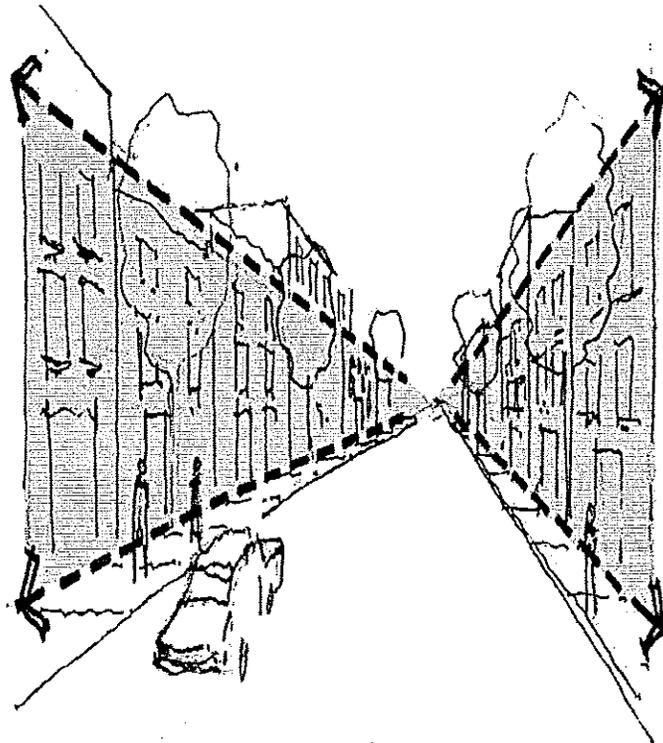


### 3

## Street Wall

The street wall has the greatest impact on the character of the street experience. The key design objectives for street walls in the Broadway Area ensure visual continuity, pedestrian scale, animation and design quality.

- ❖ A street wall of a new building should align with those of neighbouring buildings or have the same setback as the predominant buildings on the block.
- ❖ The height of the street wall should be consistent with historic heights of no greater than 3 storeys and no less than 2 storeys. Levels above the street wall should be set back to reinforce a low-rise interface with the sidewalk.
- ❖ The height of ground-level floors should be visually prominent and no less than 4.5 metres for commercial and 4.0 metres for residential uses.



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## 4

### Heritage Contexts

New buildings on Broadway Avenue should complement, rather than detract from, the character of older buildings.

#### General Guidelines

New buildings should avoid historical misrepresentation by not replicating past architectural styles.

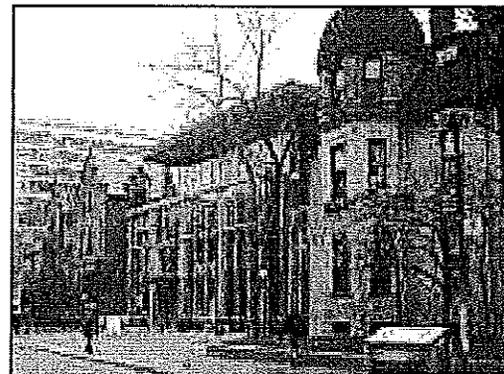
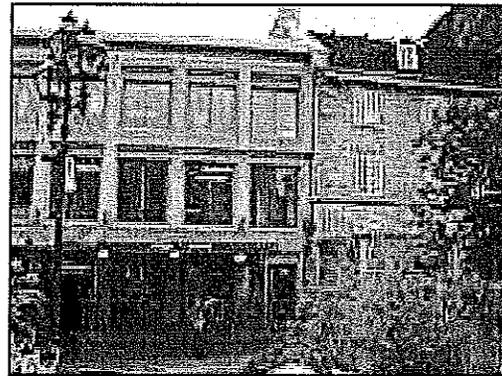
New buildings should consider and respect the scale, material and massing of adjacent heritage significant buildings.

#### Façade Articulation

New buildings should respect the pattern of façade division by ensuring the horizontal and vertical architectural orders, including windows and entries, are aligned with neighbouring heritage buildings or the established pattern on the block.

#### Façade Materials

New buildings should consider materials and colours evident in existing heritage significant properties. Building materials should be chosen for their functional and aesthetic quality. Exterior finishes should exhibit quality of workmanship, sustainability, permanence, and ease of maintenance.

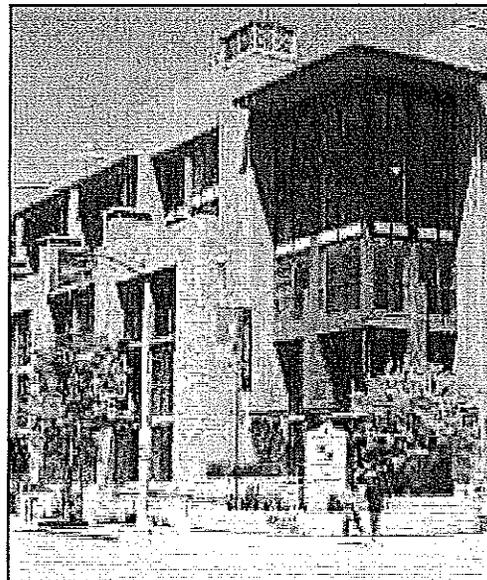


# 5

## Corner Sites

Corner buildings have a greater visual prominence given that they front onto two streets and frame intersections. To enhance the distinction of new buildings at Key Corner Sites, modest exceptions to setbacks and height restrictions should be permitted to encourage massing and designs that accentuate the visual prominence of the site.

- ❖ New developments on all corner sites should orient to both street frontages.
- ❖ Corner entrances should be encouraged wherever possible to address the two street frontages.



## 6

### Storefronts

Well proportioned and designed storefronts can provide animation and visual interest at the sidewalk.

- ❖ To reflect the existing character and context, storefronts should generally have a frontage in the range of 7.5 metres but not greater than 15 metres.
- ❖ Where frontages are greater than 7.5 metres, they should articulate narrow storefronts in the design of the facade.
- ❖ Storefronts should have a minimum of 75% glazing to maximize visual animation.
  - Clear glass should be used for windows and doors along the street-level façade. Dark tinted, reflective or opaque glazing should be discouraged for storefronts.
- ❖ Storefront entrances should be highly visible and clearly articulated. Entrances should be located at or near grade.
- ❖ Storefront signage should be consistent with the signage guidelines, but add diversity and interest to the street.
- ❖ Weather protection for pedestrians is encouraged through the use of awnings and canopies.



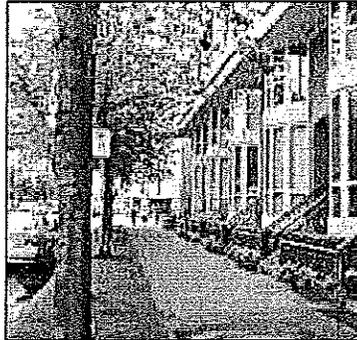
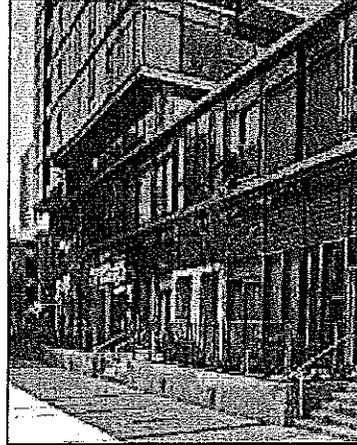
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# 7

## Residential Street Access Units

Where retail is not required, and residential uses are proposed at-grade, the following guidelines apply:

- ❖ Residential uses at-grade should include individual units accessed from the street.
- ❖ Appropriate front yard privacy measures should be considered such as setbacks, landscaping, and porches.
- ❖ Access to the individual units should be clearly visible, and the scale, rhythm and articulation of the street wall should be consistent with the residential character of adjacent neighbourhoods. Grade-level units should be designed to accommodate live-work opportunities and potential conversion into commercial or retail uses.

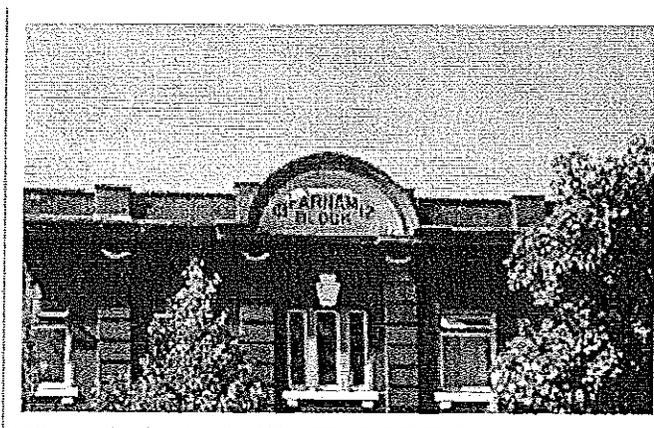
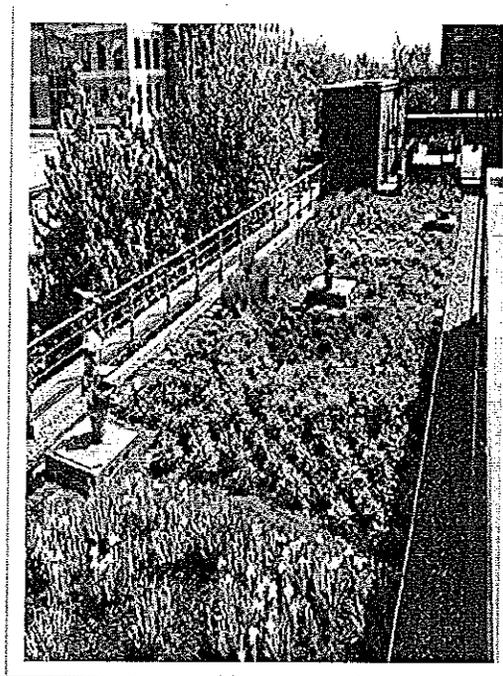


# 8

## Roof Treatment

The design of the roof can make an impact on the character of the streetscape, especially from great distances. Roofs are also seen from other buildings of equal or greater height.

- ❖ The expression of the building top and roof should be clearly distinguished from the rest of the building through treatments such as stepbacks, change in materials, cornices lines, and overhangs.
- ❖ Mechanical penthouses should be integrated with the architectural treatment of roofs and/or screened from view.
- ❖ Green roofs should be encouraged.



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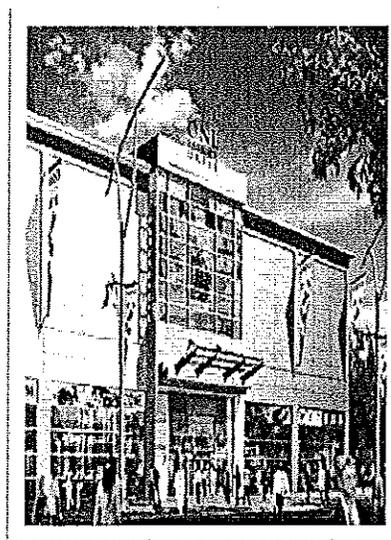
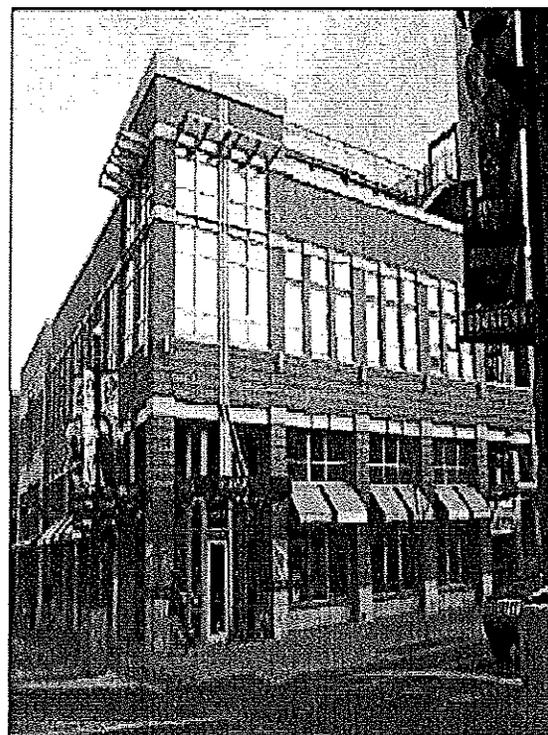
## AC2 – B5B – Architectural Control Overlay District

### 9

#### Above Grade Parking

Wherever possible, parking for new developments should be provided at the rear or below-grade and accessed off the rear lane. However, where parking is provided above-grade within the base building, the following guidelines address the design and quality of such structures.

- ❖ Direct access for parking, loading, and service areas from the street should be discouraged.
- ❖ Where an above-grade parking facility fronts on a street, the ground-level frontage should incorporate retail, public or other active uses.
- ❖ Above-grade parking structures should be designed in such a way that they reinforce the intended built character and blend into the streetscape.



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# 10

## Material & Architectural Quality

New developments should ensure excellence in architectural design and in the use of high-grade materials, particularly at street-level. A key objective of the Broadway 360° Development Plan is to achieve a balance between consistencies in design quality and street interface, while enabling individual expression in new developments. Key guidelines for architectural and material quality include:

- ❖ The Broadway area has a rich history of development that is reflected in the Prairie-style ‘main street’ buildings that are constructed in a variety of materials. New developments should seek to contribute to this mix and variety.
- ❖ Building materials should be chosen for their functional and aesthetic quality and exterior finishes should exhibit quality of workmanship, longevity, sustainability and ease of maintenance.
- ❖ Building materials recommended for new construction include brick, stone, wood, glass, in-situ concrete and pre-cast concrete.
- ❖ In general, the appearance of building materials should be true to their nature and should not mimic other materials.
- ❖ Vinyl siding, plastic, plywood, concrete block, darkly tinted and mirrored glass and metal siding utilizing exposed fasteners should be discouraged.

Recommended Materials	Not Recommended Materials
Brick	Vinyl siding
Stone	Plastic
Wood	Plywood
Glass	Darkly tinted or mirrored glass
In-situ concrete	Concrete Block
Pre-cast concrete	Metal siding with exposed fasteners



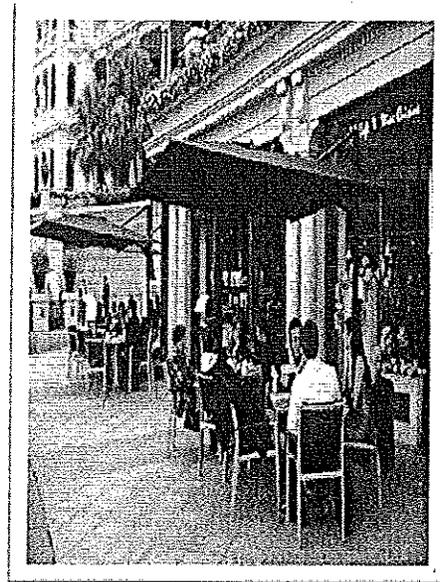
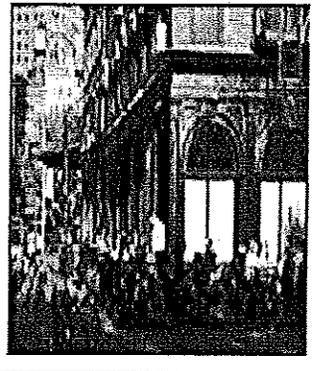
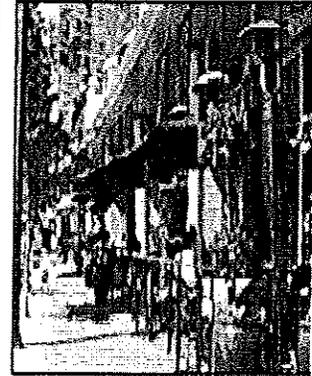
# 11

## Sidewalk Cafes

Sidewalk cafés enhance the vibrancy of street life, further enable social interaction, and are major destinations in the warmer months.

Sidewalk cafés should be encouraged throughout the Broadway Area provided there are no conflicts with adjacent land uses and they are able to be accommodated within the existing sidewalk width dimensions without encumbering pedestrians.

- ❖ Where permitting, small sidewalk cafés should be encouraged along streets with narrower sidewalks as well.
- ❖ Sidewalk cafés should be designed to contribute and integrate into the streetscape.
- ❖ Curb bump-outs should be encouraged at all corners to provide for additional sidewalk café opportunities.
- ❖ Rear yard and roof top patios should be directed to properties that are not directly adjacent to residential neighbourhood.



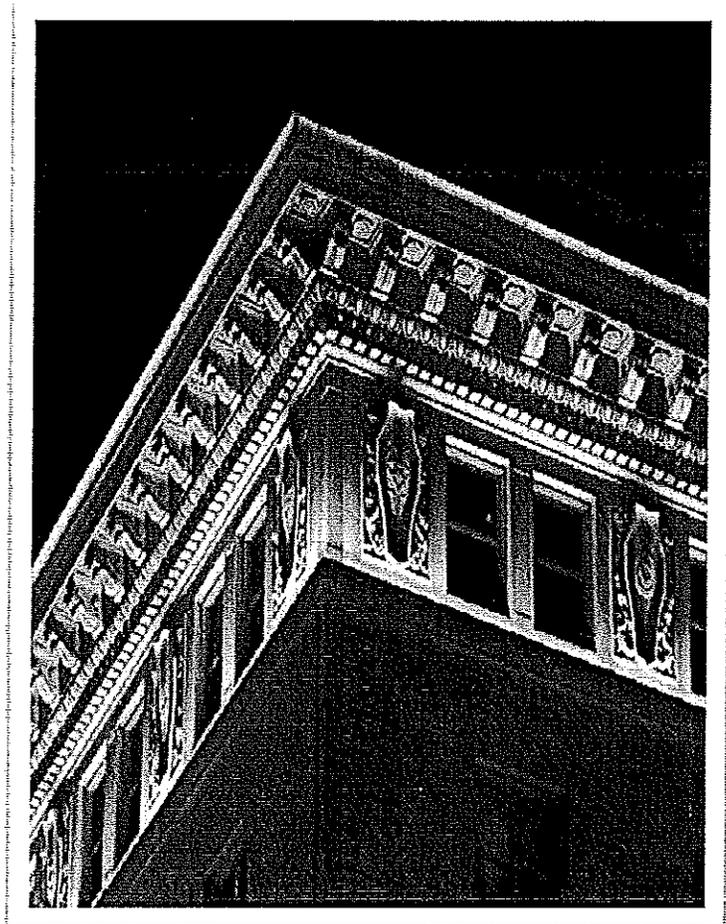
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# 12

## Building Lighting

The image and experience at night is an important aspect of any mixed-use area

- ❖ Attractive landscape and architectural features can be highlighted with spot-lighting or general lighting placement.
- ❖ Heritage and institutional buildings, as well as landmark elements such as public art, steeples or distinctive rooflines, should be illuminated.
- ❖ Subtle night-lighting of retail display windows should be encouraged.



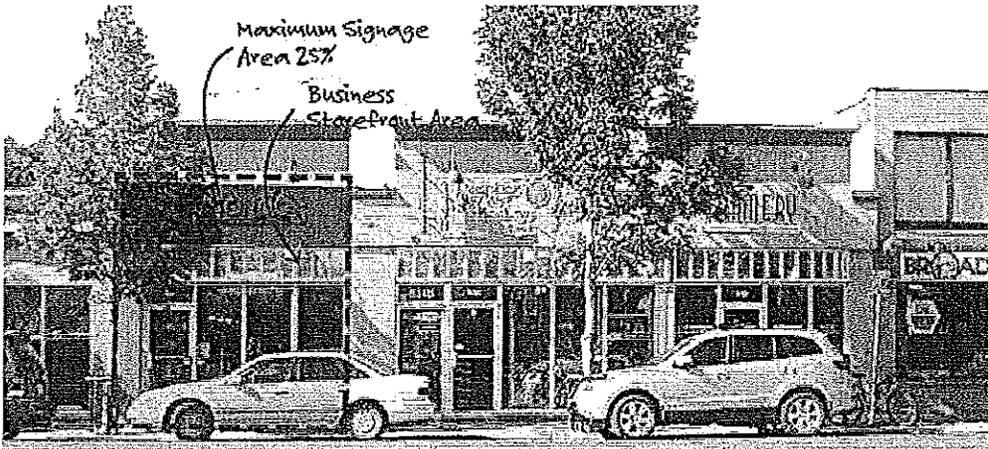
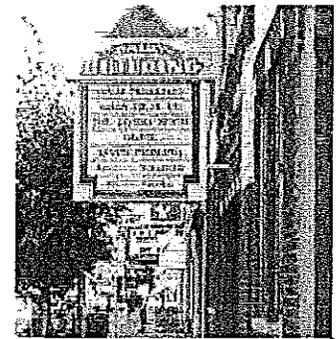
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# 13

## Signage

Signage plays an important role in the overall image of any area. Signs should contribute to the quality of individual buildings and the overall streetscape. This includes compatibility with heritage buildings, where appropriate. High quality, imaginative, and innovative signs are also encouraged.

- ❖ The maximum signage area for storefront signs should be no more than 25% of the business storefront.
- ❖ Back lit illuminated rectangular sign boxes are discouraged.
- ❖ Signage should not obscure windows, cornices or other architectural elements.
- ❖ Signage should aid pedestrians and drivers in navigating the area, especially at night.
- ❖ Billboards, super boards, and roof mounted signs are not permitted.



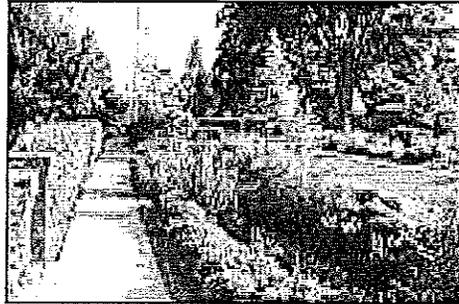
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# 14

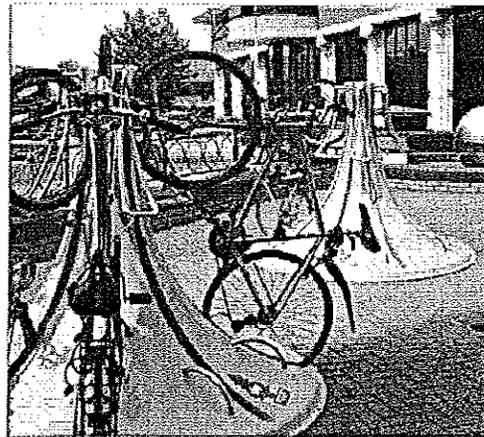
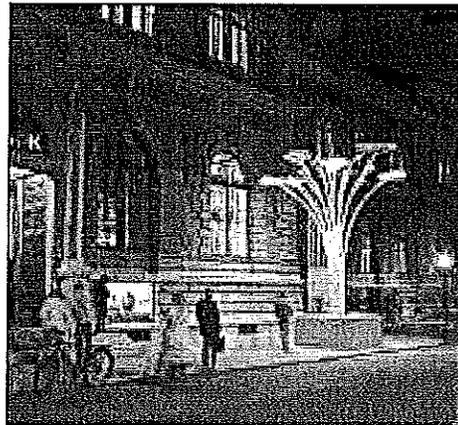
## Sustainable Design

Conservation of natural resources and systems should be a primary consideration in the planning, design, and construction process. To achieve this, all proposed projects should strive for sustainable building practices. This includes public as well as private development, and encompasses streets, parks, and buildings.

New additions, exterior alterations, or related new construction should not destroy historic materials, features, and spatial relationships that characterize the property. The new work should be differentiated from the old and should be compatible with the historic materials, features, size, scale, height, proportion and massing to protect the integrity of the property and its environment.



Good view towards from the street is captured in a natural setting.



## List of Terms

- ❖ **Animation** concerns spaces that have an animated quality; liveliness; movement; activity.
- ❖ **Articulation** is the emphasis or accentuation of different parts of a building so that they are distinct and stand out clearly.
- ❖ **Building setback** is the distance at-grade (ground level) that the building is set back from the property line.
- ❖ **Building stepback** is the distance, above the base building, that the remaining portion of the building (building cap) is set back from the face of the base building.
- ❖ **Cornice** is a decorative molding that crowns a building.
- ❖ **Floor Area Ratio (FAR)** is the ratio of gross floor area of the building divided by the site area.
- ❖ **Glazing** is the part of a wall or window that is glass.
- ❖ **Mechanical Penthouse** is the covering or enclosure on the roof of a building that houses mechanical systems or equipment for the building.
- ❖ **Rhythm**, in architecture, is the repetitive or alternating use of visual elements to create a pattern.
- ❖ **Transparency** refers to the degree to which people can see or perceive what lies beyond the street edge, often through windows, doors, fences and landscaping.
- ❖ **Parapet** is a low wall projecting from the edge of a platform, terrace, or roof.
- ❖ **Storefront** refers to an area on the frontage of a building that is delineated by features to indicate a separate or distinctive “frontage”. Larger building frontages should be divided into narrower storefronts to create visual animation and visual interest at the sidewalk.

**360** BROADWAY  
creating our neighbourhood plan

**Broadway 360 Technical Workshop  
Cosmopolitan Senior Citizens Centre – 614 11<sup>th</sup> Street East  
January 24, 2012  
5:30 pm**

On behalf of the Broadway 360 Steering Committee, Sarah Marchildon, Executive Director of the Broadway Business Improvement District, thanked everyone for attending this technical workshop on proposed land use changes. The purpose of tonight's meeting is to review the proposed B5B Zoning District and proposed Architectural Control District for the Broadway commercial area. Tim Steuart will give a presentation with question period following. After formal part of meeting, the Steering Committee will be around for more one on one discussion.

**Broadway 360 Land Use Implementation  
Tim Steuart, Senior Planner, Business License & Zoning Compliance Section**

A bit of background. This came together from a unique cooperative effort between property owners, residents, and the BID to make Broadway a better place. A comprehensive study was done with the goal of recognizing that Broadway is a special area, a cool area created with pedestrian environment. Everyone involved didn't want to assume it will stay this way forever, but there was a desire try to ensure it does. The Steering Committee has come up with a very good plan. One issue was zoning and to ensure the zoning treatment is satisfactory to all.

All properties in the Broadway Commercial Area currently zoned B5 (Inner-City Commercial Corridor) are proposed to be rezoned to the new B5B (Commercial Zoning District), a zoning district designed exclusively for Broadway.

The new B5B includes changes to the permitted uses and development standards. Also, all B5B-zoned properties would be subject to an Architectural Control District.

Saskatoon is growing strongly and we need to grow up as well as out, creating the urban living room.

- The first three storeys matter most.
- Density done properly has many benefits.
- The stepback enhances the pedestrian experience.

What factors will affect the overall height of a building?

- Gross Floor Area Ratio (7:1) which means that the maximum building volume can be 7 times the site area
- Site size (bigger, taller buildings can be built on larger sites)
- Height of base building (must be a minimum 7.5 metres to a maximum of 12 metres)
- Setback and stepbacks (must meet minimum requirements, setbacks affect the overall building form and height)
- Parking
  - Amount of required parking

- Location of parking (at grade at rear of site, below grade in parking structure, or above grade in parking structure)

Do not want building setback from the street. Requirement that at least 70% of building face the street, setback for courtyard. Removed service stations and commercial parking lot from permitted uses. Parking provision is currently 1.25 parking spaces for residents and 1 visitor for every 8 dwellings. This has been reduced to 1 parking space for residents while visitor parking has remained the same. Nothing set for commercial buildings and not proposing changes as it is usually self-regulating since developers understand that providing parking helps to attract potential tenants, so a minimum parking provision does not seem necessary.

The proposed B5B Zoning District would be subject to an Architectural Control District (ACD) intended to preserve the physical character of the area.

The ACD would contain a set of design guidelines, known as the Broadway Commercial Area Design Plan that all new development in the B5B District must conform to. Establishing an Architectural Control District (ACD) allows for enforceable design controls in the Broadway Commercial Area.

Currently, the only ACD in Saskatoon is River Landing, which for the most part, is new buildings and the land was mostly owned by City. This would be a first in Saskatoon with infill or a character area, maybe even the first in Saskatchewan. This has long been standard in other provinces and the U.S.

Councillor Clark stated this process involved a collection of groups that came together with different views and have created something not only for Saskatoon, but the whole province. Everyone walked down the street and pointed out their issues and these issues helped develop the guidelines through this consultative process.

The goal now is to find out what the public thinks of this proposal, before taking it to City Council.

The process for ACD is an application for development permit which is then reviewed by a committee of design professionals from landscape, architectural, community planning backgrounds. The committee has approximately 15 members, with 3 selected to review each application. The entire process usually takes about 60 days. It is on a professional level and not just someone's pet peeves or personal preferences.

## **There are 14 design guidelines in the Broadway Commercial Area Design Plan**

### **1. Building Expressions**

- Buildings should reinforce a *base*, *middle*, and *top* in their design.
- Within the first three storeys of a building, a clearly defined *base* contributes to the quality of the pedestrian environment.
- The *middle* or body of a building should contribute to the overall quality of the streetscape.

- The *top* or roof should be distinguished from the rest of the building.

## 2. Orientation & Placement

- All buildings should orient to the street with clearly defined entry points that directly access the sidewalk.
- A minimum of 70% of the front building line shall be located within 1.0 metre of the front property line.
- Buildings can enhance the pedestrian environment by creating a sense of enclosure.

## 3. Street Wall

- A street wall of a new building should align with those of neighbouring buildings or have the same setback as the predominant buildings on the block.
- The height of the street wall should be consistent with historic heights of no greater than 3 storeys and no less than 2 storeys.
- Levels above the street wall should be set back to reinforce a low-rise interface with the sidewalk.

## 4. Heritage Context

- New buildings on Broadway Avenue should complement, rather than detract from, the character of older buildings.
- General Guidelines – New buildings should avoid historical misrepresentation by not replicating past architectural styles, and should respect the scale, material and massing of adjacent heritage buildings.
- Façade Articulation – New buildings should ensure the horizontal and vertical architectural orders including windows and entries, are aligned with neighboring heritage buildings or the established pattern on the block.

## 5. Corner Sites

- Corner buildings have a greater visual prominence given that they front onto two streets and frame intersections.
  - Designs and massing of corner buildings should accentuate the visual prominence of the site.
  - Corner buildings should orient to both street frontages and, wherever possible, have entrances that address both frontages.

## 6. Storefronts

- To provide animation and visual interest, storefronts should have:
  - A frontage in the range of 7.5 metres.
  - A minimum of 75% glazing to maximize visual animation.
  - Entrances that are highly visible and located at or near grade.
  - Signage that adds diversity and interest to the street.
- Dark tinted, reflective, or opaque glazing should be discouraged for storefront glazing

## 7. Residential Street Access Units

- In buildings where residential uses are located at-grade:
  - The individual units should be accessed from the street

- Appropriate front yard privacy measures should be taken
- Access to the units should be consistent with the residential street character in Nutana

## **8. Roof Treatment**

- Roof design should consider the following guidelines:
  - The use of stepbacks, changes in materials, cornice lines and overhangs
  - Screen mechanical penthouses from view
  - Green roofs are encouraged

## **9. Above Grade Parking**

- Where parking is provided at grade, the following guidelines address the design quality of the facility:
  - Direct access from the street is discouraged.
  - Ground level retail should be incorporated, where the parking structure fronts a street.
  - Parking structures should be designed to reinforce the built character and blend into the streetscape.
- Broadway 360° recommends that, wherever possible, parking should be provided in the rear yard or below grade, and should be accessed from the lane.

## **10. Material & Architectural Quality**

- New developments should contribute to the Prairie-style Main Street building style that exists
- High quality materials should be chosen that are both functional and aesthetically pleasing
- Materials chosen should not mimic other materials
- A key objective of Broadway 360 is to achieve a balance between consistencies in design quality & street interface, while enabling individual expression.

## **11. Sidewalk Cafes**

- Should be encouraged along all sidewalks
- Should contribute and integrate into the streetscape
- Corners with “curb bump outs” could provided additional opportunities
- Rear yard and roof-top patios should not abut residential areas
- Sidewalk cafes should be encouraged, provided they don’t cause land use conflicts or encumber pedestrian movements.

## **12. Building Lighting**

- Both landscape and architectural features can be highlighted
- Landmarks & distinctive features of buildings should be illuminated
- Subtle night lighting of retail displays should be encouraged

## **13. Signage**

- Storefront signs should be no more than 25% of the business storefront
- Should aid pedestrians & drivers, especially at night

- Backlit rectangular sign boxes should be discouraged
- Signage should not obscure building features.
- Signage Group 5, in the City of Saskatoon Zoning Bylaw is applied to Broadway Avenue

#### 14. Sustainable Design

- Projects should strive for sustainable building practices
- When adaptive reuse projects are undertaken to rehabilitate historic buildings, the old and new should be compatible in terms of historic materials, features, size, scale, height, proportion and massing to protect the integrity of the property and its environment

#### Questions:

- Could you briefly describe gross floor space exemptions for parking?

Under the B5 Zoning District currently, certain things are exempt from, like indoor parking is not counted towards the total area of the building. E.g. you have a parking floor it does not count as gross floor space. Steering committee was concerned about very large and tall buildings also providing a significant amount of above grade commercial parking. Parking above grade will count towards the ration with the exception of the parking you need for the building itself. We will look at the parking area and the parking you have to provide for dwelling units and the parking for commercial area at rate of 24 square meters plus the drive lanes and the ramps. We will credit that back to you; it will not count toward parking structure.

- Parking with mixed used development, how do you treat the artist who works and lives in place?

The live/work unit would count as a residential dwelling, so one parking space would be required.

- Are there still no parking requirements for commercial property?

No, this remains the same.

#### Comments:

- It is a huge challenge task to bring this into place. Almost to mold into a campus area. Guidelines are good to control design, but if too wide open it's difficult to administer. However, you don't want it too stringent either. Couple concerns with the first two guidelines.

- Base, middle and cap seems simple and perfect, but concern with wall aspect of a 2 or 3 storey building, not sure if good for street. Maybe need to have a restriction of number of stories as well. Need more to guide this.
- 70% frontage and street wall... How will this affect the Extra Foods or Oskayak School if they change ownership?

- Signage and indirect architectural lighting sections are good.
- Storefront is critical for this process.

Tim noted many issues will be covered off by codes as they are more stringent than guidelines can be.

- Some concern with the height allowed for building, seems allowance is too high. Buds is 5 stories and that is fine, but going 12 stories seems high.

Tim noted the ratio of the historic building height will be followed.

- Good solid principle, it allows for a bit of variety, very sound, but is there any room allowed for odd variety that still maintains the character?
- It seems like the smaller lots may not be able to follow the 7:1 ratio.

Tim noted it is important to maintain the pedestrian feel.

- Last area is sustainable design, which is about providing general encouragement and not specific guidelines.

Tim stated the design and statement of intent. It is more becoming the norm, it is good business sense. Don't really need to regulate it as it is the way of good business, much the same as the parking provision.

- **Is there a goal regarding the ratio of residential to commercial property?**

There is no specific quota, but the goal is to have mixed uses. Fact of the matter is Broadway is a very healthy area of mixed use and hope it will continue.

Tim thanked everyone for their time and reminded the group that the Steering Committee members will be around for anyone who wishes to have a more one to one conversation.

For more information please visit: [www.broadway360.ca](http://www.broadway360.ca)

**Public Open House  
Broadway 360 Land Use Implementation  
Proposed B5B and ACD  
Thursday, January 19, 2012**

**Comments:**

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  - In planning guideline has thought been given to right to sunlight. As a solar installer and local resident I can imagine objections to shading from new development.
  - Any encouragement or incentives for green buildings practices.
  - Would existing proposed development in old car was site on Main Street be subjected to new guidelines.
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  - Kudos to all the people who have put time and energy into this project.
  - I totally support it
  - Final comment – I don't want buildings higher than 12 stories but understand the ratio and compromises that had to be made.
3.
  - Excellent consultative process and result!
4.
  - A slide during the presentation briefly mentioned "sustainable design" as an architectural control. I'm curious as to what these sustainable initiatives entail. Sustainability is a fashionable word; I hope it is more than just a buzz word. Are LEED standards being considered?
5.
  - I support the ACD but I believe that some people find the term (particularly "control") threatening and scary. I understand the term's ubiquity but perhaps a terminology change could ease acceptance.
  - Unrelated: I'd like to see more attention paid to streetscaping – the current crosswalks, for example, suck. I'd like to see Farmer's Market/River Landing – style bricked crosswalks on Broadway to enhance the pedestrian experience and calm traffic.
6.
  - I am new to Saskatoon so my questions might be silly/irrelevant:

- Is Broadway 360 expected to change the current zoning bylaws? If not, which rules/plans over-rides the other?
- Is every compatible and "in line"? For example, 7:1 ratio vs what height is currently allowed.
- Might be something to be careful about

7.

- An opportunity to "fix" the building height anomaly on Broadway. The wrong message is being set to the development community that this height and building mass is what the community supports for the immediate and future. This is ludicrous. The city has the right (legally) to change these anomalies and wake certain the design future desired by citizens – not just a few landowners. Height, the B5B height, belongs downtown. Graduate the height downward away from the core. Look at S.E. Falsecreek (Vancouver) Olympic village relative to downtown Vancouver.

8.

- I like this plan a lot, very good work.

9.

- Very good to see progress of any kind in keeping our distinct livable for our families.
- Concerns:
  - "Parking should be accessed from the lane" – What is the impact on residences in the area, will there be extra upkeep on those lanes?
  - Boundaries you have excluded, the area across 8<sup>th</sup> Street (south side 8<sup>th</sup> St & Broadway). Which have been 2 areas that have seen changes (Shell Station) and the "M" Zoned area on Broadway across from the
  - Catholic Church. This is important to!! Why excluded?
  - Concerned by the "should" language like "rooftop patios should not abut residential" – does that mean they still can? Can an existing building add a rooftop because it isn't a new building?
- I realize the zoning is different but couldn't it be included in the future.

10.

- I do not notice any standard recommendations (rules) for the usage of lanes that border residential areas.

11.

- Overall I think you've done a really good job with the new proposed Zoning District and the important move to create enforceable design guidelines for new development in the complementary Architectural Control District.
- I have one comment that is aimed at improving the Architectural Overlay District Design Guidelines.
  - Under the "Storefronts" guideline, please consult the recently approved Phase One of the City Centre Plan, which the Broadway Commercial area

is a part of, on p. 74, where it gives guidelines/indicators for Attractive Ground Floor Frontages. Also see p. 126-127 Opportunities, "From a few dispersed main streets... to a network of active streets." The current "Storefronts" guideline is not strong enough to give affect to the opportunities proffered in the Public Spaces Activity and Urban Form Strategic Framework (November 2011).

12.

- We have a unique neighbourhood on Broadway as it stands right now, why do you want to change it by wanting to put up highrisers? We like it the way it is right now. We wouldn't have moved to this area if we didn't like it. Besides, the more highrisers there are, the less seen a person gets, the more cranky the people become, then there is more traffic and more crime. You people in City Hall can only think of getting more money in your pockets, so it comes down to being greedy.

13.

- My thanks to all the people who worked on this project! A lot of thought and hard work has obviously gone into keeping our neighbourhood the great place it is to live. I, like many, am a bit disappointed with the height restriction issue, but I think they came up with some good compromises. I also didn't hear anything about the number of highrisers that could be built. I fear the area will become overly congested with overly expensive apartment buildings. I appreciate the diversity we now have in Nutana and would miss it if we only lived around luxury condos. Just a thought. Hopefully, this can be passed at City Council ASAP. Thanks again. The meeting was very informative.

#### **Submitted via Broadway 360 website:**

14.

- I think the type of successful community that Broadway has been since it was redeveloped almost 30 years ago, is one that is people driven rather than architecture driven. Though heritage buildings provide a basic element, with new construction a sort of stylized ambience can be built, but the hollow act of simply inserting nice looking buildings won't prove fruitful for the continued success of the district in the long term.

My belief is that architecture should follow use and that design is for people to work, live and play in, and this can be satisfied through community building that essentially relies on establishing mixed use and mixed levels of affordability.

The things that make Broadway special are the people who live and work there every day. The owner-operator businesses provide the attraction to people coming into the area for the unique shops and restaurants. The BBID support every aspect of day-to-day life and year round events that Saskatoon has

become known for – unfortunately these independent businesses are fading with the end of each lease period and may soon be gone. Housing is a prime issue in Saskatoon; increased density can be achieved gracefully through multi-level-use planning.

What I suggest is that we, in whatever way it can be arranged, strive to put in place in a rule book to guarantee we can grow and preserve at once, for example: the B5B outlines the perimeters, sets heights defines set backs etc. But it's the 'inner workings' of a building that will contain and define whether or not a building is successfully integrated to support the community.

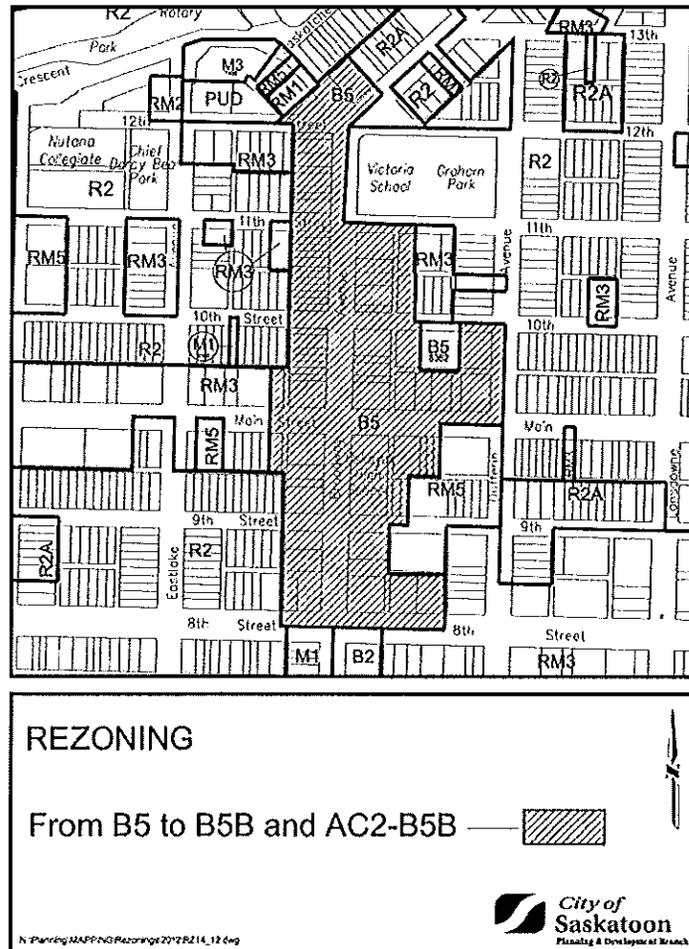
To adhere to an integration and affordability platform will ensure that each building is designed in measured percentages that include mixed use lease and resale units – a variety of sizes of each type of unit (because size determines market value, lease rate, sale price, etc), and a variety of each unit per a percentage of each of type of use; be it retail, office or residential.

That this kind of coding of multiple use, multi-layered affordability could be what in fact defines a very healthy future for Broadway. And this may be the only way we can ensure that Broadway remains a people place, that there will be something for everyone by-design regulations for sizes and uses of interior spaces. In this way the 360 plan can fulfill its mandate, be a model, impress on developers that mixed use, community affordability is incredibly important to people now and future generations of the Broadway District and for that matter, anywhere in Saskatoon.

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z19/12	<b>PROPOSAL</b> Proposed Rezoning from B5 to B5B and AC2 - B5B	<b>EXISTING ZONING</b> B5 Inner City Commercial Corridor District
<b>LEGAL DESCRIPTION</b> Various		<b>CIVIC ADDRESS</b> Various
		<b>NEIGHBOURHOOD</b> Nutana
<b>DATE</b> July 30, 2012	<b>APPLICANT</b> City of Saskatoon Planning and Development Branch 222 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 0J5	<b>OWNER</b> Various

### LOCATION PLAN



**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATIONS:**

that a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising respecting the proposal to amend Zoning Bylaw No. 8770 as outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770;
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposal to rezone the properties indicated on Attachments 2 and 3 from a B5 Zoning District to a B5B Zoning District be approved; and
- 5) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposal to apply the AC2 – B5B Architectural Control Overlay District to the properties indicated on Attachments 2 and 3 be approved.

**B. PROPOSAL**

The Planning and Development Branch is requesting that the properties in the Broadway commercial area, as indicated on Attachments 2 and 3, be rezoned from a B5 to a B5B Zoning District and that the AC2 – B5B Architectural Control Overlay Zoning District be applied to the properties.

A companion report has been submitted to amend Zoning Bylaw No. 8770 to create the B5B Broadway Commercial Zoning District and the AC2 – B5B Architectural Control Overlay Zoning District.

The Planning and Development Branch is requesting 72 properties, located in the Nutana neighbourhood (see Attachment 3), be rezoned from a B5 Zoning District to a B5B Zoning District. The B5B Zoning District contains development standards that will ensure that new development in the area enhances the existing urban environment in the Broadway Avenue commercial area.

The Planning and Development Branch is also requesting that the AC2 – B5B Architectural Control Overlay Zoning District be applied to these properties in the Broadway Avenue

commercial area. This district is an architectural overlay district containing 14 design guidelines that will ensure the quality of design for new construction.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed B5B Zoning District is intended to be applied in the Broadway Avenue commercial area. This district will provide development standards to ensure that new development enhances the existing urban environment along this important commercial corridor.

The proposed AC2 – B5B Architectural Control Overlay Zoning District is also intended to be applied in the Broadway Avenue commercial area; and it will impose design guidelines on all new developments in this area. The design guidelines will ensure that new buildings preserve character and enhance the best qualities of the Broadway Commercial Area. The proposed B5B Zoning District is a component of the implementation of the Broadway 360 Development Plan.

**D. BACKGROUND INFORMATION**

In 2007, the Broadway Business Improvement District (BID) and the Nutana Community Association proposed that a study of the Broadway Commercial Area be undertaken. It was noted by both groups that some of the recommendations contained in the 2001 Nutana Local Area Plan (LAP) had become outdated, or difficult to implement due to community or property owner resistance, and there were concerns about the future character of Broadway Avenue. The City of Saskatoon (City) was invited to participate as a partner with the Broadway BID and Nutana Community Association in the development of a “Broadway Area Plan” to address outstanding recommendations in the Nutana LAP.

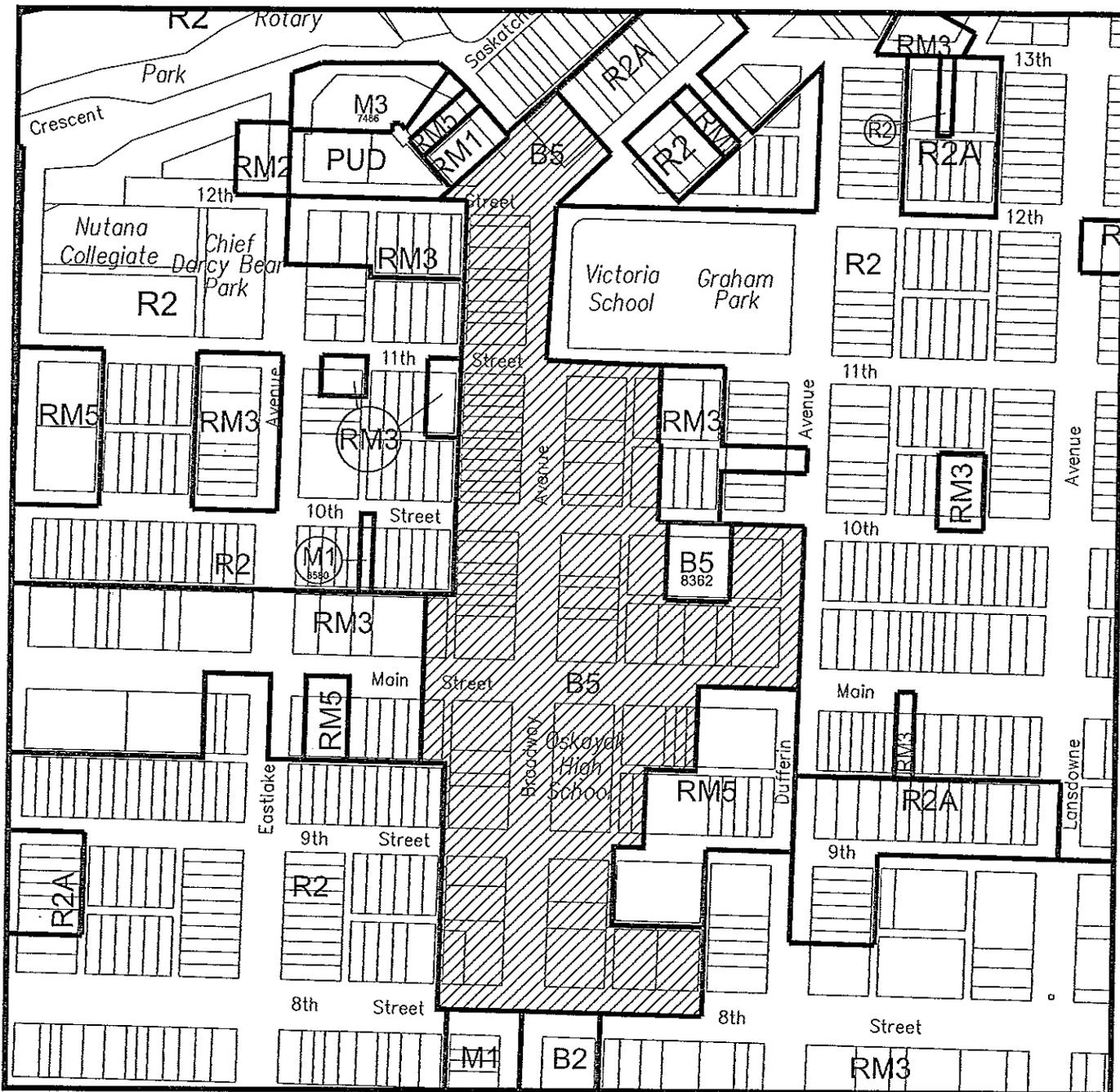
The Planning Partnership, a Toronto-based urban planning consultant firm, was hired to prepare what would become the Broadway 360 Development Plan (Plan). The Plan involved a thorough public consultation process that engaged residents, business and commercial property owners, area schools, churches, and those representing Saskatoon’s heritage community.

The Plan explored practical urban development solutions to address land use, street character, safety, parking, and traffic issues in the Broadway area.

The Steering Committee that was struck during the creation of the Plan was comprised of developers, commercial property owners, Nutana residents, the Nutana Community Association, the BID, and the Ward Councillor. The Administration worked very closely with the Steering Committee over several months to prepare detailed zoning requirements that would be acceptable to the various stakeholders. A report creating the B5B Zoning District and the AC2 – B5B Architectural Control Overlay District has been submitted separately (see Application No. Z14/12).

ATTACHMENT 1

<b>FACT SUMMARY SHEET</b>	
<b>A. Location Facts</b>	
1. Municipal Address	See Attachment 2
2. Legal Description	Various
3. Neighbourhood	Nutana
4. Ward	6
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Various
2. Proposed Use of Property	Various
3. Adjacent Land Uses and Zoning	
North	Residential – RM1, R2A, and R2
South	Institutional – M1, Commercial – B2, and Residential – RM3
East	Residential – RM5 and RM3
West	Residential – RM3 and R2
4. No. of Existing Off-Street Parking Spaces	Not applicable
5. No. of Off-Street Parking Spaces Required	Not applicable
6. No. of Off-Street Parking Spaces Provided	Not applicable
7. Site Frontage	Various
8. Site Area	Various
9. Street Classification	Arterial and Local
10. No. of Sites	72
<b>C. Development Plan Policy</b>	
1. Existing Development Plan Designation	Commercial – Special Area
2. Proposed Development Plan Designation	Commercial – Special Area
3. Existing Zoning District	B5 – Inner-City Commercial Corridor District
4. Proposed Zoning District	B5B – Broadway Commercial District AC2 – B5B Architectural Control Overlay Zoning District



# REZONING

From B5 to B5B and B5B(ACD2) — 



## List of Properties Proposed to be Rezoned from B5 to B5B and AC2 – B5B

615	10th	St	E	806	Broadway	Ave	
616	10th	St	E	810	Broadway	Ave	
634	10th	St	E	813	Broadway	Ave	
612	11th	St	E	814	Broadway	Ave	
535	8th	St	E	817	Broadway	Ave	
601	8th	St	E	818	Broadway	Ave	
617	8th	St	E	820	Broadway	Ave	
619	8th	St	E	821	Broadway	Ave	
611	9th	St	E	824	Broadway	Ave	
613	9th	St	E	834	Broadway	Ave	
1002	Broadway	Ave		835	Broadway	Ave	
1005	Broadway	Ave		906	Broadway	Ave	
1006	Broadway	Ave		912	Broadway	Ave	
1010	Broadway	Ave		916	Broadway	Ave	
1011	Broadway	Ave		919	Broadway	Ave	
616	Broadway	Ave		922	Broadway	Ave	
626	Broadway	Ave		526	Main	St	
630	Broadway	Ave		527	Main	St	
632	Broadway	Ave		616	Main	St	
638	Broadway	Ave		617	Main	St	
640	Broadway	Ave		619	Main	St	
642	Broadway	Ave		620	Main	St	
644	Broadway	Ave		621	Main	St	
650	Broadway	Ave		622	Main	St	
702	Broadway	Ave		626	Main	St	
704	Broadway	Ave		629	Main	St	
706	Broadway	Ave		639	Main	St	
707	Broadway	Ave		611	University	Dr	
708	Broadway	Ave		*628	10 <sup>th</sup>	St	E
712	Broadway	Ave			*AC2-B5B only		
714	Broadway	Ave					
715	Broadway	Ave					
718	Broadway	Ave					
720	Broadway	Ave					
723	Broadway	Ave					
724	Broadway	Ave					
726	Broadway	Ave					
730	Broadway	Ave					
732	Broadway	Ave					
733	Broadway	Ave					
735	Broadway	Ave					
801	Broadway	Ave					
802	Broadway	Ave					

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- A slide during the presentation briefly mentioned "sustainable design" as an architectural control. I'm curious as to what these sustainable initiatives entail. Sustainability is a fashionable word; I hope it is more than just a buzz word. Are LEED standards being considered?

5.

- I support the ACD but I believe that some people find the term (particularly "control") threatening and scary. I understand the term's ubiquity but perhaps a terminology change could ease acceptance.
- Unrelated: I'd like to see more attention paid to streetscaping – the current crosswalks, for example, suck. I'd like to see Farmer's Market/River Landing – style bricked crosswalks on Broadway to enhance the pedestrian experience and calm traffic.

6.

- I am new to Saskatoon so my questions might be silly/irrelevant:

- o Is Broadway 360 expected to change the current zoning bylaws? If not, which rules/plans over-rides the other?
- o Is every compatible and "in line"? For example, 7:1 ratio vs what height is currently allowed.
- o Might be something to be careful about

7.

- An opportunity to "fix" the building height anomaly on Broadway. The wrong message is being set to the development community that this height and building mass is what the community supports for the immediate and future. This is ludicrous. The city has the right (legally) to change these anomalies and wake certain the design future desired by citizens – not just a few landowners. Height, the B5B height, belongs downtown. Graduate the height downward away from the core. Look at S.E. Falsecreek (Vancouver) Olympic village relative to downtown Vancouver.

8.

- I like this plan a lot, very good work.

9.

- Very good to see progress of any kind in keeping our distinct livable for our families.
- Concerns:
  - o "Parking should be accessed from the lane" – What is the impact on residences in the area, will there be extra upkeep on those lanes?
  - o Boundaries you have excluded, the area across 8<sup>th</sup> Street (south side 8<sup>th</sup> St & Broadway). Which have been 2 areas that have seen changes (Shell Station) and the "M" Zoned area on Broadway across from the
  - o Catholic Church. This is important to!! Why excluded?
  - o Concerned by the "should" language like "rooftop patios should not abut residential" – does that mean they still can? Can an existing building add a rooftop because it isn't a new building?
- I realize the zoning is different but couldn't it be included in the future.

10.

- I do not notice any standard recommendations (rules) for the usage of lanes that border residential areas.

11.

- Overall I think you've done a really good job with the new proposed Zoning District and the important move to create enforceable design guidelines for new development in the complementary Architectural Control District.
- I have one comment that is aimed at improving the Architectural Overlay District Design Guidelines.
  - o Under the "Storefronts" guideline, please consult the recently approved Phase One of the City Centre Plan, which the Broadway Commercial area

is a part of, on p. 74, where it gives guidelines/indicators for Attractive Ground Floor Frontages. Also see p. 126-127 Opportunities, "From a few dispersed main streets... to a network of active streets." The current "Storefronts" guideline is not strong enough to give affect to the opportunities proffered in the Public Spaces Activity and Urban Form Strategic Framework (November 2011).

12.

- We have a unique neighbourhood on Broadway as it stands right now, why do you want to change it by wanting to put up highrisers? We like it the way it is right now. We wouldn't have moved to this area if we didn't like it. Besides, the more highrisers there are, the less seen a person gets, the more cranky the people become, then there is more traffic and more crime. You people in City Hall can only think of getting more money in your pockets, so it comes down to being greedy.

13.

- My thanks to all the people who worked on this project! A lot of thought and hard work has obviously gone into keeping our neighbourhood the great place it is to live. I, like many, am a bit disappointed with the height restriction issue, but I think they came up with some good compromises. I also didn't hear anything about the number of highrises that could be built. I fear the area will become overly congested with overly expensive apartment buildings. I appreciate the diversity we now have in Nutana and would miss it if we only lived around luxury condos. Just a thought. Hopefully, this can be passed at City Council ASAP. Thanks again. The meeting was very informative.

**Submitted via Broadway 360 website:**

14.

- I think the type of successful community that Broadway has been since it was redeveloped almost 30 years ago, is one that is people driven rather than architecture driven. Though heritage buildings provide a basic element, with new construction a sort of stylized ambience can be built, but the hollow act of simply inserting nice looking buildings won't prove fruitful for the continued success of the district in the long term.

My belief is that architecture should follow use and that design is for people to work, live and play in, and this can be satisfied through community building that essentially relies on establishing mixed use and mixed levels of affordability.

The things that make Broadway special are the people who live and work there every day. The owner-operator businesses provide the attraction to people coming into the area for the unique shops and restaurants. The BBID support every aspect of day-to-day life and year round events that Saskatoon has

become known for – unfortunately these independent businesses are fading with the end of each lease period and may soon be gone. Housing is a prime issue in Saskatoon; increased density can be achieved gracefully through multi-level-use planning.

What I suggest is that we, in whatever way it can be arranged, strive to put in place in a rule book to guarantee we can grow and preserve at once, for example: the B5B outlines the perimeters, sets heights defines set backs etc. But it's the 'inner workings' of a building that will contain and define whether or not a building is successfully integrated to support the community.

To adhere to an integration and affordability platform will ensure that each building is designed in measured percentages that include mixed use lease and resale units – a variety of sizes of each type of unit (because size determines market value, lease rate, sale price, etc), and a variety of each unit per a percentage of each of type of use; be it retail, office or residential.

That this kind of coding of multiple use, multi-layered affordability could be what in fact defines a very healthy future for Broadway. And this may be the only way we can ensure that Broadway remains a people place, that there will be something for everyone by-design regulations for sizes and uses of interior spaces. In this way the 360 plan can fulfill its mandate, be a model, impress on developers that mixed use, community affordability is incredibly important to people now and future generations of the Broadway District and for that matter, anywhere in Saskatoon.

**Broadway 360 Technical Workshop**  
**Cosmopolitan Senior Citizens Centre – 614 11<sup>th</sup> Street East**  
**January 24, 2012**  
**5:30 pm**

On behalf of the Broadway 360 Steering Committee, Sarah Marchildon, Executive Director of the Broadway Business Improvement District, thanked everyone for attending this technical workshop on proposed land use changes. The purpose of tonight's meeting is to review the proposed B5B Zoning District and proposed Architectural Control District for the Broadway commercial area. Tim Steuart will give a presentation with question period following. After formal part of meeting, the Steering Committee will be around for more one on one discussion.

**Broadway 360 Land Use Implementation**  
**Tim Steuart, Senior Planner, Business License & Zoning Compliance Section**

A bit of background. This came together from a unique cooperative effort between property owners, residents, and the BID to make Broadway a better place. A comprehensive study was done with the goal of recognizing that Broadway is a special area, a cool area created with pedestrian environment. Everyone involved didn't want to assume it will stay this way forever, but there was a desire try to ensure it does. The Steering Committee has come up with a very good plan. One issue was zoning and to ensure the zoning treatment is satisfactory to all.

All properties in the Broadway Commercial Area currently zoned B5 (Inner-City Commercial Corridor) are proposed to be rezoned to the new B5B (Commercial Zoning District), a zoning district designed exclusively for Broadway.

The new B5B includes changes to the permitted uses and development standards. Also, all B5B-zoned properties would be subject to an Architectural Control District.

Saskatoon is growing strongly and we need to grow up as well as out, creating the urban living room.

- The first three storeys matter most.
- Density done properly has many benefits.
- The stepback enhances the pedestrian experience.

What factors will affect the overall height of a building?

- Gross Floor Area Ratio (7:1) which means that the maximum building volume can be 7 times the site area
- Site size (bigger, taller buildings can be built on larger sites)
- Height of base building (must be a minimum 7.5 metres to a maximum of 12 metres)
- Setback and stepbacks (must meet minimum requirements, setbacks affect the overall building form and height)
- Parking
  - Amount of required parking

- Location of parking (at grade at rear of site, below grade in parking structure, or above grade in parking structure)

Do not want building setback from the street. Requirement that at least 70% of building face the street, setback for courtyard. Removed service stations and commercial parking lot from permitted uses. Parking provision is currently 1.25 parking spaces for residents and 1 visitor for every 8 dwellings. This has been reduced to 1 parking space for residents while visitor parking has remained the same. Nothing set for commercial buildings and not proposing changes as it is usually self-regulating since developers understand that providing parking helps to attract potential tenants, so a minimum parking provision does not seem necessary.

The proposed B5B Zoning District would be subject to an Architectural Control District (ACD) intended to preserve the physical character of the area.

The ACD would contain a set of design guidelines, known as the Broadway Commercial Area Design Plan that all new development in the B5B District must conform to. Establishing an Architectural Control District (ACD) allows for enforceable design controls in the Broadway Commercial Area.

Currently, the only ACD in Saskatoon is River Landing, which for the most part, is new buildings and the land was mostly owned by City. This would be a first in Saskatoon with infill or a character area, maybe even the first in Saskatchewan. This has long been standard in other provinces and the U.S.

Councillor Clark stated this process involved a collection of groups that came together with different views and have created something not only for Saskatoon, but the whole province. Everyone walked down the street and pointed out their issues and these issues helped develop the guidelines through this consultative process.

The goal now is to find out what the public thinks of this proposal, before taking it to City Council.

The process for ACD is an application for development permit which is then reviewed by a committee of design professionals from landscape, architectural, community planning backgrounds. The committee has approximately 15 members, with 3 selected to review each application. The entire process usually takes about 60 days. It is on a professional level and not just someone's pet peeves or personal preferences.

## **There are 14 design guidelines in the Broadway Commercial Area Design Plan**

### **1. Building Expressions**

- Buildings should reinforce a *base*, *middle*, and *top* in their design.
- Within the first three storeys of a building, a clearly defined *base* contributes to the quality of the pedestrian environment.
- The *middle* or body of a building should contribute to the overall quality of the streetscape.

- The *top* or roof should be distinguished from the rest of the building.

## 2. Orientation & Placement

- All buildings should orient to the street with clearly defined entry points that directly access the sidewalk.
- A minimum of 70% of the front building line shall be located within 1.0 metre of the front property line.
- Buildings can enhance the pedestrian environment by creating a sense of enclosure.

## 3. Street Wall

- A street wall of a new building should align with those of neighbouring buildings or have the same setback as the predominant buildings on the block.
- The height of the street wall should be consistent with historic heights of no greater than 3 storeys and no less than 2 storeys.
- Levels above the street wall should be set back to reinforce a low-rise interface with the sidewalk.

## 4. Heritage Context

- New buildings on Broadway Avenue should complement, rather than detract from, the character of older buildings.
- General Guidelines – New buildings should avoid historical misrepresentation by not replicating past architectural styles, and should respect the scale, material and massing of adjacent heritage buildings.
- Facade Articulation – New buildings should ensure the horizontal and vertical architectural orders including windows and entries, are aligned with neighboring heritage buildings or the established pattern on the block.

## 5. Corner Sites

- Corner buildings have a greater visual prominence given that they front onto two streets and frame intersections.
  - Designs and massing of corner buildings should accentuate the visual prominence of the site.
  - Corner buildings should orient to both street frontages and, wherever possible, have entrances that address both frontages.

## 6. Storefronts

- To provide animation and visual interest, storefronts should have:
  - A frontage in the range of 7.5 metres.
  - A minimum of 75% glazing to maximize visual animation.
  - Entrances that are highly visible and located at or near grade.
  - Signage that adds diversity and interest to the street.
- Dark tinted, reflective, or opaque glazing should be discouraged for storefront glazing

## 7. Residential Street Access Units

- In buildings where residential uses are located at-grade:
  - The individual units should be accessed from the street

- Appropriate front yard privacy measures should be taken
- Access to the units should be consistent with the residential street character in Nutana

#### **8. Roof Treatment**

- Roof design should consider the following guidelines:
  - The use of stepbacks, changes in materials, cornice lines and overhangs
  - Screen mechanical penthouses from view
  - Green roofs are encouraged

#### **9. Above Grade Parking**

- Where parking is provided at grade, the following guidelines address the design quality of the facility:
  - Direct access from the street is discouraged.
  - Ground level retail should be incorporated, where the parking structure fronts a street.
  - Parking structures should be designed to reinforce the built character and blend into the streetscape.
- Broadway 360° recommends that, wherever possible, parking should be provided in the rear yard or below grade, and should be accessed from the lane.

#### **10. Material & Architectural Quality**

- New developments should contribute to the Prairie-style Main Street building style that exists
- High quality materials should be chosen that are both functional and aesthetically pleasing
- Materials chosen should not mimic other materials
- A key objective of Broadway 360 is to achieve a balance between consistencies in design quality & street interface, while enabling individual expression.

#### **11. Sidewalk Cafes**

- Should be encouraged along all sidewalks
- Should contribute and integrate into the streetscape
- Corners with “curb bump outs” could provided additional opportunities
- Rear yard and roof-top patios should not abut residential areas
- Sidewalk cafes should be encouraged, provided they don’t cause land use conflicts or encumber pedestrian movements.

#### **12. Building Lighting**

- Both landscape and architectural features can be highlighted
- Landmarks & distinctive features of buildings should be illuminated
- Subtle night lighting of retail displays should be encouraged

#### **13. Signage**

- Storefront signs should be no more than 25% of the business storefront
- Should aid pedestrians & drivers, especially at night

- Backlit rectangular sign boxes should be discouraged
- Signage should not obscure building features.
- Signage Group 5, in the City of Saskatoon Zoning Bylaw is applied to Broadway Avenue

#### 14. Sustainable Design

- Projects should strive for sustainable building practices
- When adaptive reuse projects are undertaken to rehabilitate historic buildings, the old and new should be compatible in terms of historic materials, features, size, scale, height, proportion and massing to protect the integrity of the property and its environment

#### Questions:

- **Could you briefly describe gross floor space exemptions for parking?**

Under the B5 Zoning District currently, certain things are exempt from, like indoor parking is not counted towards the total area of the building. E.g. you have a parking floor it does not count as gross floor space. Steering committee was concerned about very large and tall buildings also providing a significant amount of above grade commercial parking. Parking above grade will count towards the ration with the exception of the parking you need for the building itself. We will look at the parking area and the parking you have to provide for dwelling units and the parking for commercial area at rate of 24 square meters plus the drive lanes and the ramps. We will credit that back to you; it will not count toward parking structure.

- **Parking with mixed used development, how do you treat the artist who works and lives in place?**

The live/work unit would count as a residential dwelling, so one parking space would be required.

- **Are there still no parking requirements for commercial property?**

No, this remains the same.

#### Comments:

- It is a huge challenge task to bring this into place. Almost to mold into a campus area. Guidelines are good to control design, but if too wide open it's difficult to administer. However, you don't want it too stringent either. Couple concerns with the first two guidelines.
  - Base, middle and cap seems simple and perfect, but concern with wall aspect of a 2 or 3 storey building, not sure if good for street. Maybe need to have a restriction of number of stories as well. Need more to guide this.
  - 70% frontage and street wall... How will this affect the Extra Foods or Oskayak School if they change ownership?

- Signage and indirect architectural lighting sections are good.
- Storefront is critical for this process.

Tim noted many issues will be covered off by codes as they are more stringent than guidelines can be.

- Some concern with the height allowed for building, seems allowance is too high. Buds is 5 stories and that is fine, but going 12 stories seems high.

Tim noted the ratio of the historic building height will be followed.

- Good solid principle, it allows for a bit of variety, very sound, but is there any room allowed for odd variety that still maintains the character?
- It seems like the smaller lots may not be able to follow the 7:1 ratio.

Tim noted it is important to maintain the pedestrian feel.

- Last area is sustainable design, which is about providing general encouragement and not specific guidelines.

Tim stated the design and statement of intent. It is more becoming the norm, it is good business sense. Don't really need to regulate it as it is the way of good business, much the same as the parking provision.

- **Is there a goal regarding the ratio of residential to commercial property?**

There is no specific quota, but the goal is to have mixed uses. Fact of the matter is Broadway is a very healthy area of mixed use and hope it will continue.

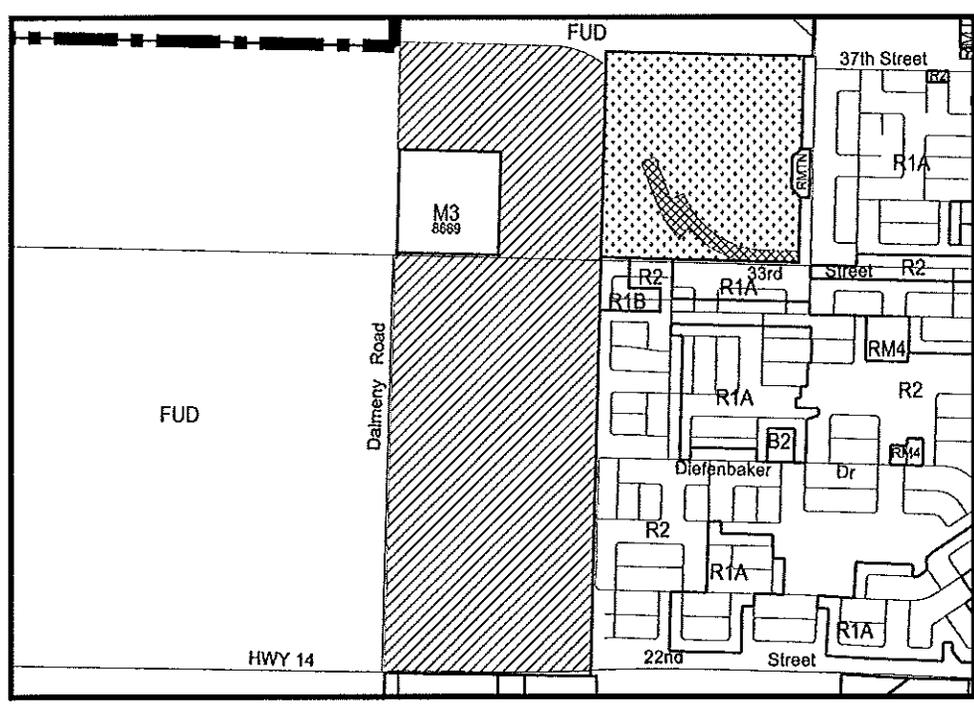
Tim thanked everyone for their time and reminded the group that the Steering Committee members will be around for anyone who wishes to have a more one to one conversation.

For more information please visit: [www.broadway360.ca](http://www.broadway360.ca)

## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z15/12	<b>PROPOSAL</b> Proposed Rezoning from R1A(H) to R1A, FUD to R1A, and R1A(H) to R1B	<b>EXISTING ZONING</b> R1A (H) and FUD
<b>LEGAL DESCRIPTION</b> Various (see Attachment 1)	<b>CIVIC ADDRESS</b> N/A	
	<b>NEIGHBOURHOOD</b> Kensington	
<b>DATE</b> July 19, 2012	<b>APPLICANT</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7	<b>OWNER</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7

### LOCATION PLAN



### PROPOSED REZONING

- From FUD to R1A
- From R1A(H) to R1B
- From R1A(H) to R1A

File No. RZ15-2012

Planning & Development Branch

**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

That a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendments to Zoning Bylaw No. 8770 be approved:
  - a) that the properties identified in Attachment 2 be rezoned from R1A(H) - One-Unit Residential District with a Holding symbol, and FUD - Future Urban Development District to R1A - One-Unit Residential District; and
  - b) Phase I of the Kensington neighbourhood development, as shown on the Plan of Proposed Subdivision (see Attachment 3), be rezoned from:
    - i. R1A(H) - One-Unit Residential District with a Holding symbol, to R1B - Small Lot One-Unit Residential District (Lots 1 to 26, Block 100, Lots 1 to 29, Block 101, Lots 1 to 30, Block 103); and
    - ii. R1A(H) - One-Unit Residential District with a Holding symbol to R1A - One-Unit Dwelling Residential District (Lots 1 to 15, Block 102, and Lots 71 to 76, Block 103).

**B. PROPOSAL**

The Planning and Development Branch has received an application from the City of Saskatoon (City), Land Branch requesting that the identified properties within the Kensington Neighbourhood Concept Plan area be rezoned from R1A(H) - One-Unit Residential District with a Holding symbol, and FUD - Future Urban Development District to R1A - One-Unit Residential District; and to rezone Kensington Phase I from R1A(H) - One-Unit Residential District with a Holding symbol, to R1A - One-Unit Residential District and R1B - Small Lot One-Unit Residential District.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed rezoning of the identified properties would facilitate residential development that is consistent with the Kensington Neighbourhood Concept Plan.

**D. BACKGROUND INFORMATION**

During its April 16, 2012 meeting, City Council approved the Kensington Neighbourhood Concept Plan, which provides a wide range of housing options, as well as neighbourhood commercial services.

**E. JUSTIFICATION**

1. Community Services Department Comments

a) Planning and Development Branch

The proposed rezoning is consistent with the approved Concept Plan. Properties in the northwest part of the Kensington neighbourhood will be zoned R1B District and R1A District to accommodate development of Phase I of the neighbourhood. The remainder of the properties identified will be zoned as a R1A District. Any parcels intended for other forms of development will be subject to future rezoning when detailed survey plans are prepared.

2. Comments by Others

a) Infrastructure Services Department

The proposed Zoning Bylaw No. 8770 amendment is acceptable to the Infrastructure Services Department.

b) Utility Services Department, Transit Services Branch

The Transit Services Branch has no easement requirements regarding the above referenced properties. At present, the Transit Services Branch has no service within 450 metres.

## FACT SUMMARY SHEET

FACT SUMMARY SHEET	
<b>A. Location Facts</b>	
1. Municipal Address	N/A
2. Legal Description	LSD 3, 5, and 6 2-37-6-W3; NW¼ 35-36-6-W3; Part of SW¼ 35-36-6-W3; Parcel A, Plan No. 98SA07556; Parcel C, Plan No. 00SA28118; Parcel E, Plan No. 101709783; and Lots 1 to 26, Block 100, Lots 1 to 29, Block 101, Lots 1 to 30, Block 103 as shown on Plan of Proposed Subdivision showing Part of SE¼ 2-37-6-W3, Plan No. 101836076
3. Neighbourhood	Kensington
4. Ward	3
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Residential with a Holding symbol - R1A(H) and Future Urban Development -FUD
2. Proposed Use of Property	Residential – R1A and R1B
3. Adjacent Land Uses and Zoning	Future Urban Development, Residential, Direct Control District, Institutional, and Commercial
North - Undeveloped	Future Urban Development – FUD
South – Blairmore Suburban Centre	Direct Control District – DCD6, Commercial – B3, and Institutional – M3
East – Confederation Park Neighbourhood Pacific Heights Neighborhood	Residential – R2, R1B, and RMTN
West - Undeveloped	Future Urban Development – FUD
4. Street Classification	22 <sup>nd</sup> Street West – Major Arterial Diefenbaker Drive – Major Arterial 33 <sup>rd</sup> Street West – Major Arterial
<b>C. Official Community Plan Policy</b>	
1. Existing Official Community Plan Designation	Residential
2. Proposed Official Community Plan Designation	Residential
3. Existing Zoning District	FUD and R1A(H)
4. Proposed Zoning District	R1A and R1B

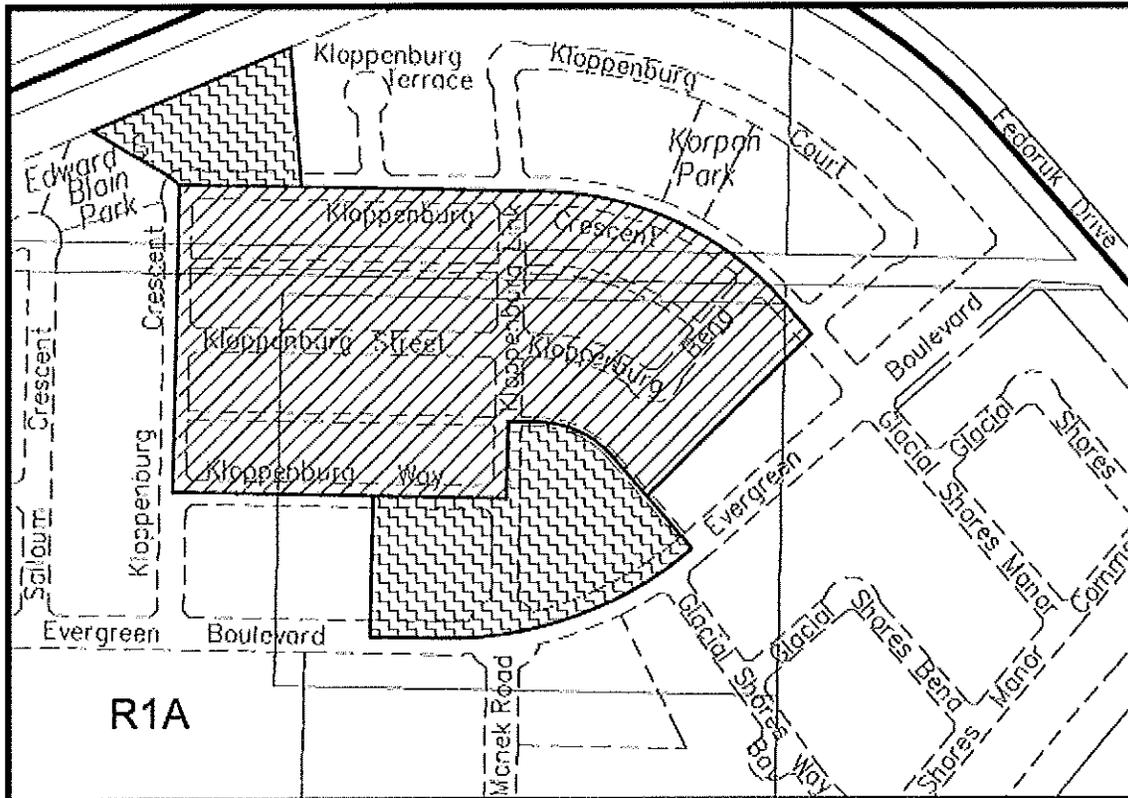




## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z17/12	<b>PROPOSAL</b> Proposed Rezoning from R1A to R1B and RMTN	<b>EXISTING ZONING</b> R1A
<b>LEGAL DESCRIPTION</b> Plan to be Approved		<b>CIVIC ADDRESS</b> Evergreen Boulevard, Kloppenburg Crescent, Bend, Link, Street, and Way
		<b>NEIGHBOURHOOD</b> Evergreen
<b>DATE</b> July 16, 2012	<b>APPLICANT</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7	<b>OWNER</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7

### LOCATION PLAN



PROPOSED REZONING

From R1A to R1B

From R1A to RMTN

City of  
Saskatoon

Planning & Development Branch

File No. RZ17-2012

**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

That a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising with respect to the proposal to rezone the properties outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notices for advertising the proposed amendments;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendment to Zoning Bylaw No. 8770 to rezone the properties identified in the attached Proposed Zoning Map from R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District, and RMTN – Townhouse Residential District, be approved.

**B. PROPOSAL**

The Planning and Development Branch has received an application from the City of Saskatoon, Land Branch, requesting that the properties identified in the attached Proposed Zoning Map (see Attachment 2) be rezoned as follows:

- 1) Blocks 1 to 7 be rezoned from R1A – One-Unit Residential District to R1B – Small Lot One-Unit Residential District; and
- 2) Parcels EE, FF, and GG, be rezoned from R1A – One-Unit Residential District to RMTN – Townhouse Residential District.

The rezoning of these lands would accommodate small lot one-unit and townhouse residential development.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed rezoning will permit the development of the aforementioned lands in a manner which is consistent with the Evergreen Neighborhood Concept Plan (Concept Plan).

**D. BACKGROUND INFORMATION**

During its June 1, 2009 meeting, City Council approved the Concept Plan. The subject

sites are currently under the blanket zoning of a R1A District (One-Unit Residential District), which was applied to the area after the approval of the Concept Plan. The Concept Plan provides a wide range of housing options, as well as neighbourhood commercial services to serve the area. The proposed amendments will change the zoning designations for the specified areas of the neighbourhood in order to accommodate the variety of residential development that is desired within the Evergreen neighbourhood.

## **E. JUSTIFICATION**

### **1. Community Services Department Comments**

#### **a) Planning and Development Branch**

The proposed rezoning is in compliance with the approved Concept Plan and will accommodate a diversity of housing types in the Evergreen neighbourhood. Future development on this site will comply with the development standards identified in the R1B and RMTN Zoning Districts.

### **2. Comments by Others**

#### **a) Infrastructure Services Department**

The proposed amendment is acceptable to the Infrastructure Services Department.

#### **b) Utility Services Department, Transit Services Branch**

The Transit Branch has no easement requirements regarding the above referenced property. At present, the Transit Branch has no service within 450 metres, but has services within this development.

## **F. COMMUNICATION PLAN**

Once this application has been considered by the Municipal Planning Commission, it will be advertised in accordance with Public Notice Policy No. C01-021, and a date for a public hearing will be set. The property owners affected by this rezoning will be notified in writing. The Planning and Development Branch will also notify the Community Consultant and the Ward Councillor of the public hearing date by letter. A notice will be placed in The StarPhoenix two weeks prior to the public hearing. Notice boards will also be placed on the site.

**G. ENVIRONMENTAL IMPLICATIONS**

There are no environmental and/or greenhouse gas implications.

**H. ATTACHMENTS**

1. Fact Summary Sheet
2. Proposed Zoning Map

Written by: Daniel Gray, Planner 16

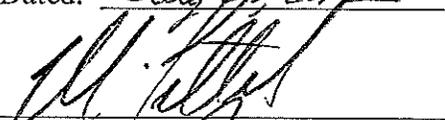
Reviewed by:

  
Alan Wallace, Manager  
Planning and Development Branch

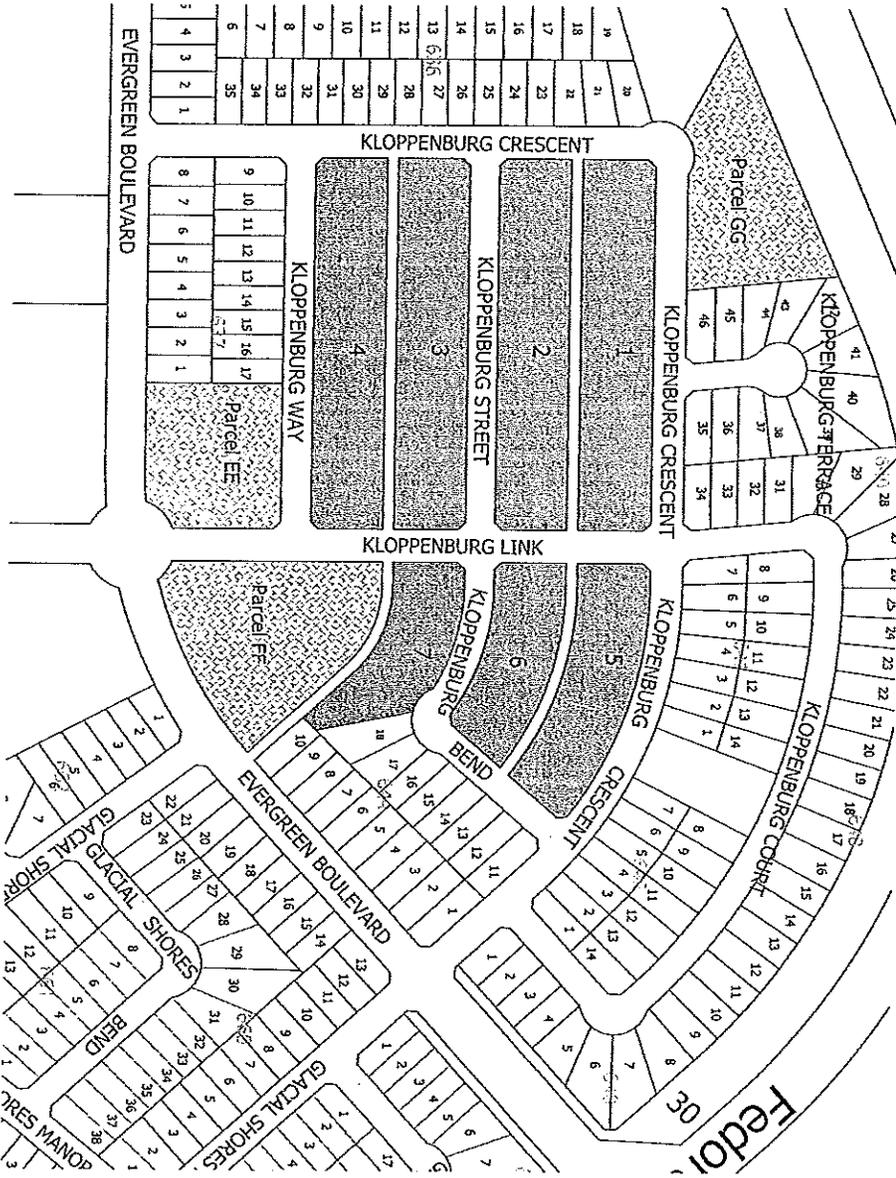
Approved by:

  
Randy Grauer, General Manager  
Community Services Department  
Dated: July 23, 2012

Approved by:

  
Murray Totland, City Manager  
Dated: July 26/12.

<b>FACT SUMMARY SHEET</b>	
<b>A. Location Facts</b>	
1. Municipal Address	Evergreen Blvd, Kloppenburg Crescent, Bend, Link, Street, and Way
2. Legal Description	Plan to be Approved
3. Neighbourhood	Evergreen
4. Ward	10
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Residential – R1A
2. Proposed Use of Property	Residential – R1B and RMTN
3. Adjacent Land Uses and Zoning	Residential
North - Undeveloped	Future Urban Development - FUD
South - Evergreen Neighbourhood	Residential – R1A
East - Evergreen Neighbourhood	Residential – R1A
West - Evergreen Neighbourhood	Residential – R1A
4. No. of Existing Off-Street Parking Spaces	N/A
5. No. of Off-Street Parking Spaces Required	N/A
6. No. of Off-Street Parking Spaces Provided	N/A
7. Site Frontage	N/A
8. Site Area	N/A
9. Street Classification	Evergreen Boulevard – Major Collector Kloppenburg Crescent, Bend, Link, Street, and Way – Local Street
<b>C. Official Community Plan Policy</b>	
1. Existing Official Community Plan Designation	Residential
2. Proposed Official Community Plan Designation	Residential
3. Existing Zoning District	R1A
4. Proposed Zoning District	R1B and RMTN



**EVERGREEN**

**Legend:**

-  R1B  
Parcel 1:  
Part of LSD 3-18-37-04-3 Ext 33 & Part of  
NW 1/4 37-4-W3M
-  R1B  
Parcels 2 - 4:  
Part of Parcel A Registered Plan  
#78534536 & Part of NW 1/4 37-4-W3M
-  R1B  
Parcel 5:  
Part of LSD 3-18-37-04-3 Ext 33 & Part of  
Registered Plan #78534536
-  R1B  
Parcel 6:  
Part of Parcel A Registered Plan  
#78534536
-  R1B  
Parcel 7:  
Part of Parcel A Registered Plan  
#78534536 & Part of NE 1/4 37-4-W3M
-  RMTN  
Parcels EE & FF:  
Part of Parcel A Registered Plan #78534536
-  R1B  
Parcel GG:  
Part of LSD 4-18-37-04-3 Ext 32 & Part of  
LSD 3-18-37-04-3 Ext 33

**7. Communications to Council – (Requests to speak to Council regarding reports of Administration and Committees)**

**8. Communications to Council (Sections B, C, and D only)**

**9. Question and Answer Period**

**10. Matters of Particular Interest**

**11. Enquiries**

**12. Motions**

**13. Giving Notice**

**14. Introduction and Consideration of Bylaws**

Bylaw No. 9037 - The Zoning Amendment Bylaw, 2012 (No. 9)

Bylaw No. 9038 - The Official Community Plan Amendment Bylaw, 2012 (No. 3)

Bylaw No. 9044 - The Zoning Amendment Bylaw, 2012 (No. 11)

Bylaw No. 9045 - The Adult Services Licensing Bylaw, 2012

Bylaw No. 9046 - The Tax Discount and Penalties Amendment Bylaw, 2012

Bylaw No. 9047 - The Traffic Amendment Bylaw, 2012 (No. 2)

## **COMMUNICATIONS TO COUNCIL**

### **MEETING OF CITY COUNCIL – WEDNESDAY, AUGUST 15, 2012**

#### **A. REQUESTS TO SPEAK TO COUNCIL**

- 1) James Frie, Khodr Bardouh and Marwan Bardouh, Comfort Cabs Limited dated August 7**
- 

Requesting permission to address City Council with respect to the issuance of wheel chair accessible plates to taxi companies. (File No. CK. 307-4)

**RECOMMENDATION:** that a representative of Comfort Cabs be heard.

**B. ITEMS WHICH REQUIRE THE DIRECTION OF CITY COUNCIL**

**1) Alma Day, undated**

Submitting copies of articles regarding prayer. (File No. CK. 150-1)

**RECOMMENDATION:** that the information be received.

**2) Gertrude Armbrust, dated July 13**

Commenting on volunteers. (File No. CK. 150-1)

**RECOMMENDATION:** that the information be received.

**3) Brian Jones, dated July 16**

Commenting regarding Canadian Soldiers. (File No. CK. 150-1)

**RECOMMENDATION:** that the information be received.

**4) Patrick J. Montgomery, Lieutenant-Commander, Commanding Officer, Her Majesty's Canadian Ship SASKATOON, dated June 12**

Providing information and gratitude for support of the HMCS SASKATOON.  
(File No. CK. 205-1)

**RECOMMENDATION:** that the information be received.

**5) Westfab Limited, Steel Fabricators, dated June 25**

Expressing support for municipal Free and Fair Trade Act. (File No. CK. 127-1)

**RECOMMENDATION:** that the information be received.

**Items Which Require the Direction of City Council**  
**Wednesday, August 15, 2012**  
**Page 2**

**6) Carla Paul, dated August 2**

Requesting an extension to the time where amplified sound can be heard under the Noise Bylaw, on Sunday, September 9 2012, in Lakeview Park for annual Rider Day in the Park event. (File No. CK. 185-9)

**RECOMMENDATION:** that the request for an extension to the time where amplified sound can be heard under the Noise Bylaw, on Sunday, September 9 2012, in Lakeview Park for annual Rider Day in the Park event be approved subject to any administrative conditions.

**7) Carrie Catherine, the Two Twenty, dated August 2**

Requesting temporary lane closures on 20<sup>th</sup> Street, between Avenues B and C, on Friday, September 21, 2012, for Complete Streets event. (File No. CK. 205-1)

**RECOMMENDATION:** that the request for temporary lane closures on 20<sup>th</sup> Street, between Avenues B and C, on Friday, September 21, 2012, for Complete Streets event, be approved subject to any administrative conditions.

**8) Jasmin Fookes, dated August 3**

Requesting an extension to the time where amplified sound can be heard under the Noise Bylaw, from dusk to approximately 10:00 p.m. on Tuesday, September 11, 2012, for Saskatoon Food Bank's Garden Patch volunteer appreciation event. (File No. CK. 185-9)

**RECOMMENDATION:** that the request for an extension to the time where amplified sound can be heard under the Noise Bylaw, from dusk to approximately 10:00 p.m. on Tuesday, September 11, 2012, for Saskatoon Food Bank's Garden Patch volunteer appreciation event be approved subject to any administrative conditions.

**Items Which Require the Direction of City Council**  
**Wednesday, August 15, 2012**  
**Page 3**

**9) Shellie Bryant, Secretary, Development Appeals Board, dated July 19**

Advising of Notice of Hearing of the Development Appeals Board regarding the property located at 1710 Idylwyld Drive North. (File No. CK. 4352-1)

**RECOMMENDATION:** that the information be received.

**10) Shellie Bryant, Secretary, Development Appeals Board, dated July 30**

Advising of Notice of Hearing of the Development Appeals Board regarding the property located at 36 Lindsay Drive. (File No. CK. 4352-1)

**RECOMMENDATION:** that the information be received.

C. **ITEMS WHICH HAVE BEEN REFERRED FOR APPROPRIATE ACTION**

1) **Elaine Orvedahl, dated July 8**

Commenting on issues with a tree. (File No. CK. 4139-4) **(Referred to the Administration to respond to the writer.)**

2) **John Thomson, dated July 11**

Commenting on problems with Circle Drive North. (File No. CK. 6001-1) **(Referred to the Administration for further handling.)**

3) **Kyle Gorman, dated July 11**

Commenting on a parking ticket received. (File No. CK. 5301-1) **(Referred to the Administration to respond to the writer.)**

4) **John Coutts, dated July 15**

Commenting on a school zone on Acadia Drive. (File No. CK. 6280-1) **(Referred to the Administration to respond to the writer.)**

5) **Edward Kozowy, dated July 18**

Commenting on the condition of city streets. (File No. CK. 6315-1) **(Referred to the Administration to respond to the writer.)**

6) **Edward Danneberg, dated July 18**

Commenting on transit services in Holiday Park. (File No. CK. 7310-1) **(Referred to the Administration to respond to the writer.)**

7) **Anita Hrytsak, dated July 19**

Commenting on a tree on McCormack Road. (File No. CK. 4139-4) **(Referred to the Administration for appropriate action and response to the writer.)**

**Items Which Have Been Referred for Appropriate Action**  
**Wednesday, August 15, 2012**  
**Page 2**

**8) Tyler Gilewicz, dated July 19**

Commenting on traffic in North Industrial area. (File No. CK. 6320-1) **(Referred to the Administration to respond to the writer.)**

**9) Peter Derrick, dated July 19**

Commenting on Master Plan Development – Inner City North End. (File No. CK. 4110-1) **(Referred to the Administration to respond to the writer.)**

**10) Ryan Harder, dated July 19**

Commenting on the need for rent control. (File No. CK. 750-1) **(Referred to the Administration to respond to the writer.)**

**11) Ken Helgeson, dated July 20**

Commenting on red light countdown timers. (File No. CK. 5300-8) **(Referred to the Administration to respond to the writer.)**

**12) Nathan Rushton, dated July 23**

Commenting on grass cutting on berms. (File No. CK. 4200-1) **(Referred to the Administration to respond to the writer.)**

**13) Barbara Szpunar, dated July 23 (WITHDRAWN, AUGUST 13, 2012)**

**14) Denis Vally, dated July 23**

Commenting on parking meter technology. (File No. CK. 6120-2) **(Referred to the Administration to respond to the writer.)**

**Items Which Have Been Referred for Appropriate Action**  
**Wednesday, August 15, 2012**  
**Page 3**

**15) Almabella Mascarina, dated July 23**

Commenting on the alley between Preston and Argyle Avenues. (File No. CK. 6315-1) **(Referred to the Administration to respond to the writer.)**

**16) Peter Derrick, dated July 23**

Commenting on design plan of hotel/condo/retail development in South Downtown. (File No. CK. 4129-15) ) **(Referred to the Administration to respond to the writer.)**

**17) Myron Kowalsky, dated July 24**

Commenting on noise from city buses. (File No. CK. 375-2) **(Referred to the Administration to respond to the writer.)**

**18) Sylvia J. Carpenter, dated July 26**

Commenting on street closures. (File No. CK. 6295-1) ) **(Referred to the Administration to respond to the writer.)**

**19) Garnet Fehr, dated July 26**

Commenting on speed limits in detours. (File No. CK. 6320-1) ) **(Referred to the Administration to respond to the writer.)**

**20) Darla Taylor, dated July 28**

Commenting on a water main break on Melrose Avenue. (File No. CK. 6315-1) ) **(Referred to the Administration to respond to the writer.)**

**21) Charlie Bueckert, dated August 1**

**Items Which Have Been Referred for Appropriate Action**  
**Wednesday, August 15, 2012**  
**Page 4**

Commenting on vehicles driving in Kinsmen Park. (File No. CK. 4205-1) ) **(Referred to the Administration to respond to the writer.)**

**22) Mary Anne Jenks, dated August 1**

Commenting on transit routes in Montgomery Park. (File No. CK. 7310-1) ) **(Referred to the Administration to respond to the writer.)**

**23) Brenda Proulx, dated August 3**

Commenting on the need for programs for children with disabilities. (File No. CK. 5500-1) ) **(Referred to the Administration to respond to the writer.)**

**24) Mae Juleff, dated August 5**

Commenting on a crosswalk on Broadway and Main and about cyclists not dismounting on sidewalks and bridges. (File Nos. CK. 6150-1 and 5200-4) ) **(Referred to the Administration to respond to the writer.)**

**25) Anthony (Tony) Korte, dated August 3**

Commenting on buffer strip adjacent to the sound wall on Wanuskewin Road. (File No. CK. 6295-1) **(Referred to the Administration to respond to the writer.)**

**26) Patricia Roe, Leader of Communications and Public Relations, Sherbrooke Community Centre, dated July 30**

Submitting request to purchase lane on Acadia Drive. (File No. CK. 6295-1) **(Referred to the Administration for appropriate action and response to the writer.)**

**RECOMMENDATION:** that the information be received.

**D. PROCLAMATIONS**

**1) Jim McKinny, Secretary, Remembrance Day Committee, dated July 4**

Requesting City Council proclaim November 5<sup>th</sup> to 11<sup>th</sup>, 2012, as Veterans' Week.  
(File No. CK. 205-5)

**2) Margaret Hendry, President, Canadian Federation of University Women Saskatoon Inc., dated July 13**

Requesting City Council proclaim September 14, 2012 as CFUW Day. (File No. CK. 205-5)

**3) Wilf Keller, President and Chief Executive Officer, Ag-West Bio Inc., dated July 23**

Requesting City Council proclaim September 14 to 21, 2012 as Biotech Week and extending invitation to event on September 20, 2012. (File No. CK. 205-5)

**4) Canadian Association of Family Enterprise Saskatoon Chapter, dated July 26**

Requesting City Council proclaim Thursday, October 18, 2012 as Family Business Day.  
(File No. CK. 205-5)

**5) John Brodsky, dated July 29**

Requesting City Council proclaim September 22, 2012 as Saskatoon Blades Day.  
(File No. CK. 205-5)

**6) Rick Morrell, Executive Director, Saskatchewan EcoNetwork, dated July 30**

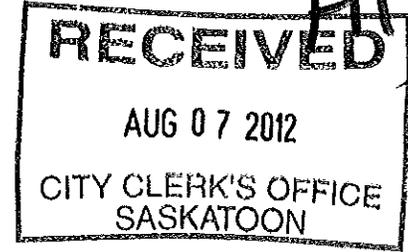
Requesting City Council proclaim September 30, 2012 as World Rivers Day in Saskatoon.  
(File No. CK. 205-5)

**Proclamations**  
**Wednesday, August 15, 2012**  
**Page 2**

7) **Kyla McKechnie, undated**

Requesting City Council proclaim September 2012 as Interstitial Cystitis Awareness Month.  
(File No. CK. 205-5)

- RECOMMENDATION:**
- 1) that City Council approve all proclamations as set out in Section D; and
  - 2) that the City Clerk be authorized to sign the proclamations, in the standard form, on behalf of City Council.



1224 Ontario Ave  
Saskatoon, SK S7K 1S5  
Office 306-664-5244  
Fax: 306-664-6477

August 7, 2012

To: Janice Mann (City Clerk)

Honorable Don Atchison and All Members of City Council

We are writing this letter in regard to the issuance of wheel chair accessible plates in the city of Saskatoon to the three taxi companies i.e. United Cabs, Comfort Cabs and Radio Cabs. At present United Cab has four wheel chair plates with approximately 80 taxis and Radio Cabs has four wheel chair plates with approximately 20 taxi plates. On the other hand, Comfort Cabs has only two wheel chair plates with a taxi fleet of approximately 65 vehicles.

This past fall when the city of Saskatoon was issuing seasonal plates, city administration decided that the number of plates issued was going to be four plates for United Cab, four plates for Comfort Cab and two plates for Radio Cab.

However, the manager of Radio Cab approached city Council and argued that each company should be treated the same, regardless of how many taxi plates each company had. Council decided to grant each company four plates. At that time Comfort Cab agreed with the proposal. Unfortunately the same formula was not implemented when Comfort Cabs applied for wheel chair accessible taxi plates in 2009.

The fact of not having the same number of wheel chair plates as the other two companies is not only increasing the waiting time for our wheel chair customers, but we are also risking losing our regular taxi business. As a customer will not use our regular taxi service if their loved ones who are bound to a wheel chair are not served in a timely manner.

We feel that Comfort Cabs did not get their share of the wheel chair plates, and therefore we are requesting that you grant us an additional two wheel chair plates, so all three companies can have an equal number of wheel chair plates. Comfort Cabs is a rapidly expanding and well respected company that has won the consumer choice award three times in a row. We are extremely proud of the reputation we have developed, and hope that council will consider our request and grant us two additional wheel chair plates as soon as possible.

We thank you for your time and kind consideration.

*Requesting to Speak*

✓ James Frie, Khodr Bardouh and Marwan Bardouh (Owners of Comfort Cabs Ltd)

***Ride in Comfort!***

Dion, the Minister of Intergovernmental Affairs, said yesterday."

Just a minute! Mr. Dion. An increasing number of Canadians are doing a little homework and study to find out why there is so much frustration and discontent with our federal government throughout the Western provinces, and so much separatist sentiment in Quebec. And we're finding that most of the frustration and concern stems from the unconstitutional manipulation and usurpation of provincial jurisdictions and sovereignty by Ottawa this past several decades. Just turn to Page 31 of our Constitution, and under Section 92 read the areas of *exclusive* provincial jurisdiction — and then note how many of those provincial jurisdictions your Federal Governments have invaded and usurped.

The reality is that frustration, alienation and separatist sentiment in several regions today is the direct result of the federal governments' years of increasingly unconstitutional manipulation and meddling in *exclusively* provincial affairs.

So come again, Mr. Dion, with a more valid story — after you've checked our Constitution. Yes, you now have a copy, because we sent you one several weeks ago.

### Kansas Senate Prayer 2000

Our March 8th incoming e-mail brought us the following incident in American politics. (But, of course, our Speaker of the House, I'm sure, wouldn't dare to let anything like this happen in our more staid, prudent and politically-correct conception of public decorum and propriety in our public institutions.):

"An interesting prayer was given recently in Kansas at the opening session of their Senate in 2000. It seems prayer still upsets some people.

"When Minister Joe Wright was asked to open the new session of the Kansas Senate with a prayer, everyone was expecting the usual generalities, but this is what they heard:

"Heavenly Father, we come before you today to ask Your forgiveness and to seek Your direction and guidance.

"We know Your Word says, 'Woe to those who call evil good,' but that is exactly what we have done. We have lost our spiritual equilibrium and

reversed our values. We confess that

"We have ridiculed the absolute truth of Your Word and call it 'pluralism.'

"We have worshipped other gods and called it 'multiculturalism.'

"We have endorsed perversion and called it 'alternative lifestyle.'

"We have exploited the poor and called it 'the lottery.'

"We have rewarded laziness and called it 'welfare.'

"We have killed our unborn and called it 'choice.'

"We have shot abortionists and called it 'justifiable.'

"We have neglected to discipline our children and called it 'building self-esteem.'

"We have abused power and called it 'politics.'

"We have coveted our neighbour's possessions and called it 'ambition.'

"We have polluted the air with profanity and pornography and called it freedom of 'expression.'

"We have ridiculed the time-honoured values of our forefathers and called it 'enlightenment.'

"Search us, Oh, God, and know our hearts today. Cleanse us from every sin and set us free. Guide and bless these men and women who have been sent to direct us to the centre of Your will, to open our hearts and ask it in the name of Your Son, the living Saviour, Jesus Christ.

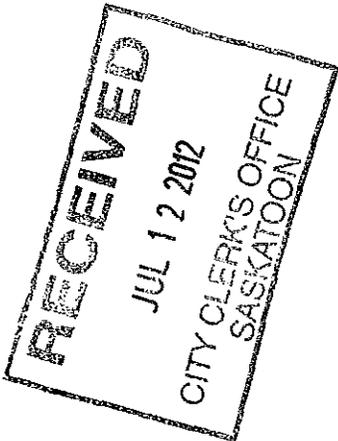
"Amen.

"The response was immediate. A number of legislators walked out during the prayer in protest. In 6 short weeks, Central Christian Church, where Rev. Wright is pastor, logged more than 5,000 phone calls with only 47 of those responding negatively. The church is now receiving international requests for copies of this prayer from India, Africa and Korea.

"Commentator Paul Harvey aired this prayer on 'The Rest of the Story' on the radio and received a larger response to this program than any other he has ever aired.

"With the Lord's help, may this prayer sweep over our nation and wholeheartedly become our desire, that we again can be called one nation under God.

For Council



Alma Day  
1994 Marisa CRT  
P.O. Box 67  
Bright Grove, ON  
N0N 1C0

Please acknowledge receipt. I have no postal code or street address so I'll never know if it was received.  
Don't buckled down to destroyers of our Canadian Heritage

150 BID

Procult political institute and once had a store front on Water Street in Vancouver's toney Gastown area. He had a lifelong fascination with numbers and for years immersed himself in the Kabbalarian practice of numerology. More recently, he had developed an immensely complex programme for forecasting astrologically advantageous times for risky activities like gambling. Born Jud Ireland, he followed Kabbalarian philosophy and changed his name to Cyllorn.

Jud was intensely bright and opinionated. His dark suit had been a characteristic since his teen years. At 16, he'd hitch-hiked from Ontario to New York City and was picked up by many drivers impressed by the suit on this young man. Jud was a proud European and was outraged at the government engineered replacement of the European population of his adopted British Columbia. Jud never shied away from controversy and once faced down a rowdy mob of communists protesting a talk by the late Ron Gostick at Jud's Water Street offices. Jud had a winning smile and a puckish sense of humour. A frequent caller to radio open line shows, Jud was adept at countering an opponent's punch. On one show about banned historian David Irving, the host thought he'd silence Jud by demanding: "Do you deny 6 million-Jews died? Do you? Do you?"

Jud sidestepped the trap. "I'm okay with that figure," he said disarmingly. "I'll go for 7 million, if that's what you say," he offered reasonably. "But, I'm okay with 8 million, if you say so," he added. By now he had effectively satirized the iron power of Zionist political correctness which seeks to make it impossible to even question the Hollyweird version of World War II. Jud was a speaker at the March, 1996 Second Canadian Free Speech Conference in Vancouver, sponsored by the Canadian Association for Free Expression and at several annual George Orwell Dinners sponsored by the Canadian Free Speech League.

In 1991, Jud authored a hefty book entitled Stop Apologizing! He hammered political correctness, guiltmongering and self-hating Whites. European sacrifice and hard work built Canada. We owe whining minorities nothing, he argued. Stop apologizing. [The gist of his views appears in the following article.] The book was widely distributed to opinion makers and to all MPs. There was the predicable storm of politically correct denunciations are dark calls in Parliament for a Sec. 319 "hate law" prosecution.

I shall miss Jud's good humour, wit, sharp intelligence and lifelong loyalty to his people, the European founding settler people of the Dominion of Canada. -- Paul Fromm



## THE INTEGRITY OF RACE

I am the son of a soldier, a taxpayer, a risk taker and an unauthorized guardian of what my Dad perceived as the 'collective good' in our society. From paying his bills on time, to never living beyond his means, an honest day's pay for an honest day's work, to respecting people as well as the law - these were the principles that I saw respected each and every day. Throughout the 160-year history as Upper Canadians as well as Canadians, my ancestors cleared land, financed and built docks, barns, bridges, roads, municipal halls, lobbied railway companies to build a station in their small town, volunteered as policemen and firemen, sat on school and church building committees and, like my own father, volunteered as soldiers when patriotism came knocking.

With each year that passed, a greater sense of permanence was created than the year before. A progressive ethic dominated the times that enabled pre-confederation Canadians to continually see the fruits of their labour before them. They were truly the 'Sons of Progress.' These people asked for little tolerance from society; yet, generation after generation they earned much respect. This is my Canadian heritage and for my insistence on the achievements of my ancestry and their peers being justifiably recognized, rather than today's relentless condemnations, I am called a "racist." In the last thirty years, the philosophy of those individuals that built this country has been found guilty of 'Sins of Progress', and there has been an obvious lack of recognition for any of the benefits of this philosophy. Those who shared no part in the progress of Canada becoming a nation present themselves as victims of the peculiar sin of nation-building. Thus, today's media-driven society continually demands that these Nation-building Canadians ignore their progressive heritage and make amends for their 'sins.'

My ancestral heritage has historically defied the natural environment, thus creating the progressive changes from building landing docks to financing and building railroads that future generations of immigrants from the old world immediately benefited from upon their arrival in Canada. My defiance of the natural environment is no different from my Great-Great-Great Grandfather's in 1832. His goal upon landing in the Bay of Chaleur in New Brunswick was to create, not destroy. It was to build, to forge and to grow, not to destroy something of anyone else's. He sought to develop a society that mirrored his own beliefs, that protected people of any race, that insisted on freedom of the press, that protected children under the legal age and strongly believed that every man was due a fair trial. Individually and collectively through a common ethic these early Canadians established a social order that was different than that of both Britain and the neighbouring United States, yet similar to all at the same time. Foreign immigrants, after Confederation, witnessed a combination of Britain's family class and the American's moneyed class upon their arrival to Canada. Thus the British family class provided the institutional order (e.g. the common law/court system) and the American moneyed class provided the opportunities to the early Canadian social order.

This made assimilation easier in Canada than in the rest of the colonies. Each individual, as well as social group, earned its own respect as they marked out their own social territories. Irish, Finns, Norseman, Swedes, Poles, Ukrainians, French or Jews they all had their own level of assimilation; individually or in concert. When someone called a newly-arrived immigrant a name, defaming their race, in actuality, it was pointing out their naiveté or indifference to the social laws of the land. There were no Human Rights Tribunals to run to, nor was the media a sympathetic ear to voice one's complaints. Records of newspapers published in British Columbia 100 years ago

will prove that there was a higher disregard for the non-assimilating immigrants by the media than we've been led to believe. Prior to 1960, immigrants to this continent either adopted the social laws of the land, lived in isolation, or faced being ostracised in the more urban areas. But that's all gone now.

Today, I'm to respect everyone else's heritage but my own. I have the only heritage scarred by the 'Sins of Progress.' There are societies within the new Canadian culture that consume but don't progress. And apparently, according to our courts, these societies are not responsible for any of the 'Sins of Progress.' Our courts demand tolerance of other societies' social codes without examination or debate, while I witness standards of my own heritage being crushed under the weight of judicially controlled morality. The word "racist" has now become the all-encompassing word that shrieks out the names of the Politically Incorrect, those 'responsible' for the evil deeds of history. It gives the accuser an unrelenting position, aided by a morally-rudderless society and a righteous media. The accused has no defence. He must stand trial before the media court that, like his heartless accusers, will prosecute and sentence the accused in absentia, to a political and/or financial death.

→ Note  
The people who share the Western Culture are being attacked by a politically funded army of 'Culture Crushers' whose religion is power and whose fuel is fear. These 'Culture Crushers' have demanded, in the name of equality and rights, that I stand trial for the sins of my heritage. Now that I realize that the progressive philosophy of the Western Culture carries a large Reparation Tax, passed by law, to pay off those who claim to be the victims of that progressive philosophy, I find that I cannot accept this as being fair. For those persons who share the bond of the Western Culture, all must now begrudgingly carry a larger tax load because of these 'sins.' If we protest, we will be tried by the political/ media courts for racism and sentenced by the court of controlled public opinion to a political death. My ancestral heritage is that of resisting plutocratic oligarchies (e.g. Magna Carta). Resisting such oppression - socially, economically and politically - is part of my culture, an inherent right built into the societal code of my own people. Remember, Western man said everyone was equal before the eyes of God and the law. They were not equal in any other way; thus the philosophy of respect for each other as individuals rather than a legislated form of equality. I come from a heritage of self-governing people and the overlying tone to the laws they created was fairness not equality. What courtroom do I stand before and argue that my heritage is being vandalized by placing someone's political rights ahead of what is fair for all?

Today's politicians and our forever righteous media laud those still suffering from historical wrongs and ruthlessly condemn any one who dares challenge politically acceptable interpretations of historical events. My response to this dogma was, in the beginning, to acquire more knowledge. Based on what I discovered, I now find myself in a state of aggressive contempt for those who, because of their self-serving beliefs, gender, sexual preference or skin colour, have beneficial status in the new quota system called 'affirmative action.'

This is our own version of 60s bussing, but for adults not school children. Rather than swapping urban ghetto kids for suburban kids like as Americans did, Canadians affirmatively displace some people and replace them with different people based on their sex, skin colour, etc. Any who dares question the logic of this special, largely racial 'quota system' will be tried as a racist and sentenced within 48 hours. When American legislators are asked today about bussing, they all conveniently find nice politically correct places to hide by saying, 'We meant well but how were we to know?' Those accused of racism by special interest groups, or by our pervasively righteous media, have

no access to the 'People's Court' (television or print). Thus the death sentence proceeds with the accused not even being allowed to address the charge. Having no medium for defence, the accused is guaranteed a quick death and no funeral. Would a philosophical mind not ask the accusers just how adequately equipped are they to judge the racist acts or thoughts of others? Are they ready to sentence someone to a political/financial death, ignoring the consequences of being wrong? Did they have to write a qualifying exam? Is there a racist identification programme at some redundant college that certifies graduates as having 'Racist Identifying Privileges' (R.I.P.s)? Is there a quota on who gets in the programme? Finally, is it funded by a 'special' federal programme?

I think my beliefs as a Canadian with 160 years of heritage on this continent have been sentenced to death by those with R.I.P.'s But I am not going to die. I am also not going to roll over and play dead nor become a living ghost as has happened to many of those who have, for example, questioned the financial affairs of war in this century. My heritage is that of making things better than they were before and, unlike many others, I can prove that -- not only on this continent but on two others. In a society where money rules man, my heritage will always lose. The Shackles of Guilt are the means used to surrender the proven benefits of creative individualism to the dogmas of the political 'World Order'. But soon the buildings, the bridges, the highways, the telephone lines, the hospitals, the sewers and the social order will all need rebuilding. Please remember these creations are only the physical expression of the philosophically progressive attitudes of Western thinking. I dare any one to examine those societies most in need of repair and note the degree of rejection of Western philosophy by its inhabitants.

Those among us today who have crushed the progressive culture for their indefinable Equality, will be first to demand that society be repaired. Our post '60s society has taught our youth, as well as new Canadians, to demand repairs to the tangible and intangible structures supporting our society, but not how to create them. Will those 'equalled' go to the equalizers (our courts, social workers or media) to fix the dysfunctional infrastructure or will they go to the global equalizers for technical expertise? When our nation's 'equals' dial 911 (getting put on hold for an indefinite time) and their call is no longer an emergency but a catastrophe, will they cease to believe the Egalitarian messengers? Nowhere has 'equality' created market order, only long lines of disillusioned souls waiting patiently for the promise of unearned rewards. My goals, like my God, are always fair, as were those of the five generations of Canadians before me. I seek the association of similar philosophies as a form of protection and secondly, but most importantly, to enhance the heritage of progress. I do this for my own benefit, for the benefit of all those around me and for those Canadians who are now not seen nor heard but will be responsible for the type of society Canada becomes 50 to 100 years from now. For this I am called a "racist"? So be it. -- **Jud Cyllorn** (This article appeared in Council of Public Affairs Digest, Vol. VIII, Nos. 9 and 10 (September 1 October 1992) under the title 'Culture Crushers'.)

### **On Again Off Again "Roma"**

The **Genius Trust**, otherwise known as "Canadian immigration officials, are once again contending with a large influx of Roma asylum seekers from the Czech Republic, prompting fears recently lifted visa requirements will be reinstated less than a year after they were lifted. According to **Immigration and Refugee Board** records, a total of 267 Czechs have applied for refugee status between Nov, 1 and March 31. In contrast, only 123 refugee claims were filed by Czech citizens between 2001 and 2007. [Gosh, could lower numbers somehow be associated with the visa then in effect?] Only five

WALKER

# We Are Survivors

(For those born Before 1940 . . .)

We were born before television, before penicillin, polio shots, frozen foods, Xerox, contact lenses, videos and the pill. We were before radar, credit cards, split atoms, laser beams and ballpoint pens, before dish-washers, tumble driers, electric blankets, air con-ditioners, drip-dry clothes . . . and before man walked on the moon.

We got married first and then lived together (how quaint can you be?). We thought 'fast food' was what you ate in Lent, a 'Big Mac' was an oversized raincoat and 'crumpet' we had for tea. We existed before house husbands, computer dating and 'sheltered accommodation' was where you waited for a bus.

We were before day care centres, group homes and disposable nappies. We never heard of FM radio, tape decks, artificial hearts, word processors, or young men wearing earrings. For us 'time sharing' meant togetherness, a 'chip' was a piece of wood or fried potato 'hardware' meant nuts and bolts and 'software' wasn't a word.

Before 1940 'Made in Japan' meant junk, the term 'making out' referred to how you did in your exams, 'stud' was something that fastened a collar to a shirt and 'going all the way' meant staying on a double-decker bus to the terminus.

In our day, cigarette smoking was 'fashionable', 'grass' was mown, 'coke' was kept in the coalhouse, a 'joint' was a piece of meat you ate on Sundays and 'pot' was something you cooked in. 'Rock Music' was a fond mother's lullaby, 'Eldorado' was an ice-cream, a 'gay person' was the life and soul of the party, while 'aids' just meant beauty treatment or help for someone in trouble.

We who were born before 1940 must be a hardy bunch when you think of the way in which the world has changed and the adjustments we have had to make. No wonder there is a generation gap today . . . BUT

By the grace of God . . . we have survived!

## THE NEW SCHOOL PRAYER

This was written by a teen in Bagdad, Arizona.  
This is incredible!

Now I sit me down in school  
Where praying is against the rule  
For this great nation under God  
Finds mention of Him very odd.  
If Scripture now the class recites,  
It violates the Bill of Rights.  
And anytime my head I bow  
Becomes a federal matter now.  
Our hair can be purple, orange or green,  
That's no offence; it's a freedom scene.  
The law is specific, the law is precise.  
Prayers spoken aloud are a serious vice.  
For praying in a public hall  
Might offend someone with no faith at all.  
In silence alone we must meditate,  
God's name is prohibited by the state.  
We're allowed to cuss and dress like freaks,  
And pierce our noses, tongues and cheeks.  
They've outlawed guns, but first the Bible.  
To quote the Good Book makes me liable.  
We can elect a pregnant Senior Queen,  
And the 'unwed daddy,' our Senior King.  
It's "inappropriate" to teach right from wrong,  
We're taught that such "judgments" do not belong.  
We can get our condoms and birth controls,  
Study witchcraft, vampires and totem poles.  
But the Ten Commandments are not allowed,  
No word of God must reach this crowd.  
It's scary here I must confess,  
When chaos reigns, the school's a mess.  
So, Lord, this silent plea I make:  
Should I be shot;  
My soul please take!  
Amen ✍

# Prayer doesn't belong at municipal events: CCLA

SASKATOON — Prayers would no longer have a place at government events in Saskatoon if the Canadian Civil Liberties Association has their way.

Sheetal Rawal of the CCLA sent a letter to Mayor Don Atchison calling for an end to prayer at municipal events.

"The state should have no role in imposing, endorsing or promoting a particular religion over others, nor should it pressure or coerce an individual into a religious practice," Rawal said in the letter dated June 5.

The letter also expressed support for Ashu M.G. Solo, who filed a complaint against Atchison, city councillor Randy Donauer, and the City of Saskatoon with the Saskatchewan Human Rights Commission on May 1. This,

after Solo felt discriminated against when Atchinson called on Donauer to lead a multi-religious audience in a Christian prayer at a Volunteer Appreciation Banquet organized by the mayor's office.

"I thought that was extremely inappropriate, extremely offensive and extremely discriminatory because it gives primacy to their religion over all other religions, it violates the separation of religion and government and it's an abuse of office in that they are trying to impose their religious beliefs on others who don't share them," Solo said.

Rebekah Bennetch, a member of the atheist community in Saskatoon and professor of professional communication at the University of Saskatchewan, agrees with Solo's original

complaint that asked for a public apology, but says he is going too far by launching a human rights complaint.

"This isn't necessarily a human rights issue," Bennetch said.

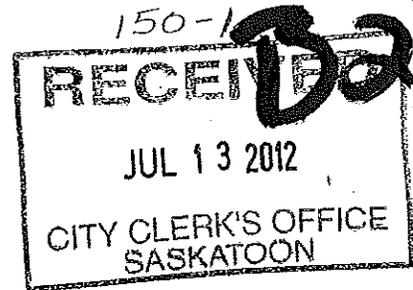
"I might not appreciate it as a non-believer, but I don't think necessarily my human rights were in jeopardy by having to sit through a 30 second prayer at a meal," she said. "I think it was insensitive - cultural insensitive - but I don't think legislating was the way to go."

Solo said the Saskatchewan Human Rights Commission is currently assessing his complaint to determine how to proceed.

Both Atchison and Donauer were unavailable for comment.

— Lisa Mrazek, QMI Agency

*Nobody asked him to come here  
If he doesn't like it he is free to leave  
When in Rome do as the Romans do*



Ms. Gertrude Armbrust  
1508-330 Dixon Road, Toronto, ON, M9R 1S9  
Ph. 416-242-3823  
July 13, 2012

Dear City Council of Canada,

Money needs to be trusted and inspected with a financial volunteer committee.

Every level of government, associations, school boards, condominiums and households are in financial trouble because they give themselves statements and not control over original bills and receipts. This is how millions get lost. The city counselors fail to protect the grass route people by not giving them protection by passing the law to have a bulletin board reserved for volunteers in every neighborhood. For example, loneliness is the number one illness causing financial stress as well. In my building, employees make money doing private work during job hours.

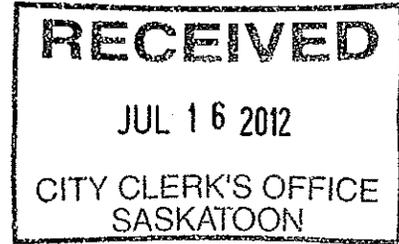
When will the city counselors consider the mental and financial state of the citizens of Canada? The city council should think about empowering neighbors to make a better world in volunteering like the high school students in Ontario.

Yours truly,

Ms. Gertrude Armbrust

B3)

From: Web E-mail - Mayor's Office  
Sent: July 16, 2012 11:48 AM  
To: Web E-mail - City Clerks  
Subject: FW: A Letter to the Mayor of Saskatoon



FYI

-----Original Message-----

From: [brian.jones6@sympatico.ca](mailto:brian.jones6@sympatico.ca) [<mailto:brian.jones6@sympatico.ca>]  
Sent: July 13, 2012 3:54 PM  
To: Web E-mail - Mayor's Office  
Subject: A Letter to the Mayor of Saskatoon

First Name: Brian  
Last Name: Jones  
Organization:  
Address: 10 McConnell Lane  
City: Newmarket  
Province: On  
Postal Code: L3Y 7M1  
Phone: 905 836 9503  
Fax:  
Email: [brian.jones6@sympatico.ca](mailto:brian.jones6@sympatico.ca)  
Comments: YNH Books

c/o  
Brian Jones  
10 McConnell Lane  
Newmarket, On  
L3Y 7M1

[brian.jones6@sympatico.ca](mailto:brian.jones6@sympatico.ca)

Attn: Mayor and all members of Council.

Dear Mayor Atchison,

Our country is blessed by a very diverse cultural mosaic. As a relatively new country our history is short. During this time Canadians have been called upon by our allies to join in battle against enemies that threaten the common values and way of life we share.

Thoughts of Our Canadian Soldiers at War is a book of short stories I wrote that reflects how our soldiers and their families have been affected by Canada's participation in the Boer War through to our most recent mission in Afghanistan.

The stories are meant to act as a catalyst for Canadians of all ages to preserve and strengthen the respect, honour and knowledge of what these fine people have sacrificed and continue to do so for the lifestyle we are so fortunate to have.

As a member of Council and as a proud Canadian, I am asking you to consider purchasing a copy for yourself, a family member, for your office or a member of your community, now or as we approach Remembrance Day.

The cost of a copy is \$8.00 net plus postage. If you are interested in purchasing a copy or more please contact me at:

brian.jones6@sympatico.ca

I would be pleased to forward the electronic version of the cover and text (pdf's) to you, upon your request, for your evaluation.

Yours Sincerely,

Brian Jones

YNH Books

Her Majesty's Canadian Ship SASKATOON  
PO Box 17000 Stn Forces  
Victoria, BC V9A 7N2

12 June 2012

MM 709: 1000-1 (CO)

His Worship Donald Atchison  
Mayor of Saskatoon  
222 Third Avenue North  
Saskatoon SK S7K 0J5

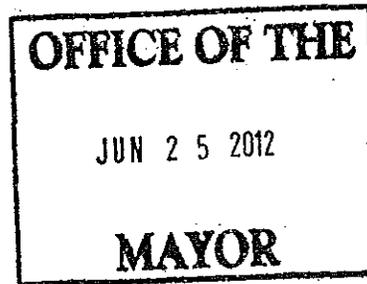
Dear Mayor Atchison,

It is my distinct privilege to update you on the activities of your namesake ship, HMCS SASKATOON in the first half of 2012. The ship has seen approximately 75 days away from Esquimalt since the end of January, so this has been an extraordinarily busy new year!

In late January, we spent a week escorting the U.S. submarine USS JACKSONVILLE while conducting trials off of Nanoose, B.C. This was followed by a two week exercise in February called PACIFIC GUARDIAN where several hundred personnel were involved in a port security exercise based in Vancouver. Although it was an intense period of time, quite a bit of training was accomplished owing during this opportunity. In the last week of February, I sent several members of the regular ship's company ashore for the week and embarked 25 members from HMCS UNICORN, Saskatoon's Naval Reserve Division. We sailed as together as one ship's company giving an outstanding opportunity and experience to UNICORN's personnel as well as SASKATOON's, and the week was such a success that we are already looking ahead to repeating this next year.

Our next task was to support the trial programme for the submarine HMCS VICTORIA, and we spent several weeks assisting her on the path to operational readiness. The ship's company enjoyed the unique opportunity to sail in immediate proximity to a Canadian submarine, and took a good deal of pride in watching the progress. Finally, during the last few months, we conducted training while helping support the workup programs for two sister ships, and spent some time conducting underwater operations with the Bottom Object Inspection Vehicle embarked.

As you can imagine, the past few months have been busy and exciting, as well as gratifying to observe all of the training milestones accomplished. I was very pleased to have been able to visit the city for the recent Battle of the Atlantic ceremony, and enjoyed the hospitality immensely. As I mentioned during my last visit to the city, I will be relinquishing command soon, to Lieutenant Commander Larry Moraal. Invitations should be forthcoming in several weeks, but the date has been confirmed as September



6<sup>th</sup>. In the meantime, I am looking forward to my last deployment this summer to the San Diego operating areas as part of the multinational exercise RIMPAC.

I would like to thank you and the city for your support during the past two years of my command of HMCS SASKATOON. I am certain that the incoming Commanding Officer will continue to nurture this relationship, and receive support through the excellent community connections that are in place. The experience of being a ship's Captain is one that I will always treasure and I thank my good fortune for having been given the opportunity and honour of commanding one of Her Majesty's Canadian Ships.

Best wishes for the future,



Patrick J. Montgomery  
Lieutenant-Commander  
Commanding Officer

127-1  
BS)

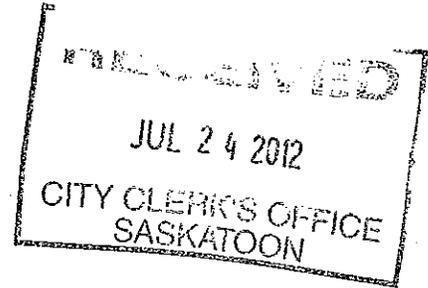
June 25, 2012

His Worship Donald J. Atchison

Office of the Mayor

222 3<sup>rd</sup> Avenue North

Saskatoon, SK. S7K 0J5



Dear Mayor Atchison,

I am writing to express the strong support of Weldfab Ltd. in Saskatoon, SK. for the **municipal Free and Fair Trade Act**.

In today's globalized economy, the trade related policies and actions of foreign governments and offshore corporations can drastically impact our local markets, the quality of our built environment and ultimately our economic wellbeing. In recent years, the Canadian steel construction market has experienced a significant surge in offshore imports primarily from steel producing countries that are experiencing structural overcapacity resulting in a surplus of production.

In an effort to offload their excess capacity, many companies in these offshore markets are aggressively resorting to unfair trade practices - undercutting prices, dumping their products, and leveraging substantial government subsidies in their home country to gain a competitive edge in the Canadian marketplace. These practices are clearly in contravention of the World Trade Organization's guidelines and other international trade related treaty obligations, and severely undermine the ability of Canadian firms to compete, ultimately threatening Canadian jobs and economic recovery.

Steel products that are manufactured overseas also, in many instances, do not conform to the quality, environmental and labour standards mandated in Canada resulting in the long term degradation in the quality of our built environment.

An important lesson for us is the recent experience of the Australian steel construction industry who has seen their domestic businesses crippled and jobs decimated as a direct result of unfair trade practices by offshore companies. The state and federal governments are now dealing with the resultant fallout of high unemployment and structural market changes.

The Canadian steel construction industry is prepared to compete based on fair and open markets, and will continue to challenge unfair market distortions. Long-term progress on this

problem will require that trade rules be strengthened and strongly enforced, and that meaningful action is taken to address the growth in excess capacity that continues in many steel producing countries.

The **municipal Free and Fair Trade Act**, proposed by the Canadian Institute of Steel Construction, provides a framework that will ensure that only products that meet the highest quality and safety standards, and are procured through fair and open trade practices, are marketed in Canada. The Act proposes the adoption of the following key principles by municipal governments in their decision making:

1. Municipal Governments shall give fair and preferential treatment to Canadian construction products, materials and engineering, and all other municipal procured products and services in the conditions where:
  - a. The country of origin artificially deflates or undervalues their currency in relation to the Canadian dollar or
  - b. The product, material and or service from the country of origin is produced in a facility that does not meet or exceed the Canadian, Provincial and Municipal environmental regulations or
  - c. The product or service from the country of origin does not meet or exceed the Canadian, Provincial and Municipal health and safety standards or
  - d. The foreign country, state, or municipality does not allow similar, fair and equal access to their markets for the same product and or services
  - e. The country of origin appears to be *dumping* the product, material, or service (or ingredient, component, or base materials within each) into Canada

Time is of the essence. As a member of the Canadian steel construction community and a constituent, we urge you to support the introduction and passage of this bill in your municipality.

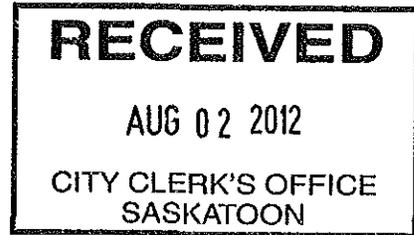
Thank you

A handwritten signature in black ink, consisting of a large, stylized initial 'P' followed by a series of horizontal strokes that extend to the right.

Sincerely

B6) 5-9

**From:** CityCouncilWebForm  
**Sent:** August 02, 2012 11:00 AM  
**To:** City Council  
**Subject:** Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Carla Paul  
301 385 Kingsmere Blvd  
Saskatoon  
Saskatchewan  
S7J 4J6

EMAIL ADDRESS:

[carlajpaul@gmail.com](mailto:carlajpaul@gmail.com)

COMMENTS:

Lakeview Community Association is hosting its 2nd annual Rider Day in the Park on Sunday September 9th.

Rider Day in the Park is an annual family-fun event that brings together families and individuals from the area to watch the Saskatchewan Roughriders play Winnipeg Blue Bombers on the big screen under a tent in Lakeview Park. This year the Rider Game kicks off at 11am and we will begin to set up our equipment at 10 am and will begin having amplified noise starting at 10:30 am.

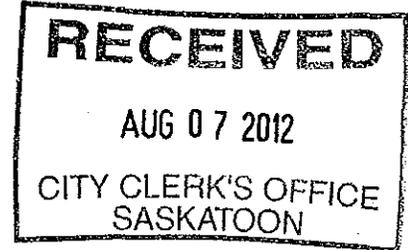
We ask City Council to ammend the noise bylaw from 1pm to 10:30am to accommodate our special event on Sunday September 9th.

Thanks for your time and look forward in hearing from you.

Sincerely,  
Lakeview Community Association

02 August 2012

Office of the City Clerk  
2nd Floor, City Hall  
222 3rd Ave. North  
Saskatoon, SK S7K 0J5



Dear His Worship the Mayor and Members of City Council,

I am writing to request traffic lane closures to facilitate an exciting community event in Riversdale and Broadway this September called **Our Saskatoon**. I have outlined our request below and also included some background information on **Our Saskatoon** to convey the larger context of the event.

## REQUEST

**Our Saskatoon** wants to demonstrate a Complete Streets concept, where streets facilitate a number of transportation alternatives: cars, bikes, public transportation and foot traffic. To this end, we are working with Saskatoon Cycles to bring safe bike traffic to 20<sup>th</sup> Street on Friday, September 21. We are requesting the closure of two traffic lanes to create bike lanes on 20<sup>th</sup> Street between Avenue B and Avenue C.

Currently, biking on 20<sup>th</sup> Street is prohibitive because of the street width and priority given to cars. During **Our Saskatoon**, we want to demonstrate that proper bike lanes make cycling an attractive and safe alternative to driving. By closing down two lanes of traffic, we can work with Saskatoon Cycles and the City of Saskatoon to create a viable option for travel along 20<sup>th</sup> Street for the day.

We have considered requesting the closure of the entire block for the day. However, **Our Saskatoon** is focused on creating activities and installations that demonstrate real, long-term opportunities for urban planning. 20<sup>th</sup> Street is an important roadway for car traffic, and our intention is not to disrupt but augment that with other options for transportation.



## ABOUT OUR SASKATOON

On Friday, September 21 2012, people around the world will participate in Park(ing) Day, an annual event where designers, citizens and businesses transform metered parking spots into temporary public parks. In 2011, Park(ing) Day had 975 parks created in 162 cities and 35 countries. In 2012, **Our Saskatoon** will participate in this global movement with a local demonstration that transforms Riversdale into a world-class example of sustainability and creativity.

On Thursday, Sept 20 **Our Saskatoon** begins with a kick-off event at Broadway Theatre, featuring art performances, guest speakers Ken Greenberg and Allan Wallace, and Park(ing) Day demonstrations.

Park(ing) Day begins on Fri, September 21 with metered parking spots transformed into public parks around the University, Broadway and Riversdale.

In Riversdale, a range of activities will position the neighborhood as a sustainable, bike-friendly complete street where community and business thrive. These activities include:

- The parking lot at 20<sup>th</sup> Street and Avenue B will be transformed into a public square with green space & public seating. The public square will feature:
  - Buskers, dancers and visual artists
  - Public games (bocci and horseshoes)
  - Graffiti wall, with professional artists mentoring youth
  - Storytelling
  - Yoga classes by Hot Yoga on 20<sup>th</sup>
  - Installation by the Saskatoon Engineering Students' Society. They will bring a real-life recreation of the Park(ing) Day logo: an upside down car with a garden on top
  - Movie night projection after sundown
- Popup retail stores showcasing local fashion (Saskatoon Fashion and Design Festival) and art (The Stall Gallery), with music and bar in the evening
- Farmer's Market activities
- A MIXED MEDIA 3 party at the Two Twenty that evening featuring DJs, bar and food

Collectively, these activities demonstrate that in healthy communities, streets are not just intended for moving cars. They are critical community hubs that serve as focal points of commerce and culture, where people shop, dine, stroll, play, and meet. Streets that are conducive to walking, biking, public transportation *and* cars, move more people, more efficiently and at a lower cost than typical car-oriented roads.

Our Saskatoon is an expression of conversations that are shaping the city—from Saskatoon Speaks, to the City of Saskatoon's Culture Plan, and Great Places events. Our citizens, businesses and organizations are talking about our future as a sustainable, creative city with a high quality of life that attracts residents, business and investment. With the support of His Worship the Mayor and Members of City Council, we are confident **Our Saskatoon** will help our community realize its full potential.

Following is a list of the partners who have committed to participating in **Our Saskatoon**: Saskatoon Cycles, Saskatoon Fashion & Design Festival, the Two Twenty, deezine.ca, Mix & Match Marketplace, Stunn Collective, Parviz Yazdani, SKARC, AODBT, Saskatoon Engineering Students' Society, Hot Yoga on 20<sup>th</sup>, Village Guitars & Amp Co., Nutana Community Association, Broadway BID, Riversdale BID, Broadway Theatre, Stantec, Road Map Saskatoon, Meewasin Valley Authority, Great Places, U of S Regional and Urban Planning Program, The Stall Gallery, Mane Productions, Susan Busse

Please let me know if you require more information. I look forward to hearing from you.

Sincerely,



Carrie Catherine  
Our Saskatoon Event Organizer

B8)  
785-1

From: CityCouncilWebForm  
Sent: August 03, 2012 1:49 PM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Jasmin Fookes  
202 Ave C South  
Saskatoon  
Saskatchewan  
S7M 1N2

EMAIL ADDRESS:

[jasmin.f@saskatoonfoodbank.org](mailto:jasmin.f@saskatoonfoodbank.org)

COMMENTS:

Hello,

I am the Urban Agriculture Coordinator for the Saskatoon Food Bank's Garden Patch on the 900 blk of 2nd Ave N. On the evening of Tuesday September 11th we would like to hold a volunteer appreciation evening. We would like to screen a free movie in the garden commencing at dusk. I am writing to request a noise bylaw exemption for this purpose.

Warmly,  
Jasmin

[www.sfbgardenpatch.com](http://www.sfbgardenpatch.com)  
[@sfbgardenpatch](#)



*City of*  
**Saskatoon**

Saskatoon Development  
Appeals Board

B9)

---

c/o City Clerk's Office      ph 306•975•8002  
222 - 3rd Avenue North      fx 306•975•7892  
Saskatoon, SK S7K 0J5

July 19, 2012

His Worship the Mayor  
and Members of City Council

Ladies and Gentlemen:

**Re:    Development Appeals Board Hearing  
      Refusal to Issue Development Permit  
      Landscaping Plan Condition of Building Permit #2421/07  
      (Landscaping Deficiency)  
      1710 Idylwyld Drive North – IL1 Zoning District  
      101184134 Saskatchewan Ltd.  
      (Appeal No. 27-2012)**

In accordance with Section 222(3)(c) of *The Planning and Development Act, 2007*, attached is a copy of a Notice of Hearing of the Development Appeals Board regarding the above-noted property.

Yours truly,

**Shellie Bryant**  
Secretary, Development Appeals Board

SB:drs

Attachment

Templates\DABs\Mayor.dot



City of  
**Saskatoon**

Saskatoon Development  
Appeals Board

c/o City Clerk's Office      ph 306•975•8002  
222 - 3rd Avenue North      fx 306•975•7892  
Saskatoon, SK S7K 0J5

NOTICE OF HEARING - DEVELOPMENT APPEALS BOARD

<b>DATE:</b>	Monday, August 13, 2012	<b>TIME:</b>	4:00 p.m.
<b>PLACE:</b>	Committee Room E, Ground Floor, South Wing, City Hall		
<b>RE:</b>	Refusal to Issue Development Permit Landscaping Plan Condition of Building Permit #2421/07 (Landscaping Deficiency) 1710 Idylwyld Drive North – IL1 Zoning District 101184134 Saskatchewan Ltd. (Appeal No. 27-2012)		

TAKE NOTICE that Ksum Holdings has filed an appeal under Section 219(1)(b) of *The Planning and Development Act, 2007*, in connection with the City's refusal to issue a development permit based on refusal of a revised landscaping plan issued for 1702 Idylwyld Drive North.

The property is located in an IL1 Zoning District.

Section 11.1.8(1) of the Zoning Bylaw states that a landscaped strip of not less than 4.5 metres in depth throughout lying parallel to and abutting the front site line shall be provided. Based on the proposed site plan, an area of 4.5 metres adjacent to the property line containing 1.5 metres of landscaping and a 3 metre brick area resulting in a landscaping deficiency of 3 metres.

Section 7.1(1)(i) of the Zoning Bylaw states that one tree is required for every 6 linear metres along required frontages and one tree is required for every 9 metres along required flankages. Based on the information provided, the landscaped area on the proposed site plan would be considered the frontage and therefore would need trees spaced every 6 metres. The proposed span is noted as having 3 trees spaced at 9 metres apart. A total of 5 trees are required spaced 6 metres apart resulting in a deficiency of 2 trees.

Section 11.1.8(1) of the Zoning Bylaw states that a landscaped strip of not less than 4.5 metres in depth throughout lying parallel to and abutting the front site line shall be provided on every site and shall be used for no purposed except landscaping and the necessary driveway access to the site. The submitted plan notes that the brick in the 4.5 metre area adjacent to the front property line is a car lift. Parking of vehicles in this area would not be permitted.

**The Appellant is seeking the Board's approval to allow the landscaping deficiency.**

Development Appeals Board  
Appeal 2012-27

Anyone wishing to provide comments either for or against this appeal can do so by writing to the Secretary, Development Appeals Board, City Clerk's Office, City Hall, Saskatoon, Saskatchewan, S7K 0J5 or email [development.appeals.board@saskatoon.ca](mailto:development.appeals.board@saskatoon.ca). Anyone wishing to obtain further information or view the file in this matter can contact the Secretary at 975-2783.

Dated at SASKATOON, SASKATCHEWAN, this 19<sup>th</sup> day of July, 2012.

Shellie Bryant, Secretary  
Development Appeals Board

Templates\DABs\Dab-A



*City of*  
**Saskatoon**

Saskatoon Development  
Appeals Board

**B10)**

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c/o City Clerk's Office      ph 306•975•8002  
222 - 3rd Avenue North    fx 306•975•7892  
Saskatoon, SK S7K 0J5

July 30, 2012

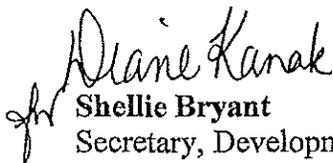
His Worship the Mayor  
and Members of City Council

Ladies and Gentlemen:

**Re:    Development Appeals Board Hearing  
      Refusal to Issue Development Permit  
      Proposed Detached Accessory Building (Swimming Pool Enclosure)  
      (Exceeding Maximum Allowable Height and Total Floor Area)  
      36 Lindsay Drive – R1 Zoning District  
      Sabah Shaya  
      (Appeal No. 28-2012)**

In accordance with Section 222(3)(c) of *The Planning and Development Act, 2007*, attached is a copy of a Notice of Hearing of the Development Appeals Board regarding the above-noted property.

Yours truly,

  
Shellie Bryant

Secretary, Development Appeals Board

SB:drs

Attachment

Templates\DABs\Mayor.dot

**NOTICE OF HEARING - DEVELOPMENT APPEALS BOARD**

<b>DATE:</b>	<b>Monday, August 13, 2012</b>	<b>TIME: 4:00 p.m.</b>
<b>PLACE:</b>	<b>Committee Room "E", Ground Floor, South Wing, City Hall</b>	
<b>RE:</b>	<b>Refusal to Issue Development Permit Proposed Detached Accessory Building (Swimming Pool Enclosure) (Exceeding Maximum Allowable Height and Total Floor Area) 36 Lindsay Drive – R1 Zoning District Sabah Shaya (Appeal No. 28-2012)</b>	

TAKE NOTICE that Sabah Shay has filed an appeal under Section 219(1)(b) of *The Planning and Development Act, 2007*, in connection with the City's refusal to issue a Development Permit to construct a detached accessory building (swimming pool enclosure) at 36 Lindsay Drive, which is located in an R.1 Zoning District.

Section 5.7(3)(b) of the Zoning Bylaw states no detached accessory building shall have any part of its roof in excess of 5.0 metres (16.4 feet) in height. Based on the plans submitted by the applicant, the height of the detached accessory building will be 5.64 metres (18.52 feet). This results in the maximum height being exceeded by 0.64 metres (2.12 feet).

Section 5.7(3)(e) of the Zoning Bylaw states no detached accessory building to a one-unit dwelling shall have a total floor area greater than 87.0 square metres (936.46 feet). Based on the plans submitted by the applicant, the proposed detached accessory building will have a floor area of 122.41 square metres (1317.56 square feet) and an existing detached accessory building located on the site has a floor area of 49.53 square metres (533.14 square feet), for a total floor area of 171.94 square metres (1850.7 square feet). This results in a total floor area exceeding the maximum permitted floor area by 84.94 square metres (914.24 square feet).

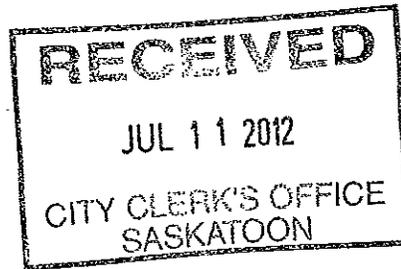
**The Appellant is seeking the Board's approval of the proposed height and total floor area for the detached accessory building.**

Anyone wishing to provide comments either for or against this appeal can do so by writing to the Secretary, Development Appeals Board, City Clerk's Office, City Hall, Saskatoon, Saskatchewan, S7K 0J5 or email [development.appeals.board@saskatoon.ca](mailto:development.appeals.board@saskatoon.ca). Anyone wishing to obtain further information or view the file in this matter can contact the Secretary at 975-2783.

Dated at SASKATOON, SASKATCHEWAN, this 30th day of July, 2012.

Shellie Bryant, Secretary  
Development Appeals Board

4/39 - 4



CD

July 8, 2012

Elaine Orvedahl  
828 Temperance St  
Saskatoon, SK S7N 0N1

City Council  
City Hall, 222 3<sup>rd</sup> Ave North  
Saskatoon, SK S7K 0J5

To Whom It May Concern:

It is no news to anyone in Saskatoon that we had a terrific windstorm last week. In fact, one of the many trees which blew down was across Temperance Street and down the length of my neighbour's lot on Colony Street. It took a couple of days before the city removed it from Temperance Street because it had caused no damage to property. How does this affect me?

Well, I have contacted the city on more than one occasion about the poplar in front of my house which has a huge limb which overhangs my living room. Although Mr. McLeod tells me that it is a cottonwood, the truth is that cottonwood is just another name for poplar and poplars have a limited lifespan. I have no desire to wake up in the middle of the night, stuck in my bedroom because the tree has wiped out my living room and blocked the door to my bedroom. Since I live on the east side of the street and the prevailing winds are from the northwest the possibility is certainly there.

I am particularly concerned because the tree has the habit of losing its leaves every June as well as every late fall so I do not believe that it is healthy. Not only have I contacted the city with this concern, but my late husband also made complaints before he died. In addition, the previous owner of the property, Sophia Dixon, also contacted the city about the tree. Mrs. Dixon was very old and really hated the mess the leaves created not once but twice each year. I am not asking for the whole tree to be removed, merely the branch which overhangs my house. It is your tree and your responsibility. If it were my tree I would have dealt with it a few years ago.

This letter is intended to inform you of the problem because a friend of mine tripped on a part of the sidewalk which the city had not repaired and was told that the city would not pay for her broken glasses because they were not aware there was a problem. I am taking the precaution of having the letter to you and the one for my records stamped by a commissioner of oaths so there will be no doubt that the city has been informed of the problem in case that limb does come down onto my house and I have to sue for costs not covered by my insurance policy.

Sincerely,

A handwritten signature in cursive script that reads "Elaine Orvedahl". The letters are fluid and connected, with a prominent loop at the end of the last name.

Elaine Orvedahl

# Notarial Certificate

I, Donna Rederburg, of the City of Saskatoon, in the Province of Saskatchewan, a Notary Public, in and for the Province of Saskatchewan, by Royal Authority duly appointed;

**Do Hereby Certify** that hereunto annexed is a true copy of:

Letter dated July 8, 2012 from Elaine Orvedahl to City Council, City of Saskatoon, City Hall.

The said copy having been compared by me with the said original document; an act whereof being requested, I have granted under my notarial form and seal of office to serve and avail as occasion shall or may require.

DATED AT THE CITY OF SASKATOON, IN THE PROVINCE OF SASKATCHEWAN, THIS 9<sup>th</sup> DAY OF JULY, 2012.



A Notary Public in and for  
the Province of Saskatchewan.  
Expiry Date May 31, 2014



6001-1  
(ca)

July 11, 2012

Mayor Atchison and Council  
222 3<sup>rd</sup> Ave. N.  
Saskatoon, SK S7K 0J5



Mayor Atchison and Council

The Circle Dr North parking lot (aka Circle Dr) appears to be at a point of complete breakdown. I believe it is time the City via Saskatoon Police Services to take some steps to help mediate some of the everyday problems Monday – Friday.

While the design of the street is the major flaw, the number of vehicles adds to the problem, next the number of semi units using this roadway and finally by the mindless flow of drivers that follow the herd over the cliff!

A constant and endless number of drivers mindlessly drive into an intersection along Circle Dr. at Northridge, Faithful, or Quebec Ave. and end up not being able to complete the movement out of the intersection which then prevents traffic flow in the opposite direction. It is time that Saskatoon Police Services makes an impression on drivers to stop this practice.

While obviously assuring the safety of police officers it might be wise to have a blitz where these drivers that block intersections are rewarded with a ticket. From observations I think it would be quite possible to issue a ticket during the time the vehicle sits in the intersection without impeding traffic flow – at a minimum the officers would be able to get the plate number and request they pull over to an assigned area to complete the process.

This practice not only occurs along Circle Dr. but other areas of the City and would be a great opportunity to train/re-train old and new drivers. Waiting for the South Bridge project to be completed in the hope it will reduce the traffic on Circle Dr. is burying ones' head in the sand – deal with this problem that will continue regardless the status of the South Bridge project.

Sincerely,

John Thomson  
202 Nixon Cres.  
Saskatoon, SK

PS – why is nothing being done about the rut/hole caused by improper paving on the over-pass on Circle Dr. over Idylwyld Dr. eastbound – I've called about it, nothing is done and has been like this since the repaving along Circle.

5301-1  
C3

From: kylejgorman@yahoo.ca  
Sent: July 11, 2012 7:39 PM  
To: Web E-mail - City Clerks  
Subject: parking complaint  
  
Importance: High

**RECEIVED**  
JUL 12 2012  
CITY CLERK'S OFFICE  
SASKATOON

## City of Saskatoon Website 'Contact Us' Message

To: City Clerk's Office  
From: kyle gorman "[kylejgorman@yahoo.ca](mailto:kylejgorman@yahoo.ca)"  
Subject: parking complaint

Message: To Whom It May Concern,

I am writing to inform you of an unfortunate incident that took place last week with one of the city's parking attendants.

This is in regards to ticket number 59164020.

My kid brother and I were doing errands last week and we had parked downtown. Realizing our metre was about to expire my younger brother went back to the car while I finished our last errand.

While I was inside, my brother was approached by the parking attendant. He informed my brother that if he plugged the metre by the time he finished writing the ticket, we would not receive the ticket. So, my brother entered our car and plugged the metre with one dollar. It was at this point that I was walking down the sidewalk and witnessed what took place next.

Even though my brother had plugged the metre, the attendant continued to enter the ticket into the electronic system they carry. I heard my brother say:

"But you said we wouldn't get a ticket if I plugged the metre". There was about 5 seconds of silence at which my brother again asked why the attendant was continuing to enter the ticket. As I arrived, the attendant did print the ticket off the machine, and place the ticket on the windshield.

My brother at that point looked at me and said "I don't understand what's happening" to which the attendant responded:

"too late, that's your ticket, BUDDY".

And then he walked away.

I said 'excuse me', as to get the attendants' attention, but he plainly ignored me and continued to walk away.

Now, I understand having a bad day and going on a bit of a power trip, I can almost understand lying to my kid brother about giving a ticket after he had plugged the metre, but what I cannot understand is the ageist, elitist behavior I witnessed this gentleman display towards a young teenager!

Quite frankly, the derogatory term "buddy", as it was used in this situation, can carry very serious connotations for those involved. The tone of sarcasm and disrespect with which this term was directed at my brother is completely unacceptable by any sort of societal standards. Words cannot express my disappointment in the attendant's behavior.

The attendant should consider himself fortunate that both my brother and I are not at all confrontational, because if we were to act towards him with the same level of disrespect and impertinence he displayed towards us, that situation would have been awful.

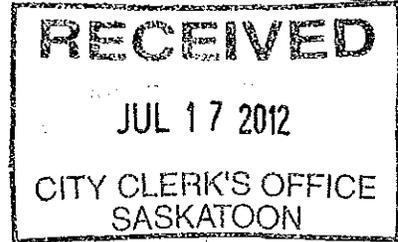
My little brother was quite adversely affected by this encounter, and I was extremely disappointed in the way the attendant treated him. I know my brother, he is a quiet, soft-spoken and respectful kid who wouldn't say a mean thing to anyone, it's just the way he is.

I am not writing this to get out of a ticket, I could care less about the 20 dollars. I am writing this because it is one week after the incident, and I am still appalled at the behavior I witnessed. I would appreciate a response with some sort of resolution to the above situation.

Many Thanks,  
Kyle Gorman, MD, FRCPC  
Staff Anesthesiologist, Regina Qu'Appelle Health Region

6280-1  
C4

July 15, 2012



His Worship the Mayor and City Council  
Saskatoon City Hall  
222 3<sup>rd</sup> Avenue North  
SASKATOON SK S7K0J5

It has come to my attention that there is a significant anomaly in the School Zone on Acadia Drive at the western most intersection with Champion Cres and Simon Fraser Cres. It appears one portion of the School Zone is approximately four times longer, for vehicles proceeding in an eastbound direction, than that portion of the School Zone for vehicles heading in a westbound direction. The problem appears to arise from an attempt to utilize the western most School Zone sign for a purpose for which school zone signs were not intended to be used.

My understanding is that it is the intent of the city with respect to School Zones to:

- 1). Minimize the number and size of Schools Zones to ensure maximum compliance and minimum inconvenience to the motoring public while insuring the safety of school children,
- 2). the design of school zones is to comply as closely as possible with the Uniform Standards for Municipalities for ease of use by the motoring public while maintaing public safety.

It appears to me the unintended use of the School Zone sign may create a hazardous situation where inattention or complacency on the part of parents or children, or the failure of motorists to realize they are entering a School Zone so far from the school, could a endanger child and cause serious injury or even death.

Is my belief that moving the western most school zone sign approximately 95 meters east would resolve the issue. It would, in my view create a school zone of equal size in both directions in the school zone, in keeping with Uniform Standards, without compromising safety.

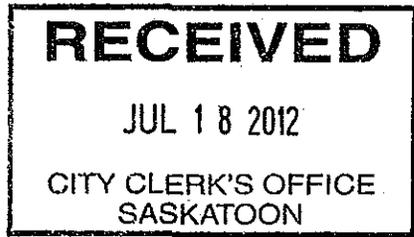
As a concerned taxpayer of Saskatoon I would strongly encourage the City to review the situation and take remedial action to avoid an unfortunate situation that may precipitate in litigation against the City of Saskatoon.

John Coutts

421 Guelph Cres  
Saskatoon SK  
S7H 4R2

6315-1  
C5

From: CityCouncilWebForm  
Sent: July 18, 2012 4:41 PM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP, THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Edward Kozowy  
1314 Wiggins Avenue S  
Saskatoon  
Saskatchewan  
S7H 2J6

EMAIL ADDRESS:

[edkozowy@gmail.com](mailto:edkozowy@gmail.com)

COMMENTS:

City of Saskatoon Road improvement task force should ask the City of Calgary how they keep thier streets and roads in such good shape compared to Saskatoon.

When I moved here a few months ago I was shocked at the substandard condition of most neighborhood streets. Although I applaud Saskatoon on the alternative transportation message, I would suggest re-paving a few streets so that Cyclists / Boarders/small Automobiles have a chance of making it between pot-holes.

Let me know what measures are being taken to improved the impoverished look of our roads.

Sincerley

Ed Kozowy

7310-1  
C6

From: CityCouncilWebForm  
Sent: July 18, 2012 7:31 PM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Edward Danneberg  
1418 Ave N South  
Saskatoon  
Saskatchewan  
S7M2R3

EMAIL ADDRESS:

[edanneberg@gmail.com](mailto:edanneberg@gmail.com)

COMMENTS:

Re: Transit changes in Holiday Park

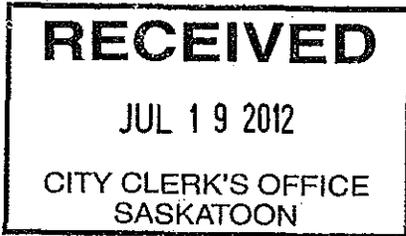
Once again we would like to have the transit situation in Holiday Park looked at and re-addressed. After more than a year, it is plain to see that the change in route from Schuyler, to Avenue M, Wellington and Avenue N, has been a mistake. The ridership is almost ZERO, at any time of the day or night and the negative affects of having bus traffic in a residential neighborhood are many. The added noise, street surface damage, danger to children (in a high-density family area) and property encroachment vs. the very few riders that use the stops on Avenue M or Wellington just do not make sense. One of the main reasons given for the change was to service Arbor Green, the "retirement complex" on Avenue M. In close monitoring by several residents, it is plainly obvious that few - if any - of the Arbor Green residents use transit. They generally have their own cars, or have special transit choices to choose from. As well, something we have seen on a regular basis, is the close calls children have been in as they play on and around Wellington in front of the Embassy Gardens. At some point something terrible will happen.

Please re-address this issue with all parties fairly represented.

Sincerely,  
E. Danneberg

4139-4  
C1

**From:** CityCouncilWebForm  
**Sent:** July 19, 2012 10:02 AM  
**To:** City Council  
**Subject:** Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Anita Hrytsak  
346 McCormack Rd  
Saskatoon  
Saskatchewan  
S7M4T2

EMAIL ADDRESS:

[anita.hrytsak@producer.com](mailto:anita.hrytsak@producer.com)

COMMENTS:

City tree removal:

This is regarding the tree next to my house.  
Tenants #344 McCormack Rd. (SK. Housing Duplex)

Front tree needs the branches removed where it's  
broken and dead.

Thank you.

6320-4  
C8

From: CityCouncilWebForm  
Sent: July 19, 2012 9:49 AM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Tyler Gilewicz  
235 Adilman Drive  
Saskatoon  
Saskatchewan  
S7K 7R7

EMAIL ADDRESS:

[tyler@excaliburequip.com](mailto:tyler@excaliburequip.com)

COMMENTS:

Hi there,

I work in the North Industrial and I must say it is a giant mess. Circle Drive between Millar and Ave. C is so backed up it takes 45 minutes just to get from Faithful to Millar.

This North Bridge should help, but its not going to solve all the problems. When there is a train during rush hour (which should NEVER happen but it does), or construction on Warman or the on-ramps from Warman to Circle, the ENTIRE North end locks up. Its stupid. The North end is the stupidest design ever.

What ever happened with the original plans to have the current Circle Drive being Westward only, and East was to be beside the railroad tracks. Separate roads. And why would Faithful not meet at Quebec? That causes SO MANY issues.

The railroad crossing all down Millar are so bad people have to come to a complete stop to go over in order to keep their cars in one piece.

I love this city, but the North End street systems being so messed up will only get worse as the city grows, which you all want. As do I.

Thanks for your time, hopefully someone wakes up and starts fixing the North end.

Tyler Gilewicz

4-110-1  
C9)

From: CityCouncilWebForm  
Sent: July 19, 2012 12:44 PM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Peter Derrick  
1850 Main Street  
Saskatoon  
Saskatchewan  
S7H 5T2

EMAIL ADDRESS:

[peterderrick@gmail.com](mailto:peterderrick@gmail.com)

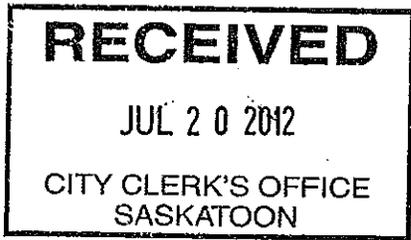
COMMENTS:

Please explain why we need to pay \$500,000.00 for a consultant to develop a "master" plan for the inner city north-end?

Do we not have qualified city planners with enough experience and exposure to what other cities are doing in terms of developing similar plans?

The cost is a concern - do we not have this expertise in-house and if not - why not?

From: CityCouncilWebForm  
Sent: July 19, 2012 5:24 PM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Ryan Harder  
108-406 Nelson Road  
Saskatoon  
Saskatchewan  
S7S 1N5

EMAIL ADDRESS:

[rjharder@gmail.com](mailto:rjharder@gmail.com)

COMMENTS:

I am writing today to implore city council to consider implementing rent control policies.

My fiance and I support ourselves and our daughter on a modest income of approximately \$60,000 a year combined. I am a substitute teacher and work a second job on weekends, and my fiance works a summer job for four months and a part time job while pursuing her commerce degree.

We are currently in the unenviable position of being trapped in a renting situation. We would like to buy an approximately \$180,000 3 bedroom townhouse to support our family which we hope will be getting larger in the future. However, as a substitute teacher and a university student, our income is not secure or high enough to get a mortgage.

Meanwhile, our landlord (Weidner Apartment Homes) has raised our rent from a hefty \$1190 per month to a ridiculous \$1360 per month for our 800 sq ft 2 bedroom apartment. They also gave us no notice of the increase, with their justification being that as a lease expires and you go month to month, no notice is required. We are going to the rentalsman...but regardless, it is clear that our living costs will continue to skyrocket even if we choose not to have children.

There are many mortgage assistance programs in place to help people in my income bracket escape renting. There is also the Mortgage Flexibilities initiative put forward by the city. However, neither of these help us, because we would still first need to be approved by CMHC - who in their recent crackdown on "wild" indebtedness, have deemed us and people in our situation too untrustworthy to pay an \$850 a month mortgage.

The city needs to put a lid on the ever profitable gouging of the middle class by landlords. Please consider helping growing families who are being shut out of the housing market by taming out of control rent. We are deemed "too poor" to afford \$850 a month, but apparently not too poor to afford nearly \$1400 a month.

At a time of record low mortgage rates, and record high rents, it is the young working class who find themselves suffering the consequences in the middle.

Thank you.

**From:** CityCouncilWebForm  
**Sent:** July 20, 2012 11:01 AM  
**To:** City Council  
**Subject:** Write a Letter to City Council

**RECEIVED**

JUL 20 2012

CITY CLERK'S OFFICE  
SASKATOON

TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Ken Helgeson  
837 Ave. T North  
Saskatoon  
Saskatchewan  
s7l3b8

EMAIL ADDRESS:

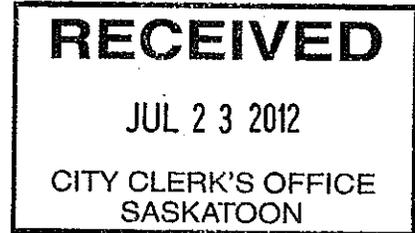
[k.helgeson@sasktel.net](mailto:k.helgeson@sasktel.net)

COMMENTS:

I wanted to know as well as many others i talk to, WHY on the Red light cameras the walk/don't walk lights are not switched over to the number count down walk lights. I think it will be much safer for people. Because i don't really feel like getting rear ended or rear end someone just so the city can make some money off the lights. At least with count down a person can see if the light is going to change soon, a person can slow down and stop safely instead of jamming on the brakes and getting smoked from behind. You have the lights down town.. just wish this city got got with the times and did something right.

Ken Helgeson

**From:** CityCouncilWebForm  
**Sent:** July 23, 2012 8:42 AM  
**To:** City Council  
**Subject:** Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Nathan Rushton  
323 Klassen Cres  
Saskatoon  
Saskatchewan  
S7R0E9

EMAIL ADDRESS:

[nathan@cpttechnical.ca](mailto:nathan@cpttechnical.ca)

COMMENTS:

Hi there,

Just wondering if the berm behind my house is on a rotation for getting the grass cut? It hasn't been cut yet this year and the mosquito's are quite bad. Also the weeds turn to seed and blow into my yard. Can you please put this on a regular rotation for mowing.

Thanks

Nathan

6120-3  
C19

From: CityCouncilWebForm  
Sent: July 23, 2012 11:55 AM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Denis Vally  
6-11355, Cottonwood Drive  
Maple Ridge  
British Columbia  
V2X 2C6

EMAIL ADDRESS:

denis.vally@ipsgroupinc.com

COMMENTS:

Re - Single Space Parking Meter Technology.

A few months ago Council rejected the submissions for renewal / replacement of the parking meters/equipment installed in the city - RFP #11-0973  
From reports in the press it appears that there is a perception that single space meters are "old technology" This may have been true four years ago but is clearly not the case today. We respectfully request that we (IPS Group Inc.) be allowed to address the council at your meeting on 15th August to brief council on the current technology that is available in single space meters.  
We would need ten to fifteen minutes of your time  
We are aware that council afforded one of our competitors some time to present their case regarding this RFP.

6315-1  
C15

From: CityCouncilWebForm  
Sent: July 23, 2012 10:50 AM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Almabella Mascarina  
1319 Preston Ave South  
Saskatoon  
Saskatchewan  
S7H 2V5

EMAIL ADDRESS:

[alma\\_0826@yahoo.com](mailto:alma_0826@yahoo.com)

COMMENTS:

A pleasant day to you.

First and foremost I really dont know where to address this issue on our alley. The alley between Argyle St and Preston Avenue. There are already lots of pot holes making it hard for vehicles to pass through. I hope you can help us on this matter.

Thank you so much.

And have a wonderful day

4129-15  
C16

**From:** Peter Derrick [peterderrick@gmail.com]  
**Sent:** July 23, 2012 3:28 PM  
**To:** Web E-mail - City Clerks  
**Cc:** info@djcarhitect.com  
**Subject:** Email - Communications to Council - Design plan of hotel condo retail development south downtown - File No. CK. 4129-15



To: City Clerks; Please pass this on to Mayor & Council, thank you.

Dear Mayor & Council of Saskatoon,

I felt compelled to echo the sentiments of the Star Phoenix letterwriter below with respect to the design plan as envisioned by Victory Majors Investments & Co., incorporating a hotel, retail and condo development for the south downtown part of our city. A complex of this magnitude and significance should be overseen by a designer who is most connected to our landscape and therefore I would like to strongly recommend that an architect such as Douglas Cardinal be commissioned to undertake this project. His particular aesthetic and extensive portfolio make him arguably the most appropriate Canadian architect to take on this proposal and make it a truly Saskatchewan inspiration. His well known architectural work and award winning accomplishments make him the truly stand-out candidate to design such an important part of our new downtown. So this is a plea for Council to encourage those responsible for this project to woo the best in Canada and lobby for Douglas Cardinal to produce a great organic design that would best fit the proposed site near where our river turns. It would be great if the sinuous curves of our river could be included in the design elements for this complex. Why do we have to settle for the rectangular box-like structures that seem to dominate our architectural landscape? I am sure that I am not alone in this opinion and that this letter gets heard by those who can truly make a difference and produce something truly iconic for this city and this great province of Saskatchewan. Thank you.

Sincerely,

Peter J. Derrick  
1850 Main Street  
Saskatoon, SK  
S7H 5T2 306 651 3666

*"After waiting decades for the south downtown to be developed, we're finally on the cusp of the major push. Here we have a once-in a-century opportunity to make a major, bold individual architectural statement a la Dubai or Shanghai or Bangkok, one that would make Saskatoon truly shine. So what do we have presented by the builders instead? Design so bland and boring, so lacking in creativity, so out of the late 1950s that it looks like something drawn up by a C student in architecture 101. I can't describe my disappointment. It's bad enough that we're stuck with that folly of a condo whose design was based on the even uglier technical school, happily trucked to the dump where it belongs. Now much of the rest of the most valuable property in the city is being further squandered in the desperation to Get Something Up. Builders: Do you want to be lauded for all time for creating something brilliant, inspiring and imaginative in River Landing? Or do you want to be remembered for burdening us with River Letdown?"*

*Jason Schoonover  
Saskatoon  
Star Phoenix July 23rd 2012*

375-2  
C17

From: CityCouncilWebForm  
Sent: July 24, 2012 7:37 PM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Myron Kowalsky  
523 Brookmore Cres.  
Saskatoon  
Saskatchewan  
S7V1C2

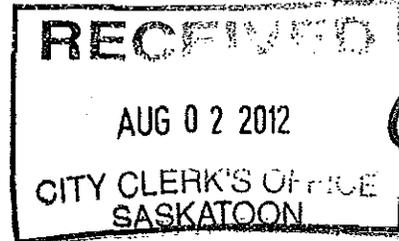
EMAIL ADDRESS:

[pmk@sasktel.net](mailto:pmk@sasktel.net)

COMMENTS:

Dear City Mothers and Fathers,  
Regarding noise:

One thing you can do to reduce noise pollution is to pay attention to what can be done to reduce the decibel level of the city buses.  
I reside across the street from a bus stop. Too often when the bus driver "steps on it" the noise exceeds the racket of any other vehicle on Briarwood Road.  
Please consider converting to quieter mufflers, and being mindful of the loudness when purchasing replacement buses.  
In the mean time, reminding the drivers that a "sweet retreat" is preferable to "putting the pedal to the metal".  
Regards,  
Myron K.



Sylvia J. Carpenter

42 Pondarosa Trailer Court,

Swift Current,

Sask. S9H 3X6.

12/00/26,

Saskatoon City Council,

Star Phoenix, Saskatoon.

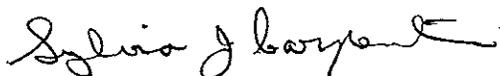
Dear Sirs,

I would like to bring to your attention your handling of Street closures in your city.

This is not the first time I have written to you about this problem. July 13<sup>th</sup> I was on my way to your City to see the NEIL DIAMOND Show. I was going to go to my Daughter and family's place in Warman coming from Swift Current I was on highway #7. I arrived just about 1 PM rush hour in any city, so not to add to the congestion in the city I decided to go up on the Dalmeny Rd.

After making a left hand turn at the lights at the junction of #7 and #14 I found that this road was closed no detour signs or information was evident on #7 about the closure. I might add I was not the only driver to have to make a U turn at this point. Do you want people to spend money in you city or not? This attitude to the travelling public is not exceptable. I fully realize that the roads and streets need repairs, but would it take much time to put up notices in advance of the closure? I also realise that the weather does not co-operate with the repair crews but, customer service is worth a lot to the public travelling to your city of Saskatoon.

Yours Sincerely,



Sylvia J Carpenter.

From: CityCouncilWebForm  
Sent: July 26, 2012 4:43 PM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Garnet Fehr  
241 Dalhousie Cres  
Saskatoon  
Saskatchewan  
S7H 3R9

EMAIL ADDRESS:

[garnet.fehr@saskatoon.ca](mailto:garnet.fehr@saskatoon.ca)

COMMENTS:

Often when road/crew workers have a primary intersection or street shutdown, traffic gets redirected through quiet neighborhoods. I would like to recommend that when this happens a temporary speed restriction of 30 km/h is also enforced. Kids playing in and walking through quiet neighborhoods won't be anticipating the mad frenzy of traffic that will be pushing speeds of 50+ km/h because of drivers who are trying to meet their deadlines. I seen this first hand in my own crescent earlier in the spring, and was completely shocked to find high volumes of traffic race down our narrow street like it was Acadia. I was very concerned for not only my own kids, but the many kids in the neighborhood.

Hearing that Preston North will be shut down on Friday reminded me that the parallel streets will be seeing high volumes of drivers - likely driving aggressively to keep on schedule.

Please understand that redirecting heavy traffic through quiet neighborhoods with no restrictions on speed creates a dangerous and risky environment for unsuspecting residents and children.

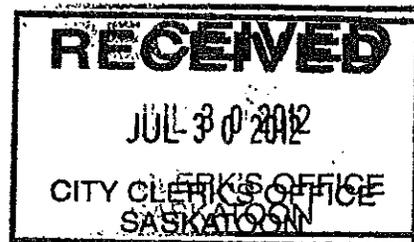
Drivers should be required to moderate their speeds accordingly when taking a detour through a quiet neighborhood; otherwise they should be finding an alternative route altogether.

Thank you for listening.

Garnet

CaO  
6315-1

From: CityCouncilWebForm  
Sent: July 28, 2012 9:46 AM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Darla Taylor  
2813 Melrose Ave  
Saskatoon  
Saskatchewan  
S7J 0W1

EMAIL ADDRESS:

[superstardar99@hotmail.com](mailto:superstardar99@hotmail.com)

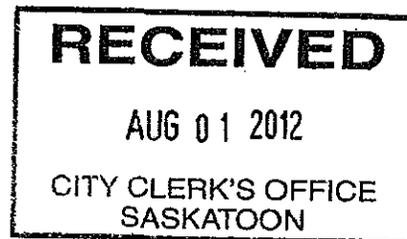
COMMENTS:

February 6, 2012 there was a water main break right in front of my house, which caused the city to come in and tear up the road in front of the house to fix it. They still have not come and fixed the road, they continue to put gravel on the area that was effected, however, this is just causing more issues. I have rock chips on my car and jeep, as well, I am having to constantly wash my car and jeep, inside and out to keep it clean. As well there is a big hole in front of my house that they just stuck a pylon in to show that the hole was there, now the pylon is gone and the hole is still there. We also have had flooding issues on the street in front of our house. Every time we have a heavy rain we have to call the city to come and unplug the drains, which in return we have had a water and sewer back up in our basement. I have called to find out when they are going to fix the road and they said they don't have it scheduled yet. Down the same street they have come and filled in a couple holes that in my opinion didn't even need to be done. This is getting ridiculous, we also received a letter to make sure our garbage bin is in our yard and not left in the alley. That is fine, but we are expected to keep up our yard and we pay taxes to the city to have the area around our house kept up to better standards than this. This has gone on long enough and they need to come and pave the road in front of our house and fix the huge hole beside the sidewalk. What are my taxes going toward if they cannot even maintain the road right in front of my house. Can you please check into this issue and please do something about it soon.

Thank you,  
Darla Taylor

Cal  
4205-9

From: CityCouncilWebForm  
Sent: August 01, 2012 1:16 PM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Charlie Bueckert  
1302 Avenue P North  
Saskatoon  
Saskatchewan  
S7L 2W9

EMAIL ADDRESS:

COMMENTS:

My family and I were enjoying an evening out at Kinsmen Park last night (July 31/12) when a vehicle drove around the concession building and proceeded to drive around the other side on the walkway. I would like to see a No Vehicles Allowed sign, posts erected or fencing put up. There were children playing in the area and I don't believe any vehicles should be allowed there. Any concessions being delivered should be carted or wheeled from the parking lot. It's only a matter of time before a kid runs into the vehicle or vice versa. Roads are for vehicles, not parks!

Please let me know what action you can take on this matter. Thank you.

7310-1 C22)

**From:** CityCouncilWebForm  
**Sent:** August 01, 2012 1:21 PM  
**To:** City Council  
**Subject:** Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Mary Anne Jenks  
3302 Caen Street  
Saskatoon  
Saskatchewan  
S7M 3P2

EMAIL ADDRESS:

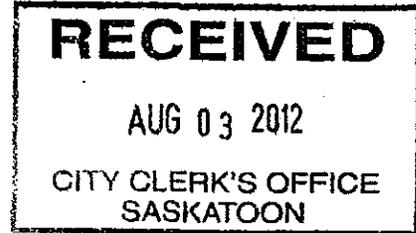
[maryanne\\_jenks@yahoo.ca](mailto:maryanne_jenks@yahoo.ca)

COMMENTS:

With regards to the #23 Montgomery bus route and schedule, I find the schedule great for the weekdays but the Saturday schedule needs to be changed because there are a lot of Montgomery residents that work on Saturday ( including myself) and need half hour buses in Montgomery from Confederation Mall, which isn't happening right now, please help rectify this. With regards to the route I think it's great, I think the route it much shorter than it was when it was part of the #3 Route and I find I can make the 7:30 shift at work on Saturdays and this is good because the boss now knows he has backup if the other person can't make it.

C23)  
5500-7

From: CityCouncilWebForm  
Sent: August 03, 2012 2:06 PM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Brenda Proulx  
504 1 st street east  
hepburn  
Saskatchewan  
s0k1z0

EMAIL ADDRESS:

[bren\\_j@live.ca](mailto:bren_j@live.ca)

COMMENTS:

hi i'am writing to you today because i like many other parents with young disabled children don't have the same opportunities as others for our kids. my son just turned three years old and he is unable to walk. and i have been looking for programs and activities in the city where he can be a little more independent. he is a very bright and energetic little boy. we planned on putting my neices and daughter into summer and fall programs and asked them what the wanted to do and when my son said he wanted to do something there was no answer i could give him. he understand alot about his condition and knows there are things he will not get to do but he will try anyway. i have found plenty of programs for older disabled kids but unfortunately i have only found one program for the smaller children with disabilities and its the tumbleweeds program the uofs puts on. i do my best to not treat him any differently then other. unfortunately there are times when i'am trying to plan activities with my son, and my 2 nieces and my daughter and i have to limit what i can do with them because he cant do a lot of the stuff because there are no options or facilities that adapt to physicyly disabled kids. i have tried to reasurch and i have found stuff that is out of province but i really cant afford to keep driving all over the country for things if anyone knows of anything that is out there it would be greatly apreciated.

6150-1  
x5200-4

C24)

From: CityCouncilWebForm  
Sent: August 05, 2012 9:47 AM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Mae Juleff  
3707 Duke Road  
Victoria  
British Columbia  
V9C 4B6

EMAIL ADDRESS:

[maejae@shaw.ca](mailto:maejae@shaw.ca)

COMMENTS:

Saskatoon is a very beautiful city and I have been visiting every year for approximately 35 years. I stay in the Broadway/Main Street area.

I have two questions that I would like to be addressed.

1/ I'm wondering why there is not a button at the crosswalk on the Broadway/Main Street in front of the Extra Food store to cross over Broadway. The other three corners have one and it would be a lot more convenient to have one at the above corner.

2/ It is great to see so many people riding bikes around the city and every year there are more. I see signs all over that say riders must dismount on the sidewalk, but they don't and lots of the riders ring their bell to let you know they are coming and expect the pedestrian to move over for them. It is especially bad on the bridge. Is this a law or a suggestion? Also, I would like to know what the law is regarding bike helmets as most of the riders do not wear them.

I was going to send this to the city manager, but could not find a e-mail contact address.

I look forward to a reply.

Thanks,  
Mae

6295-1  
C25)

From: CityCouncilWebForm  
Sent: August 03, 2012 8:00 AM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Anthony (Tony) Korte  
131 Nordstrum Road  
Saskatoon  
Saskatchewan  
S7K6P9

EMAIL ADDRESS:

tony.korte@shaw.ca

COMMENTS:

To all the members of City Council, and Randy Donauer councillor for Ward 5  
Good morning Randy,  
Thank you Randy for stepping up to the many duties and issues demanded by Ward 5 since the by-election. I congratulate you and endorse many of your thoughts regarding the city's issues. I don't contact councillors normally, but I feel this letter has been received with less than positive response.  
The buffer strip closure is not necessary. To put up gates on both ends is not the answer. This area is used on the occasional time frame by the residents to access backyards. We use the strip for access for hauling garbage out, renovations in, etc. This council or infrastructure change creates a major inconvenience that is not necessary.  
First of all, there are ruts in the normally lawn area in the buffer strip, however, these ruts were done by contractors working on the wall, and with the wet year we've had, has never had a chance to dry up. If there would be some maintenance done in this buffer strip, the ruts could be controlled and the area would actually dry up. If anyone, contractor, city worker, whoever, would use the area now, it would make matters worse. By putting up a barrier, sure, you'll stop the traffic from going back there, but I feel this is just another 'control' issue the city administration uses to solve an issue that the citizens who live here, will, and have not abused in the past. These same citizens and taxpayers did not abuse the area in the past.  
Secondly, we live eight houses in from the Russell Road end of the buffer strip. We have NEVER had anyone drive back there that has caused any sign of abuse. I have not seen anyone drive back there, in fact, other than the contractors that did the work on the wall. There is no need ofr this inconvenience.  
Thirdly, if the city would put a little time and effort into the maintenance of this area, like lawn cutting, etc, it would be more desirable to use this area for a walking trail, biking trail, etc. Right now, its overgrown with tall grass and weeds under the trees and open areas.  
Fourth item, I was, totally, in favor of the sound attenuation wall for the purpose of sound attenuation. I was hoping that the wall would not isolate the area too much to create a "trouble" area for hooligans and rif-raf to hang around and cause 'issues' and vandalism. It has done any of that to this point... a little graffiti, that's it... by closing it off.. you'll be making a more isolated area for crime activities to take place, because it'll take

more time for authorities and police officers to gain access to that area. This is not a desirable end to the issue of traffic in this area.

Fifth item, more related to the third item. make this area 'nice and pretty' and well maintained and the problem take care of itself. As I said earlier, there have been NO issues on our end of the buffer strip.

Please have this letter forwarded to City Council and Infrastructure Service, Traffic Management Engineer, Shirley Matt.

Please have myself included in any corresponding communication regarding this issue, as I feel very strong about citizens controlling themselves with less government control.

Regards,  
Tony Korte  
131 Nordstrum Road  
Saskatoon, Sk.,  
p. 934-6738  
c. 270-3894

6295-1

RECEIVED

AUG 07 2012

CITY CLERK'S OFFICE  
SASKATOON



July 30, 2012

City Clerk  
City of Saskatoon  
222 3<sup>rd</sup> Avenue North  
Saskatoon Sask. S7K 0J5

Re: Lane between 333, 301 and 401 Acadia Drive

Dear Mayor and City Councillors.

We are pleased to submit to you our request to acquire the lane between 333, 301 and 401 Acadia Drive. As of November 2005 Sherbrooke Community Society Inc. (SCSI) became the owner of all three properties which are now separated by the lane. SCSI is a not for profit charitable organization.

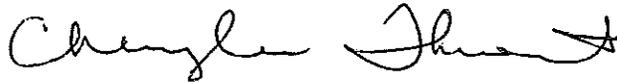
SCSI (through Sherbrooke Foundation Inc.) would like to acquire the lane referred to above, for the reasons listed.

1. Sherbrooke maintains the lane; this includes snow removal and maintenance. *As a result the city has not incurred any costs associated with the lane, all costs for maintenance have been borne by Sherbrooke.*
2. Sherbrooke would be able to monitor and enforce parking in the lane. Cars parked inappropriately can impede medical and fire emergencies, and put residents at risk.
3. Sherbrooke in accordance with the Saskatoon Health Region is a non smoking facility. By acquiring the lane it would also become a non smoking area, which would further deter residents and staff from smoking on our property.
4. The traffic through the lanes both in front and behind Sherbrooke is significant at times coincidental to the start and the end of the school day for the high school in the neighbourhood. Speeding is a problem which puts residents, visitors and staff at risk.
5. The historic main purpose for the lane was to provide access to the Sisters of Sion building. This building is no longer there and Sherbrooke owns all adjacent property.



We appreciate your consideration of our request to acquire the lane by Sherbrooke Community Society Inc. (through Sherbrooke Foundation Inc.). As we are a not for profit organization we would ask that you consider transferring the property to us at no cost in consideration of the management, maintenance, and security costs that have been covered by Sherbrooke.

Sincerely,



Cherylee Theoret

EFO



Patricia Roe

Leader of Communications & Public Relations

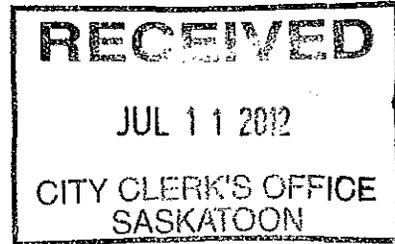
cc:

Darren Hill, Ward 1 Councillor, City of Saskatoon  
Pat Lorje, Ward 2 Councillor, City of Saskatoon  
Ann Iwanchuk, Ward 3 Councillor, City of Saskatoon  
Myles Heidt, Ward 4 Councillor, City of Saskatoon  
Randy Donauer, Ward 5 Councillor, City of Saskatoon  
Charlie Clark, Ward 6 Councillor, City of Saskatoon  
Mairin Loewen, Ward 7 Councillor, City of Saskatoon  
Glenn Penner, Ward 8 Councillor, City of Saskatoon  
Tiffany Paulsen, Ward 9 Councillor, City of Saskatoon  
Bev Dubois, Ward 10 Councillor, City of Saskatoon

Eric Olauson, Ward 8 Candidate, City of Saskatoon  
Ainsley Robertson, Ward 8 Candidate, City of Saskatoon  
Dave Wilton, Ward 8 Candidate, City of Saskatoon  
Karen Rooney, Ward 8 Candidate, City of Saskatoon

REMEMBRANCE DAY COMMITTEE

205-5  
DI



July 4, 2012

Mayor and Members of City Council  
City Of Saskatoon  
222-3<sup>rd</sup> Ave. N.  
Saskatoon, Sk. S7K 0J5

RE: PROCLAMATION FOR VETERAN'S WEEK NOVEMBER 5<sup>th</sup> TO 11<sup>th</sup>, 2012

Dear Mayor and Council:

On behalf of all Veterans and the Remembrance Day Committee of Saskatoon we are requesting that the City of Saskatoon proclaim the week of November 5<sup>th</sup> to 11<sup>th</sup> as Veteran's week.

Yours truly

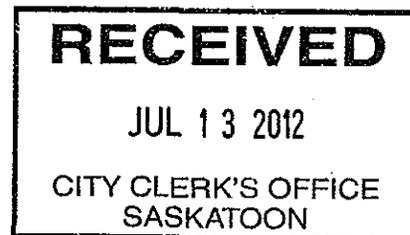
A handwritten signature in black ink, appearing to read "Jim McKinny". The signature is fluid and cursive, with a large loop at the beginning and a long horizontal stroke at the end.

Jim McKinny – Secretary Remembrance Day Committee

CC: Brent Wignes  
Don McDonald

205-5  
D2

From: CityCouncilWebForm  
Sent: July 13, 2012 11:02 AM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Margaret Hendry  
1323 Broadway Avenue  
Saskatoon  
Saskatchewan  
S7H 2A5

EMAIL ADDRESS:

[margaret.hendry@shaw.ca](mailto:margaret.hendry@shaw.ca)

COMMENTS:

City Clerk's Office  
City of Saskatoon  
222-23rd Street East  
Saskatoon  
S7K 0J5

To: Mayor and City Councillors:

The Canadian Federation of University Women Saskatoon Inc is a non-profit group advocating for all aspects of education. In particular we work to ensure that women have access to furthering their aspirations in all fields. To support this end we have provided financial aid for women since the 1930s. In the last year we provided \$29,500 in scholarships to students at the University of Saskatchewan and to a student graduating from Royal West Campus. We also support the music festival and the science fair in Saskatoon and donate a painting by a local artist to a Saskatoon elementary school each year

This is financed by donations from our past and current members and by the profits from our mammoth book sale held each October. Our members and supporters contribute many hours of labour to bring this about.

In 2012 the provincial government will again declare September 14 as CFUW Day in Saskatchewan. CFUW Saskatoon would like to celebrate the occasion by asking you to declare September 14, 2012 as CFUW Day in Saskatoon. We would appreciate this recognition of our contribution to the community. We look forward to continuing with our work.

I would be happy to speak to council on this matter and to provide further information. Thank you for your consideration

Sincerely

Margaret A. Hendry  
President  
Canadian Federation of University Women Saskatoon Inc

Box 7405  
Saskatoon S7K 4J3  
306-373-8693  
email: [info@cfuw-saskatoon.org](mailto:info@cfuw-saskatoon.org)  
website: <https://sites.google.com/site/cfuwsaskatoon/>



205-5  
D3)



July 23, 2012

Mayor Don Atchison  
c/o City Clerk's Office  
City of Saskatoon  
222 - Third Ave North  
Saskatoon, SK S7K 0J5

**RE: Proclamation of Biotech Week – September 14 - 21, 2012**  
**Invitation to speak at Biotech & Beer at Boffins – September 20, 4:45pm**

---

Dear Mayor Atchison,

We would like to begin by thanking you for your support of Canada's National Biotech Week (NBW) by participating in events and by proclaiming Biotech Week in the City of Saskatoon in past years. We would be grateful if the City of Saskatoon would proclaim September 14-21 Biotech Week for 2012.

This countrywide event celebrates the imagination, opportunities and success of biotechnology innovators across Canada. The Saskatchewan NBW steering committee has planned a wide range of exciting activities to showcase the province's achievements and encourage involvement in the life sciences and growth in our bioeconomy.

Your proclamation will complement a variety of events that promote and celebrate the biotechnology industry. Two education outreach events are planned this year: **The Amazing Biotech Race** for post-secondary students (Sept. 20), and **Biotech Blast** for students in grades 7 and 8 (Sept. 18). We are also planning luncheons at Innovation Place. And once again, we toast the science of biotechnology with the **Fourth Annual Biotech & Beer at Boffins**.

**I would like to extend an invitation to you to attend Biotech & Beer at Boffins (4:00 to 7:00pm, Thursday, Sept. 20)** and hope you will relay a few words of wisdom and encouragement to the young scientists, business people and seasoned researchers who will be in attendance.

Saskatchewan hosts over 30 percent of Canada's agricultural biotechnology activity, with many other life science areas taking hold here, including genomics, plant-made pharmaceuticals and diagnostics. Two university campuses and three research parks, along with numerous research institutions and technical training centres, create a significant science cluster - a great benefit to businesses, from start-ups to international success stories. Life science discoveries foster new business opportunities in the areas of biofuels and bioproducts, functional foods and natural health products. Outstanding science R&D infrastructure, a talented workforce, quality raw materials and a solid business environment make Saskatchewan the perfect place for bio-business!

For more information about Biotech Week activities in Saskatchewan, visit the Ag-West Bio website: [www.agwest.sk.ca](http://www.agwest.sk.ca), or contact Communications Director Jackie Robin by email: [jackie.robin@agwest.sk.ca](mailto:jackie.robin@agwest.sk.ca), or by telephone: 306-668-2656. For information about activities across the country, visit the BioteCanada's National Biotechnology Week website: [www.imagenation.ca](http://www.imagenation.ca).

Thank you again for your outstanding support for Saskatchewan's bioeconomy. We look forward to working with you to celebrate excellence in biotechnology in our province and country.

Sincerely,

Dr. Wilf Keller  
President & CEO  
Ag-West Bio Inc.

205-5  
D41

Jenkins, Sandy (Clerks)

---

From: CityCouncilWebForm  
Sent: July 26, 2012 10:32 AM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

Canadian Association of Family Enterprise Saskatoon Chapter  
2366 Ave C North  
Saskatoon  
Saskatchewan  
S7L 5X5

EMAIL ADDRESS:

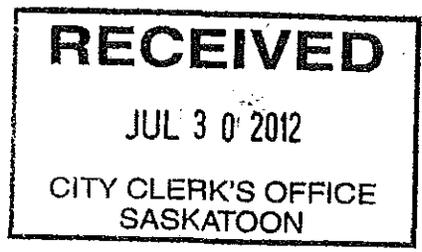
[saskatoon@cafecanada.ca](mailto:saskatoon@cafecanada.ca)

COMMENTS:

The Saskatoon Chapter of the Canadian Association of Family Enterprise requests that City Council proclaim Thursday, October 18th, 2012 as Family Business Day in Saskatoon. The day will be celebrated with a dinner highlighting family business achievements and attended by the Saskatoon business community. We thank you in advance for your consideration of this request.

205-5) D5

From: CityCouncilWebForm  
Sent: July 29, 2012 3:35 PM  
To: City Council  
Subject: Write a Letter to City Council



TO HIS WORSHIP THE MAYOR AND MEMBERS OF CITY COUNCIL

FROM:

John Brodsky  
201-3515 Thatcher Ave.  
Saskatoon  
Saskatchewan  
S7R 1C4

EMAIL ADDRESS:

[john@saskatoonblades.com](mailto:john@saskatoonblades.com)

COMMENTS:

To whom it may concern:

On Saturday, September 22, 2012 the Saskatoon Blades will be kicking off our 2012-2013 season with a game against the Prince Albert Raiders. This will be a special year for the Blades as we will be hosting the 2013 MasterCard Memorial Cup - an event that has garnered outstanding support from the Saskatoon community. We will be celebrating the start of this momentous year in our franchise's long history with a tail gate party in Credit Union Centre's parking lot and with pre-game ceremonies to take place prior to the opening face off.

We would like City Council to make September 22, 2012 "Saskatoon Blades Day" to help us make this important event even more special. We would really appreciate the opportunity to have the City of Saskatoon involved in our festivities.

Thank you very much for taking the time to consider this request and please feel free to contact me directly should you have any questions or concerns.

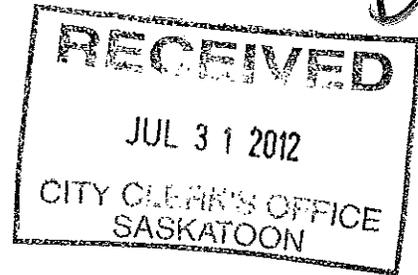
Regards,

John Brodsky  
Director of Business Operations  
Saskatoon Blades Hockey Club



Saskatchewan  
**EcoNetwork**  
 connecting | engaging | informing

205-56) Db)



535 - 8th Street East, Saskatoon SK S7H 0P9  
 Phone (306) 692-1275 • Info@econet.sk.ca • website: econet.ca

Mayor D. J. Atchison  
 City of Saskatoon  
 222- 3<sup>rd</sup> Avenue North  
 Saskatoon, SK, S7K 0J5

July 30, 2012

fax 975-2784

Your worship,

On behalf of the Saskatchewan EcoNetwork, I am writing this letter to ask that the City of Saskatoon proclaim September 30, 2012 as World Rivers day in Saskatoon.

IN 2003, the United Nations, out of concern for the impending global water and sanitation crisis, decided to dedicate a full decade to highlight the importance of these issues and to encourage people everywhere to take steps to protect the world's freshwater resources. From this came the International Decade for Action "Water for Life" (2005-2015) of which World Rivers Day was born. World Rivers Day 2011 was celebrated by several million people in over 60 countries. We want to continue to build that momentum this year. The attention that a City proclamation can bring to the event would be very welcome, helping us to educate people in Saskatoon about the importance of our river and how to protect it into the future.

As a primary feature of the landscape of Saskatoon and the source of the water supply, the South Saskatchewan River is certainly a worthy subject of celebration. Along with other community groups, we will be celebrating World Rivers Day and drawing attention to the need for our rivers to be protected and cherished.

We ask that the City of Saskatoon add its voice to ours by declaring September 30, 2012 World Rivers Day. If Council could consider our request at its earliest convenience, we would be grateful. We hope that Council will agree that the South Saskatchewan River merits this kind of attention, as without the river, Saskatoon would not be such a wonderful City as it is!

Thank you, your worship for any consideration you may be able to give to this request.

Yours truly,

Rick Morrell  
 Executive Director  
 Saskatchewan EcoNetwork

D7)

IC AWARENESS MONTH - SEPTEMBER 2012  
"WE STAND TOGETHER - FOR RESPECT, COMPASSION & CHIEF"  
WWW.ICAWARENESS.ORG



Dear City Council

My life and/or the life of a family member or friend has been severely disrupted by interstitial cystitis, a devastating pelvic pain condition. The challenge many patients face is a prompt diagnosis. Many physicians and family members are simply unaware of this condition or, worse, tell patients that their bladder symptoms are "all in their heads." I hope that you will consider issuing a proclamation on behalf of IC Awareness Month to help educate our community. Patients need to be treated with dignity and respect.

**IC Awareness Month Proclamation**

*Whereas, 3 to 8 million women in the USA and 1 to 4 million men of all ages suffer the devastating effects of this chronic pelvic pain disorder; and*

*Whereas, medical research efforts into interstitial cystitis/bladder pain syndrome have yet to discover the cause nor cure for this disabling medical condition and federal research money has been severely reduced in recent cuts; and*

*Whereas, many physicians are unaware of the symptoms and effects of interstitial cystitis/bladder pain syndrome, causing patients to suffer for many years before they obtain a correct diagnosis and medical treatment; and*

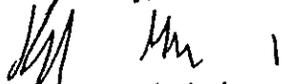
*Whereas, there is a critical need to educate and support individuals and families affected by interstitial cystitis;*

**Now, Therefore, Be It Resolved** that September 2012 is hereby designed as Interstitial Cystitis Awareness Month during which patients, support groups and organizations call for increases in public and private sector funding for medical research, targeted educational programs for health professionals, patients and the public and recognition of urological disorders as a significant public health issue.

Proclaimed This Day, 1 September 2012

I thank you for your consideration.

Sincerely,

  
Kyla McKechnie

IC Awareness Month is a project of Interstitial Cystitis Network, a social advocacy and health education company founded by IC patients for IC patients. Learn more about IC by visiting: <http://www.ic-network.com>

- 15. Communications to Council – (Section A - Requests to Speak to Council on new issues)**

3a)

## BYLAW NO. 9037

### The Zoning Amendment Bylaw, 2012 (No. 9)

The Council of The City of Saskatoon enacts:

#### Short Title

1. This Bylaw may be cited as The Zoning Amendment Bylaw, 2012 (No. 9).

#### Purpose

2. The purpose of this Bylaw is to rezone the land described in the Bylaw from an R1A District and an FUD District to an R1B District, respectively.

#### Zoning Bylaw Amended

3. Zoning Bylaw No. 8770 is amended in the manner set forth in this Bylaw.

#### R1A District to R1B District

4. The Zoning Map, which forms part of Bylaw 8770, is amended to rezone the lands described in this Section and shown as  on Appendix "A" to this Bylaw from an R1A District to an R1B District:
  - (a) Lots 14 – 26, Block 20 as shown on Plan of Proposed Subdivision of part of N.E. ¼ Sec. 18 and part of Parcel BB, Plan No. 101875394 all in Twp. 36 – Rge. 4 – W.3<sup>rd</sup> Mer. Saskatoon, Saskatchewan by R.A. Webster, S.L.S. dated March 30, 2011 and revised January 30, 2012 and May 16, 2012.

#### FUB District to R1B District

5. The Zoning Map, which forms part of Bylaw 8770, is amended to rezone the lands described in this Section and shown as  on Appendix "A" to this Bylaw from

an FUD District to an R1B District:

- (a) Block 19 as shown on Plan of Proposed Subdivision of part of N.E. ¼ Sec. 18 and part of Parcel BB, Plan No. 101875394 all in Twp. 36 – Rge. 4 – W.3<sup>rd</sup> Mer. Saskatoon, Saskatchewan by R.A. Webster, S.L.S. dated March 30, 2011 and revised January 30, 2012 and May 16, 2012.

**Coming into Force**

6. This Bylaw shall come into force on the day of its final passing.

Read a first time this \_\_\_\_\_ day of \_\_\_\_\_, 2012.

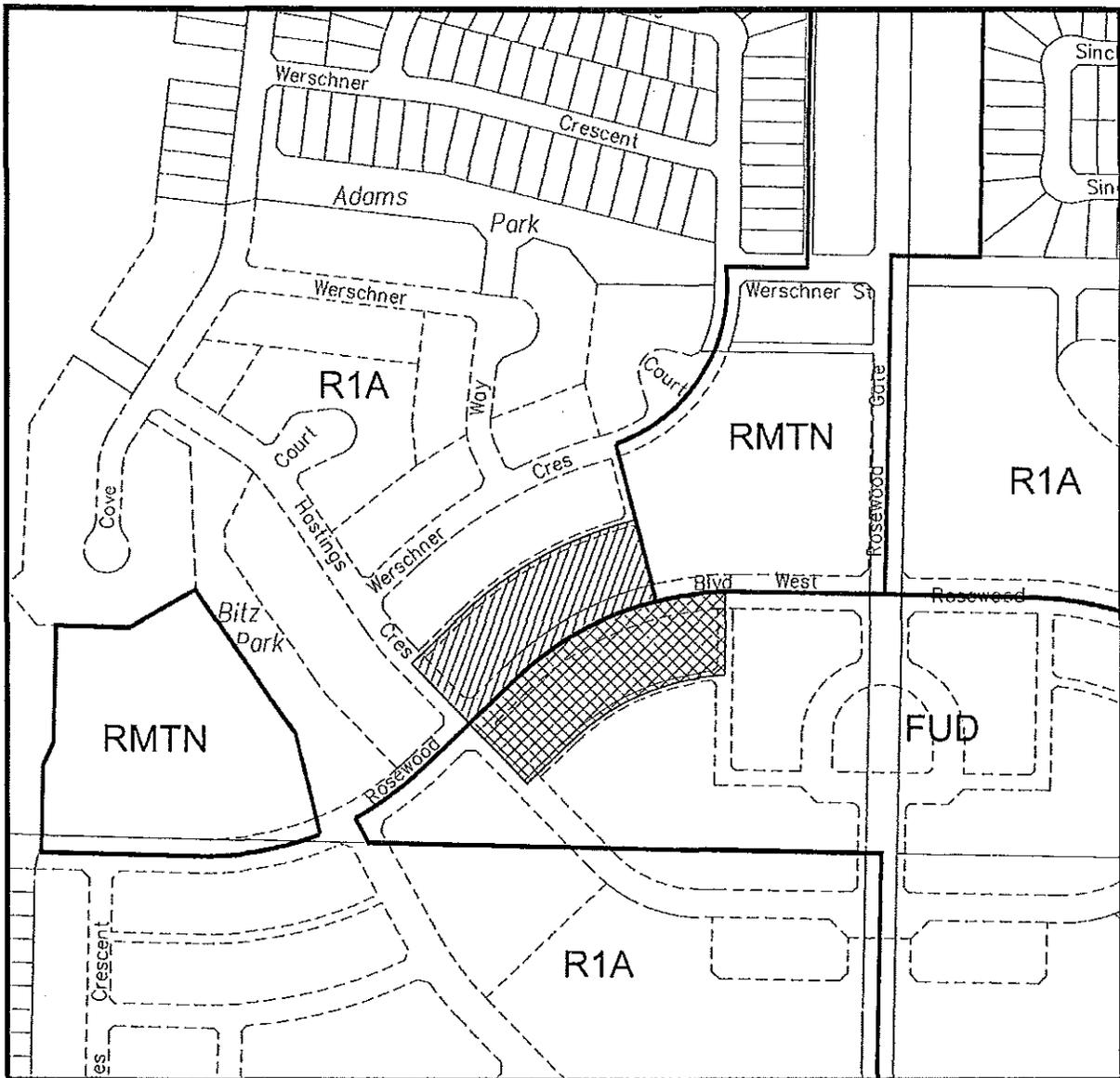
Read a second time this \_\_\_\_\_ day of \_\_\_\_\_, 2012.

Read a third time and passed this \_\_\_\_\_ day of \_\_\_\_\_, 2012.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

### Appendix "A"



### REZONING

From R1A to R1B 

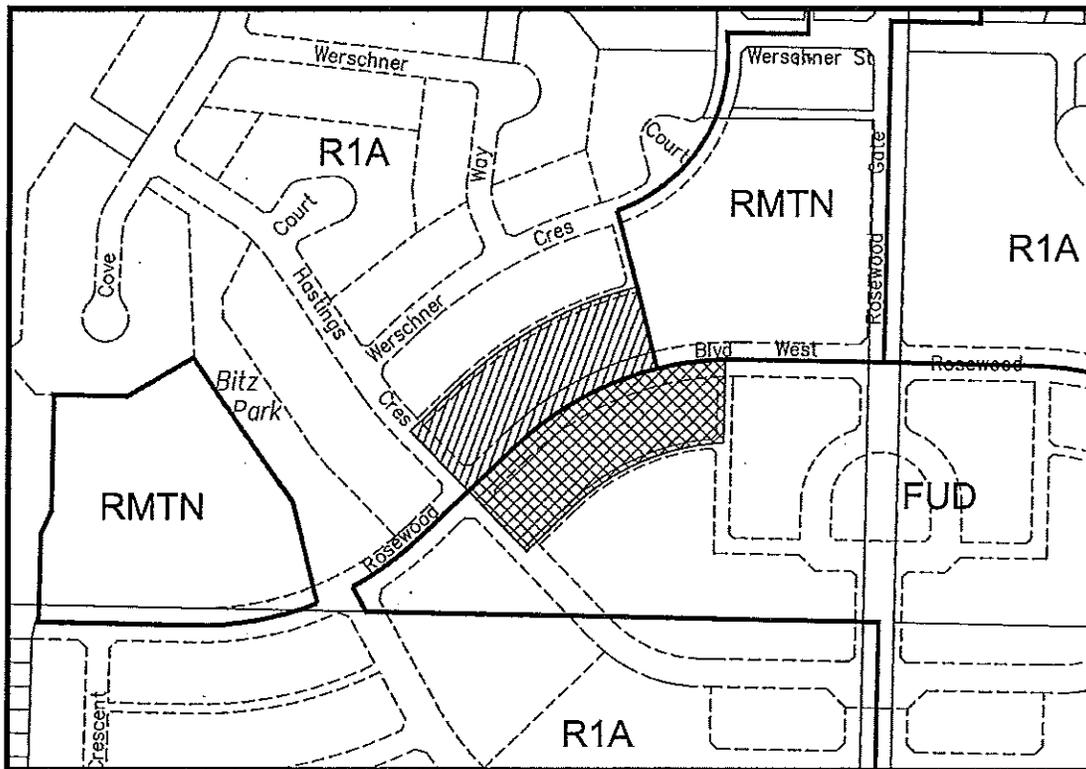
From FUD to R1B 



## COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> Z8/12	<b>PROPOSAL</b> Proposed Rezoning from FUD District and R1A District to R1B District	<b>EXISTING ZONING</b> FUD and R1A
<b>LEGAL DESCRIPTION</b> Lots 14 to 26, Block 21, and Lots 1 to 15, Block 22, Plan to be Registered		<b>CIVIC ADDRESS</b> 302 to 358 and 303 to 351 Rosewood Boulevard West
		<b>NEIGHBOURHOOD</b> Rosewood
<b>DATE</b> May 15, 2012	<b>APPLICANT</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7	<b>OWNER</b> City of Saskatoon, Land Branch 201 3 <sup>rd</sup> Avenue North Saskatoon SK S7K 2H7

### LOCATION PLAN



**PROPOSED REZONING**

From R1A to R1B

From FUD to R1B

**City of Saskatoon**  
Planning & Development Branch

File No. RZ08-2012

**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

That at the time of the public hearing City Council consider the Administration's recommendation that the proposed amendment to the Zoning Bylaw No. 8770 to rezone 302 to 358 and 303 to 351 Rosewood Boulevard West from FUD - Future Urban Development District, and R1A - One-Unit Residential District, to R1B - Small Lot One-Unit Residential District, be approved.

**B. PROPOSAL**

The Planning and Development Branch has received an application from the City of Saskatoon, Land Branch requesting that 302 to 358 and 303 to 351 Rosewood Boulevard West (see Attachment 2) be rezoned from FUD - Future Urban Development District, and R1A - One-Unit Residential District, to R1B - Small Lot One-Unit Residential District. The rezoning of these lands would accommodate small lot, one-unit residential development.

**C. REASON FOR PROPOSAL (By Applicant)**

The proposed rezoning of the aforementioned lands would ensure the land uses are consistent with the Rosewood Neighborhood Concept Plan (Concept Plan).

**D. BACKGROUND INFORMATION**

During its May 20, 2008 meeting, City Council approved the Concept Plan. The subject sites are currently zoned FUD District and R1A District. The Concept Plan provides a wide range of housing options, as well as neighbourhood commercial services. In order to accommodate future development, Zoning Bylaw No. 8770 must be amended to allow development to occur in accordance with the Concept Plan.

**E. JUSTIFICATION**

1. Community Services Department Comments

a) Planning and Development Branch

The proposed Zoning Bylaw No. 8770 amendment will change the land use from its current zoning to an R1B District. This zoning district is used to provide small lot residential development in the form of one-unit dwellings, as well as related community uses and is similarly found in close proximity to the core of Saskatoon's new neighbourhoods. This proposal is in compliance with the approved Concept Plan and will add to the diversity of

housing types in the Rosewood neighbourhood. Future development on this site will comply with the development standards identified within the R1B District.

2. Comments by Others

a) Infrastructure Services Department

The proposed Zoning Bylaw No. 8770 amendment is acceptable to the Infrastructure Services Department.

b) Utility Services Department, Transit Services Branch

The Transit Services Branch has no objections to this proposal, and will provide service as outlined within the Concept Plan.

F. COMMUNICATION PLAN

Once this application has been considered by the Municipal Planning Commission, it will be advertised in accordance with Public Notice Policy No. C01-021, and a date for a public hearing will be set. The Planning and Development Branch will notify the Community Consultant of the public hearing date by letter. A notice will be placed once in The StarPhoenix two weeks prior to the public hearing. Notice boards will also be placed on the site. The property owners affected by this rezoning will also be notified in writing.

G. ENVIRONMENTAL IMPLICATIONS

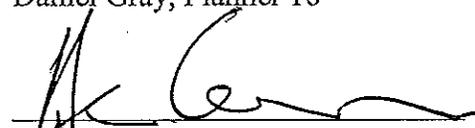
There are no environmental and/or greenhouse gas implications.

H. ATTACHMENTS

1. Fact Summary Sheet
2. Plan of Proposed Subdivision

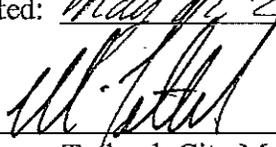
Written by: Daniel Gray, Planner 16

Reviewed by:

  
Alan Wallace, Manager  
Planning and Development Branch

Z8/12  
Rosewood Boulevard West  
May 15, 2012

Approved by:   
\_\_\_\_\_  
Randy Grauer, General Manager  
Community Services Department  
Dated: May 17, 2012

Approved by:   
\_\_\_\_\_  
Murray Totland, City Manager  
Dated: May 18/12

<b>FACT SUMMARY SHEET</b>	
<b>A. Location Facts</b>	
1. Municipal Address	302 to 358 and 303 to 351 Rosewood Boulevard West
2. Legal Description	N/A
3. Neighbourhood	Rosewood
4. Ward	9
<b>B. Site Characteristics</b>	
1. Existing Use of Property	Residential – R1A
2. Proposed Use of Property	Residential – R1B
3. Adjacent Land Uses and Zoning	Residential
North	Residential – R1A
South	Residential – R1A
East	Residential – RMTN
West	Residential – R1A
4. No. of Existing Off-Street Parking Spaces	N/A
5. No. of Off-Street Parking Spaces Required	N/A
6. No. of Off-Street Parking Spaces Provided	N/A
7. Site Frontage	N/A
8. Site Area	N/A
9. Street Classification	Rosewood Boulevard West – Major Collector Rosewood Gate North – Major Collector Hastings Crescent – Proposed
<b>C. Official Community Plan Policy</b>	
1. Existing Official Community Plan Designation	Residential
2. Proposed Official Community Plan Designation	Residential
3. Existing Zoning District	R1A
4. Proposed Zoning District	R1B

N.E. 1/4 Sec. 18-36-4-3

N.E. 1/4 Sec. 18-36-4-3

BB

Plan No. 101875394

EE

Plan No. 102028566

# ROSEWOOD City of Saskatoon Phase 2

PLAN SHOWING PROPOSED SUBDIVISION OF PART OF  
N.E. 1/4 SEC. 18 -  
AND PART OF  
PARCEL BB, PLAN NO. 101875394  
ALL IN  
TWP. 36 - RGE. 4 - W.3rd MER.  
SASKATOON, SASKATCHEWAN.  
BY : R.A. WEBSTER, S.L.S.  
2011



Registered Land Surveyor Mark 200, 2011  
Registration No. 2011

**LEGEND**  
Shaded areas show area to be subdivided and shown in grey.  
Shaded areas are approximately 100' x 100' by 10' wide.  
Lines shown are hypothetical and may change upon completion of survey.  
Portion of this plan, prepared for subdivision is entitled with a Survey License No. and  
contains 10/10th Edition (10/10th) notes.  
Distances are shown as per length.  
1 & 2 follow the 10/10th Edition.

- 1 Existing easements are shaded grey
- 2 3m Bank Easement
- 3 2m BankPower, Sewal, and Show Cables/Systems
- 4 2m Bank Easement, Sewal, and Show Cables/Systems
- 5 City of Saskatoon - for water & sewer
- 6 2.0m x 2.0m BankPower easement for transformer
- 7 2.2m x 2.0m BankPower easement for padlock

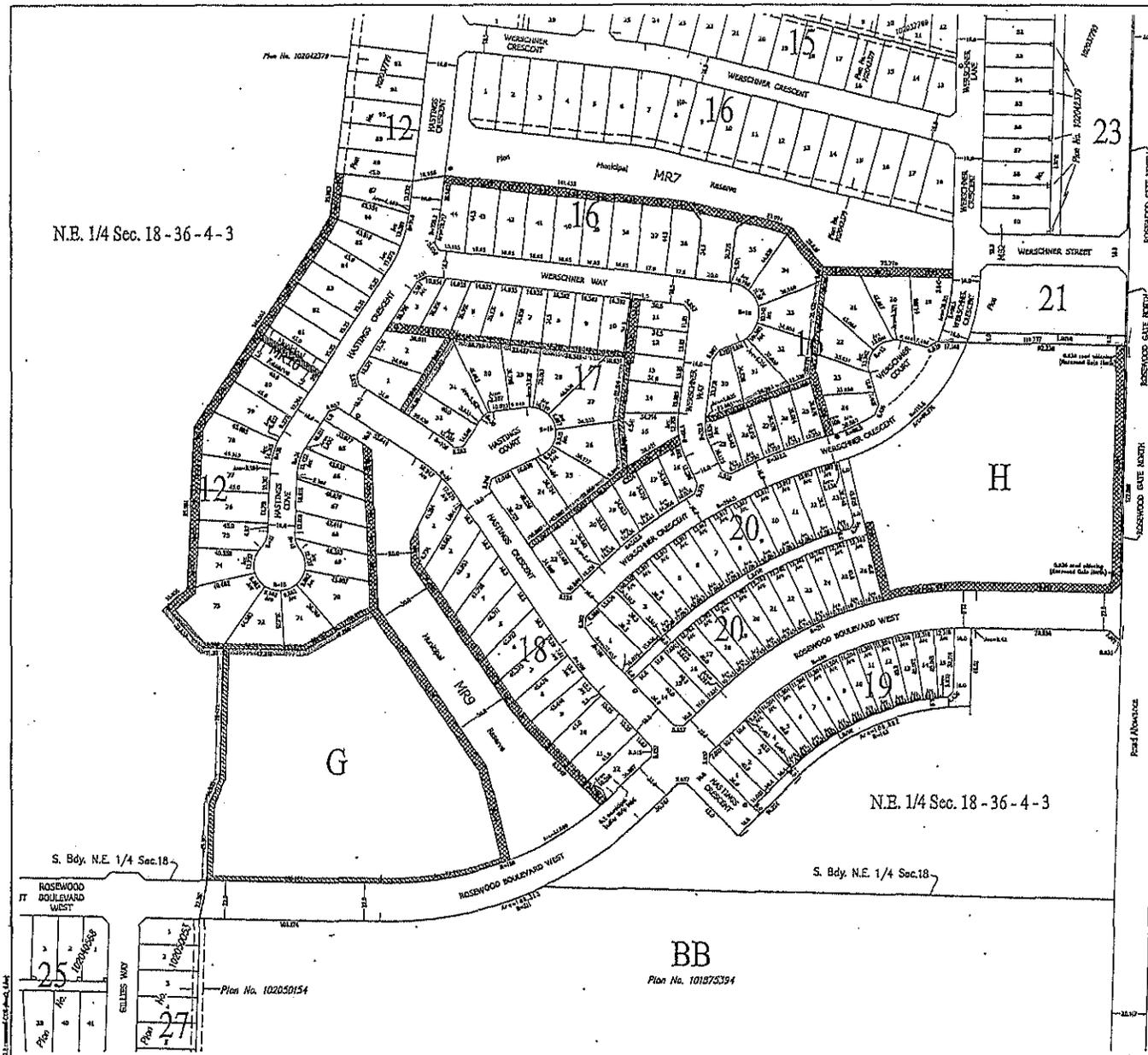
EXAMINED : CITY OF SASKATOON

SENIOR PLANNER, LAND BRANCH OF THE COMMUNITY SERVICES DEPARTMENT  
A.D. 2011.

EXAMINED : CITY OF SASKATOON  
Approved under the provisions of Bylaw No. 6257 of the City of Saskatoon.

General Manager of the Community Services Department  
A.D. 2011.

N.W. 1/4 Sec. 17-36-4-3



The following is a copy of **Clause 1, Report No. 3-2012** of the **Municipal Planning Commission**, which was **ADOPTED** by City Council at its meeting held on **June 18, 2012**:

1. **Proposed Rezoning from FUD District and R1A District to R1B District**  
**302 to 358 and 303 to 351 Rosewood Boulevard West**  
**Rosewood Neighbourhood**  
**Applicant: City of Saskatoon, Land Branch**  
**(File No. CK. 4351-012-8)**
- 

- RECOMMENDATION:**
- 1) that City Council approve the advertising with respect to the proposed amendment to Zoning Bylaw No. 8770 to rezone 302 to 358 and 303 to 351 Rosewood Boulevard West from FUD – Future Urban Development District, and R1A – One-Unit Residential District, to R1B – Small Lot One-Unit Residential District;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
  - 3) that the City Solicitor be requested to prepare the required Bylaw; and
  - 4) that at the time of the public hearing, City Council consider the Municipal Planning Commission’s recommendation that the proposed rezoning be approved.

Attached is a report of the General Manager, Community Services Department dated May 15, 2012, with respect to the above proposed rezoning.

Your Commission has reviewed the report with the Administration and supports the above recommendations.

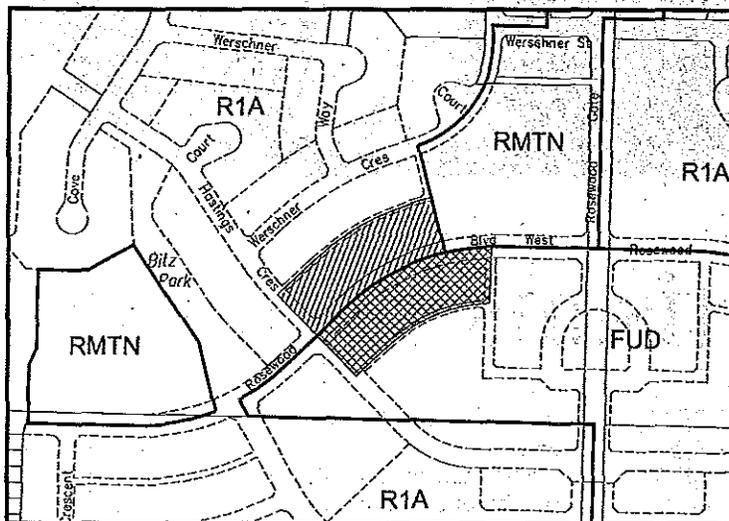
**ZONING NOTICE**

**ROSEWOOD NEIGHBOURHOOD  
PROPOSED ZONING BYLAW AMENDMENT – BYLAW NO. 9012**

Saskatoon City Council will consider an amendment to the City's Zoning Bylaw (No. 8770). Through Bylaw No. 9037, the Zoning Amendment Bylaw 2012 (No. 9), the properties in the Rosewood Neighbourhood as shown in the map below will be rezoned from R1A-One-Unit Residential District to R1B – Small Lot One-Unit Residential District.

**LEGAL DESCRIPTION** – Lots 14-26, Block 21; and Lots 1-15, Block 22, Plan to be registered.

**CIVIC ADDRESS** – Rosewood Boulevard West



**PROPOSED REZONING**

From R1A to R1B	—	
From FUD to R1B	—	

File No. R208-2012



**REASON FOR THE AMENDMENT** – The proposed rezoning would facilitate the construction of small lot one-unit residential development in a manner consistent with the Rosewood Neighbourhood Concept Plan.

**INFORMATION** - Questions regarding the proposed amendment or requests to view the proposed amending Bylaw, the City of Saskatoon Zoning Bylaw and Zoning Map may be directed to the following without charge:  
Community Services Department, Planning and Development Branch  
Phone: 975-7723 (Daniel Gray)

**PUBLIC HEARING** - City Council will hear all submissions on the proposed amendment, and all persons who are present at the City Council meeting and wish to speak on **Wednesday, August 15, 2012 at 6:00 p.m. in City Council Chambers, City Hall, Saskatoon, Saskatchewan.**

All written submissions for City Council's consideration must be forwarded to:  
His Worship the Mayor and Members of City Council  
c/o City Clerk's Office, City Hall  
222 Third Avenue North, Saskatoon SK S7K 0J5

All submissions received by the City Clerk by 10:00 a.m. on Wednesday, August 15, 2012 will be forwarded to City Council. City Council will also hear all persons who are present and wish to speak to the proposed Bylaw.

3 b)

## BYLAW NO. 9038

### The Official Community Plan Amendment Bylaw, 2012 (No. 3)

The Council of The City of Saskatoon enacts:

#### Short Title

1. This Bylaw may be cited as The Official Community Plan Amendment Bylaw, 2012 (No. 3).

#### Purpose

2. The purpose of this Bylaw is:
  - (a) to amend the Official Community Plan – Phasing Map to re-designate the land referred to in the Bylaw from Phase II to Phase I; and
  - (b) to change the land use designation of the land described in the Bylaw from Urban Holding to Residential.

#### Official Community Plan Amended

3. The Official Community Plan, annexed as Schedule “A” to Bylaw No. 8769 and forming part of the Bylaw, is amended in the manner set forth in this Bylaw.

#### Phasing Map Amended

4. The Phasing Map, which is referred to in Section 3.2.2 of the Official Community Plan and which forms part of the Plan, is amended by changing the phasing designation of the lands described in this Section and shown as  on Appendix “A” to this Bylaw from Phase II to Phase I:
  - (a) Surface Parcel No. 135680643  
Legal Land Description: LSD 5-02-37-06-3 Ext 7  
As described on Certificate of Title 70S00161,  
description 7;

- (b) Surface Parcel No. 135680665  
Legal Land Description: LSD 6-02-37-06-3 Ext 8  
As described on Certificate of Title 70S00161,  
description 8;
  - (c) Surface Parcel No. 135669088  
Legal Land Description: LSD 4-02-37-06-3 Ext 4  
As described on Certificate of Title 98SA02962,  
description 4;
  - (d) Surface Parcel No. 135680621  
Legal Land Description: LSD 3-02-37-06-3 Ext 6  
As described on Certificate of Title 70S00161,  
description 6;
  - (e) Surface Parcel No. 117153008  
Legal Land Description: Blk/Par A, Plan No. 98SA07556 Ext 0  
As described on Certificate of Title 98SA17521;
  - (f) Surface Parcel No. 118172257  
Legal Land Description: NW 35-36-06-3 Ext 0  
As described on Certificate of Title 82S04896;
  - (g) Surface Parcel No. 152959539  
Legal Land Description: Blk/Par D, Plan 101709783 Ext 0;
  - (h) Surface Parcel No. 136167420  
Legal Land Description: Blk/Par C, Plan 00SA28118 Ext 1  
As described on Certificate of Title 00SA28119;
  - (i) Surface Parcel No. 152959540  
Legal Land Description: Blk/Par E, Plan 101709783 Ext 0;
- and,
- (j) Surface Parcel No. 152959551  
Legal Land Description: SW 35-36-06-3 Ext 21  
As shown on Plan 101709783.

**Urban Holding to Residential**

5. The Official Community Plan Land Use Map is amended by changing the land use designation of the lands described in this Section and shown as  on Appendix "B" to this Bylaw from Urban Holding to Residential:

- (a) Surface Parcel No. 135680643  
Legal Land Description: LSD 5-02-37-06-3 Ext 7  
As described on Certificate of Title 70S00161,  
description 7;
- (b) Surface Parcel No. 135680665  
Legal Land Description: LSD 6-02-37-06-3 Ext 8  
As described on Certificate of Title 70S00161,  
description 8;
- (c) Surface Parcel No. 135680621  
Legal Land Description: LSD 3-02-37-06-3 Ext 6  
As described on Certificate of Title 70S00161,  
description 6;
- (d) Surface Parcel No. 117153008  
Legal Land Description: Blk/Par A, Plan No. 98SA07556 Ext 0  
As described on Certificate of Title 98SA17521;
- (e) Surface Parcel No. 118172257  
Legal Land Description: NW 35-36-06-3 Ext 0  
As described on Certificate of Title 82S04896;
- (f) Surface Parcel No. 152959539  
Legal Land Description: Blk/Par D, Plan 101709783 Ext 0;
- (g) Surface Parcel No. 136167420  
Legal Land Description: Blk/Par C, Plan 00SA28118 Ext 1  
As described on Certificate of Title 00SA28119;
- (h) Surface Parcel No. 152959540  
Legal Land Description: Blk/Par E, Plan 101709783 Ext 0;

and,

- (i) Surface Parcel No. 152959551  
Legal Land Description: SW 35-36-06-3 Ext 21  
As shown on Plan 101709783.

**Coming into Force**

- 6. This Bylaw shall come into force upon receiving approval of the Minister of Government Relations.

Read a first time this \_\_\_\_\_ day of \_\_\_\_\_, 2012.

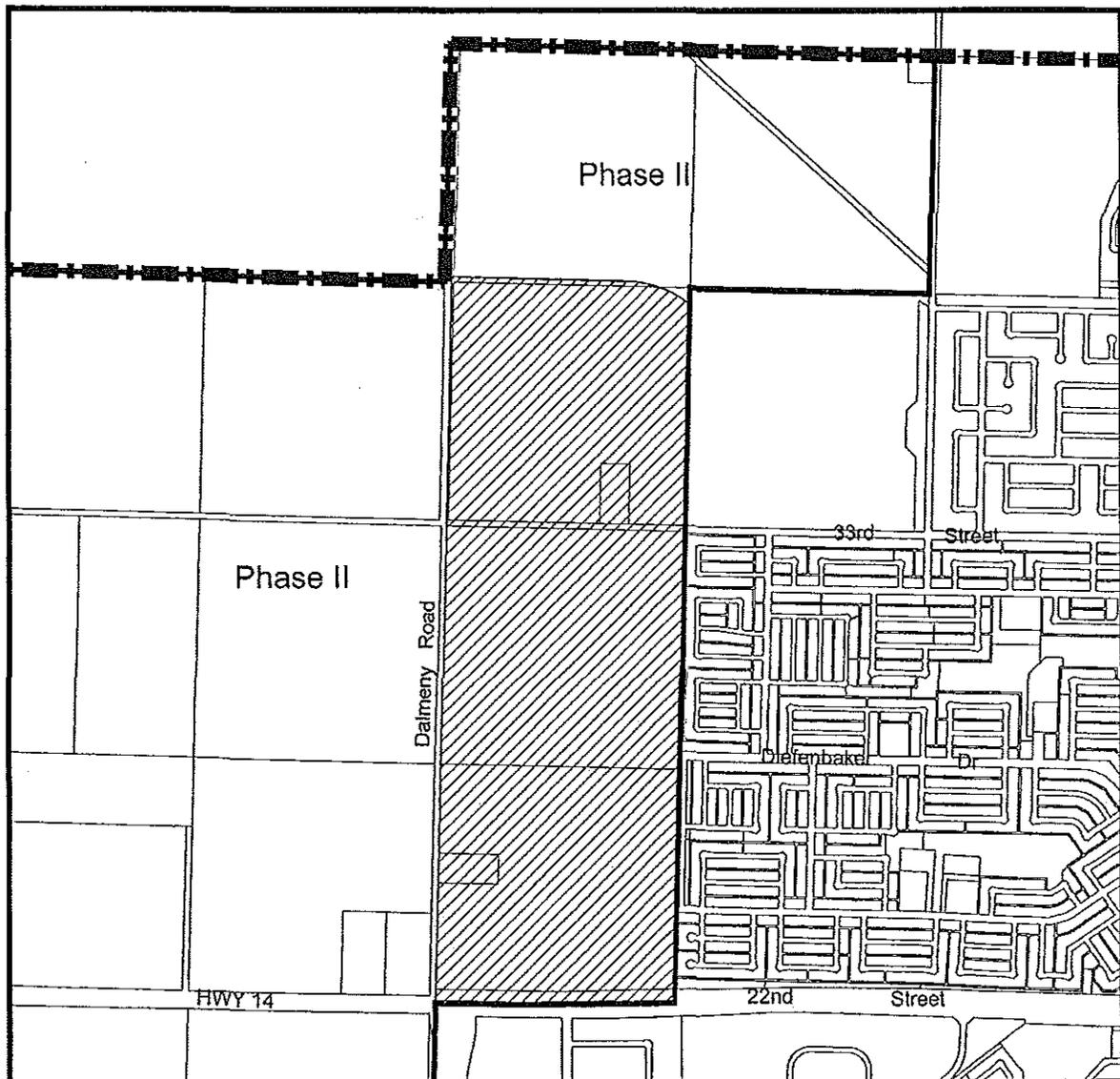
Read a second time this \_\_\_\_\_ day of \_\_\_\_\_, 2012.

Read a third time and passed this \_\_\_\_\_ day of \_\_\_\_\_, 2012.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

# Appendix "A"

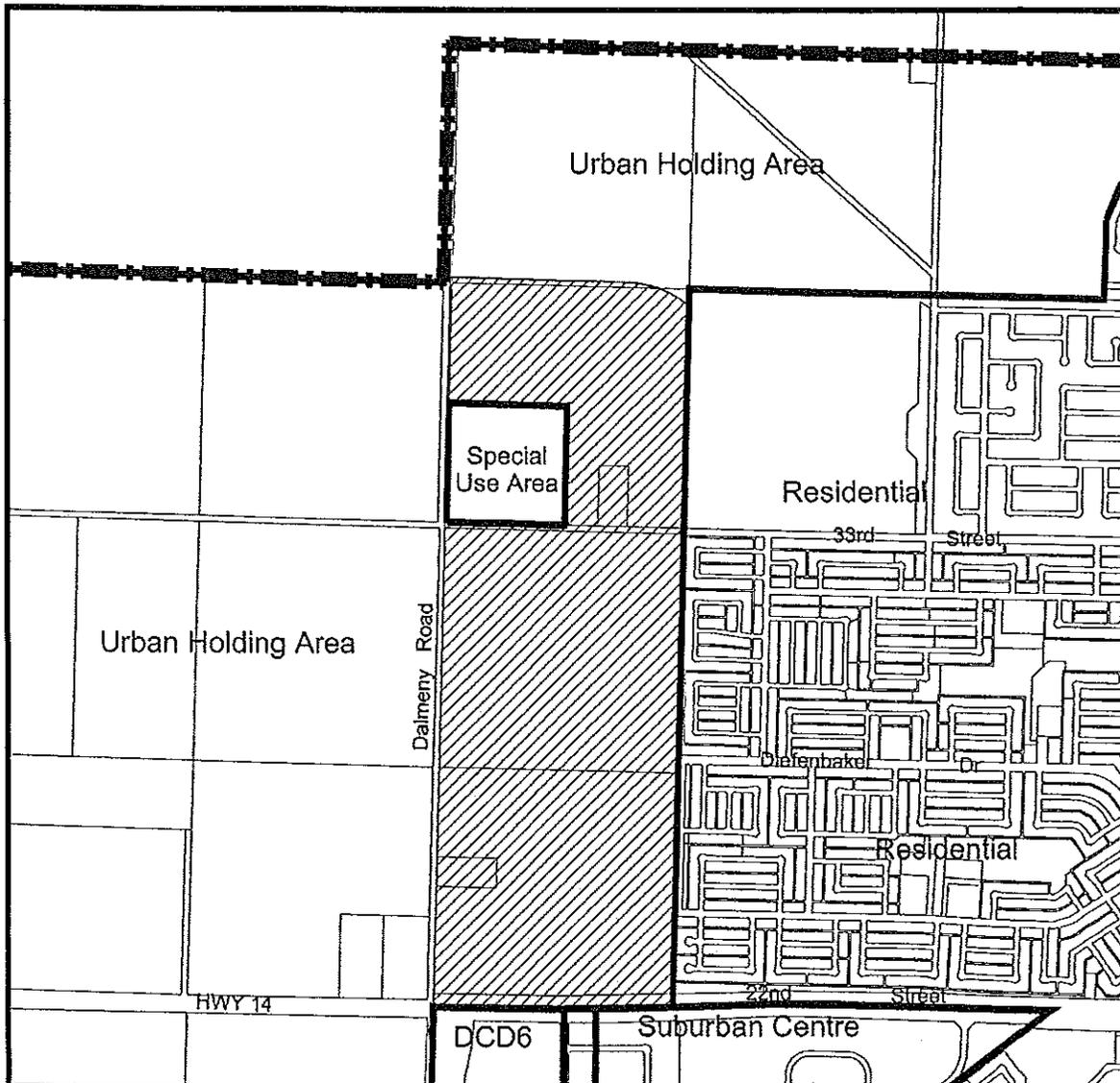


## AMENDMENT TO THE OFFICIAL COMMUNITY PLAN - PHASING MAP

From Phase II to Phase I — 



# Appendix "B"



## AMENDMENT TO THE OFFICIAL COMMUNITY PLAN - LAND USE MAP

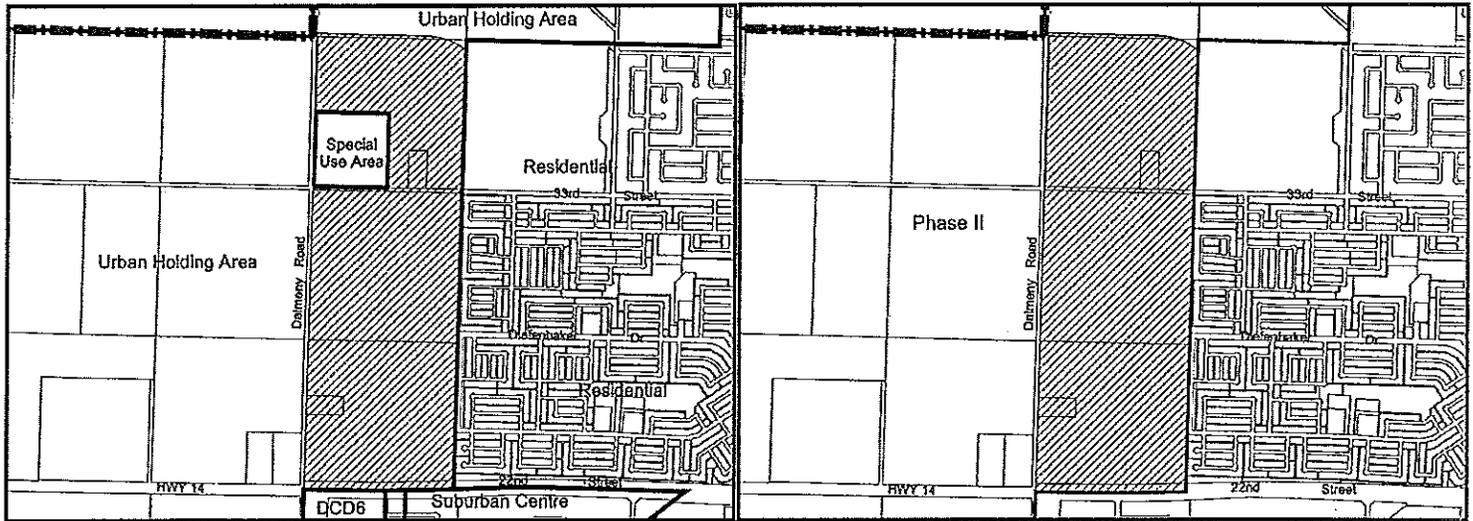
From Urban Holding to Residential



# COMMUNITY SERVICES DEPARTMENT

<b>APPLICATION NO.</b> OCP 13/11	<b>PROPOSAL</b> Official Community Plan Amendments: 1. Urban Holding to Residential; and 2. Phase 2 to Phase 1	<b>EXISTING ZONING</b> FUD and R1A(H)
<b>LEGAL DESCRIPTION</b> W ½ 35-36-6-W3, and LSD 3, 5, and 6, on S ½ 2-37-6-W3		<b>CIVIC ADDRESS</b> Not Applicable
		<b>NEIGHBOURHOOD</b> Kensington
<b>DATE</b> May 15, 2012	<b>APPLICANT</b> City of Saskatoon	<b>OWNER</b> City of Saskatoon 201 3rd Avenue South Saskatoon SK S7K 2H7

**LOCATION PLANS:**



**PROPOSED AMENDMENT TO THE OFFICIAL COMMUNITY PLAN - LAND USE MAP**  
 From Urban Holding to Residential —   
 City of Saskatoon Planning & Development Branch

**PROPOSED AMENDMENT TO THE OFFICIAL COMMUNITY PLAN - PHASING MAP**  
 From Phase II to Phase I —   
 City of Saskatoon Planning & Development Branch

A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:

that at the time of the public hearing, City Council consider the Administration's recommendation that the proposed amendments to the Official Community Plan Bylaw No. 8769 to reclassify the land use designation of W ½ 35-36-6-W3, and LSD 3, 5, and 6, on S ½ 2-37-6-W3 from Urban Holding Area to Residential within the Official Community Plan Land Use Map, and the Official Community Plan Phasing Map from Phase 2 to Phase 1, be approved.

B. PROPOSAL

An application from the Land Branch has been received, requesting that the Official Community Plan (OCP) Land Use Map be amended to reclassify the land use of W ½ 35-36-6-W3, and LSD 3, 5, and 6, on S ½ 2-37-6-W3 from Urban Holding Area to Residential; and that these properties are moved from Phase 2 to Phase 1 through an amendment to the OCP Phasing Map.

C. REASON FOR PROPOSAL

To facilitate development in accordance with the approved Kensington Neighbourhood Concept Plan. A rezoning application will follow requesting various zoning changes upon adoption of the proposed OCP amendments.

D. BACKGROUND INFORMATION

During its April 16, 2012 meeting, City Council approved the Kensington Neighbourhood Concept Plan. Kensington will be a 512 acre residential neighbourhood bounded on the east by Confederation Park and Pacific Heights; on the south by 22nd Street West and the Blairmore Suburban Centre; and on the west and north by agricultural lands. The neighbourhood will accommodate one-unit dwellings; low-density townhouse units; medium-density, apartment style, and stacked townhouse-style units; along with mixed-use units, combining residential with neighbourhood retail, office, and service uses. When fully developed, the neighbourhood will have an estimated population of approximately 8,300 residents, with a projected density of 7.2 units per gross acre. Completion of this neighbourhood is estimated to be five to seven years, with the land currently being serviced to provide infrastructure capacities for future neighbourhood development.

**E. JUSTIFICATION**

1. Community Services Department Comments

a. Planning and Development Branch

The Planning and Development Branch supports the two proposals:

- i) to amend the OCP Land Use Map from Urban Holding to Residential – This is a standard land use practice as new neighbourhoods are developed in Saskatoon. As lands are acquired and Neighbourhood Concept Plans are being developed, lands capable of being serviced with a full range of utilities are given a blanket land use designation of Urban Holding. This signifies pending development upon approval of the Concept Plan. After the Concept Plan approval, the land use within the neighbourhood is designated and provides clarity toward required amendments to address the pending development. In this case, the Land Branch has requested that the lands be reclassified as Residential to accommodate a variety of housing types; and
- ii) to amend the OCP Phasing Map from Phase 2 to Phase 1 – the OCP Phasing Map provides a rational and efficient phasing system for the servicing and development of urban land. Phasing Sequence No. 1 indicates those areas considered to be suitable for development within the next five years; where Phasing Sequence No. 2 indicates areas suitable for development beyond the next five years, but within scope of the OCP. The required infrastructure and servicing is currently being put in place for the development of the aforementioned lands; therefore, the proponent has proceeded with a request to amend the OCP Phasing Map to accommodate the development of the lands.

b. Building Standards Branch

The Building Standards Branch has no objections to the proposed OCP application. The site plans submitted have not been reviewed for code compliance.

A building permit is required to be obtained before any construction on this project begins.

2. Comments by Others

a. Infrastructure Services Department

The proposed amendment is acceptable to the Infrastructure Services Department.

b. Utility Services Department, Transit Services Branch

The Transit Services Branch has no objection toward these two proposals. Currently the lands in question are undeveloped; however, future transit plans for this area will remain consistent with plans identified in the Kensington Neighbourhood Concept Plan.

F. COMMUNICATION PLAN

Once this application has been considered by the Municipal Planning Commission, both proposals will be advertised in accordance with Public Notice Policy No. C01-021, and a date for two public hearings will be set. The Planning and Development Branch will notify the Confederation Park and Pacific Heights Community Associations, as well as the Community Consultant of the public hearings by letter. A one-time notice will be placed in The StarPhoenix two weeks prior to the public hearing. Notice boards will also be placed on the site. The property owners affected by these amendments will also be notified in writing.

G. ENVIRONMENTAL IMPLICATIONS

There are no environmental and/or greenhouse gas implications.

H. ATTACHMENT

1. Fact Summary Sheet

Written by: Daniel Gray, Planner 16

Reviewed by:

  
Alan Wallace, Manager  
Planning and Development Branch

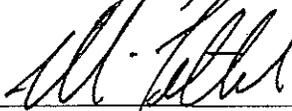
Approved by:



Randy Grauer, General Manager  
Community Services Department

Dated: May 17, 2012

Approved by:



Murray Totland, City Manager

Dated: May 18/12

<b>FACT SUMMARY SHEET</b>	
<b>A. <u>Location Facts</u></b>	
1. Municipal Address	W ½ 35-36-6-W3, and LSD 3, 5, and 6, on S ½ 2-37-6-W3
2. Legal Description	N/A
3. Neighbourhood	Kensington
4. Ward	3
<b>B. <u>Site Characteristics</u></b>	
1. Existing Use of Property	Vacant
2. Proposed Use of Property	Residential
3. Adjacent Land Uses and Zoning	
North	Residential
South	Residential and FUD
East	Residential
West	Residential
4. No. of Existing Off-Street Parking Spaces	N/A
5. No. of Off-Street Parking Spaces Required	N/A
6. No. of Off-Street Parking Spaces Provided	N/A
7. Site Frontage	N/A
8. Site Area	N/A
9. Street Classification	N/A
<b>C. <u>Development Plan Policy</u></b>	
1. Existing Development Plan Designation	Urban Holding
2. Proposed Development Plan Designation	Residential
3. Existing Zoning District	Urban Holding
4. Proposed Zoning District	Residential

The following is a copy of **Clause 2, Report No. 3-2012** of the **Municipal Planning Commission**, which was **ADOPTED** by City Council at its meeting held on **June 18, 2012**:

- 2. Proposed Official Community Plan Amendments:  
Urban Holding to Residential; and Phase 2 to Phase 1  
Kensington Neighbourhood  
Applicant: City of Saskatoon  
(File No. CK. 4351-012-6)**
- 

- RECOMMENDATION:**
- 1) that City Council approve the advertising with respect to the proposed amendments to the Official Community Plan Bylaw No. 8769 to reclassify the land use designation of W ½ 35-36-6-W3, and LSD 3, 5, and 6, on S ½ 2-37-6-W3 from Urban Holding Area to Residential within the Official Community Plan Land Use Map, and the Official Community Plan Phasing Map from Phase 2 to Phase 1, as indicated in the May 15, 2012, report of the General Manager, Community Services Department;
  - 2) that the General Manager, Community Services Department be requested to prepare the required notice for advertising the proposed amendment;
  - 3) that the City Solicitor be requested to prepare the required Bylaw; and
  - 4) that at the time of the public hearing, City Council consider the Municipal Planning Commission's recommendation that the proposed amendments to the Official Community Plan Bylaw be approved.

Attached is the report of the General Manager, Community Services Department dated May 15, 2012, with respect to the proposed Official Community Plan Amendments.

Your Commission has reviewed the report with the Administration and is supporting the above recommendations.

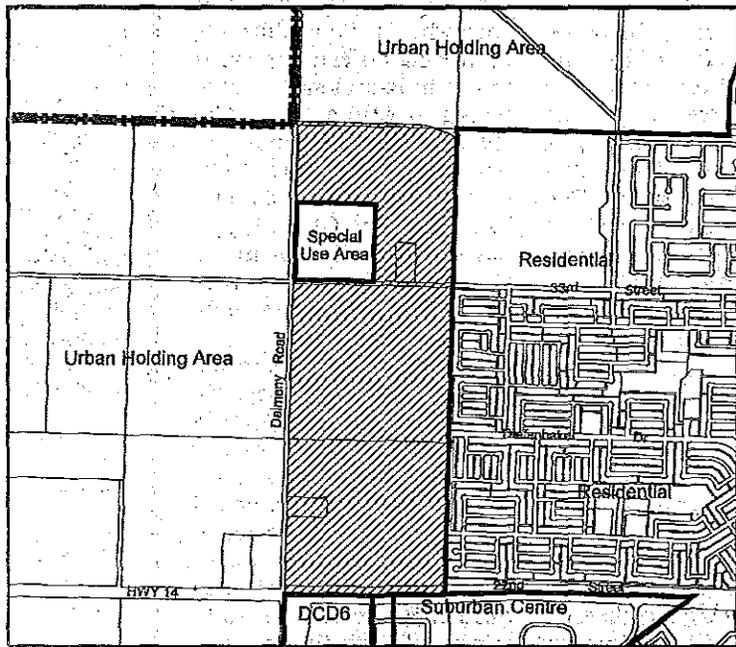
**OFFICIAL COMMUNITY PLAN NOTICE**

**KENSINGTON NEIGHBOURHOOD  
PROPOSED AMENDMENT TO THE OFFICIAL COMMUNITY PLAN  
- PHASING MAP - BYLAW NO. 9038**

Saskatoon City Council will consider an amendment to the Official Community Plan, Bylaw No. 8769. Through Bylaw No. 9038, The Official Community Plan Amendment Bylaw, 2012 (No. 3), the property highlighted in the map below will be reclassified from Urban Holding to Residential land use within the Official Community Plan Land Use Map and redesignated from Phase II to Phase I Development within the Official Community Plan - Phasing Map.

**LEGAL DESCRIPTION** - W 1/2 35-36-6-W3 and LSD 3,5, and 6 on S 1/2 2-37-6-W3

**CIVIC ADDRESS** - N/A

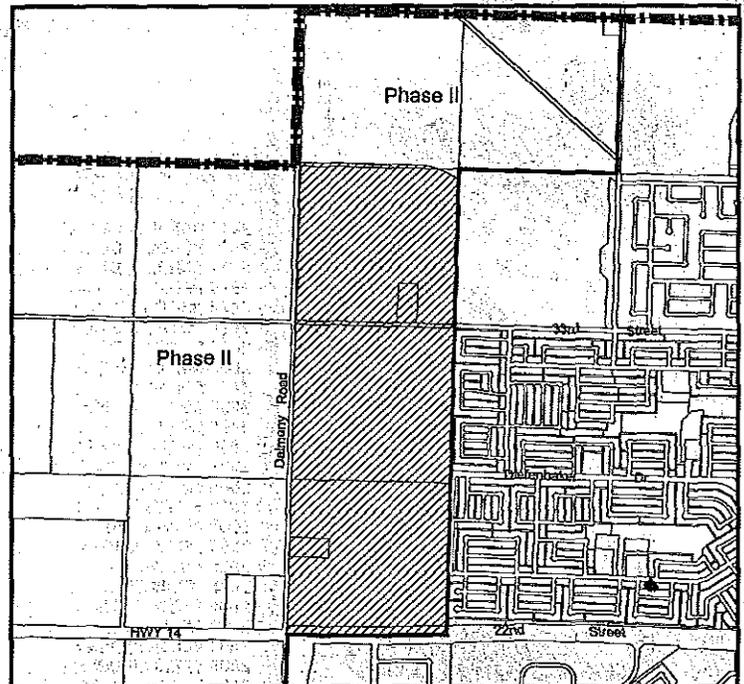


**AMENDMENT TO THE OFFICIAL COMMUNITY PLAN - LAND USE MAP**

From Urban Holding to Residential



N:\Planning\MAPPING\Official Comm Plan\2011 OCP\OCP13\_11 Aug (Amended)



**AMENDMENT TO THE OFFICIAL COMMUNITY PLAN - PHASING MAP**

From Phase II to Phase I



N:\Planning\MAPPING\Official Comm Plan\2011 OCP\OCP13\_11 Aug (Amended)

**REASON FOR THE AMENDMENT** - The proposed amendment would reclassify the land identified in the map above from Urban Holdings to Residential land use and revise the phased development of the land identified in the map above from Phase II to Phase I, to accommodate future residential development in the Kensington Neighbourhood.

**INFORMATION** - Questions regarding the proposed amendment or requests to view the proposed amending Bylaw, the City of Saskatoon Zoning Bylaw and Zoning Map may be directed to the following without charge:  
Community Services Department, Planning and Development Branch  
Phone: 975-7723 (Daniel Gray)

**PUBLIC HEARING** - City Council will hear all submissions on the proposed amendment, and all persons who are present at the City Council meeting and wish to speak on **Wednesday, August 15, 2012, at 6:00 p.m. in City Council Chambers, City Hall, Saskatoon, Saskatchewan.**

All written submissions for City Council's consideration must be forwarded to:  
His Worship the Mayor and Members of City Council  
c/o City Clerk's Office, City Hall  
222 Third Avenue North, Saskatoon SK S7K 0J5

All submissions received by the City Clerk by 10:00 a.m. on Wednesday, August 15, 2012, will be forwarded to City Council. City Council will also hear all persons who are present and wish to speak to the proposed Bylaw.

3c)

## BYLAW NO. 9044

### The Zoning Amendment Bylaw, 2012 (No. 11)

The Council of The City of Saskatoon enacts:

#### Short Title

1. This Bylaw may be cited as The Zoning Amendment Bylaw, 2012 (No. 11)

#### Purpose

2. The purpose of this Bylaw is to amend the regulations in the Zoning Bylaw governing Direct Control District 1 (DCD1) to:
  - (a) provide for greater building height and development density on the three development parcels located in Phase II of River Landing;
  - (b) address appropriate siting for hotel development;
  - (c) ensure that parking and loading facilities are appropriately located; and
  - (d) clarify the applicable sign regulations.

#### Zoning Bylaw Amended

3. Zoning Bylaw No. 8770 is amended in the manner set forth in this Bylaw.

#### Clause 13.1.3.1(a) Amended

4. The chart contained in Clause 13.1.3.1(a) is amended:
  - (a) by adding the following in the column "Uses" for the Category "Hotels":

"Hotels are prohibited on Parcel A, Plan No. 101977274 Extension 0 (410 Avenue C South) and Parcel BB, Plan No. 101977274 Extension 0 (426 Avenue B South).";

- (b) by adding the following in the column “Uses” under the Category “Residential”:

“Townhouses and Live/Work Units are prohibited on 19<sup>th</sup> Street west of Avenue B South, Avenue B South between 19<sup>th</sup> Street West and Sonnenschein Way, Sonnenschein Way between Avenue A South and Avenue B South and on Avenue A South.”

**Clause 13.1.3.3(a) Amended**

5. Clause 13.1.3.3(a) is amended by adding the following:

“Buildings proposed to be constructed on Parcel A, Plan No. 101977274 Extension 0 (410 Avenue C South) and Parcel BB, Plan No. 101977274 Extension 0 (426 Avenue B South) will be subject to specific review at the time of a development application, to ensure that the height is appropriate to the scale and design of the building, the nature of the adjacent street and landscape and that appropriate sunlight and wind protection are provided to adjacent areas.”

**Clause 13.1.3.4(c) Amended**

6. Clause 13.1.3.4(c) is repealed and the following is substituted:

- “c) Signage

The regulations applicable to Signage Group No. 5 of Appendix “A” – **Sign Regulations** will govern the use of signs in the DCD1 except that portable signs, billboards, superboards, electronic message centres and electronic message centres (mobile) are prohibited.”

**Clause 13.1.3.5(b) Amended**

7. Clause 13.1.3.5(b) is repealed and the following is substituted:

- “b) Parking Location

All off-street parking must be enclosed, covered, underground, within, or upon permitted buildings.”

**Clause 13.1.3.5(c) Amended**

8. Clause 13.1.3.5(c) is repealed and the following is substituted:

“c) Parking Access

Direct access from parking garages is not permitted onto Sonnenschein Way, Avenue A south of Sonnenschein Way, 2<sup>nd</sup> Avenue, 19<sup>th</sup> Street west of Idylwyld Drive, Avenue B South between 19<sup>th</sup> Street and Sonnenschein Way and Spadina Crescent. Parking access for Parcel D, Plan No. 65S12125 Extension 2 as shown on Plan 101977274 (422 Avenue A South) should be accessed from the rear of the site. Where parking access cannot be provided from the rear of the site, the access must be designed in a manner which does not detract from the adjacent pedestrian realm.”

**Clause 13.1.3.5(d) Amended**

9. Clause 13.1.3.5(d) is repealed the following is substituted:

“d) Direct access to garbage collection areas and service loading entrances is not permitted onto Sonnenschein Way, Avenue A south of Sonnenschein Way, 2<sup>nd</sup> Avenue, Spadina Crescent and 19<sup>th</sup> Street west of Idylwyld Drive. Service entrances to Parcel D, Plan No. 65S12125 Extension 2 as shown on Plan 101977274 (422 Avenue A South) must be located at the rear of the site.”

**Map No. 2 Amended**

10. DCD1 Maximum Building Height Map No. 2 is repealed and replaced by the DCD1 Maximum Building Height Map No. 2 attached as Appendix “A” to this Bylaw.

**Coming into Force**

11. This Bylaw comes into force on the day of its final passing.

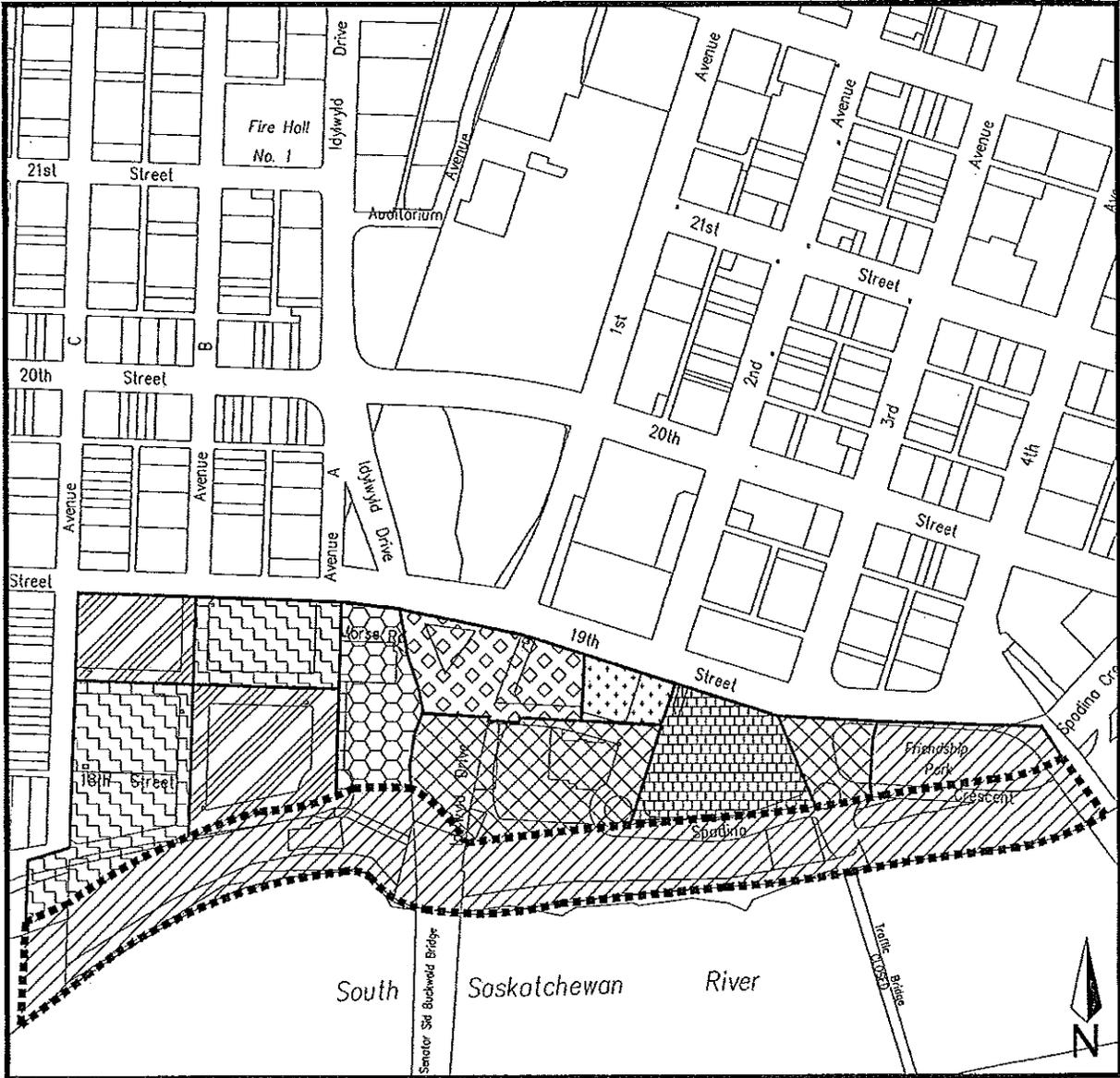
Read a first time this	day of	, 2012.
Read a second time this	day of	, 2012.
Read a third time and passed this	day of	, 2012.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

Appendix "A"

City of Saskatoon Zoning Bylaw



**DCD1 MAXIMUM BUILDING HEIGHT** MAP No. 2

Fourteen Metres - 2:1 FSR		Ninety-five Metres - 6:1 FSR	
Twenty-Four Metres - 4:1 FSR		Riverbank Area (11m) - 2:1 FSR	
Twenty-Seven Metres - 4:1 FSR		60m from 1991 Shoreline	
Forty-Eight Metres - 3:1 FSR			
Ninety-Five Metres - 5:1 FSR			
Sixty-Eight Metres - 4:1 FSR			

**City of Saskatoon**
  
 Community Services
   
 Planning & Development Branch

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**COMMUNITY SERVICES DEPARTMENT**

<b>APPLICATION NO.</b> Z9/12	<b>PROPOSAL</b> Zoning Bylaw No. 8770 Text Amendments to DCD1 District	<b>EXISTING ZONING</b>
<b>LEGAL DESCRIPTION</b>		<b>CIVIC ADDRESS</b>
		<b>NEIGHBOURHOOD</b>
<b>DATE</b> June 22, 2012	<b>APPLICANT</b> Planning and Development Branch City of Saskatoon	<b>OWNER</b>

**LOCATION PLAN**

**A. COMMUNITY SERVICES DEPARTMENT RECOMMENDATION:**

that a report be forwarded to City Council recommending:

- 1) that City Council approve the advertising respecting the proposal to amend Sections 13.1.3 of Zoning Bylaw No. 8770 as outlined in this report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
- 3) that the City Solicitor be requested to prepare the required bylaw to amend Zoning Bylaw No. 8770; and
- 4) that at the time of the public hearing, City Council be asked to consider the Administration's recommendation that the Zoning Bylaw No. 8770 text amendment be approved.

**B. PROPOSAL**

The Planning and Development Branch, in consultation with the City Manager's Office, is proposing that the Direct Control District 1 (DCD1) District be amended to provide for greater building height and development density on the three development parcels, located in Phase II of River Landing, to address appropriate siting for hotel development, to ensure that parking and loading facilities are appropriately located, and to clarify sign regulations.

**C. REASON FOR PROPOSAL (by Applicant)**

The amendments related to maximum building height, floor space ratio, and hotels will facilitate the opportunity to have a larger resident population in Phase II of River Landing in order to better utilize the subject sites and to create a critical population mass to support commercial development in the area.

The amendments related to the location of street townhouses and live/work units are intended to encourage developments, adjacent to important pedestrian thoroughfares, to have an active street frontage.

The proposed changes that relate to parking location, vehicle access, and service area access are intended to ensure that vehicle access points to these areas does not detract from the pedestrian oriented environment.

The amendments relating to signage are intended to clarify existing sign requirements.

**D. BACKGROUND INFORMATION**

These zoning amendments would apply to property, mainly in Phase II of River Landing. This area is zoned Direct Control District 1 (DCD1) and Architectural Control Overlay District (AC1). This zoning is based on the vision for the area, which was developed in the South Downtown Concept Plan 2004 and the South Downtown Local Area Design Plan. Both documents address the importance of building design and massing.

**E. JUSTIFICATION**

1. Community Services Department Comments

a) Proposed Zoning Amendments

It is proposed that the following provisions of the Direct Control District 1 (DCD1) District be amended:

- 1) Section 13.1.3.1 Linkage and Land Use – that hotels be prohibited on Parcel A (410 Avenue C South) and Parcel BB (426 Avenue B South) (see Attachment 2);
- 2) Section 13.1.3.1 Linkage and Land Use – that the location of street townhouses and live/work units be prohibited on 19<sup>th</sup> Street West, (west of Avenue B South); Avenue B South (between 19<sup>th</sup> Street West and Sonnenschein Way); Sonnenschein Way (between Avenue A South and Avenue B South); and on Avenue A South;
- 3) Section 13.1.3.3 a) Maximum Building Height – that the maximum building height for Parcel A and BB be increased from the current limit of 14 metres to 24 metres, and that the maximum building height of Parcel D be increased from 68 metres to 95 metres. The aforementioned maximum building height will be subject to specific review, at the time of a development application, to ensure that it is appropriate to the scale and design of the building, the nature of the adjacent street and landscape, and that appropriate sunlight penetration and wind protection are provided to adjacent areas;
- 4) Section 13.1.3.3 c) Maximum Floor Space Ratio – that the maximum floor space ratio for Parcels A and BB be increased from 2:1 to 4:1;

- 5) Section 13.1.3.4 c) Signage – to clarify that billboards, superboards, electronic message centres, and electronic message centres (mobile) are prohibited;
- 6) Section 13.1.3.5 b) Proposed Parking Location – that all off-street parking is required to be enclosed, covered, underground, within, or upon permitted buildings;
- 7) Section 13.1.3.5 c) Parking Access – that direct access from parking garages will not be permitted onto Sonnenschein Way or Avenue A (south of Sonnenschein Way) or 2<sup>nd</sup> Avenue or 19<sup>th</sup> Street west of Idylwyld Drive, Avenue B South between 19<sup>th</sup> Street and Sonnenschein Way, and Spadina Crescent. Parking access for Parcel D (422 Avenue A South) should be accessed from the rear of the site. When parking access cannot be provided from the rear lane, the access must be designed in a manner which does not detract from the pedestrian realm; and
- 8) Section 13.1.3.5 b) Service Areas – that direct access to garbage collection areas and service loading entrances will not be permitted onto Sonnenschein Way, Avenue A (south of Sonnenschein Way), 2<sup>nd</sup> Avenue, Spadina Crescent, or 19<sup>th</sup> Street west of Idylwyld Drive. Service entrance to Parcel D (422 Avenue A South) must be accessed from the rear of the site.

b) Policy Context

The Objectives of the DCD1 District include:

- 1) offering a dynamic blend of diverse and complementary land uses, which will attract people to the River Landing for evening, daily, and year-round activity;
- 2) supporting and enhancing existing and new commercial activities in the Downtown and Riversdale areas by encouraging both public and private investment;
- 3) producing mixed-use developments that will result in an urban environment, which is integrated with public activities conducted on or near the riverbank; and
- 4) creating a distinct identity and sense of place in Saskatoon and encouraging the recognition of the historical richness of the area.

Additional development density will serve to create the opportunity for the development of diverse and complementary land uses, which will enhance both River Landing and surrounding areas. These amendments are intended to ensure that development in River Landing provides a high

quality, well designed environment. These amendments have given detailed attention to the requirements which will serve to enhance the streetscape environment for pedestrians in this area.

Three development parcels in Phase II of River Landing, located west of Idylwyld Drive, will soon be brought to the market. The Planning and Development Branch, in conjunction with the City Manager's Office, is of the view that it is important to review the zoning, prior to the sale of these properties, to ensure that the opportunity is provided for high quality development, which will maximize the potential of River Landing. In this respect, the amendments being proposed are intended to facilitate mixed-use developments that will result in an urban environment that complements the riverfront, the Farmers' Market, and the Riversdale neighbourhood.

c) Meewasin Valley Authority (MVA)

The proposed amendments were prepared, following extensive consultation with MVA's administrative staff. It is very important that the City and MVA maintain complementary development regulations in River Landing. MVA's board will also consider these amendments in due course.

2. Comments by Others

a) Infrastructure Services Department

The proposed amendments, as noted above, are acceptable to the Infrastructure Services Department.

F. COMMUNICATION PLAN

Your Administration has undertaken an extensive consultation process with respect to these amendments. A public information meeting was held last fall addressing the potential for increased density and building height. Overall, support was expressed for increased height and density. Additional comments were noted with respect to retaining public uses, ensuring high quality architectural design, parking issues, and environmental sustainability.

In addition, your Administration held a series of meetings with developers, builders, architects, and real estate professionals regarding these amendments. They noted the significant quality of the development sites in River Landing and noted that opportunities for increased density would enhance the viability of these sites. The feedback received has been incorporated into draft amendments.

Your Administration also met with representatives from the Riversdale Business Improvement District and the Riversdale Community Association. Overall, the proposed amendments were viewed favourably. There were some questions related to the height and density of the future development, as well as concerns about parking in the area. Your Administration will continue to monitor the situation and work with the community with respect to this issue.

If this application is approved for advertising by City Council, a notice will be placed in The StarPhoenix once a week for two consecutive weeks. Notice boards will also be placed on the site. The Riversdale Community Association and the Riversdale Business Improvement District have been notified of this amendment.

**G. ATTACHMENTS**

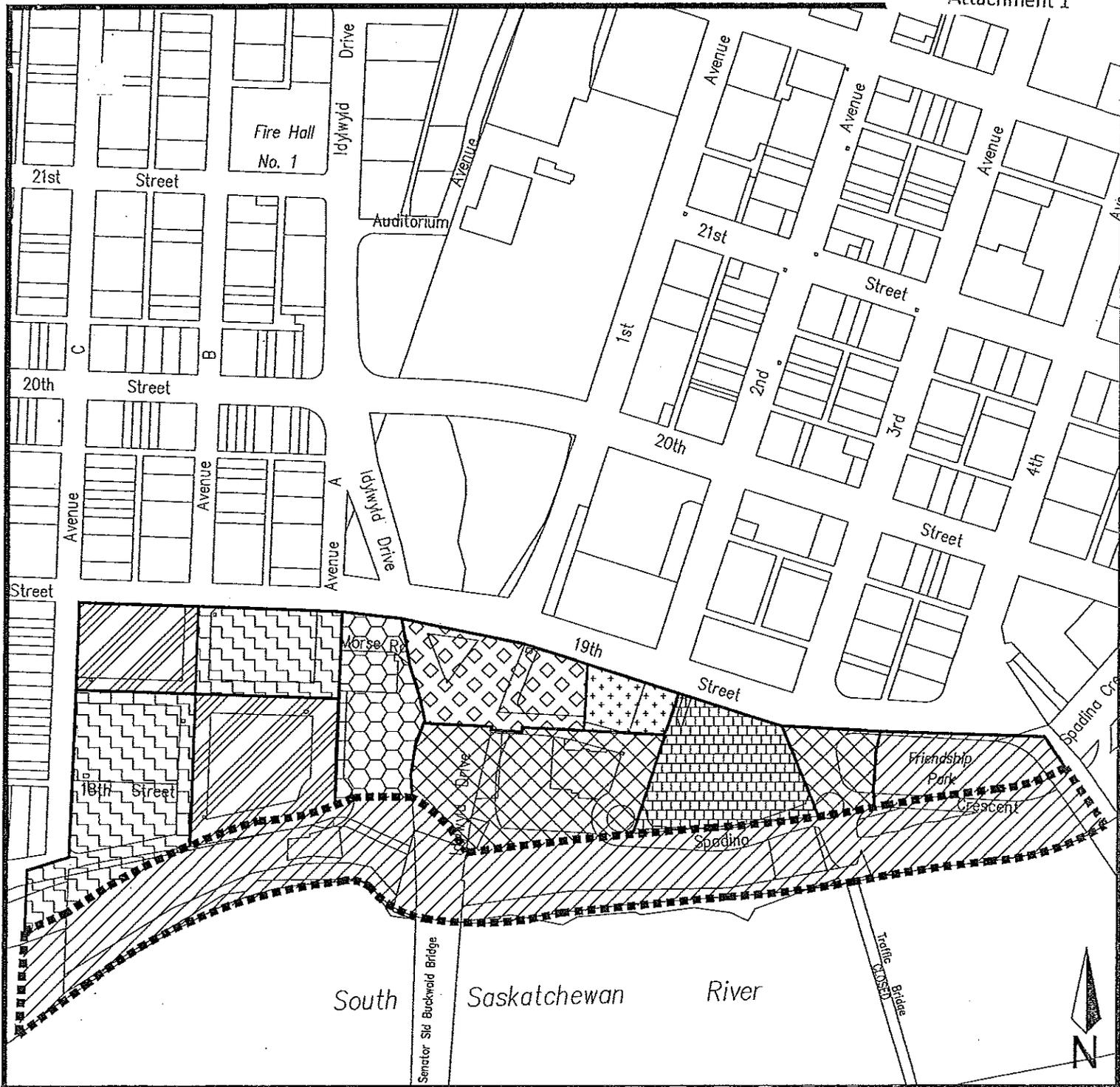
1. DCD1 Maximum Building Height Proposed – Map No. 2
2. Plan of Survey – River Landing – Phase II

Written by: Paula Kotasek-Toth, Heritage and Design Coordinator

Reviewed by: “Alan Wallace”  
Alan Wallace, Manager  
Planning and Development Branch

Approved by: “Randy Grauer”  
Randy Grauer, General Manager  
Community Services Department  
Dated: “June 29, 2012”

Approved by: “Jeff Jorgenson” for  
Murray Totland, City Manager  
Dated: “July 4, 2012”



# DCD1 MAXIMUM BUILDING HEIGHT PROPOSED

MAP No. 2

Fourteen Metres - 2:1 FSR



Twenty-Four Metres - 4:1 FSR



Twenty-Seven Metres - 4:1 FSR



Forty-Eight Metres - 3:1 FSR



Ninety-Five Metres - 5:1 FSR



Sixty-Eight Metres - 4:1 FSR



Ninety-five Metres - 6:1 FSR



Riverbank Area (11m) - 2:1 FSR



60m from 1991 Shoreline





The following is a copy of **Clause 2, Report No. 5-2012** of the **Municipal Planning Commission**, which was **ADOPTED** by City Council at its meeting held on **July 18, 2012**:

**2. Zoning Bylaw No. 8770**  
**Text Amendments to DCD1 District**  
**Applicant: Planning and Development Branch**  
**(File No. CK. 4350-012-4)**

- RECOMMENDATION:**
- 1) that City Council approve the advertising respecting the proposal to amend portions of Section 13.1.3 of Zoning Bylaw No. 8770, as outlined in the report of the General Manager, Community Services Department dated June 27, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
  - 3) that the City Solicitor be requested to prepare the required Bylaw to amend Zoning Bylaw No. 8770; and
  - 4) that at the time of the Public Hearing, City Council consider the Municipal Planning Commission's recommendation that the Zoning Bylaw No. 8770 text amendment be approved.

Attached is a report of the General Manager, Community Services Department dated June 22, 2012, with respect to the above proposed text amendments to the Zoning Bylaw.

Your Commission has reviewed the report with the Administration and supports the above recommendations.

**ZONING NOTICE**

**DCD1 ZONING DISTRICT – RIVER LANDING  
PROPOSED ZONING BYLAW AMENDMENT – BYLAW NO. 9044**

Saskatoon City Council will consider an amendment to the City's Zoning Bylaw (No. 8770). Through Bylaw No. 9044, the Zoning Amendment Bylaw 2012 (No. 11), will provide the City of Saskatoon with greater building height and development density on three development parcels in Phase II of River Landing and provide for development controls to ensure a quality pedestrian realm.

Specifically, the following amendments to the Direct Control District 1 (DCD1) are being proposed:

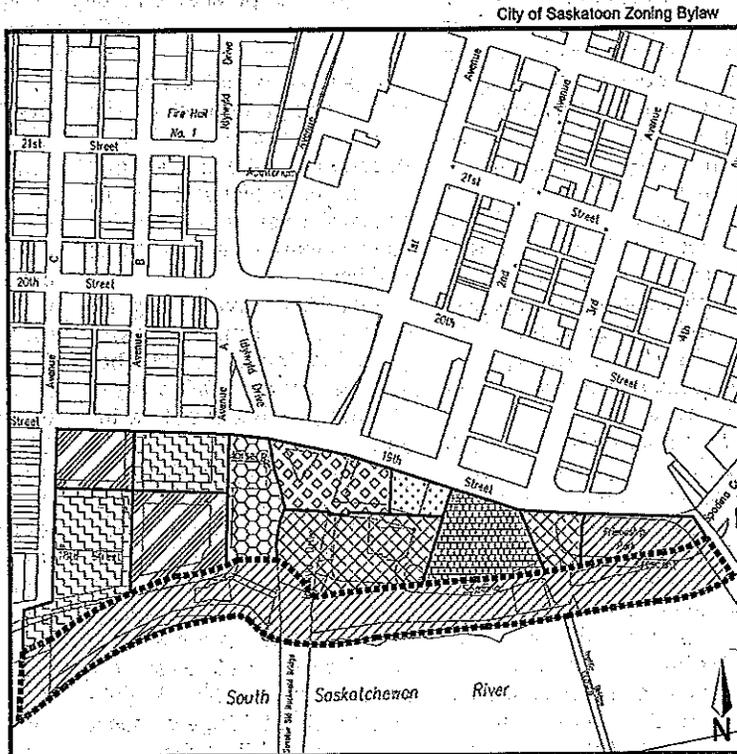
**Section 13.1.3.1 - Linkage and Land Use**

It is proposed that hotels be prohibited on Parcel A (410 Avenue C South) and Parcel BB (426 Avenue B South).

It is proposed that the location of street townhouses and live/work units be prohibited on 19th Street West (west of Avenue B South); Avenue B South (between 19th Street West and Sonnenschein Way); Sonnenschein Way (between Avenue A South and Avenue B South); and on Avenue A South.

**Section 13.1.3.3 a) Maximum Building Height**

It is proposed that the maximum building height for Parcel A and BB be increased from the current limit of 14 metres to 24 metres, and that the maximum building height of Parcel D be increased from 68 metres to 95 metres as shown on the following map.



**DCD1 MAXIMUM BUILDING HEIGHT PROPOSED**

MAP No. 2

Fourteen Metres - 2:1 FSR		Ninety-five Metres - 6:1 FSR	
Twenty-Four Metres - 4:1 FSR		Riverbank Area (11m) - 2:1 FSR	
Twenty-Seven Metres - 4:1 FSR		60m from 1891: Shoreline:	
Forty-Eight Metres - 3:1 FSR			
Ninety-Five Metres - 5:1 FSR			
Sixty-Eight Metres - 4:1 FSR			



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The aforementioned maximum building height will be subject to specific review, at the time of a development application, to ensure that it is appropriate to the scale and design of the building, the nature of the adjacent street and landscape, and that appropriate sunlight penetration and wind protection are provided to adjacent areas.

**Section 13.1.3.3 c) Maximum Floor Space Ratio**

It is proposed that the maximum floor space ratio for Parcels A and BB be increased from 2:1 to 4:1.

**Section 13.1.3.4 c) Signage**

It is proposed that billboards, superboards, electronic message centres, and electronic message centres (mobile) are prohibited.

**Section 13.1.3.5 b) Proposed Parking Location**

It is proposed that all off-street parking is required to be enclosed, covered, underground, within, or upon permitted buildings.

**Section 13.1.3.5 c) Parking Access**

It is proposed that that direct access from parking garages will not be permitted onto Sonnenschein Way or Avenue A (south of Sonnenschein Way) or 2nd Avenue or 19th Street west of Idylwyld Drive, Avenue B South between 19th Street and Sonnenschein Way, and Spadina Crescent. Parking access for Parcel D (422 Avenue A South) should be accessed from the rear of the site. When parking access cannot be provided from the rear lane, the access must be designed in a manner which does not detract from the pedestrian realm.

**Section 13.1.3.5 b) Service Areas**

It is proposed that direct access to garbage collection areas and service loading entrances will not be permitted onto Sonnenschein Way, Avenue A (south of Sonnenschein Way), 2nd Avenue, Spadina Crescent, or 19th Street west of Idylwyld Drive. Service entrance to Parcel D (422 Avenue A South) must be accessed from the rear of the site.

**REASON FOR THE AMENDMENTS** – The amendments related to maximum building height, floor space ratio, and hotels will facilitate the opportunity to have a larger resident population in Phase II of River Landing in order to better utilize the subject sites and to create a critical population mass to support commercial development in the area.

The amendments related to the location of street townhouses and live/work units are intended to encourage developments, adjacent to important pedestrian thoroughfares, to have an active street frontage.

The amendments that relate to parking location, vehicle access, and service area access are intended to ensure that vehicle access points to these areas do not detract from the pedestrian oriented environment. Those regarding signage are meant to clarify existing sign requirements.

**INFORMATION** - Questions regarding the proposed amendment or requests to view the proposed amending Bylaw, the City of Saskatoon Zoning Bylaw and Zoning Map may be directed to the following without charge: Community Services Department, Planning and Development Branch Phone: 975-2993 (Paula Kotasek-Toth)

**PUBLIC HEARING** - City Council will hear all submissions on the proposed amendment, and all persons who are present at the City Council meeting and wish to speak on Wednesday, August 15, 2012 at 6:00 p.m. in City Council Chambers, City Hall, Saskatoon, Saskatchewan.

All written submissions for City Council's consideration must be forwarded to: His Worship the Mayor and Members of City Council c/o City Clerk's Office, City Hall 222 Third Avenue North, Saskatoon SK S7K 0J5

All submissions received by the City Clerk by 10:00 a.m. on Wednesday, August 15, 2012 will be forwarded to City Council.

**BYLAW NO. 9045****The Adult Services Licensing Amendment Bylaw, 2012**

The Council of The City of Saskatoon enacts:

**Short Title**

1. This Bylaw may be cited as *The Adult Services Licensing Amendment Bylaw, 2012*.

**Purpose**

2. The purpose of this Bylaw is to amend *The Adult Services Licensing Bylaw, 2012* to specify a minimum distance that two or more adult service agencies or independent adult service agencies must be separated from one another.

**Bylaw No. 9011 Amended**

3. *The Adult Services Licensing Bylaw, 2012* is amended in the manner set forth in this Bylaw

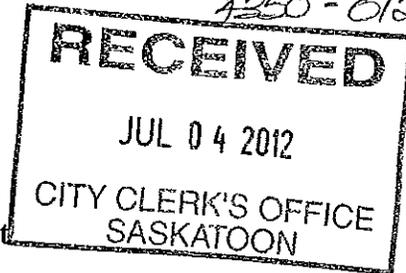
**New Section 16.1**

4. The following section is added after section 16:

**“Separation Distances for Adult Service Agencies and Independent Adult Service Agencies**

- 16.1 (1) Where an adult service agency or independent adult service agency is otherwise a permitted use, an adult service agency or independent adult service agency shall not be located within 160 metres of another adult service agency or independent adult service agency, as measured from property line to property line.
- (2) When a person applies for a licence for an adult service agency or independent adult service agency, and there exists another adult service agency or independent adult service agency within 160 metres of the applicant’s proposed location, the application shall be denied by the City.





**TO:** Secretary, Municipal Planning Commission  
**FROM:** General Manager, Community Services Department  
**DATE:** June 29, 2012  
**SUBJECT:** Adult Services Land Use Review – Separation Distance Between Adult Service Agencies  
**FILE NO.:** PL 4350-Z12/12

**RECOMMENDATION:** that a report be submitted to City Council recommending:

- 1) that City Council approve the advertising with respect to the proposal to amend the Adult Services Licensing Bylaw, 2012, Bylaw No. 9011, as indicated in the attached report;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
- 3) that the City Solicitor be requested to prepare the required amendments to the Adult Services Licensing Bylaw, 2012, Bylaw No. 9011; and
- 4) that at the time of a public hearing, City Council consider the Administration's recommendation that the Adult Services Licensing Bylaw, 2012, Bylaw No. 9011 be amended to provide for a minimum 160 metre separation distance between adult service agencies that may provide in-call services located in the IL1 – Light Industrial and IH - Heavy Industrial Districts be approved.

**BACKGROUND**

During its May 29, 2012 meeting, the Municipal Planning Commission (MPC) considered proposed amendments to Zoning Bylaw No. 8770 pertaining to adult service agencies. The MPC supported the recommendation for advertising the proposed amendments to Zoning Bylaw No. 8770 and resolved, in part:

- “6) that the Administration be requested to report further with respect to strategies to limit concentration of adult service activities in any one area of the city.”

During its June 18, 2012 meeting, City Council resolved:

- “1) that City Council approve the advertising to amend Zoning Bylaw No. 8770, as follows, and as further described in the report and attachments:

- a) to provide a definition of adult service agencies;
  - b) to permit adult service agencies as a home based business on an out-call basis only;
  - c) to limit in-call adult service agencies to the IL1 - General Light Industrial District and the IH – Heavy Industrial District; and
  - d) to include a 160 metre separation distance between in-call adult service agencies and residential properties, schools, parks, and active and passive recreational facilities;
- 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendments;
  - 3) that the City Solicitor be requested to prepare the required amendments to Zoning Bylaw No. 8770;
  - 4) that at the time of the public hearing, City Council consider the Administration's recommendation that the bylaw amendments be approved; and
  - 5) that City Council endorse the concept of separation distances between in-call adult service agencies to ensure clustering of adult service businesses does not occur, and that the Administration report back in due course on an implementation strategy."

## **REPORT**

During its July 18, 2012 meeting, City Council will hold a public hearing to consider amendments to Zoning Bylaw No. 8770 to regulate adult services as a land use, and also provide a separation distance of 160 metres as per City Council Resolution No. 1 above.

This report deals with Resolution No. 6 from the May 29, 2012 MPC meeting, and Resolution No. 5 from the June 18, 2012 City Council meeting.

### **Authority to separate Adult Service Agencies**

*The Cities Act* provides City Council with the authority to specify a minimum distance that two or more businesses within a class, or two or more classes of business, must be separated from one another. This provision of *The Cities Act* was used a few years ago to provide a separation distance of 160 metres between pawn shops in Business License Bylaw No. 8075 in response to a concentration of pawn shops along 20<sup>th</sup> Street West. The 160 metres was used to ensure that no more than one pawn shop would be established on a block.

### Proposed Separation between Adult Service Agencies

In response to concerns noted by the MPC and City Council over the potential impact on safety, real or perceived, and neighbourhood perceptions resulting from the concentration of adult service agencies, a similar separation distance as that used for pawn shops could be applied to adult service agencies.

It is not anticipated that concentration of adult service agencies will be an immediate issue in the city. However, it is possible, over time, for two or three adult services agencies to locate on the same block. This may create a negative perception within the community. In this regard, your Administration is recommending that the Community Services Department proceed with amendments to the Adult Services Licensing Bylaw, 2012, Bylaw No. 9011 to provide for a separation distance of 160 metres between adult services businesses located in the IL1 – Light Industrial (IL1) and IH - Heavy Industrial (IH) Districts. This separation distance would ensure that there is no more than one adult service agency per block.

The decision to act sooner rather than later has been based on the fact that any concentration problems that arise cannot be easily solved later. Once an adult service agency is established, it can only be grand-fathered out if a separation distance is imposed after the fact. In this regard, it is the opinion within the Community Services Department that it is prudent to include a separation distance within the Adult Services Licensing Bylaw, 2012, Bylaw No. 9011 for adult service uses.

### Other Municipalities

A review of other Canadian municipalities that currently license adult service businesses was undertaken and previously provided to the MPC. Information was obtained from the City of Calgary, the City of Edmonton, the City of Red Deer, and the City of Winnipeg. Relevant information on separation distances obtained from these municipalities is outlined below.

#### 1. City of Calgary

Recently, Calgary's City Council approved amendments to their Massage Bylaw to differentiate between massage categories. Massage practitioners who are not members to one of the four massage associations in Alberta would be re-classified as "Body Rub Practitioners" and would be subject to enhanced license requirements, such as a separation distance of 500 metres from other body rub centres or a residence. The intent of the amendments is to improve consumer protection and minimize negative impacts created in, or adjacent to, residential uses. Separation distance was particularly established to ensure body rub centres are not "clustering" together and creating body rub districts. These separation distances do not apply to dating and escort services or exotic entertainment businesses.

2. City of Edmonton

The City of Edmonton does not have separation distance requirements.

3. City of Red Deer

Adult entertainment establishments must be located 150 meters from any other drinking establishment or residential district. The City of Red Deer does not have a separation distance for escort service businesses.

4. City of Winnipeg

Adult service businesses and/or adult entertainment establishments located in commercial or industrial districts must be located 1,000 feet (305 metres) or more away from a residential district; park or recreational district; any place of worship; any elementary, middle, or high school; or any other adult service or entertainment use. The separation distance was implemented when the City of Winnipeg approved their X-Rated Stores Bylaw in 1993. The separation distance regulation does not apply to escort agency businesses.

### **OPTIONS**

The MPC does have the option of proposing alternate separation distances; however, your Administration would recommend using the same separation distance as pawn shops because it addresses the same issues, such as negative perceptions within a neighbourhood.

### **POLICY IMPLICATIONS**

Amendments to the text of the Adult Services Licensing Bylaw, 2012, Bylaw No. 9011 will be required to provide a separation distance of 160 metres between adult service agencies located in the IL1 and IH Districts.

### **FINANCIAL IMPLICATIONS**

There are no financial implications.

### **ENVIRONMENTAL IMPLICATIONS**

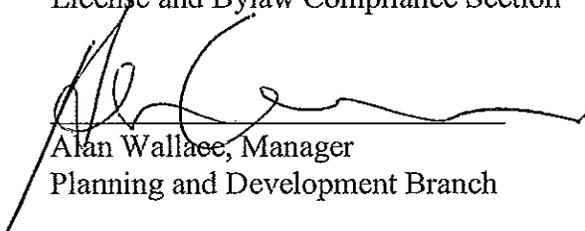
There are no environmental and/or greenhouse gas implications.

### **PUBLIC NOTICE**

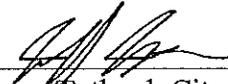
If the advertising for the text amendment to the Adult Services Licensing Bylaw, 2012, Bylaw No. 9011 is approved by City Council, the amendment will be advertised in accordance with

Public Notice Policy No. C01-021, and a date for a public hearing will be set. A notice will be placed in The StarPhoenix one week prior to the date on which the matter will be considered by City Council.

Written by: Melissa Austin, Planner; and  
Darryl Dawson, Manager, Business  
License and Bylaw Compliance Section

Reviewed by:   
Alan Wallace, Manager  
Planning and Development Branch

Approved by:   
Randy Grauer, General Manager  
Community Services Department  
Dated: June 29/12

Approved by:   
for Murray Totland, City Manager  
Dated: July 4/2012

The following is a copy of **Clause 1, Report No. 5-2012** of the **Municipal Planning Commission**, which was **ADOPTED** by City Council at its meeting held on **July 18, 2012**:

**1. Adult Services Land Use Review – Separation Distance  
Between Adult Service Agencies  
(File No. CK. 4350-012-2)**

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- RECOMMENDATION:**
- 1) that City Council approve the advertising with respect to the proposal to amend the Adult Services Licensing Bylaw, 2012, Bylaw No. 9011, as outlined in the report of the General Manager, Community Service Department dated June 29, 2012;
  - 2) that the General Manager, Community Services Department, be requested to prepare the required notice for advertising the proposed amendment;
  - 3) that the City Solicitor be requested to prepare the required amendments to the Adult Services licensing Bylaw, 2012, Bylaw No. 9011; and
  - 4) that at the time of the Public Hearing, City Council consider the Municipal Planning Commission's recommendation that the amendment to Adult Services Licensing Bylaw, 2012, Bylaw No. 9011, to provide for a minimum 160 metre separation distance between adult service agencies that may provide in-call services located in the IL1 – Light Industrial and IH – Heavy Industrial Districts, be approved.

Attached is a report of the General Manager, Community Services Department dated June 29, 2012, with respect to the above proposed amendments to the Adult Services Licensing Bylaw, 2012, Bylaw No. 9011.

Your Commission has reviewed the report with the Administration and heard representation from the Executive Director of the Riversdale BID in support of the initiatives as proposed. Your Committee supports the above recommendations.

## **BYLAW NOTICE**

### **PROPOSED ADULT SERVICES LICENSING BYLAW AMENDMENT – BYLAW NO. 9045**

Saskatoon City Council will consider an amendment to The Adult Services Licensing Bylaw, 2012 (No. 9011) to provide for a 160 metre separation distance between any adult service agency or independent adult service agency that may have client visits and is located in the IL1 – Light Industrial District or IH – Heavy Industrial District.

Through Bylaw No. 9045, Section 16.1 will be added to indicate that where an adult service agency or independent adult service agency is otherwise a permitted use, an adult service agency or independent adult service agency shall not be located within 160 metres of another adult service agency or independent adult service agency.

**REASON FOR THE AMENDMENT** – The reason for the amendment is to limit concentration of adult service businesses by establishing a minimum distance that two or more adult service agencies or independent adult service agencies, that may have client visits, must be separated from one another.

**INFORMATION** - Questions regarding the proposed amendment or requests to view the proposed amending Bylaw or the City of Saskatoon Adult Services Licensing Bylaw, 2012 can be directed to the following without charge:  
Community Services Department,  
Planning and Development Branch  
Phone: 975-2658 (Melissa Austin)

**PUBLIC HEARING** - City Council will hear all submissions on the proposed amendment and all persons who are present at the Council meeting and wish to speak on **Wednesday, August 15, 2012 at 6:00 p.m. in Council Chambers, City Hall, Saskatoon, Saskatchewan.**

All written submissions for City Council's consideration must be forwarded to:

His Worship the Mayor and Members of City Council  
c/o City Clerk's Office, City Hall  
222 Third Avenue North, Saskatoon, SK S7K 0J5

All submissions received by the City Clerk by 10:00 a.m. on Wednesday, August 15, 2012 will be forwarded to City Council. City Council will also hear all persons who are present and wish to speak to the proposed Bylaw.

4(a)

PROPOSED U-TURN

PROPOSED MEDIAN CLOSURE

25TH STREET

25TH STREET EXTENSION

IDYLWYLD DRIVE

PLAN DESCRIPTION/REVISIONS

4	
3	
2	
1	

DRAWN BY BAJ  
 DATE 2012-JUL-31

SCALE : HOR. 1:1000 VERT. \_\_\_\_\_



**City of Saskatoon**  
 Infrastructure Services Department

PROPOSED MEDIAN CLOSURE  
 25TH STREET & IDYLWYLD DRIVE

APPROVED  
  
 GENERAL MANAGER  
 ENGINEER

ENGINEER \_\_\_\_\_  
 PLAN NO. 250-0068-005r001

# PUBLIC NOTICE

## Proposed Median Closure and New Median Opening – Idylwyld Drive at 25th Street

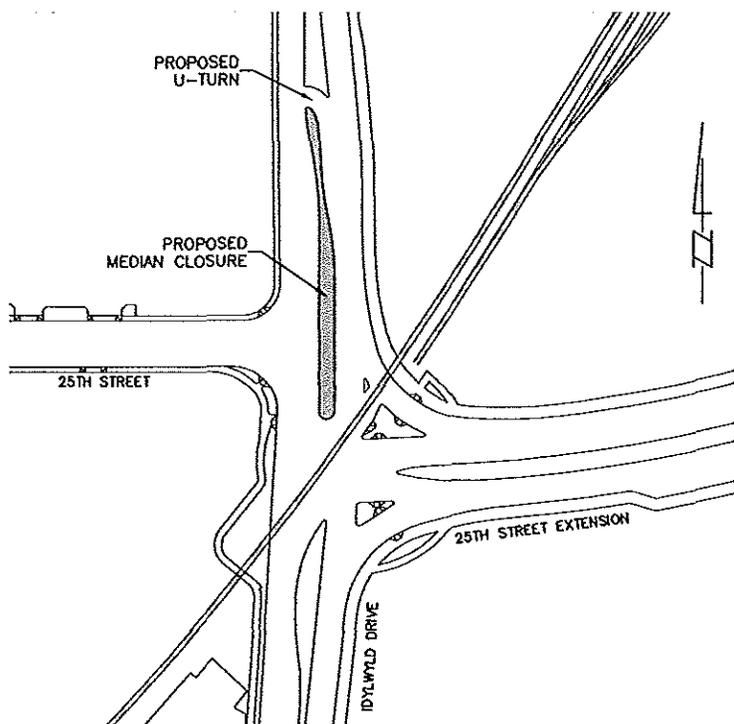
Attachment 2.

In accordance with the City Council Policy Number C01-021 - Public Notice, public notice is required before Council considers permanently closing or creating a median opening.

The proposed median closure on Idylwyld Drive is necessary to facilitate the 25th Street extension project, as stipulated by Transport Canada rules due to its proximity to the at-grade railway crossing. A new median opening that will accommodate the northbound U-Turn movement is proposed a short distance to the north.

Should this closure be approved by City Council, the existing opening will be permanently closed. Notices have been sent to parties affected by this closure.

**INFORMATION** - Questions regarding the proposal may be directed to: Infrastructure Services Department, Transportation Branch  
Phone: 975-2896 (Goran Lasic)



**PUBLIC MEETING** - City Council will consider the above matter and hear all persons present at the City Council meeting and wish to speak on Wednesday, August 15, 2012, at 6:00 p.m. in the Council Chambers, City Hall, Saskatoon, Saskatchewan.

All written submissions for City Council's consideration must be received by 10:00 a.m. on Wednesday, August 15, 2012 to:

His Worship the Mayor and Members of City Council  
c/o City Clerk's Office, City Hall  
222 Third Avenue North, Saskatoon, SK S7K 0J5