

# Connecting Avenue C Walking & Cycling Improvement Project

## Phase 3 Public Engagement: What We Heard Report

August 2023



## Executive Summary

The City of Saskatoon is committed to promoting active transportation and providing transportation choices that are safe and comfortable for people of all ages and abilities all year round.

Saskatoon's Active Transportation Plan (2016) identified Avenue C as a future All Ages and Abilities (AAA) cycling and walking route to help address community and infrastructure needs for cycling, walking, and other modes of active transportation in Saskatoon.

Three phases of engagement were conducted as part of the evaluation and design process for cycling facilities on Avenue C from Spadina Crescent to 45th Street. Phase 1 Engagement was complete as of June 2022, Phase 2 Engagement was complete as of December 2022, and Phase 3 Engagement was complete as of July 2023.

A full breakdown of the first and second phases of engagement and the themes that emerged can be found in the Phase 1 and the Phase 2 What We Heard Reports.

### Phase 3

The objectives of the third phase of engagement, conducted May – July 2023, focused on sharing and collecting feedback on the proposed design before taking the recommended design to Council.

A stakeholder session was held in the afternoon on June 13, 2023 and had approximately 12 attendees. A public engagement session was held in the evening on June 13, 2023 and had 55 to 60 attendees. An online public survey was open for responses from May 29 – June 30, 2023 and received 527 responses. One paper survey was submitted at the public engagement session. A total of four emails were received from the public and four comments were submitted on the project Engage Page.

*Common themes / comments from the stakeholder session included:*

- General Support for the 30 km/h speed limit.
- Support for proposed bike parking.
- Snow clearing is a concern.
- Support for curb ramps and sidewalk improvements.
- Parking impact is a concern.

*Common themes / comments from the public open house included:*

- *Suggestion for secure bike parking.*
- *Concerns with personal safety and crime rates.*
- *Accessibility issues are a concern.*
- *Concerns and questions around cost.*
- *Snow clearing was noted as a concern.*
- *Concerns about loss of parking spaces.*
- *Questions about how many people want / would use cycling facilities on Avenue C.*

*Common themes / comments from the survey responses included:*

- General support for reduced, 30 km/h speed limit.
- Concerns around the removal of parking on certain segments of Avenue C.
- Concerns for cyclist safety in areas without separate and protected cycling lanes proposed.
- Safety in general for cyclists and pedestrians is a recurrent concern / priority.

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## 1. Background

This document outlines feedback received from 2023 public engagement events in relation to the City of Saskatoon’s Connecting Avenue C Walking & Cycling Improvement Project (the “Project”). The Project focuses on the design of All Ages and Abilities (AAA) cycling facilities and improvements to walking facilities on Avenue C from Spadina Crescent to 45th Street in Saskatoon to enhance connectivity, safety, and accessibility.

The route is designed as a safe and inclusive space for all modes of transportation that connects the people of Saskatoon to each other and to many destinations in the City.

Several key factors will be considered in the planning and design of AAA walking and cycling facilities along Avenue C, including:



## 2. Stakeholder Groups

Representatives from the stakeholder groups were invited to the targeted stakeholder session. A comprehensive list of stakeholders identified as having the potential to be impacted by or interested in the construction of active transportation facilities along Avenue C was developed, including:

### Local Residents/ Homeowners

Those who live or own property on or near Avenue C between Spadina Crescent and 45th Street.

### Business Owners & Community Service Organizations

Those who own or operate businesses and/or community service organizations on or near Avenue C between Spadina Crescent and 45th Street.

### Cyclists, Pedestrians, Drivers and Mobility Device Users

Those who walk, cycle, drive or use mobility devices to travel along Avenue C between Spadina Crescent and 45th Street.

The targeted stakeholder list is a living resource to be developed and continuously refined to include people who are either directly or indirectly impacted by the project. Concerted efforts were made to identify any vulnerable and marginalized segments of the community, or community organizations who service vulnerable or marginalized segments of the community, to ensure they are invited to share their perspectives. The stakeholder list can be found in **Appendix A**.

## 3. Engagement Activities

Phase 3 Engagement included an in-person targeted stakeholder session and an in-person public engagement session to collect feedback that will inform the final design for All Ages and Abilities (AAA) cycling facilities and improvements to walking facilities on Avenue C.

Both the stakeholder and public sessions ran in conjunction with an online and paper public survey.

Participants	Level of Influence	Objective	Engagement Goal	Engagement activity
Targeted Stakeholders	Consult	Share information and obtain feedback and ideas	Phase 3: Receive input on the final active transportation facilities proposed for Avenue C and address questions and concerns.	Stakeholder session Public survey – online format Engage Page Sent email updates to the stakeholder group / subscribers list
Community/Residents	Consult	Share information and obtain feedback and ideas	Phase 3: Receive input on the final active transportation facilities proposed for Avenue C and address questions and concerns.	Public session Public survey – online and paper format Engage Page Correspondence with project team via email and phone

## 4. What We Heard

### 4.1 Phase 3 Stakeholder Session

#### 4.1.1 Purpose

The purpose of the Phase 3 stakeholder engagement session was to present the proposed walking and cycling improvements that were developed using the input gathered in the first and second phase of engagement to community leaders and key stakeholders and to collect feedback, comments, suggestions, answer questions and address concerns related to the proposed design. The stakeholder session was held on June 13, 2023.

#### 4.1.2 Marketing Techniques

Key community groups and partners were directly invited via email to participate in this session, see Appendix A.

#### 4.1.3 Input Received

Approximately 12 attendees participated in the in-person stakeholder session. Participants were encouraged to provide their feedback and ask questions/state concerns on the recommended design proposed for Avenue C. The presentation slide deck for the in-person stakeholder session can be found in **Appendix B**.

Recommended walking and cycling facilities were presented for each segment of Avenue C from Spadina Crescent to 45th Street, and attendees were asked to share their perspectives, thoughts, and concerns on each of the sections.

Feedback from session participants is broken down by street segment and has been summarized by common/recurrent themes, as outlined below.

#### Road Segment: Spadina Crescent to 19th Street – Commercial Area

- Support for the 30 km/h speed limit.
- Participants noted that this block of Avenue C is very narrow. On garbage days, people put bins in travel lane which essentially narrows the roadway to one travel lane.
- Support for proposed bike parking location at Isinger Park.
- Participants noted that there should be secure bike parking provided at the future Farmer's Market (should be located near Farmer's Market which is not on Avenue C).
- Support for maintaining parking on this block.
- One participant left a note on map: "No R.P.P. (Residential Parking Permit) to accommodate parking near houses."

### Road Segment: 19th Street to Jamieson Street – Commercial/Residential Area

- Accessibility concerns for people with visual impairments crossing bike lane. People with visual impairments cannot hear cyclists which increases the conflict potential. It was suggested that the pedestrian crossing area be highlighted to bring more awareness to cyclists that pedestrians will be crossing the bike lane and visual obstructions be minimized so cyclists can better see pedestrians. It was also noted that using a ramp so pedestrians cross the bike lane at street level is better for those with visual impairments as they are able to detect the bike lane; however, this could lead to an issue for those with mobility impairments if there isn't enough space between the bike lane and travel lane to properly ramp up then down.
- It was noted that existing accessible parking spaces should be accommodated.
- There are a lot of people walking to/from the Salvation Army building. It was noted that providing a bike lane will remove cyclists from the sidewalk which could improve safety.
- Future parking needs for the Riversdale area (with future development plans) should be considered with the removal of the east side parking lane on Avenue C between 19th Street and Jamieson Street.
- Support for secure bike parking at Avenue C and 23rd Street with future BRT station.
- Bike parking should be located outside of the sidewalk area.
- Snow clearing of the bike lane is a concern due to lack of snow storage area.
- One participant (BIZ) left a note on map: "Sharrows with 30 km/h speeds and leave parking" and "30 km/h through to 23rd".

### Road Segment: Jamieson Street to 33rd Street – Residential Area

- It was recommended that bike parking be provided with proposed bus barn redevelopment site.
- Support for the 30 km/h speed limit.
- Comment that people don't abide by stop signs; however, it was recognized that stop signs slow vehicles more than yield signs.
- Concerns noted regarding the wide driveways between 24th and 25th Street and the lack of curbs/grass edge to guide those with visual impairments.
- Support for curb ramp improvements to help those with visual and mobility impairments. It was noted that some intersections don't have curb ramps, or the existing curb ramps are not currently in the correct location to direct pedestrians to the crosswalk.

### Road Segment: 33rd Street to 41st Street – Residential and Commercial/Industrial Area

- It was noted that pedestrians want to cross on both sides of the 33rd Street intersection with Avenue C.
- Cyclists moving through the crosswalk at 33rd Street make crossing tough for the visually impaired community as cyclists are silent.
- It was recommended that parking be provided for cyclists visiting 33rd Street.
- Issue with trucks turning into grocery store at 33rd Street.
- Curb ramps and sidewalk connections as well as tactile pavement should be provided at all intersections.



## Road Segment: 41st Street to 45th Street – Commercial/Industrial Area

- It was recommended to quantify impact of any change made at the intersection of Circle Drive and Avenue C in terms of safety and operations. Multiple comments that this is a very busy intersection with a history of high collisions. Some noted that they did not feel it was safe for pedestrians or cyclists to cross at this intersection.
- Personal safety was identified as a concern in this area.
- It was recommended that Cynthia Street be considered for the cycling route, as opposed to Avenue C at the north end. Cynthia Street is much less busy than Avenue C in this area and leads directly to the existing pedestrian bridge over Idylwyld Drive.
- Snow clearing was noted as a concern. Where will snow be stored after the multi-use path and sidewalks are installed?
- It was noted that this is a heavy vehicle route and there could be driveway conflicts.
- Private parking impact was noted as a concern.

### 4.2 Phase 3 Public Open House

#### 4.2.1 Purpose

The purpose of the Phase 3 public open house was to present the proposed walking and cycling improvements that were developed using the input gathered in the first and second phases of engagement to the general public and to collect feedback, comments, suggestions, answer questions and address concerns. The open house was held on June 13, 2023.

#### 4.2.2 Marketing Techniques

Phase 3 engagement was advertised on the City's Engage page, through Saskatoon's social media feeds, and by direct email to stakeholder groups. Flyers with information about the engagement were delivered to the residents near Avenue C. An email update was sent to project subscribers. Four mini billboards were placed along Avenue C to promote the survey at the following locations:

- Cynthia Street & Robin Crescent
- 30th Street West & Avenue D North
- 302 22nd Street West
- Avenue C North & Circle Drive

Letters were delivered via Canada Post to businesses near Avenue C in the Riversdale Business Improvement District (BID), 33rd Street BID, and business along Avenue C between Circle Drive and 45th Street. The stakeholder group was encouraged to share the survey with their networks.

#### 4.2.3 Input Received

Approximately 55 to 60 attendees participated in the public open house. Similar to the stakeholder session, participants were encouraged to provide their feedback and ask questions/state concerns. The boards for the in-person public engagement session can be found in **Appendix B**.



The proposed walking and cycling facilities were presented for each segment of Avenue C from Spadina Crescent to 45th Street, and attendees were asked to share their perspectives, thoughts, concerns and given the opportunity to ask questions on each of the sections.

Feedback / comments from open house participants are summarized and broken down by road segment as outlined below:

### **Road Segment: Spadina Crescent to 19th Street – Commercial Area**

- Parking utilization during festivals and farmer's market were noted.
- Suggestion for secure bike parking at the farmer's market.

### **Road Segment: 19th Street to Jamieson Street – Commercial Area and Residential Area**

- Concerns with personal safety were noted.
- Some concerns about parking loss along this stretch for the businesses.

### **Road Segment: Jamieson Street to 33rd Street – Residential Area**

- Concerns with personal safety were noted.
- Question was asked about why we did not consider making Avenue C a one-way street and adding two-way bike lanes along the entire corridor.
- Comment that it is safe to bike here now so it is not necessary to add pavement markings and signage.
- Suggestion to add bike parking in the redevelopment area.
- Comment that curb ramp improvements would help people with mobility/visual impairments.
- Comment that the project is not needed for 4 months of the year.

### **Road Segment: 33rd Street to 41st Street – Residential Area and Commercial/Industrial Area**

- Concern regarding safety of 33rd Street and Avenue C intersection and the difficulty of crossing the 33rd Street Intersection.
- Personal safety and crime rates were noted as concerns.
- It was noted that high traffic was moved in front of park on Avenue D with diverter at Avenue C/38th Street.
- Concerns about cost of multi-use path. Is this cost effective?
- Question about data to show that people will use facilities.
- Question as to why Avenue C was chosen for this project?
- Comment that parking for cyclists visiting 33rd Street is needed.
- How many kids need to die on 33rd Street before attention is put here?
- Cannot access 33rd Street intersection.
- Suggestion for pedestrian facilities at intersections.
- Concern regarding City debt and cost.

## Road Segment: 41st Street to 45<sup>th</sup> Street - Commercial/Industrial Area

- Crime rates noted as a concern.
- Perceived low existing pedestrian and cycling use in industrial area.
- Concerns with narrowing the road (south of the rail line) to accommodate multi-use path and safety concerns about getting out of vehicle when parked.
- Concern regarding City's parking and travel lane width standards. Don't think they are real / sufficient.
- Snow clearing was noted as a concern.
- Questions on how to ensure cyclist safety in industrial area with large trucks and semis.
- Concerns about garbage pick up as there is no room on the road.
- Concerns from adjacent businesses on the impact to their business with the multi-use path and sidewalk implementation on Avenue C north of Circle Drive. They don't think anyone will use the facility and it is not needed for connections to transit stops, as no one uses transit.

## General Comments

- Questions about how many people want / would use cycling facilities on Avenue C.
- Questions about why Avenue C is the chosen route.
- Concerned about the loss of 125 parking spaces over the entire length of Avenue C.
- Several people expressed that they wanted this project cancelled and believed that this project is not needed.
- Several people expressed concerns over the funding gap (potential property tax increase) and how much this project would cost.
- Someone noted: "I can't believe this is actually being proposed. How many people have no safe way to bike to Avenue C?" – suggesting that there could be connectivity issues.
- Some people noted crime rates and that they didn't feel safe (personal safety) walking / cycling on Avenue C.
- Note to ensure that bike parking is outside of sidewalk area.
- Given the atmosphere of the room (large group of people in non-support for project), some people that were supportive of project expressed that they didn't feel comfortable adding notes or expressing their support publicly.
- Concerns regarding: "15-Minute Cities" and "UN agenda".
- Concerns about Council not listening to them.

## 4.3 Survey: Survey Monkey

### 4.3.1 Purpose

A survey was prepared in both paper and online format to gather feedback on the proposed cycling facility options. The online version was prepared using SurveyMonkey. The survey was open during the months of May and June for a total of 32 days. The survey captured 527 online participants.

*Of note, these were self-administered, non-random surveys and thus results cannot be considered to be statistically significant or representative of the opinions of all residents. As with other consultation tools,*

*the survey findings should not be considered in isolation, but instead factored into the context of other community input and assessment methodologies.*

### 4.3.2 Marketing Techniques

The survey was advertised on the City's Engage page, through Saskatoon's social media feeds, by direct email to stakeholder groups, and during the stakeholder and public Phase 3 engagement sessions. Flyers with information about the project and survey were delivered to the residents along Avenue C. Mini billboards were placed along Avenue C to promote the survey.

### 4.3.3 Input Received

#### 4.3.3.1 Demographics & Supplemental Information

##### Age Range

Survey respondents **largely** represented the age cohorts of:

- 35-44 years (27%),
- 45-54 years (21%),
- 25-34 years (20%), and
- 55-64 years (17%).

### What is your age range?

Answered: 441 Skipped: 86

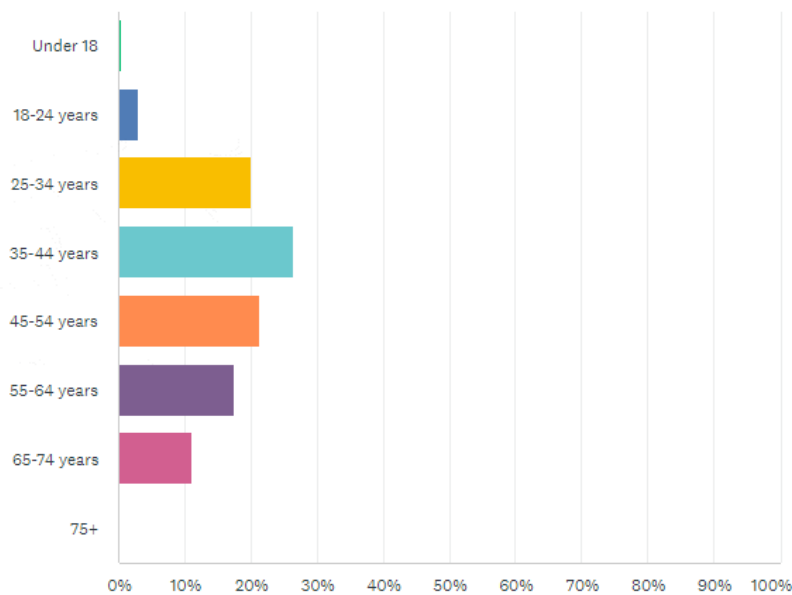


Figure 1 Age Cohort

## Gender and Identity

### Please indicate your gender identity

Answered: 438 Skipped: 89

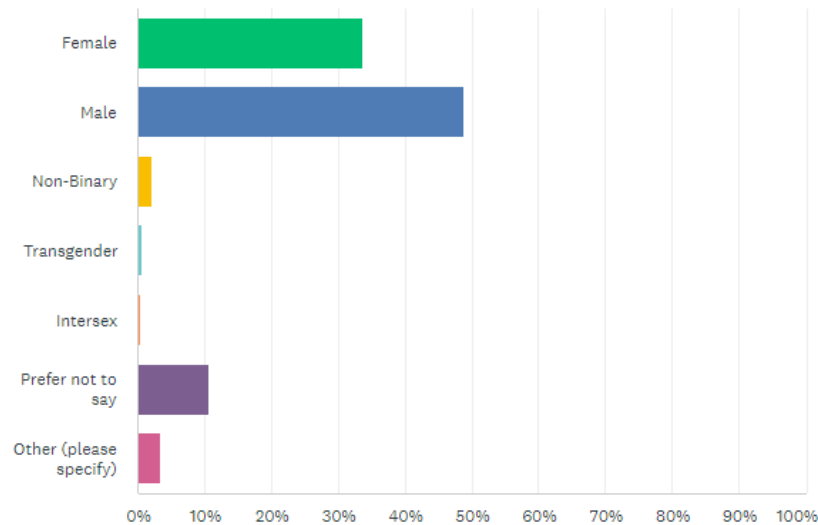


Figure 2 Gender Identity

Males represented 49% of participants and females represented 34%. 2% of participants identified as non-binary, 0.68% as transgender and 0.46% as intersex.

12% of respondents identified as having a disability.

10% of respondents identify as being part of a visible minority group.

When asked whether participants are Indigenous, 3% identified as First Nations, and 5% identified as Métis.

## Vehicles per Household

The majority of respondents (74%) indicated they have 1-2 vehicles available in their household, while 22% have 3 or more. The remaining 4% of respondents do not have a vehicle or did not specify.

## Travel on Avenue C

The next set of questions focused on how and why participants travel on Avenue C. When asked why participants travel on Avenue C, the top three reasons identified were to travel to work (49%), to access shopping and restaurants (47%), and to access the river (28%).

## For what reasons do you travel on Avenue C? (select all that apply)

Answered: 441 Skipped: 86

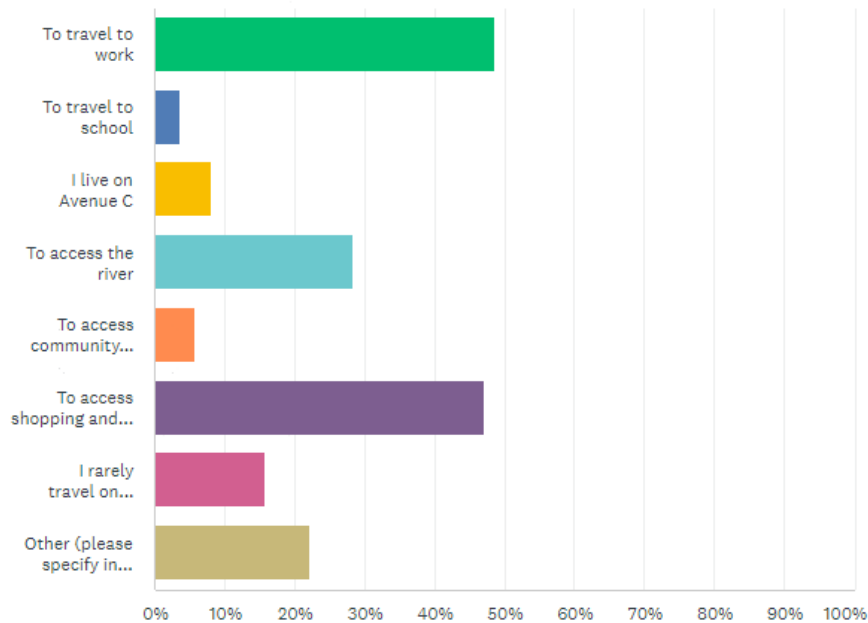


Figure 3 Reasons for Travelling on Avenue C

Other reasons included to connect to downtown bike lanes, access businesses and services, live in close proximity to Avenue C, as an access route to other parts of the City, to visit family and friends, to avoid traffic on Circle Drive, and to walk dog(s) to dog park.

### Modes of Transportation

Participants were asked what mode of transportation they use and how often these modes are used on Avenue C. Travelling via personal vehicle was ranked the highest with at least 40% of respondents traveling by this mode every day and 30% every week. Walking was ranked the second highest for everyday use with 22% followed by biking at 13%. In contrast, transit consistently ranked the lowest as a mode of transportation on Avenue C, which may be a result of the few transit routes currently available on Avenue C.

## What modes of transportation do you use and how often do you use this mode of transportation on Avenue C? (select all that apply to you)

Answered: 447 Skipped: 80

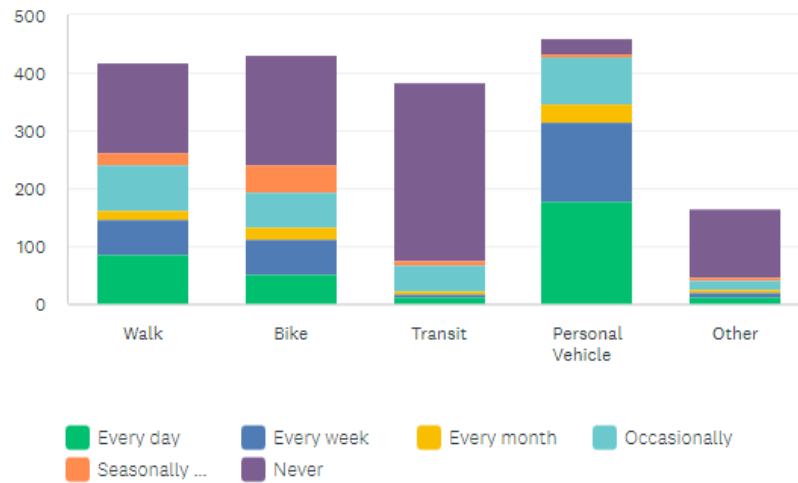


Figure 4 Modes and Frequency of Transportation

### 4.3.3.2 Option Rating and Feedback

In this section of the survey, participants were asked to review each of the segment options proposed and provide feedback. The feedback collected is summarized below.

#### Road Segment: Spadina Crescent to 19th Street – Commercial Area

##### Proposed Facility: Neighbourhood Bikeway

The first segment proposes a neighbourhood bikeway along Spadina Crescent to 19<sup>th</sup> Street.

### Proposed Cycling Facilities - Spadina Crescent to 19th Street



Participants were asked to comment on what they liked and what potential challenges they saw for the proposed cycling facilities. The likes and potential challenges identified have been categorized into themes below.

*The comments below have been categorized, summarized, and abbreviated from the raw data received in the survey.*

#### Likes Identified:

- Reduced speed limits / traffic calming measures will increase safety for all.
- Incentivising the concept of the road as a shared space.
- Good connectivity and signage.
- Does not disrupt neighbourhood vehicle access.

*"I like the raised crosswalk at 19th that is an excellent idea to slow traffic down that comes onto this street. I would have preferred to see a AAA protected bike lane in this section, but with the raised crosswalk and sidewalk bulbing this isn't a bad compromise. I appreciate that preserving parking adjacent to the park is important."*

#### Challenges Identified:

- Concern that Council will not approve the 30 km/h speed limit.
- Not a drastic enough change from what is currently in place.
- Concern for cyclist safety – need separate bike lanes or multi-use path.



- Cyclists do not feel safe and therefore use of this facility will be low.
- Cost.

*“The raised crosswalks must be bike friendly. If they have aggressive “lips” they can cause flat tires.”*

### Road Segment: 19th Street to Jamieson Street – Commercial and Residential Area

#### Proposed Facility: Unidirectional Bike Lanes

The second segment proposes unidirectional bike lanes that would provide a suitable level of separation given the traffic volumes and roadway function along Avenue C from 19th Street to 25th Street. One lane of parking would need to be removed in order to implement protected bike lanes. Participants were again asked what they liked or thought would be a challenge for this facility.

#### Proposed Cycling Facilities – 19th Street to Jamieson Street



#### Likes Identified:

- General agreement that this is a good design overall and would greatly improve safety.
- Clearly indicated bike lanes – support for the differentiation using a bright green color.
- Protected bike lanes – physical separation is safer for cyclists – support for the raised curb.
- Improvements at rail crossing.

*“Providing a clearly defined, protected space for cyclists will make it so much easier to cycle, especially with my kids. As a long time neighbourhood resident who has also leased commercial*

*property in this area, this will be a huge benefit, making the area more attractive and easy to access for residents, visitors and employees."*

Potential Challenges Identified:

- Concern for visibility at intersections and ensuring vehicles yield when turning across the path of the bike lanes.
- Concern that bike lanes won't be kept clear in winter and will therefore not be useable during that time.
- Loading/unloading passengers block bike lanes.
- Separated bike lanes should be extended to 25th Street in order to reach a safer neighborhood street.
- Concern for opening car doors into the cycling lane.
- Concern that people will park vehicles and constantly walk across bike lane which will surprise cyclists.
- Drivers claiming that cyclists "came out of nowhere" because they were obstructed from view by parked cars.
- Loss of parking.

*"Consistent winter infrastructure. I feel that if the bike lanes are not consistently cleared, people will not be able to use them and it will feed into the narrative that cycling isn't a valid form of winter transportation."*

### Road Segment: Jamieson Street to 38th Street – Residential Area

#### Proposed Facility: Neighbourhood Bikeway

Given the traffic volumes and operating speeds of the residential area along Avenue C from Jamieson Street to 38<sup>th</sup> Street, a neighbourhood bikeway was determined to be the most appropriate option for this section of the Avenue C corridor.

**Proposed Cycling Facilities – Jamieson Street to 38th Street**



**Likes Identified:**

- Does not disrupt parking.
- Support for 30 km/h speed reduction (as long as it is properly enforced).
- Curb ramps benefit others like parents with strollers and persons using wheelchairs.
- Connectivity with other cycling infrastructure.
- More trees being planted.
- Gets riders close to Saskatchewan Polytechnic Campus.
- Quickest to implement.
- Proximity / access to important landmarks / services (e.g., schools and parks).
- No interruptions in cycling routes by pedestrians.

*“I love the 30 km/h speed limit. It feels like a 30 zone there anyway (even as a driver, I couldn’t imagine going much faster on that road), and it would make the street safer.”*

**Potential Challenges Identified:**

- Does not help protect cyclists from drivers – potential for conflict.
- Concern that Council will not approve 30 km/h speed limit.
- General concern that drivers will not obey 30 km/h posted speed limit which will put cyclists in danger.
- Cost.
- Not family friendly – concern for cycling on roads with children.
- Concern that it might be cancelled.
- Concern for sharing the roads in winter conditions – safety of cyclists.
- Not enough of a change from current design.

- Concern for cyclists getting “doored” by parked cars because they're pressured to keep out of cars' way.

*“Lack of clearly defined space for cyclists means that it may not be as comfortable having kids bike through this area.”*

*“Shared lanes are still at the mercy of drivers; a little driver education re safe passing distance etc. would help a lot.”*

### Road Segment: 38th Street to 41st Street – Residential and Commercial/Industrial Area

#### Proposed Facility: Multi-Use Path

A 3.0 m wide raised multi-use path on the east side of Avenue C is proposed for 38th Street to 41st Street, which would provide a suitable level of separation from vehicles.

#### Proposed Cycling Facilities – 38th Street to 41st Street



#### Likes Identified:

- Separated and dedicated lane - bike lane is separated from the vehicles.
- A multi-use pathway will provide safety for cyclists, skateboarders, and people on scooters.
- General agreement that this is an improvement from the current design – reasonable compromise.
- Strategically removing parking.

- Provides safe cycling route to the north end.

*“I like the multi use path and that it serves as a safe space for active transportation of all types. Glad to keep the sidewalk as well.”*

Potential Challenges Identified:

- Pedestrian and cyclist path within multi-use pathway is not labeled.
- Concern that it may be cancelled.
- Ramps are sometimes quite rough and tough to bike on.
- Segregated to one side of street, difficult to access other side, difficulty in making turns to opposite side.
- Cost.
- Loss of parking.
- Not really necessary in this area.

*“Transitions from previous sections of bike lanes to this could be problematic and confusing especially as both bike lanes on same side. I would not cross over to bike lane for two blocks.”*

### Road Segment: 41st Street to 45th Street – Commercial/Industrial Area

#### Proposed Facility: Multi-Use Path on East Side and Sidewalk on West Side

A multi-use path on the east side was determined to be the most appropriate option for 41st Street to 45th Street. The 3.0 m raised multi-use path is located on the east side due to light standards near the curb on the west side and provides a suitable level of separation from vehicles. The path replaces the existing sidewalk since both pedestrians and cyclists share it. Four travel lanes are maintained; however, the northbound lanes need to be narrowed slightly from 41st Street to Circle Drive. Modifications at the Circle Drive intersection are included to improve safety for pedestrians and cyclists crossing at the intersection.

### Proposed Cycling Facilities – 41st Street to 45th Street



#### Likes Identified:

- Cyclist separation / protection from traffic.
- General agreement that this is better than current design.
- Like that it is a bi-directional MUP.
- Grade separation increases safety and access to businesses along this stretch for cyclists.
- Continuity of east side shared path.
- New sidewalk being added for pedestrians.
- Allows non-vehicle users to access services and facilities.
- Increases safety in an area that is very dangerous for cyclists and pedestrians.
- Good upgrade for employees who work in this area.
- Helps to create a more walkable/rideable area that is safer and more enjoyable.

*“This would make cycling possible on a stretch that I would never consider using otherwise.”*

#### Potential Challenges Identified:

- Preference is for a dedicated, separate cycling lane.
- There is no shade for pedestrians or cyclists.
- No wind protection due to lack of trees.
- Not on both sides of the road making it less safe for cyclists who need to cross - learning curve for traffic turning right.
- Difficulty entering/exiting the multi-use path.



- Concern of unpleasant interactions between cyclists and pedestrians.
- Electric bikes that speed on multi use paths.
- Perceived as not being needed.
- General cost and cost of extra land.

**“Almost all of the development is on the west side of Ave C in that section. So, as a cyclist, there's no way to access most of that area. Crossing Avenue C in that section is dangerous in a small car, let alone on a bike or on foot.”**

## 5. Additional Comments

Participants had the opportunity to provide additional comments at the end of the survey:

- General support for proposed improvements and hope that it goes ahead.
- Concern that facility options will never be built – project will be cancelled.
- Frustrations and questions as to why gender, race, minority status was asked / and how it is relevant to the project.
- City in need of AAA Active Transportation infrastructure – Admin must educate the public and Council on this issue.
- Sidewalks need to be improved for people in wheelchairs.
- Frustrations that facility option votes from previous survey not influencing final proposed design.
- Concerns around financial deficit and cost of project.
- Timing of project is not ideal given the economical state of the world.
- Suggest that option costs are included in survey.
- Too many open answered questions on this survey.

The community also had the opportunity to email comments to the City directly or enter comments on the project Engage Page, which are summarized below:

- *Support for the Avenue C project and the benefits it will provide the community such as:*
  - *benefits many residential areas where there are lots of families, pedestrians, schools, and community amenities*
  - *goes through a number of shopping and business areas (benefitting both those shopping and working at those destinations)*
  - *provides access to the river*
  - *provides a good north-south route for AT in the west*
  - *reduces the speed limit*
  - *adds some new sidewalk infrastructure*
  - *includes some sections of dedicated protected paths*

***“I hope that the work doesn't get bogged down by financial and political concerns as the plans are very encouraging.”***

- *Concern over City adding to City debt and cost on taxpayers for building bike lanes for usage 4 months of the year.*

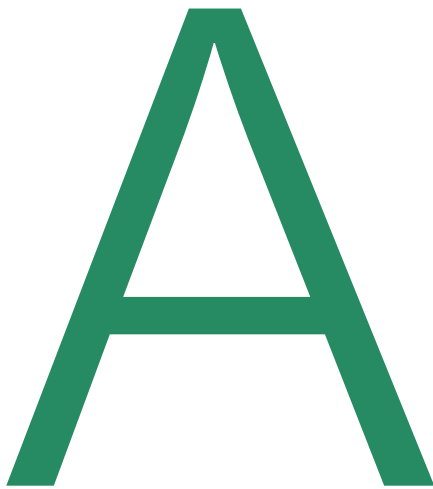


- *Concern about snow clearing and maintenance of the bike lanes during the winter months.*

## 6. Next Steps

The feedback received during Phase 3 Engagement will be used to help finalize the Connecting Avenue C design for walking and cycling facilities along Avenue C.

## APPENDIX



EXTERNAL STAKEHOLDERS
EDUCATION
University of Saskatchewan Students' Union
Caswell Community School
Mayfair Library Branch
EcoQuest school program
Saskatchewan Polytechnic
Saskatoon Community Youth Arts Programming (SCYAP)
MOBILITY/RECREATIONAL USERS
Bridge City Bicycle Co-Op
Saskatoon Cycles
Walking Saskatoon
Bus Riders of Saskatoon
EQUITY/ACCESSIBILITY/ADVOCACY
Canadian National Institute for the Blind (CNIB)
SaskAbilities
Vision Loss Rehabilitation Saskatchewan
Saskatoon Council on Aging
Saskatoon Food Bank and Learning Centre
Salvation Army
OutSaskatoon
Core Neighbourhood Youth Co-op
Inclusion Saskatchewan
Renters of Saskatoon and Area
Crocus Cooperative
COMMUNITY ASSOCIATIONS
Caswell Community Association
Hudson Bay Park Mayfair Kelsey Woodlawn Community Association
Riversdale Community Association
BUSINESS/ECONOMIC
Riversdale Business Improvement District (BID)
Downtown BID
33rd Street BID
North Saskatoon Business Association

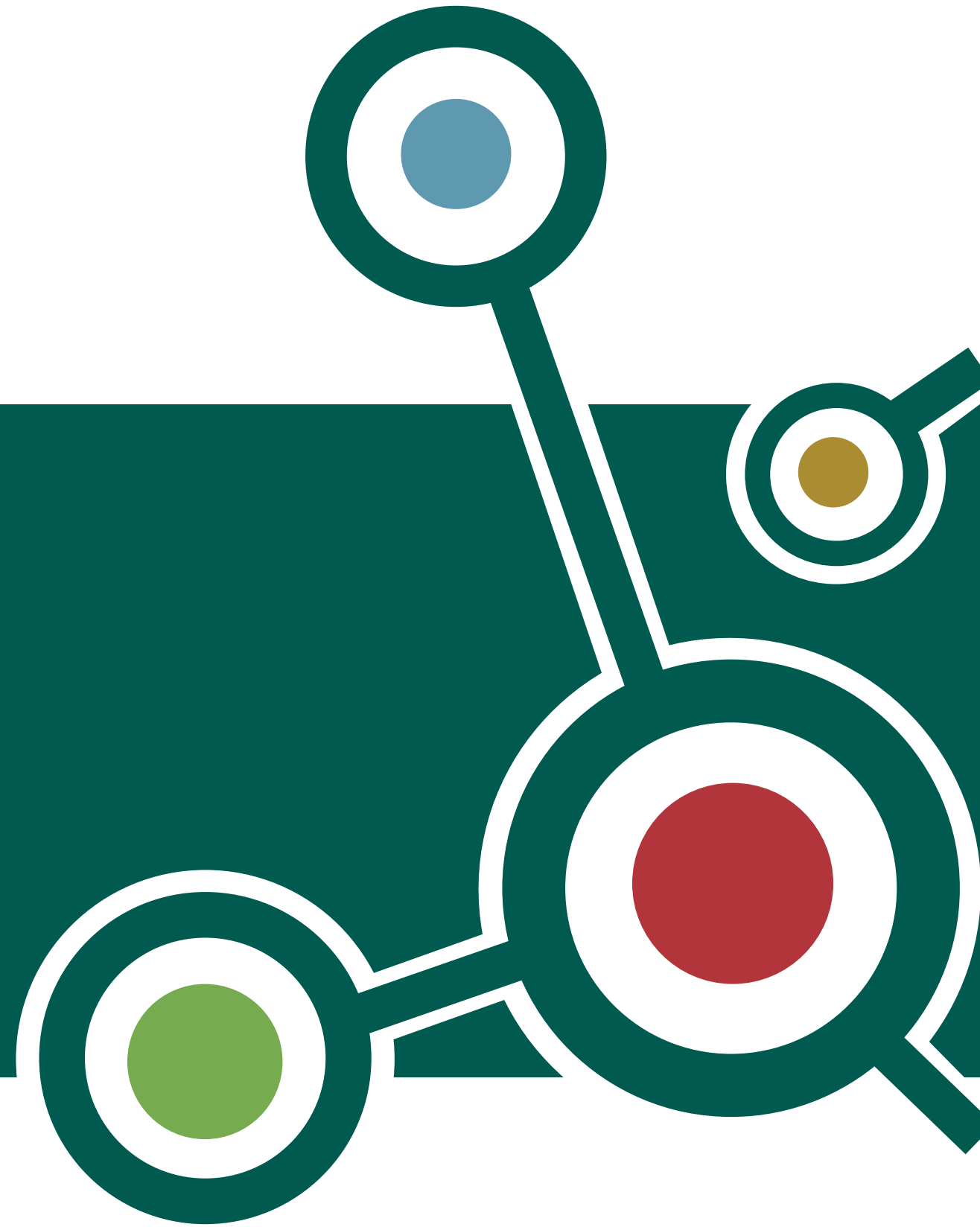
INDIGENOUS
Central Urban Métis Federation Inc. (CUMFI)
Gabriel Dumont Institute
ENVIRONMENTAL
Eco Friendly Saskatoon
Climate Justice Saskatoon
Saskatchewan Environmental Society
Meewasin Valley Authority
Saskatoon Youth Climate Committee
SOS Trees Coalition
Wild About Saskatoon
TRANSPORTATION/INFRASTRUCTURE
Airport Business Area/North Industrial
HEALTH
Saskatoon Health Authority

## APPENDIX

# B

# Connecting Avenue C

Walking and Cycling  
Improvements



# ABOUT THE PROJECT

The City of Saskatoon is committed to improving active transportation options for residents and visitors. In support of the City's active transportation goals, **Avenue C** has been identified as an **All Ages and Abilities (AAA) cycling route** to be designed as a safe and inclusive space for all modes of transportation that **connects the people of Saskatoon to each other and to many destinations in the City.**



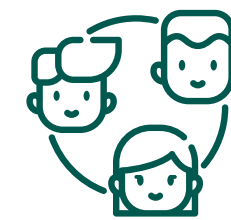
## Key goals of the study include:



Designing a **safe, comfortable, and accessible walking and cycling corridor** along Avenue C



Engaging residents throughout plan development to **understand local priorities and concerns**









Creating a plan that will **consider the needs of all users.**



# PROJECT LOCATION

The project is focused on the design of **All Ages and Abilities (AAA) cycling facilities** and **improvements to walking facilities** on Avenue C from Spadina Crescent to 45th Street in Saskatoon. The Avenue C corridor crosses many different types of land uses including commercial, residential, and industrial.

## LEGEND

-  Study Corridor
-  Future AAA Cycling Network
-  Future Multi-Modal Corridor
-  Existing Off-Street Pathway
-  Existing Neighbourhood Bikeway
-  Existing Protected Bikeway

### Commercial

### Residential

### Commercial/Industrial

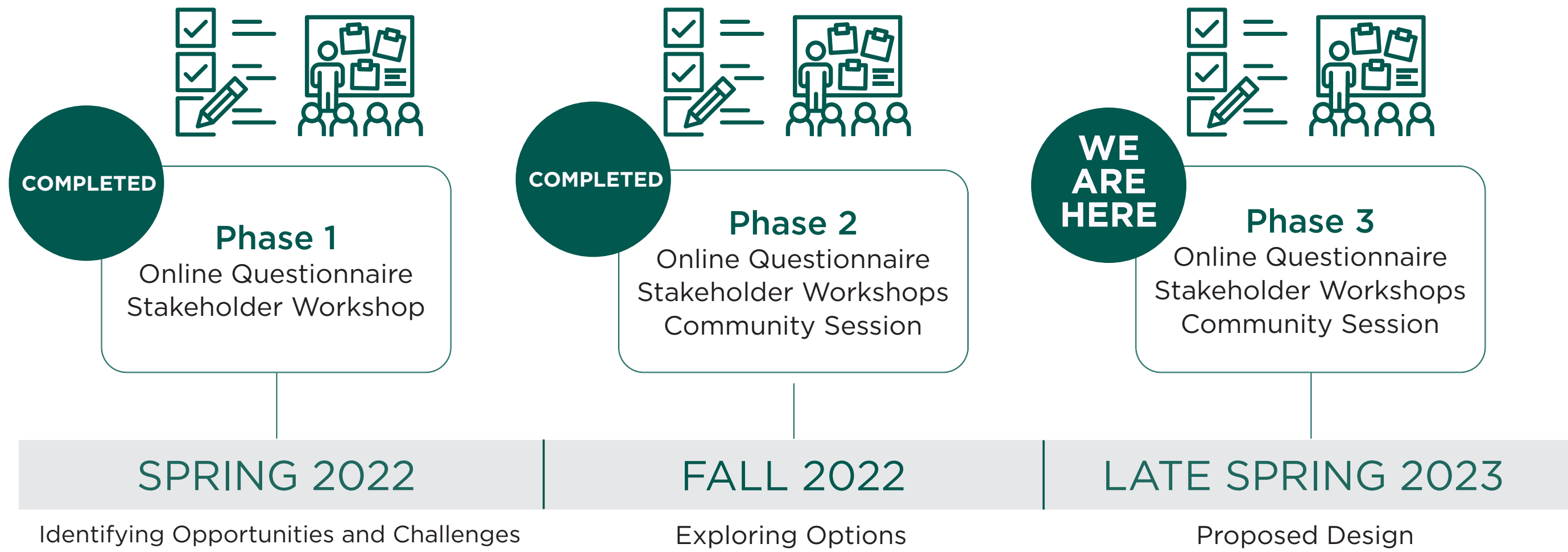






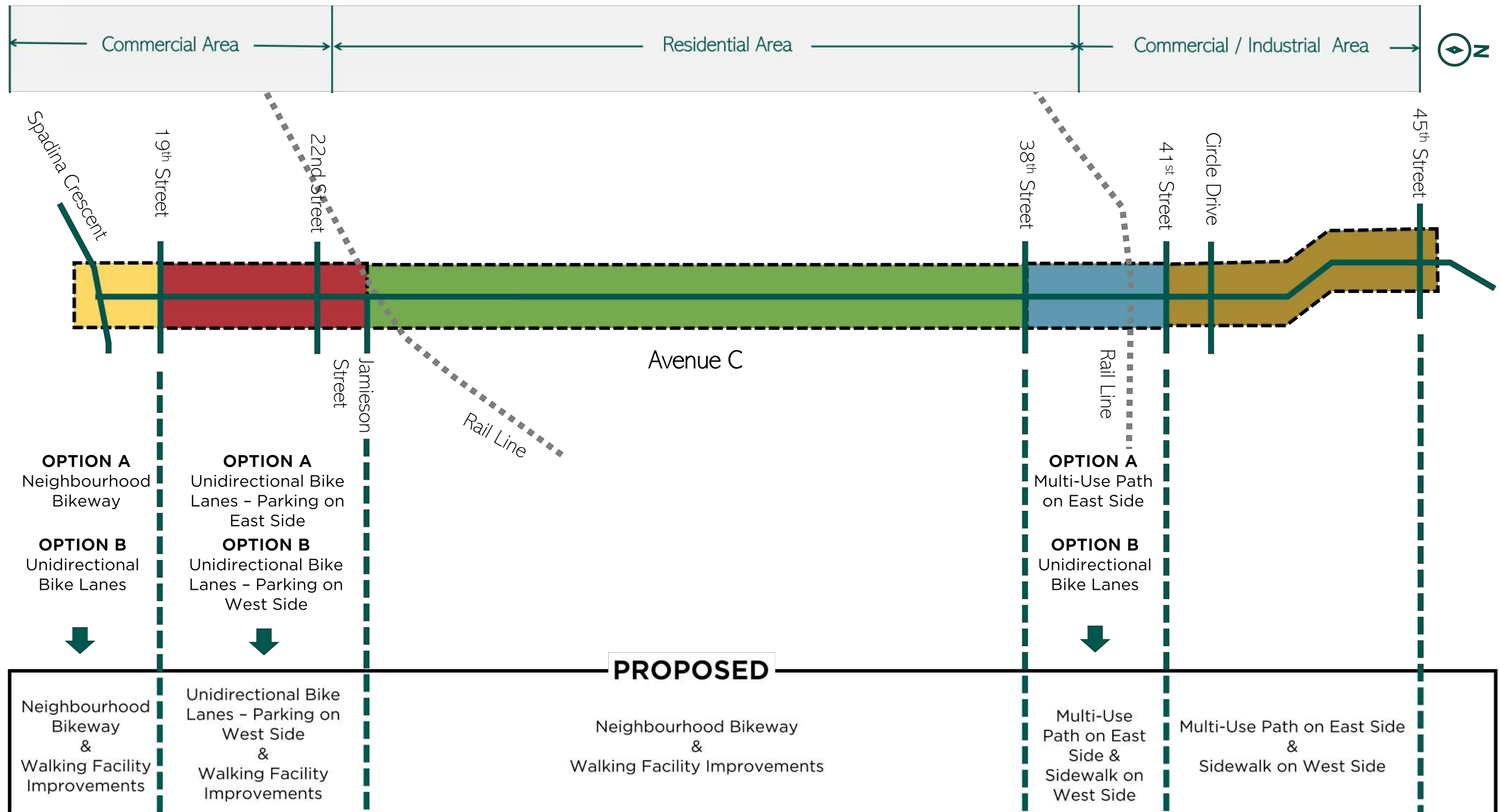
This round of engagement will be focused on us sharing and collecting feedback on the proposed designs before a final report detailing the findings and recommendations will be presented to the Standing Policy Committee on Transportation (SPCT) in Fall 2023.

## Public and stakeholder engagement will be conducted at key points throughout the project, including:



# POSSIBLE CYCLING & WALKING FACILITIES

## OPTION SUMMARY





## Common themes from the Phase 2 feedback include:

- Desire to widen sidewalks to enhance comfort and safety for all users.
- Concern with the number of pedestrians and cyclists crossing Avenue C, especially in school zones.
- Desire to prioritize pedestrians and cyclists over vehicular traffic.
- Concerns around the removal of parking on certain segments of Avenue C.
- Desire to increase landscaping along the corridor and to retain existing green space and trees.
- Concerns for cyclist safety and concerns regarding sharing the road with vehicle traffic.
- Concern regarding potential conflicts between pedestrian and cyclists on shared, multi-use paths.



# PROPOSED CYCLING FACILITIES

## SPADINA CRESCENT TO 19TH STREET



Neighbourhood bikeways are on-street routes designed to move cyclists, pedestrians, and vehicles comfortably and safely. Neighbourhood bikeways typically include a range of treatments such as signage, pavement markings and traffic calming.

The proposed design includes:

- Neighbourhood bikeway on Avenue C from Spadina Crescent to 19th Street.
- Parking on both sides of Avenue C.
- Retaining existing sidewalks on both sides of Avenue C.



Sample cross section of neighbourhood bikeway on Avenue C. Recommended posted speed 30 km/h pending Council approval.



# PROPOSED CYCLING FACILITIES

## 19TH STREET TO JAMIESON STREET



Unidirectional bike lanes are physically separated, on-street lanes designated exclusively for one-way bike travel. Cyclists will be physically separated from vehicles by a raised curb. Cyclists can enter/exit at intersections and vehicles are blocked from entering the bike lane.

The proposed design includes:

- Street level unidirectional bike lanes on Avenue C from 19th Street to Jamieson Street.
- Parking on the west side of Avenue C.
- Retaining existing sidewalks on both sides of Avenue C.

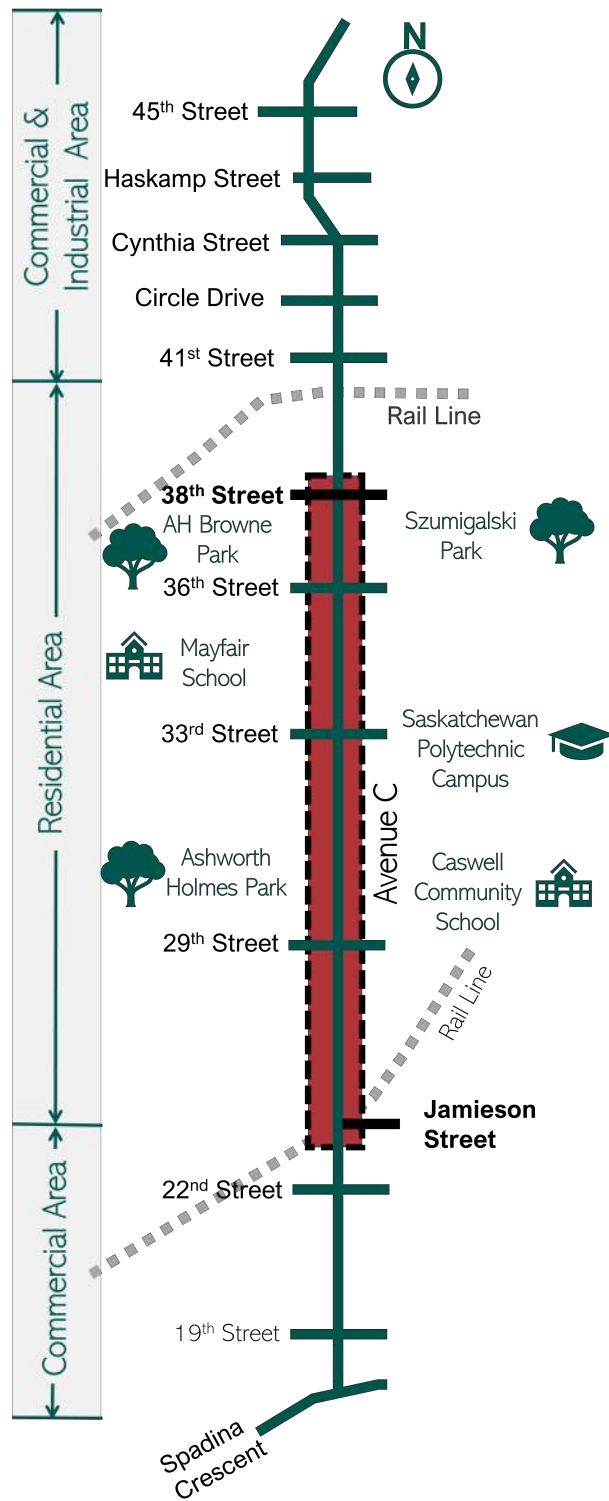


Sample cross section of unidirectional bike lanes on Avenue C.



# PROPOSED CYCLING FACILITIES

## JAMIESON STREET TO 38TH STREET



Neighbourhood bikeways typically include a range of treatments such as signage, pavement markings and traffic calming.

The proposed design includes:

- Neighbourhood bikeway on Avenue C from Jamieson Street to 38th Street with parking on both sides.
- Installing missing curb ramps along Avenue C.
- Retaining existing sidewalks on both sides of Avenue C.

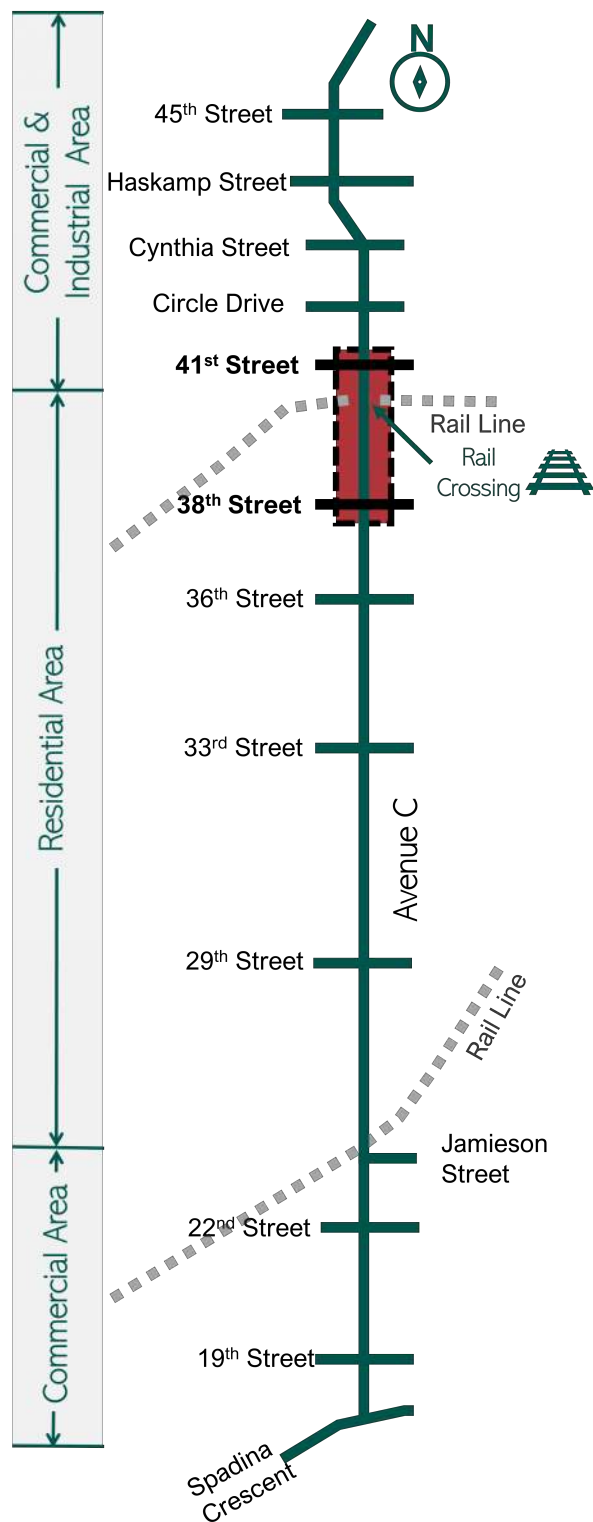


*Sample cross section of neighbourhood bikeway on Avenue C. Recommended posted speed 30 km/h pending Council approval.*



# PROPOSED CYCLING FACILITIES

## 38TH STREET TO 41ST STREET



Multi-use paths are off-street facilities that are physically separated from vehicles and run alongside or nearby roadways. These paths allow for two-way travel and are shared by pedestrians, cyclists and other non-motorized users.

The proposed design includes:

- Sidewalk level multi-use pathway on the east side of Avenue C from 38th Street to 41st Street.
- Retaining the sidewalk on the west side of Avenue C.
- Removing parking on the east side of Avenue C from 38th Street to 39th Street and retaining parking on the west side only.
- Parking would be retained on both sides of the street north of 39th Street.

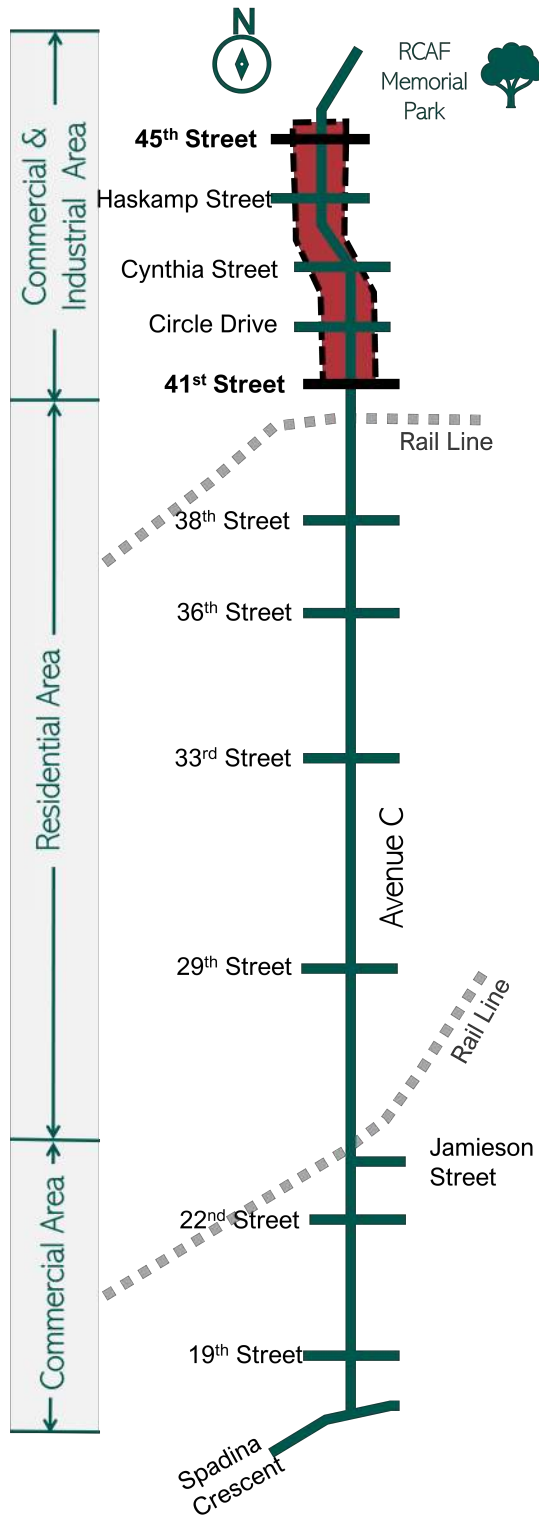


Sample cross section of multi-use pathway on Avenue C.



# PROPOSED CYCLING FACILITIES

## 41ST STREET TO 45TH STREET



Multi-use paths allow for two-way travel and are shared by pedestrians, cyclists and other non-motorized users.

The proposed design includes:

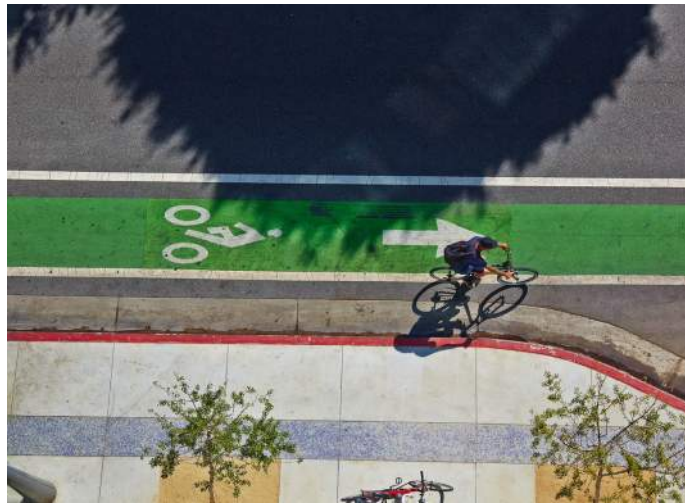
- Sidewalk level multi-use pathway on the east side of Avenue C from 41st Street to 45th Street.
- Retaining the sidewalk on the west side of Avenue C between 41st Street and Circle Drive.
- A new sidewalk on the west side of Avenue C, north of Circle Drive.



Sample cross section of multi-use pathway on Avenue C.  
Additional property would be required.



## Pavement markings



**Pavement markings** (i.e., green paint) will be used to indicate the path for cyclists through intersections and driveways. This treatment improves the safety of the intersection by increasing the visibility of the bicycle lane and helps to reinforce that cyclists have priority in these areas.

## Two-stage turn boxes



**Two-stage turn boxes** provide a safe waiting area for left-turning cyclists at some intersections.

## Curb extensions



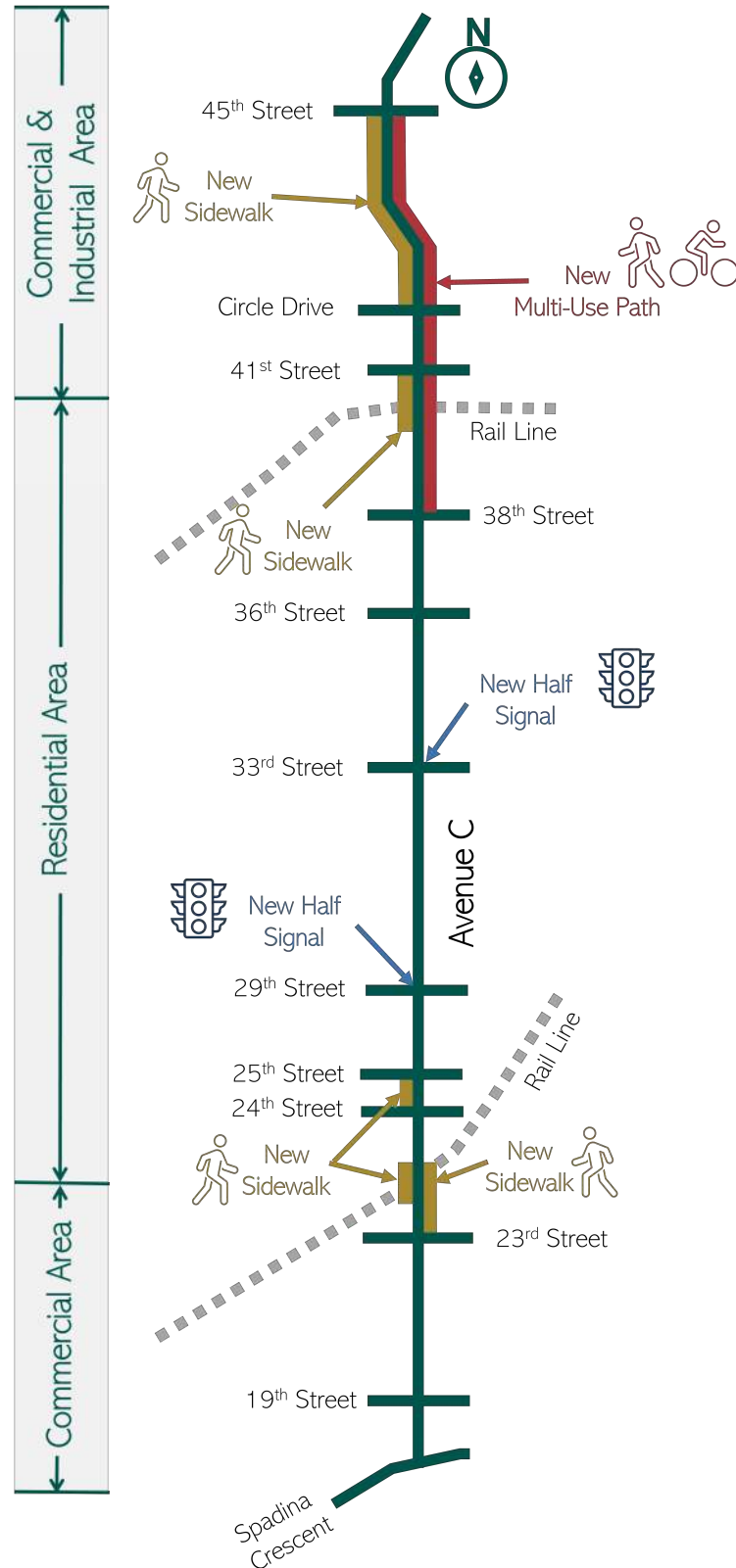
**Curb extensions** will be added at some intersections to reduce the speed of vehicles.

## Bicycle signals



**Bicycle signals** provide direction to cyclists when crossing a street.





The proposed design includes enhancements to walking facilities in the project area:

- **New sidewalks** and **multi-use paths** will be added to improve the connectivity of walking facilities.
- **Curb extensions** will be added at some intersections to decrease the pedestrian crossing distance and reduce the speed of vehicles.
- **Zebra pavement markings** will be added to some pedestrian crossing locations to enhance the crosswalk visibility.
- **Curb ramps** will be added at intersections to improve accessibility.
- **Pedestrian and cyclist actuated half signals** will be added on Avenue C at 29th Street and 33rd Street to improve safety for pedestrians and cyclists.
- **Formalized rail line crossings** will be added.



Example of Typical Sidewalk



Example of Zebra Pavement Markings

The proposed design maintains existing parking for much of the corridor; however:

- On Avenue C between 19th Street and Jamieson Street adding unidirectional bike lanes would require that 85 parking spaces be removed on the east side and 18 parking spaces be removed on the west side, resulting in the loss of 103 parking spaces.
- On Avenue C between 38th Street and 39th Street adding a multi-use path would require that parking is removed on the east side, resulting in the loss of 22 parking spaces.



Avenue C - 19th and 20th Street



Avenue C - 20th and 21st Street



Avenue C - 21st and 22nd Street



Avenue C - 22nd and 23rd Street



Avenue C - 38th and 39th Street



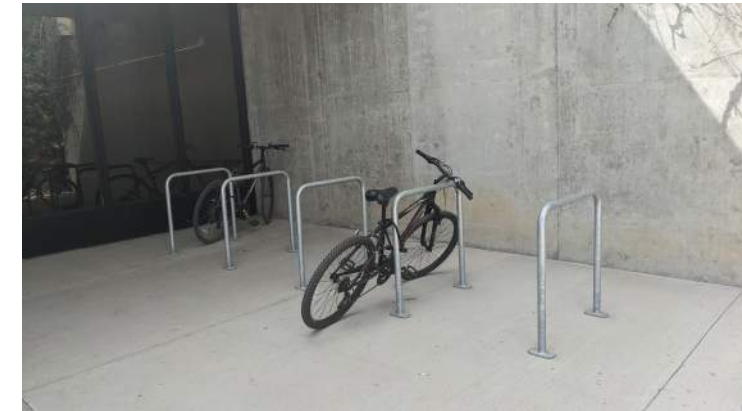
- The proposed design maintains existing landscaping and amenities for much of the corridor as most of the proposed changes are within the curb-to curb space and do not impact the existing boulevard areas.
- Potential new locations for bicycle parking have been identified throughout the corridor.
- North of 38th Street a multi-use path is proposed on the east side and a sidewalk is proposed on the west side of Avenue C in the existing boulevard space.
- Some trees and landscaping may be impacted where new sidewalks and multi-use paths are proposed.



Proposed location for bike parking  
Isinger Park



Example of bike rack



Example of bike rack



The next steps for Connecting Avenue C Walking and Cycling improvements include:



# GIVE FEEDBACK



Your input will help create a plan for connecting Avenue C that supports the needs of all users. **We look forward to hearing from you!**



Complete the project survey to **share your initial thoughts** by June 30th 2023:  
<https://www.surveymonkey.com/r/AvenueC>



Sign up to **receive updates about the project** by visiting the City of Saskatoon's Engage Page at:  
<https://www.saskatoon.ca/engage>